

Appendix A1

Pel Alternatives Analysis

Acronyms and Abbreviations

AADT	Average Annual Daily Traffic
C-470	Colorado State Highway 470
CDOT	Colorado Department of Transportation
FHWA	Federal Highway Administration
I-25	Interstate 25
ICA	Initial Corridor Assessment
MP	milepost
NEPA	National Environmental Policy Act
PEL	Planning and Environmental Linkages

I-25 Colorado Springs Denver South Connection Background of PEL Study Alternatives Analysis

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DATE: April 25, 2018
REVISION NO.: 1

This memorandum provides a summary background of the in-progress Interstate 25 (I-25) Colorado Springs Denver South Connection Planning and Environmental Linkages (PEL) study alternatives development and evaluation process. The PEL study is developing and evaluating alternative concepts for the I-25 corridor between Monument and Colorado State Highway 470 (C-470). The alternatives analysis for the PEL study was partially complete when the Colorado Department of Transportation (CDOT), the Federal Highway Administration (FHWA), and local stakeholders advanced development of the I-25 South Gap Project from the PEL study as an early action project. This memorandum describes the completed and next steps for the PEL study as background to the alternatives considered for the I-25 South Gap Project, which are described in the **I-25 South Gap Project Alternatives Analysis Technical Memorandum** included as Appendix A5 to the I-25 South Gap: Monument to C-470 Environmental Assessment (EA).

1. Background

In August 2016, CDOT began a PEL study to consider improvements along a 34-mile corridor on I-25 between Monument and C-470 in the Denver South region (**Figure 1**). The PEL study was initiated to:

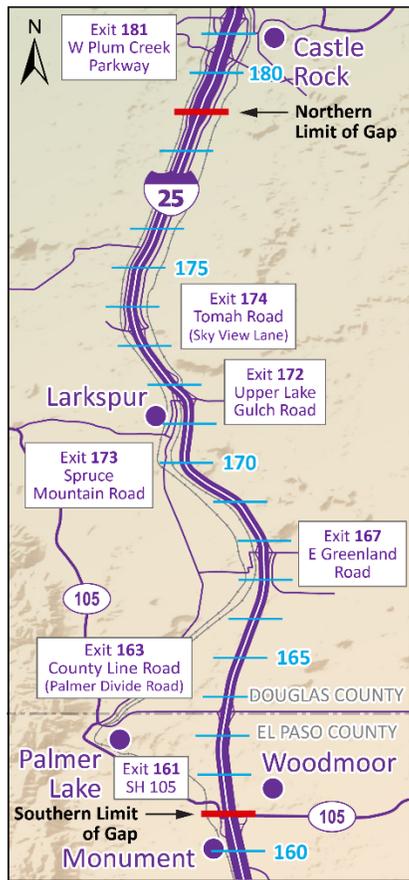
- Understand the corridor travel and safety needs and develop a vision for future improvements.
- Help CDOT define and prioritize projects based on the needs and vision.
- Determine project costs, funding, financing, and delivery options.
- Engage with local corridor communities, regional travelers, and other interested stakeholders.
- Identify significant environmental or other constraints to support an efficient transition through project delivery.
- Develop an implementation plan that allows CDOT to implement improvements incrementally according to priorities and funding.

Individual projects defined by the PEL could include localized improvements, segment-specific improvements, or corridor-wide solutions. All projects would be consistent with the long-term vision for how the interstate would operate and the demands that would be placed on the transportation network from future growth and changes in transportation preferences and technology.



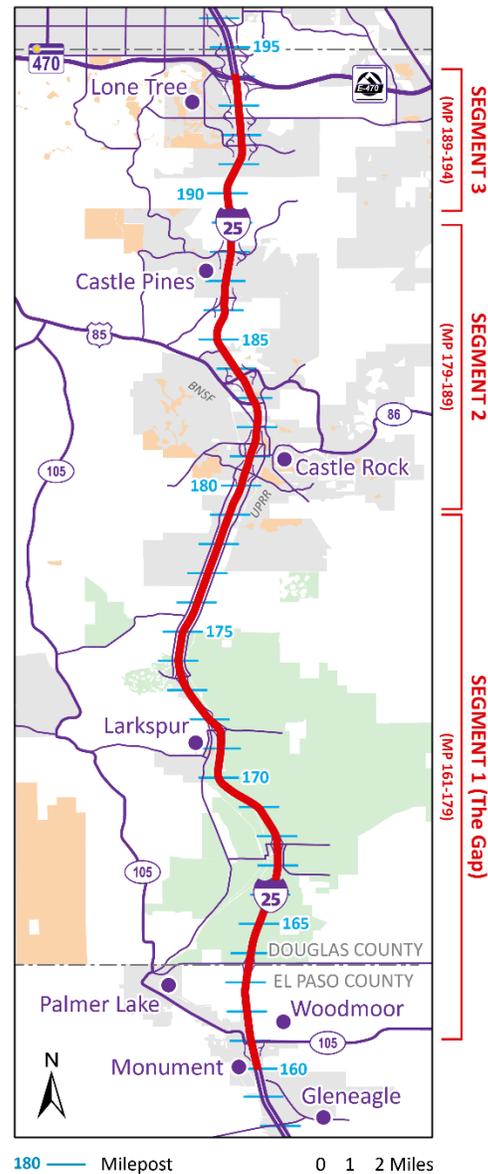
GAP_128_02

I-25 South Gap Corridor



180 — Milepost 0 1 2 Miles

I-25 South Corridor



180 — Milepost 0 1 2 Miles

Figure 1. I-25 South PEL Corridor Location

In response to significant public and political interest from the onset of the PEL study, in January 2017, CDOT and FHWA announced they would initiate design and environmental studies for a project in the Gap segment concurrently with the broader PEL study to allow project construction to be accelerated to November 2019 if funding for a Gap project could be secured. By Summer 2017, CDOT had completed a needs assessment of the PEL corridor, developed numerous concepts for improvements for each of the corridor segments, and conducted detailed engineering and environmental surveys for the Gap. By Fall 2017, CDOT and local governments had identified potential funding for about 80 percent of the \$350 million project budget and came together to apply for a federal grant (Infrastructure for Rebuilding American or INFRA grant) to complete the funding package. At this time, CDOT further accelerated the

project schedule with a new target of construction by November 2018 or sooner. Preliminary engineering and this EA were initiated, and the PEL study schedule was extended.

2. PEL Purpose and Need

A project's purpose and need provides the foundation for evaluating alternatives. The PEL purpose and need was developed after a thorough review of the existing corridor conditions and needs, which are documented in the Initial Corridor Assessment (ICA) (CH2M, 2017). The ICA presented data and analysis of infrastructure, mobility, traffic, and safety issues in the corridor, as well as context to the social and environmental conditions in the corridor. The ICA describes the three PEL corridor segments and the varying needs within those segments. The PEL segments (illustrated in Figure 2) include:

- Segment 1: Monument to Castle Rock (the Gap); mileposts (MPs) 161 to 179
- Segment 2: Castle Rock; MPs 179 to 189
- Segment 3: Denver South; MPs 189 to 194

Dividing the PEL corridor into three segments recognizes the local needs and the role the segments play in combining to serve the dominant regional corridor travel pattern.

The purpose and need for the PEL corridor improvements is to improve safety, mobility, and reliability for travel in the I-25 corridor between Colorado Springs and Denver. Under this umbrella, the needs in each of the segments is context-specific. Each of the three segments has differing traffic, land use, development, and environmental characteristics that help define the vision for and applicability of improvements through the segment. **Figure 2** summarizes these differing characteristics.



CDOT held several large public meetings to review corridor needs. Attendees provided input on problems in each of the PEL corridor segments. Most (but not all) of the attention was focused on the Gap segment (segment 1). The top three issues identified for the Gap were capacity/congestion, reliability, and safety.

Figure 2. I-25 PEL Corridor Segments

Segment 1

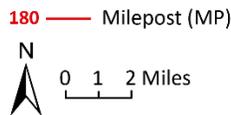
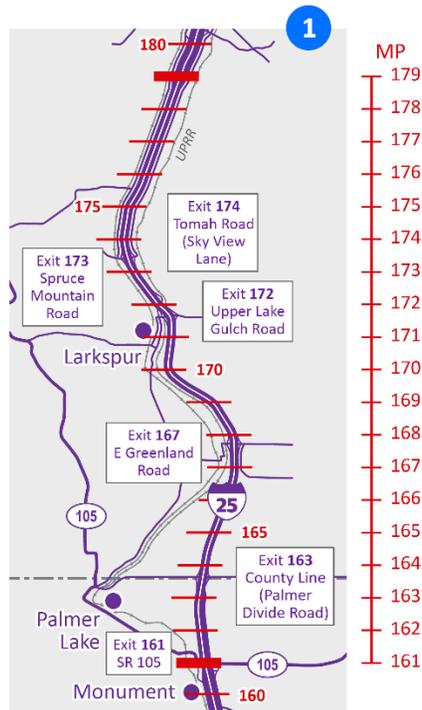
**MP 161 to MP 179:
Monument to
Castle Rock (The Gap)**

Transportation Characteristics

- Rural 4-lane
- Narrow shoulders
- Steady gradual southbound incline grade
- Hills, steep slopes, and vertically offset travel lanes
- 6 interchanges
- 2017 Average Annual Daily Traffic (AADT): 77,000-86,000 vehicles per day
- Trucks make up 8.4% of traffic

Environmental Characteristics

- High incidence of wildlife conflicts
- Many stream crossings, Preble's Meadow Jumping Mouse habitat
- Protected open space throughout



Segment 2

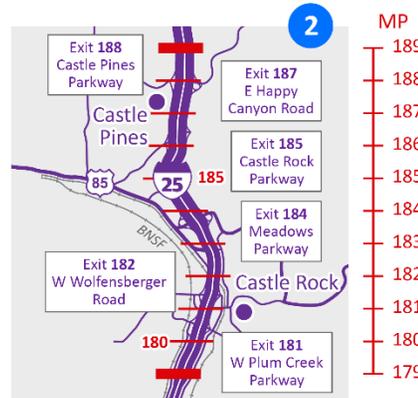
**MP 179 to MP 189:
Castle Rock to Castle Pines**

Transportation Characteristics

- Urban 6-lane
- Narrow shoulders
- 6 interchanges
- 2017 AADT: 99,000-133,000 vehicles per day
- Trucks make up 6.7% of traffic

Environmental Characteristics

- Developed and growing communities
- Nearby Plum Creek runs parallel to the highway and includes Preble's Meadow Jumping Mouse habitat



Segment 3

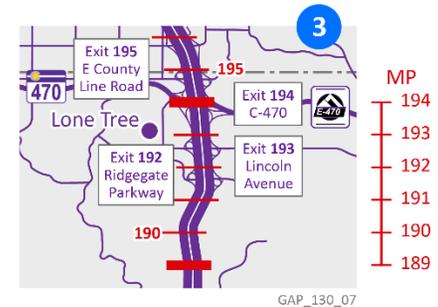
**MP 189 to MP 194:
Denver South**

Transportation Characteristics

- Urban 8-lane, recently widened on north end
- 3 interchanges
- 2017 AADT: 133,000-150,000 vehicles per day
- Trucks make up 5.3% of traffic

Environmental Characteristics

- Rapidly developing commercial and residential area



3. Alternatives Analysis Process

PEL studies are generally conducted prior to identification of project funding. The goal of PEL alternative analysis is to develop many potential solutions to transportation problems and create a broad menu of feasible actions that could be advanced into National Environmental Policy Act (NEPA) processes. The PEL process differs from the traditional NEPA project development process of narrowing alternatives to a single preferred alternative that can be advanced into design and construction. Not knowing the timeframe or funding for improvements, the PEL process recognizes that transportation needs and funding will evolve. The PEL process seeks to provide a wide array of options that could be effective, allowing agencies to be nimble in response to changing priorities or needs. Alternative concepts that may not be feasible today may be appropriate in the future; therefore, concepts are not eliminated based on current constraints.

The PEL study process conceptually follows these milestones:

1. Defining the Purpose and Need.
2. Determining criteria to measure how project solutions resolve the corridor's needs.
3. Identifying significant environmental constraints that may influence design options and/or delay project development with lengthy environmental reviews.
4. Developing a range of improvement concepts to be evaluated.
5. Refining and evaluating concepts based on the criteria.
6. Clarifying project costs and identifying necessary financing and funding options to implement improvements based on the corridor's greatest needs.

These milestones provide a framework for CDOT to engage with local corridor communities, regional travelers, and other interested stakeholders to understand their concerns and ideas for immediate and longer-term improvements. The process and outcomes support an efficient transition to NEPA processes, final design, and construction advertisement once funding is identified.

Based on the PEL purpose and need, criteria, and identified constraints (milestones 1-3 above), the PEL alternatives analysis process developed and evaluated initial improvement concepts for each of the three I-25 corridor segments between Monument and C-470. Alternative concepts were categorized as Core Concepts and Supplemental Elements.

- **Core Concepts** are standalone improvements that directly meet the PEL's purpose and need.
- **Supplemental Elements** are additional improvements that do not meet the project purpose and needs on their own but improve the Core Concepts.

Segment Alternatives are a combination of Core Concepts and Supplemental Elements. **Corridor Alternatives** are a combination of Segment Alternatives.

The I-25 PEL study includes four planned levels of alternatives evaluation:

1. Level 1 evaluation – Evaluation of Core Concepts and Supplemental Elements against purpose and need.
2. Level 2 evaluation – A more narrow review of only the Core Concepts carried forward from Level 1.
3. Level 3 – Assemble Segment Alternatives from those Core Concepts that were evaluated in Level 2 and package them with Supplemental Elements that were carried forward from Level 1.
4. Level 4 – Frame the Corridor Alternatives and establish an Implementation Plan.

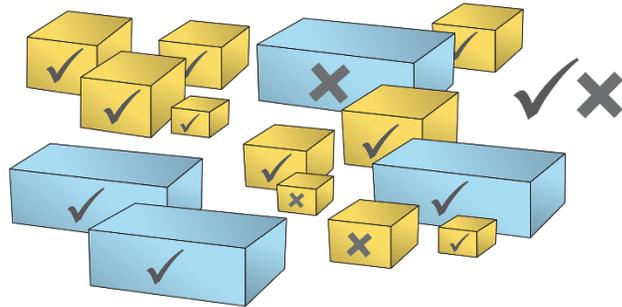
The process is illustrated in **Figure 3**. The first two levels have been completed, and results are presented in this memorandum. When the I-25 South Gap Project was advanced to NEPA, CDOT committed to an

accelerated project development and paused the PEL study. The PEL study, including developing and evaluating corridor-wide alternatives and implementation plan, will resume in Summer 2018. When the PEL study resumes, the I-25 South Gap project would be part of the new existing conditions for the PEL corridor.

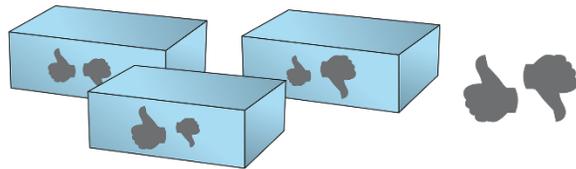
Figure 3. I-25 South PEL Alternatives Evaluation Process

EVALUATE CONCEPTS AND ELEMENTS...

Level 1 – Develop Core Concepts and Supplemental Elements



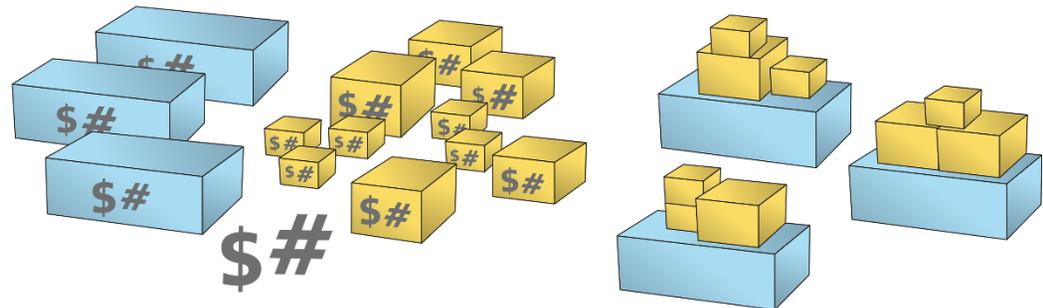
Level 2 – Refine Core Concepts



...FOR ASSEMBLING ALTERNATIVES

Level 3 – Create Segment Alternatives

Core Concepts + Supplemental Elements → Assemble Alternatives for Each PEL Segment

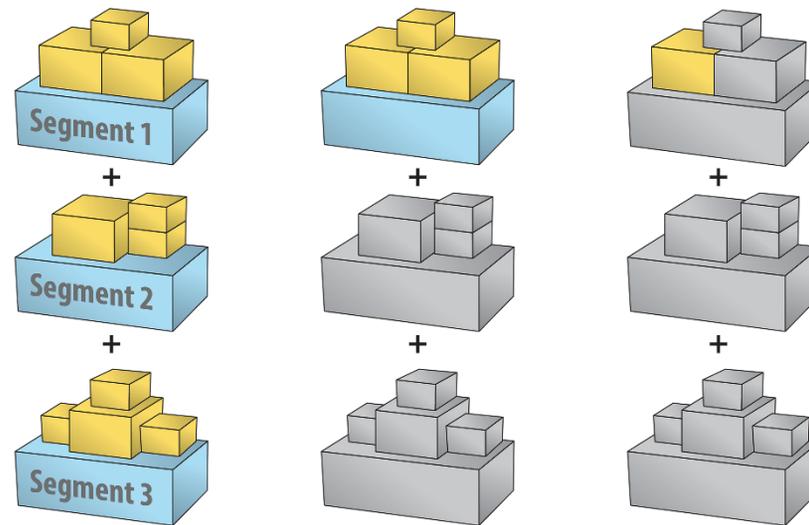


Level 4 – Build Corridor Alternatives and Endorse Implementation Plan

Corridor Alternatives
(One Corridor Alternative Shown Below)

Corridor Alternative Phased by Segment

Corridor Alternative Phased by Element



DEFINITIONS

Core Concept

Standalone improvement that directly meets purpose and need for I-25 and provides foundation for segment alternatives

Supplemental Element

Additional improvement that is necessary or improves performance of core concepts to meet project needs for segment alternatives

Segment Alternative

Core concept combined with supplemental elements to form a standalone alternative that could be implemented within a segment

Corridor Alternative

Combination of segment alternatives that addresses corridorwide needs

A. Level 1 PEL Alternatives Evaluation

Level 1 evaluation involved a high-level review of Core Concepts and Supplemental Elements to determine which concepts might solve one or more of the PEL project needs to advance safety, improve reliability, enhance regional and/or local mobility, and/or improve incident management on I-25. Any concept that addressed a need was carried forward for further evaluation. The Level 1 evaluation resulted in concepts being classified as:

- Carried forward as a Core Concept
- Carried forward as a Supplemental Element
- Not recommended
- Eliminated as not feasible

More than 100 alternative concepts were developed for the three segments, and most were carried forward for further evaluation. Core Concepts developed for the I-25 PEL consisted of improvements for various lane and shoulder configurations, transit, and alternate routes. Supplemental Elements consisted of localized improvements, such as interchanges, frontage roads, wildlife crossings, truck facilities, and intelligent transportation system elements such as ramp metering.

Segment 1 (MP 161-179): The Gap, Monument to Castle Rock¹

- Ten Core Concepts were carried forward for further evaluation, and only 1 concept (reversible lanes) was not.
- Of the 59 Supplemental Elements considered, 43 were carried forward, 10 were not recommended, and 6 were eliminated from further consideration

Segment 2 (MP 179-189): Castle Rock to Castle Pines

- All 11 of the Core Concepts were carried forward for further evaluation.
- Of the 47 Supplemental Elements considered, 24 were carried forward, 4 were not recommended, and 19 were eliminated from further consideration because they were infeasible or imprudent.

Segment 3 (MP 189-194): Denver South

- All 8 of the Core Concepts were carried forward for further evaluation.
- Of the 33 Supplemental Elements considered, 18 were carried forward, 2 were not recommended, and 13 were eliminated from further consideration.

The summary of the Level 1 evaluation is included in **Attachment 1** to this memorandum.

B. Level 2 PEL Alternatives Evaluation

The purpose of the Level 2 Evaluation was to conduct a more in-depth evaluation of the Core Concepts carried forwarded from Level 1, using evaluation criteria based on the PEL purpose and need,

¹ CDOT and local officials also identified dozens of initial actions that could be implemented to immediately improve safety and travel through the Gap. Initial actions considered ranged from improving communications and traveler information to small-scale investments in curve warning delineators and lighting. CDOT acted on a number of the PEL study's recommendations for initial actions, including expanding Bustang service to weekends, coordinating variable message and travel time signs to provide accurate and timely travel information, expanding courtesy patrol in the corridor, increasing enforcement (with the Douglas County Sheriff and Colorado State Patrol) on the frontage road between Plum Creek Parkway and Tomah Road, and coordinating traffic control and communication with the Renaissance Festival in Larkspur. CDOT also proceeded with review and recommendations for incident management in the corridor and launched a Mind the Gap safety campaign to enhance information for corridor travelers. In conjunction with these initial actions, Douglas County evaluated options for the Tomah Road/frontage road intersection and is proceeding with a project to add turn lanes at this intersection. Douglas County also initiated a study of variable speed limits and is working with CDOT to initiate a PEL study along SH 83, which is used as an alternate route to I-25.

compatibility with local and regional community planning goals, and environmental impacts. The Level 2 Evaluation identified how well the Core Concepts could address the Segment's needs; differences among Core Concepts that may lead to relatively higher impacts; and conflicts that require further design development to address.

Level 2 Evaluation included focused evaluation of the Core Concepts. As with the Level 1 Evaluation, the Technical Working Group helped CDOT evaluate the concepts, and most were carried forward for packaging as Segment alternatives in Level 3. The concepts were presented at April 2018 public meetings.

The results of the Level 2 evaluation are presented in **Attachment 2**. While some of the Core Concepts carried forward had technical or political issues as noted in the comments section of the evaluation, only one Core Concept, located in the Gap segment, was eliminated from further consideration. It is anticipated that the Level 3 and 4 packaging of Segment and Corridor alternatives would not recommend some of the Core Concepts due to conflicts with adjoining I-25 segments.

1. Segment 1: The Gap

Based on the Level 1 evaluation, 10 Core Concepts were considered in the Level 2 evaluation:

- Add One General-Purpose Lane Each Direction (three lanes)
- Add One Express Lane Each Direction (three lanes)
- Add One General-Purpose Lane and One Express Lane Each Direction (four lanes)
- Add Two General-Purpose Lanes Each Direction (four lanes)
- Add Two Express Lanes Each Direction (four lanes)
- Add Auxiliary Lanes between Tomah Exit and Spruce Mountain Road
- Add Southbound Truck Climbing Lane(s) or Passing Lane(s) at Monument Hill
- Add New Elevated Travelway Each Direction (three lanes)
- Add Express Lane and Convert Existing General-Purpose Lanes to Express Lanes (three lanes)
- Add/Convert Peak Period Shoulder

All but one of the 10 Core Concepts were retained for further consideration in the PEL Level 3 evaluation. The elevated travelway was not recommended because of the high costs and impacts of building an elevated section through the rural Gap and because a vertically separated lane did not provide maneuverability or incident response benefits of building at-grade.

2. Segment 2: Castle Rock

Based on the Level 1 evaluation, 11 Core Concepts were considered in the Level 2 evaluation:

- Add One General-Purpose Lane Each Direction South of Founder's Parkway (four lanes)
- Add One Express Lane Each Direction South of Founder's Parkway (four lanes)
- Add Two General-Purpose Lanes Each Direction South of Founder's Parkway and One General-Purpose Lane Each Direction North of Founder's Parkway (five lanes)
- Add One General-Purpose and One Express Lane Each Direction South of Founder's Parkway and One Express Lane or General-Purpose Lane Each Direction North of Founder's Parkway (five lanes)
- Add Two Express Lanes Each Direction South of Founder's Parkway and one Express Lane North of Founder's Parkway (five lanes)

- Add One Reversible Lane (one new lane, operating in the northbound direction in the morning and southbound in the afternoon)
- Add New Elevated Travelway Each Direction (four/five lanes)
- Convert One General-Purpose Lane to an Express Lane (three/four lanes)
- Convert All General-Purpose Lanes to Express Lanes (three/four lanes)
- Add/Convert Peak Period Shoulder
- Add Auxiliary Lanes

All 11 of the Core Concepts were retained for further consideration in the PEL Level 3 evaluation.

3. Segment 3: Denver South

Based on the Level 1 evaluation, 8 Core Concepts were considered in the Level 2 evaluation:

- Add One Express Lane Each Direction (five lanes)
- Add One Reversible Lane (one new lane, operating in the northbound direction in the morning and southbound in the afternoon)
- Add New Elevated Travelway Each Direction (five lanes)
- Convert One General-Purpose Lane to an Express Lane (four lanes)
- Add a New Truck Climbing Lane at Surrey Ridge
- Add Auxiliary Lanes
- Convert All General-Purpose Lanes to Express Lanes (four lanes)
- Add/Convert Peak Period Shoulder

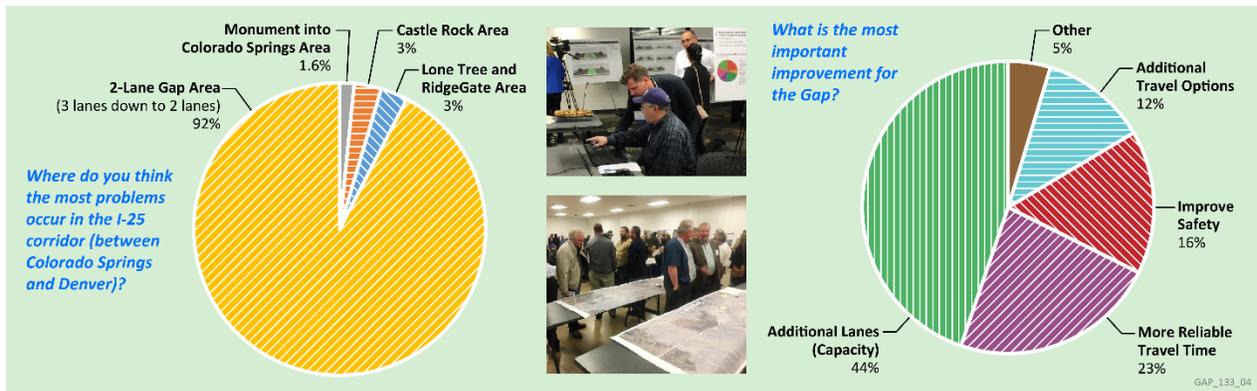
All 8 of the Core Concepts were retained for further consideration in the PEL Level 3 evaluation.

4. Public Involvement

Throughout 2017, CDOT conducted extensive outreach with corridor communities and the public to understand the most pressing travel issues through the I-25 PEL corridor and the highest priority improvements. Consistently through this process, stakeholders remained focused on prioritizing solutions for mobility and safety problems through the Gap as soon as feasible. The needs to add lane capacity; widen shoulders; improve infrastructure deficiencies; and provide wildlife crossings topped the list of capital investments. Additionally, operational improvements were also high priorities, such as truck parking and chain-up; incident management techniques; improved traveler information; and improved communication among maintenance, law enforcement, and emergency/incident responders.

PEL alternatives were developed in response to broad and specific problem areas and needs identified throughout the regional corridor CDOT and stakeholders. In January 2017, CDOT held public meetings, which were attended by more than 350 people, to present the results of the ICA and purpose and need for improvements. The meetings also sought feedback about needs from members of the public. An online survey (and corresponding printed survey distributed at the meetings) supplemented feedback provided to project team members on roll plots of the corridor and other supporting display boards.

Figure 4. Public Input into the Priority Locations and Improvement Needs for the I-25 Corridor between Colorado Springs and Denver South



Based on the identified needs, a broad range of alternative concepts was developed through an Innovations Workshop in collaboration with the Technical Working Group, a group of planning, engineering, and environmental staffs from local, state, and federal agencies that advise the project's technical progress.

Initial alternative concepts were developed for each of the corridor segments. These concepts were refined based on input from the Steering Committee (a group of local, state, and federal elected officials and representatives) and the public. The Core Concepts and Supplemental Elements developed for each of the PEL corridor segments were then presented at the April 2017 public meetings, where attendees provided feedback and suggestions for location-specific and corridor-wide solutions for the I-25 corridor. Again through the April meetings, the public remained focused on alternative concepts that improved safety and improved travel times and reliability through the Gap segment between Monument and Castle Rock. While meeting attendance was lower at the April sets of public meetings, media coverage was substantial, indicative of the increased profile of the I-25 PEL study after the January 2017 announcement to accelerate a project in the Gap.

December 2017 public meetings were held to introduce the I-25 South Gap EA and next steps for the PEL study. Input from these meetings again focused on the Gap primarily, and this input focused primarily on whether the new travel lane would be tolled. After the public meetings, CDOT conducted twelve small "listening sessions" to understand public concerns about tolling and other aspects of the project. Through these sessions, public comment began to focus on additional needs in the Gap not included in the I-25 South Gap Project, especially around a fourth travel lane and transit.

C. Next Steps for the PEL Alternatives Evaluation

Future improvements to the I-25 corridor between Colorado Springs and Denver will continue to be evaluated through the PEL study. The PEL study is anticipated to resume in Summer 2018 and be completed by the end of 2018 to fulfill its original objectives to:

- Describe the needs and vision for the regional corridor, including refining the existing corridor assessment (with the I-25 South Gap Project as part of the new "existing condition" for I-25)
- Define and prioritize projects in the corridor
- Determine project costs, funding, financing, and delivery options
- Engage with local corridor communities, regional travelers, and other interested stakeholders
- Identify significant environmental constraints and support efficient transition through project delivery

- Develop an implementation plan that allows CDOT to implement improvements incrementally according to priorities and funding

During the Level 3 Evaluation, CDOT and FHWA split out an early action project in the Gap segment to advance into design and construction. The I-25 South Gap Project is the subject of a separate Environmental Assessment. Because the I-25 South Gap Project was split out from the other PEL alternatives, the corridor needs and alternatives in the PEL study will need to be revisited and redefined if needed in light of the needs addressed by the I-25 South Gap Project. CDOT and FHWA will continue to assess additional needs for the Gap corridor and for persisting needs for travel to the north and south not addressed by the I-25 South Gap Project. Elements from the I-25 South Gap Project that were deferred to the PEL study, such as additional wildlife crossings, relocation or reconfiguration of the truck weigh station, and auxiliary or truck climbing lanes, will be included in the PEL alternatives evaluation. Additionally, the corridor limits will be reexamined and likely expanded to the south to account for the regional travel influence of El Paso County and areas south.

The Level 3 and Level 4 Evaluation of Segment and Corridor Alternatives will continue after the re-chartering of the PEL study. Core Concepts will be refined and packaged with complimentary Supplemental Elements to create alternatives for each of the I-25 PEL Segments. These Segment Alternatives will be reviewed and evaluated to be either carried forward to create corridor-wide alternatives or not recommended for further study due to comparatively negligible benefits and/or higher impacts or cost than other similarly performing alternatives. Long-term recommendations will likely have short-term project elements identified as phases or standalone projects, which will be documented in the implementation plan.

Attachment 1
PEL Level 1 Evaluation for the Gap

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 1 (MP 161-179): The Gap, Monument to Castle Rock	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
No Action	N	N	N	N	Carried Forward as Core Concept	Although the No Action does not meet the purpose and need, it is advanced as a baseline condition for comparison.
Infrastructure Elements						
LANE CONFIGURATION (NOTE: Existing lane configuration is two lanes in each direction.)						
Add One General Purpose Lane Each Direction (three lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Add One Express Lane Each Direction (three lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Add One General Purpose Lane and One Express Lane Each Direction (four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Add Two General Purpose Lanes Each Direction (four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with four lane element in Segment 2.
Add Two Express Lanes Each Direction (four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with four lane element in Segment 2.
Add Auxiliary Lanes between Tomah Exit and Spruce Mountain Road	Y	Y	Y	Y	Carried Forward as Core Concept	Could act as frontage roads.
Add Southbound Truck Climbing Lane(s) or Passing Lane(s) at Monument Hill	Y	Y	Y	Y	Carried Forward as Core Concept	
Add One Reversible Lane	Y	N	N	Y	Not Recommended for Further Evaluation	Reversible lane is not effective because volume split of traffic NB and SB does not indicate peak directions.
Add New Elevated Travelway Each Direction (three lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Add Express Lane and Convert Existing General Purpose Lanes to Express Lanes (three lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Future evaluation would need to evaluate effects on alternate routes.
Add/Convert Peak Period Shoulder	Y	Y	Y	Y	Carried Forward as Core Concept	
INTERCHANGES						
Improve County Line Road / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	Local mobility could be improved with interchange reconfiguration.
Improve Greenland Road / I-25	Y	Y	N	Y	Carried Forward as Supplemental Element	Greenland Road interchange is used to offload I-25 traffic when Monument Hill is closed. Improving the interchange could improve incident management.
Improve Upper Lake Gulch Road / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	Combine interchange with Spruce Mountain Road. Local mobility could be improved with interchange reconfiguration.
Improve Spruce Mountain Road / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	Combine interchange with Upper Lake Gulch Road. Local mobility could be improved with interchange reconfiguration.
Improve Sky View Lane (Tomah Road) / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	Local mobility could be improved with interchange reconfiguration.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

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Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 1 (MP 161-179): The Gap, Monument to Castle Rock	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Infrastructure Elements Cont.						
Add new Crystal Valley Parkway / I-25 (planned by others)	Y	Y	Y	Y	Carried Forward as Supplemental Element	New interchange would provide improved local access and mobility. EA and FONSI completed for developer-funded project.
STRUCTURES						
Replace/Rehabilitate County Line Road / I-25 bridge, MP 163.3	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Replace/Rehabilitate East Plum Creek bridge, MP 167.3	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate Greenland Road box culvert, MP 167.5	Y	N	Y	Y	Carried Forward as Supplemental Element	Local mobility improved with two-way traffic through structure.
Replace/Rehabilitate Upper Lake Gulch Road / I-25 NB bridge, MP 171.8	Y	N	N	N	Carried Forward as Supplemental Element	Bridge has a structurally deficient rating. Bridge carries I-25; therefore, improving the bridge directly improves I-25.
Replace/Rehabilitate Upper Lake Gulch Road / I-25 SB bridge, MP 171.8	Y	N	N	N	Carried Forward as Supplemental Element	Bridge has a structurally deficient rating. Bridge carries I-25; therefore, improving the bridge directly improves I-25.
Replace/Rehabilitate Railroad bridge, MP 172.0	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate East Plum Creek bridge, MP 172.2	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate Spruce Mountain Road / I-25 bridge, MP 172.3	N	N	N	N	Eliminated	Bridge has a functionally obsolete rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.
Replace/Rehabilitate Sky View Lane (Tomah Road) / I-25 bridge, MP 173.8	N	N	N	N	Eliminated	Bridge has a functionally obsolete rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.
ALTERNATE ROUTES						
Improve State Highway 83	N	Y	Y	Y	Not Recommended for Further Evaluation	Improvements to alternate routes do not directly improve the safety of I-25. While these roads do serve as detours to I-25 traffic when incidents occur, their primary function is to serve local traffic (e.g. Noe, Andrews, and Best Roads) or trips outside the PEL corridor (SH-83 and SH-105). El Paso County has several projects under construction and planned to improve the state highways in Segment 1. Improvements to local roads would be undertaken at the county or municipal levels.
Improve State Highway / County Road 105	N	Y	Y	Y	Not Recommended for Further Evaluation	
Improve Noe Road	N	Y	N	Y	Not Recommended for Further Evaluation	
Improve South Andrews Road	N	Y	N	Y	Not Recommended for Further Evaluation	
Improve Spruce Mountain Road	N	Y	N	Y	Not Recommended for Further Evaluation	
Improve East Best Road	N	Y	N	Y	Not Recommended for Further Evaluation	
Improve East Greenland Road	N	Y	N	Y	Not Recommended for Further Evaluation	
FRONTAGE ROADS						
Extend I-25 Frontage Roads (east and west of I-25) South from Sky View Lane to Spruce Mountain Road	N	Y	Y	Y	Carried Forward as Supplemental Element	
OTHER PHYSICAL ELEMENTS						
Improved Inside Shoulder (width may vary)	Y	Y	N	Y	Carried Forward as Supplemental Element	
Improved Outside Shoulder (width may vary)	Y	Y	N	Y	Carried Forward as Supplemental Element	
Infrastructure Elements Cont.						
Add Wildlife Crossings / Fencing	Y	Y	N	N	Carried Forward as Supplemental Element	

Summary: Segment 1 resulted in 10 Core Concepts and 43 Supplemental Elements being carried forward. 1 Core Concept and 10 Supplemental Elements are Not Recommended for Further Evaluation. 6 Supplemental Elements are Eliminated.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 1 (MP 161-179): The Gap, Monument to Castle Rock	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
New Carpool/Park-n-Ride Lot(s)	Y	Y	N	N	Carried Forward as Supplemental Element	
Improve Monument Bustang Park-n-Ride Facility	N	N	Y	N	Carried Forward as Supplemental Element	
Add / Improve Lighting	Y	N	Y	Y	Carried Forward as Supplemental Element	Darkness is a safety concern in some locations. Darkness is also valued for environmental reasons.
Add Emergency Parking	Y	N	Y	Y	Carried Forward as Supplemental Element	
Add Turnouts	Y	Y	N	Y	Carried Forward as Supplemental Element	
Improve and/or add emergency median crossover(s)	Y	Y	N	Y	Carried Forward as Supplemental Element	
Relocate Port of Entry / Weigh Station	Y	Y	N	N	Carried Forward as Supplemental Element	
Add / Relocate Chain-Up / Chain-Down Stations	Y	Y	N	Y	Carried Forward as Supplemental Element	
Reopen Rest Areas	Y	N	N	Y	Carried Forward as Supplemental Element	
Repurpose Rest Areas	Y	Y	N	Y	Carried Forward as Supplemental Element	
Multimodal Elements						
Add Passenger Rail along I-25 (high speed or commuter rail)	N	Y	Y	N	Carried Forward as Supplemental Element	
Add Commuter Rail along Existing BNSF/UPRR Corridor	N	N	Y	N	Not Recommended for Further Evaluation	While the BNSF/UPRR lines generally follow I-25 in Segment 1, the alignment diverges in the Palmer Lake area. Additionally, the regional nature of trip patterns through Segment 1 would not be served by a commuter rail through the Gap area, which would be forced to diverge from the PEL corridor when the tracks diverge south of Castle Rock.
Extend Light Rail E/F/R Line(s) South along I-25 to from Castle Rock to Monument	N	Y	Y	N	Not Recommended for Further Evaluation	Light Rail is not appropriate for Segment 1 (overall distance and distance between stations, speeds, seating capacity, power/grades). Passenger rail along I-25 is evaluated as a separate element.
Add Bus Rapid Transit (BRT) on Dedicated Facility	N	Y	Y	N	Not Recommended for Further Evaluation	Dedicated BRT offers little advantage over enhanced bus service in a regional setting; enhanced conventional bus service can achieve same advantages.
Expand Bustang Service	N	Y	Y	N	Carried Forward as Supplemental Element	Reliability would be improved if express lane is implemented with transit priority. Includes more round trips, and improvements to Monument Park-n-Ride, and new Bustang station in Castle Rock.
Add Trail Underpass at Upper Lake Gulch Road	N	N	Y	N	Carried Forward as Supplemental Element	
Add Trail Underpass at State Highway / County Road 105	N	N	Y	N	Carried Forward as Supplemental Element	
Add Continuous Bicycle/Pedestrian Path	N	N	Y	N	Carried Forward as Supplemental Element	

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 1 (MP 161-179): The Gap, Monument to Castle Rock	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Operations Elements						
Consider Express Lane Fee Structure	N	N	Y	N	Carried Forward as Supplemental Element	In conjunction with conversion of existing lanes to express lanes or new express lanes.
Update Incident Management Plan	Y	Y	Y	Y	Carried Forward as Supplemental Element	
Accommodate Autonomous Vehicles	Y	Y	Y	N	Carried Forward as Supplemental Element	
Add Queue Warning	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add Ramp Metering	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add Wildlife Detection and Alert System	Y	Y	N	N	Carried Forward as Supplemental Element	
Enhance ITS infrastructure	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add VMS signage	Y	Y	N	Y	Carried Forward as Supplemental Element	
Enhance Lane Markings (e.g., solar lights)	Y	Y	N	N	Carried Forward as Supplemental Element	
Implement Variable Speed Limits	Y	Y	N	N	Carried Forward as Supplemental Element	Must include enforcement to be effective.
Implement Automated Speed Enforcement	Y	Y	N	N	Carried Forward as Supplemental Element	
Add Commercial Truck Emergency Parking / Refuge	Y	Y	N	Y	Carried Forward as Supplemental Element	
Improve Special Event Traffic Control Coordination	Y	Y	N	Y	Carried Forward as Supplemental Element	
Implement Changes to Speed Limits	Y	Y	N	N	Carried Forward as Supplemental Element	
Expand Weigh in Motion	Y	Y	N	N	Carried Forward as Supplemental Element	

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 2 (MP 179-189): Castle Rock to Castle Pines	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
No Action	N	N	N	N	Carried Forward as Core Concept	Although the No Action does not meet the purpose and need, it is advanced as a baseline condition for comparison.
Infrastructure Elements						
LANE CONFIGURATION (NOTE: Existing lane configuration is three lanes each direction south of Founders Parkway and four lanes each direction north of Founders Parkway.)						
Add One General Purpose Lane Each Direction south of Founders Parkway (continuous four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Provides lane continuity with four lane section to the north.
Add One Express Lane Each Direction South of Founders Parkway (continuous four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Express lane continuity better with conversion of one existing general purpose lane each direction to express lane north of Founders Parkway. Would need to determine how express lane operations would transition / convert for existing four lane section north of Founders Parkway.
Add Two General Purpose Lanes Each Direction South of Founders Parkway; Add One General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with five lane element in Segment 3.
Add One General Purpose Lane and One Express Lane Each Direction South of Founders Parkway; Add One Express Lane or General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with five lane element in Segment 3. Would need to determine how express lane operations would transition / convert for section north of Founders Parkway.
Add Two Express Lanes Each Direction South of Founders Parkway; Add One Express Lane Each Direction North of Founders Parkway (continuous five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with five lane element in Segment 3. Express lane continuity better with conversion of one existing general purpose lane each direction to express lane north of Founders Parkway. Would need to determine how express lane operations would transition / convert for section north of Founders Parkway.
Add One Reversible Lane	Y	Y	Y	Y	Carried Forward as Core Concept	
Add New Elevated Travelway Each Direction (four/five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Would not address lane balance with three to four lane section.
Convert One Existing Lane to Express Lane (three/four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Would not address lane balance with three to four lane section.
Convert All Existing Lanes to Express Lanes (three/four lanes)	Y	Y	N	N	Carried Forward as Core Concept	Would likely push commercial truck/interstate traffic onto local road network, reducing local mobility.
Add/Convert Peak Period Shoulder	Y	Y	Y	Y	Carried Forward as Core Concept	
Add Auxiliary Lane(s)	Y	Y	Y	Y	Carried Forward as Core Concept	

Summary: Segment 2 resulted in 11 Core Concepts and 24 Supplemental Elements being carried forward. 4 Supplemental Elements are Not Recommended for Further Evaluation. 19 Supplemental Elements are Eliminated.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

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Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 2 (MP 179-189): Castle Rock to Castle Pines	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Infrastructure Elements Cont.						
INTERCHANGES						
Improve Plum Creek Parkway / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	
Improve Wilcox Street / Wolfsenberger Road / I-25	Y	Y	Y	Y	Carried Forward as Supplemental Element	
Restore US 85 / I-25 Connection (at Black Feather, by others)	Y	Y	Y	Y	Carried Forward as Supplemental Element	
Improve Meadows Parkway / Founders Parkway / I-25	N	N	N	N	Eliminated	Interchange improvements not needed.
Improve Castle Rock Parkway / I-25	N	N	N	N	Eliminated	Interchange improvements not needed.
Add New Happy Canyon Road / I-25 (new by others)	Y	Y	Y	Y	Carried Forward as Supplemental Element	
Improve Castle Pines Parkway / I-25	N	N	N	N	Eliminated	Interchange improvements not needed.
STRUCTURES						
Replace/Rehabilitate I-25 NB Over Plum Creek Parkway, MP 180.8	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate I-25 SB Over Plum Creek Parkway, MP 180.8	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate I-25 NB Over East Plum Creek, MP 181.2	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate I-25 SB Over East Plum Creek, MP 181.2	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate 5 th Street Over I-25, MP 181.5	N	N	N	N	Eliminated	Bridge has a functionally obsolete rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.
Replace/Rehabilitate I-25 Over Plum Creek, MP 181.9	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate Wolfsenberger Over I-25, MP 181.9	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Replace/Rehabilitate UPRR Over I-25, MP 182.2	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Replace/Rehabilitate County Road 107 Over I-25, MP 182.4	N	N	N	N	Eliminated	Bridge has a structurally deficient rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.
Replace/Rehabilitate Black Feather Trail Over I-25, MP 183.3	N	N	N	N	Eliminated	Bridge has a functionally obsolete rating. Bridge does not carry I-25; therefore, improving the bridge does not improve I-25.
Replace/Rehabilitate Founders Over I-25, MP 184.2	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Replace/Rehabilitate Castle Rock Parkway Over I-25, MP 185.0	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Replace/Rehabilitate Happy Canyon Over I-25, MP 187.0	N	N	N	N	Eliminated	Bridge has a structurally deficient rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.

Summary: Segment 2 resulted in 11 Core Concepts and 24 Supplemental Elements being carried forward. 4 Supplemental Elements are Not Recommended for Further Evaluation. 19 Supplemental Elements are Eliminated.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

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Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 2 (MP 179-189): Castle Rock to Castle Pines	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Infrastructure Elements Cont.						
Replace/Rehabilitate Castle Pines Parkway Over I-25 , MP 188.5	N	N	N	N	Eliminated	Bridge has a structurally deficient rating. Bridge does not carry I-25; therefore, improving the bridge does not directly improve I-25.
ALTERNATE ROUTES						
Improve US Highway 85	N	Y	Y	Y	Not Recommended for Further Evaluation	Does not improve I-25 directly and are already available as an alternate routes for incident management. Improvements to local roads would be undertaken at the county or municipal levels.
Improve State Highway 83	N	Y	Y	Y	Not Recommended for Further Evaluation	
Other Local Road Improvements	N	N	N	N	Eliminated	
FRONTAGE ROADS						
Add I-25 Frontage Road from Founders Parkway North to Castle Pines Parkway	N	N	Y	Y	Carried Forward as Supplemental Element	Front Street provides frontage travel south of Founders Parkway. North of Castle Pines Parkway, South Havana Street provides frontage travel. A frontage road in this location could assist in offloading I-25 in the event of I-25 closure north of Castle Rock.
OTHER PHYSICAL ELEMENTS						
Improve Inside Shoulder (width may vary)	Y	Y	N	Y	Carried Forward as Supplemental Element	
Improve Outside Shoulder (width may vary)	Y	Y	N	Y	Carried Forward as Supplemental Element	
New Park-N-Ride Facility	N	N	Y	N	Carried Forward as Supplemental Element	
Add Emergency Parking	Y	N	Y	Y	Carried Forward as Supplemental Element	
Add / Improve Lighting	Y	N	Y	Y	Carried Forward as Supplemental Element	
Infrastructure Elements Cont.						
Add Wildlife Crossings / Fencing	Y	Y	N	N	Carried Forward as Supplemental Element	
Multimodal Elements						
Add Passenger Rail along I-25 (high speed or commuter rail)	N	Y	Y	N	Carried Forward as Supplemental Element	
Add Commuter Rail along Existing BNSF/UPRR Corridor	N	N	N	N	Eliminated	The existing UPRR and BNSF lines bypass the majority of Segment 2, failing to serve either local or regional trips.
Extend Light Rail E/F/R Line(s) South along I-25 to Castle Rock	N	Y	Y	N	Not Recommended for Further Evaluation	Light Rail is not appropriate for Segment 2 (overall distance and distance between stations, speeds, seating capacity, power/grades). Passenger rail along I-25 is evaluated as a separate element.
Add Bus Rapid Transit (BRT) on Dedicated Facility	N	Y	Y	N	Not Recommended for Further Evaluation	Dedicated BRT offers little advantage over enhanced bus service in a regional setting; enhanced conventional bus service can achieve same advantages.

Summary: Segment 2 resulted in 11 Core Concepts and 24 Supplemental Elements being carried forward. 4 Supplemental Elements are Not Recommended for Further Evaluation. 19 Supplemental Elements are Eliminated.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

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Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 2 (MP 179-189): Castle Rock to Castle Pines	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Multimodal Elements Cont.						
Expand Bustang Service	N	Y	Y	N	Carried Forward as Supplemental Element	Reliability would be improved if express lane is implemented with transit priority. Includes more round trips, and new Bustang station in Castle Rock.
Add Front Range Trail Connection to Castle Pines Parkway	N	N	Y	N	Carried Forward as Supplemental Element	
Operations Elements						
Consider Express Lane Fee Structure	N	N	Y	N	Carried Forward as Supplemental Element	In conjunction with conversion of existing lanes to express lanes or new express lanes.
Accommodate Autonomous Vehicle Lanes	Y	Y	Y	N	Carried Forward as Supplemental Element	
Add Queue Warning	Y	Y	N	Y	Carried Forward as Supplemental Element	
Operations Elements Cont.						
Add Ramp Metering	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add Wildlife Detection and Alert System	Y	Y	N	N	Carried Forward as Supplemental Element	
Enhance ITS infrastructure	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add VMS signage	Y	Y	N	Y	Carried Forward as Supplemental Element	
Enhance Lane Markings	Y	Y	N	N	Carried Forward as Supplemental Element	
Implement Variable Speed Limits	Y	Y	N	N	Carried Forward as Supplemental Element	Must include enforcement to be effective.
Implement Changes to Speed Limits	Y	Y	N	N	Carried Forward as Supplemental Element	

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 3 (MP 189-194): Denver South	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
No Action	N	N	N	N	Carried Forward as Core Concept	Although the No Action does not meet the purpose and need, it is advanced as a baseline condition for comparison.
Infrastructure Elements						
LANE CONFIGURATION (NOTE: Existing lane configuration is four lanes each direction.)						
Add One Express Lane Each Direction (five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	Route continuity better when combined with five lane element in Segment 2.
Add One Reversible Lane	Y	Y	Y	Y	Carried Forward as Core Concept	
New Elevated Travelway Each Direction (five lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Convert One Existing Lane to Express Lane (four lanes)	Y	Y	Y	Y	Carried Forward as Core Concept	
Add New Truck Climbing Lane SB at Surrey Ridge	Y	Y	Y	Y	Carried Forward as Core Concept	
Add Auxiliary Lane(s)	Y	Y	Y	Y	Carried Forward as Core Concept	
Convert All Existing Lanes to Express Lanes Each Direction (four lanes)	Y	Y	N	Y	Carried Forward as Core Concept	Would likely push commercial truck traffic onto local road network.
Add/Convert Peak Period Shoulder	Y	Y	N	Y	Carried Forward as Core Concept	
INTERCHANGES						
Improve RidgeGate Parkway / I-25	N	N	N	N	Eliminated	Interchange improvements not needed.
Improve Lincoln Avenue / I-25	N	N	Y	Y	Carried Forward as Supplemental Element	I-25 widening would require reconfiguration of this interchange. Ultimate vision is a single point urban interchange. Will require local partnerships.
Add New Direct Express Lane Connection to E-470 and C-470	N	Y	Y	Y	Carried Forward as Supplemental Element	
STRUCTURES						
Replace/Rehabilitate I-25 Over Surrey Ridge, MP 189.7	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate I-25 Over Happy Canyon Creek, MP 191.1	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Replace/Rehabilitate I-25 Over RidgeGate Parkway, MP 192.0	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge carries I-25.
Add New Future RTD LRT Bridge Over I-25, MP TBD (by others)	N	N	N	N	Eliminated	Improvement by others.
Lincoln Avenue Over I-25, MP 193.0	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.
Add New RTD Pedestrian Bridge Over I-25, MP 193.7 (by others)	N	N	N	N	Eliminated	Improvement by others.
Add New Structures at C-470 Interchange	N	N	N	N	Eliminated	Bridge is in acceptable condition. Bridge does not carry I-25.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 3 (MP 189-194): Denver South	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Infrastructure Elements Cont.						
ALTERNATE ROUTES						
Improve RidgeGate Parkway	N	N	N	N	Eliminated	Does not improve I-25 directly. Local road improvements would be undertaken at the county or municipal levels.
Improve Lincoln Avenue	N	N	N	N	Eliminated	
Other Local Road Improvements	N	N	N	N	Eliminated	
OTHER PHYSICAL ELEMENTS						
Add Chain Up / Chain Down Station	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add Emergency Parking	Y	N	Y	Y	Carried Forward as Supplemental Element	
Add Emergency Median Crossover Between Castle Pines Parkway and RidgeGate Parkway	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add Wildlife Crossings / Fencing	Y	Y	N	N	Carried Forward as Supplemental Element	
Multimodal Elements						
Add Passenger Rail along I-25 (high speed or commuter rail)	N	Y	Y	N	Carried Forward as Supplemental Element	
Add Commuter Rail along Existing BNSF/UPRR Corridor	N	N	N	N	Eliminated	In Segment 3, the existing UPRR and BNSF lines have left the I-25 corridor and, instead, follow the US-85 corridor.
Extend E/F/R Line(s) South of RidgeGate to Castle Rock	N	Y	Y	N	Not Recommended for Further Evaluation	Light Rail is not appropriate for Segment 3 (overall distance and distance between stations, speeds, seating capacity, power/grades). Passenger rail along I-25 is evaluated as a separate element.
Add Bus Rapid Transit (BRT) on Dedicated Facility	N	Y	Y	N	Not Recommended for Further Evaluation	Dedicated BRT offers little advantage over enhanced bus service in a regional setting; enhanced conventional bus service can achieve same advantages.
Expand Bustang Service	N	Y	Y	N	Carried Forward as Supplemental Element	Reliability would be improved if express lane is implemented with transit priority. Includes more round trips.
Add Trail Underpass at Lone Tree Southern Corporate Limits	N	N	Y	N	Carried Forward as Supplemental Element	
Add Bicycle Connectivity to Centennial Trail	N	N	Y	N	Carried Forward as Supplemental Element	
Operations Elements						
Consider Express Lane Fee Structure	N	N	Y	N	Carried Forward as Supplemental Element	In conjunction with conversion of existing lanes to express lanes or new express lanes. Requires conversion of existing lane to express lane or new express lane.
Accommodate Autonomous Vehicle Lanes	Y	Y	Y	N	Carried Forward as Supplemental Element	
Add Queue Warning	Y	Y	N	Y	Carried Forward as Supplemental Element	
Add / Improve Lighting	N	N	N	N	Eliminated	No identified issues with lighting in this segment.
Enhance ITS infrastructure	Y	Y	N	Y	Carried Forward as Supplemental Element	

Summary: Segment 3 resulted in 8 Core Concepts and 18 Supplemental Elements being carried forward. 2 Supplemental Elements are Not Recommended for Further Evaluation. 13 Supplemental Elements are Eliminated.

Level I Evaluation Screening Table

NOTE: At Level 1, each segment is being evaluated individually. Subsequent levels of evaluation will include a review of the dependencies and continuity of elements, and combinations of elements, between the three segments.

Level 1 result classifications are defined as:

Carried Forward as Core Concept = has the potential to address the project's purpose and need by itself (i.e., standalone).

Carried Forward as Supplemental Element = has the potential to enhance the Core Concept(s).

Not Recommended for Further Evaluation = will not be evaluated further in the study due to comparatively negligible benefits, and/or higher impacts than other concepts/elements.

Eliminated = does not meet the Purpose and Need established within this study.

CONCEPTS for SEGMENT 3 (MP 189-194): Denver South	ADVANCES SAFETY? (potential to reduce crashes or improve infrastructure deficiencies on I-25)	IMPROVES RELIABILITY? (potential to improve travel time, improve travel time predictability, or reduce delays on I-25)	ENHANCES REGIONAL AND/OR LOCAL MOBILITY? (potential to provide improved travel options, reduces diversion to local roads)	IMPROVES INCIDENT MANAGEMENT? (potential to reduce response/clear time, improve responder safety, reduce crashes, provide route around accident, improve driver communication/coordination)	RESULT of LEVEL 1 EVALUATION	COMMENTS
Operations Elements Cont.						
Add VMS signage	Y	Y	N	Y	Carried Forward as Supplemental Element	
Enhance Lane Marking	Y	Y	N	N	Carried Forward as Supplemental Element	
Implement Variable Speed Limits	Y	Y	N	N	Carried Forward as Supplemental Element	Must include enforcement to be effective.
Implement Speed Limit Changes	Y	Y	N	N	Carried Forward as Supplemental Element	

Attachment 2
PEL Level 2 Evaluation for the Gap



I-25 PEL - Summary of Level 2 Screening Results
August 10, 2017

Lane Configuration Core Concepts – Segment 1, Segment 2, and Segment 3		
Criteria Category	Common Factors in Benefits and Impacts Among Concepts	Differences in Benefits and Impacts Among Concepts
Safety	<ul style="list-style-type: none"> Supplemental Elements will be necessary to address project needs. Level 3 evaluation will bring Supplemental Elements together with Core Concepts Additional lane would reduce congestion-related crashes Increases potential for lane changing crashes Additional lanes increase weaves at interchanges 	<ul style="list-style-type: none"> Travel speed differential varies if there is an express lane vs. GP lane vs. truck climbing lane Lane reduction influence areas may persist Peak Period Shoulders would eliminate the use of a shoulder for emergency situations such as a broken-down vehicle.
Reliability	<ul style="list-style-type: none"> Supplemental Elements will be necessary to address project needs. Level 3 evaluation will bring Supplemental Elements together with Core Concepts Reduce traffic delay from non-recurring events Improve travel times and likely reliability due to increased capacity 	<ul style="list-style-type: none"> Express lanes promote more reliable travel times
Mobility	<ul style="list-style-type: none"> Supplemental Elements will be necessary to address project needs. Level 3 evaluation will bring Supplemental Elements together with Core Concepts Reduce turbulence in traffic stream with additional passing opportunities Increase capacity Reduce pressure on local roads and alternative routes with additional capacity on I-25 Do not directly support alternative travel choices (e.g., BRT, LRT, Bikes, etc.) but could indirectly improve conditions due to reduced pressure on local roads Improves work zone flexibility Increase in maintenance burden Commercial vehicle travel improved with reduced congestion on I-25 	<ul style="list-style-type: none"> Express lanes provide more travel choices and operational flexibility Express lanes are consistent with CDOT's goals for adding capacity and managing congestion All toll concepts (i.e., converting all GP lanes to express lanes) could increase traffic on local roads for I-25 drivers avoiding tolls, especially in Segments 2 and 3 where alternate routes are available Express lane options less preferable to CMCA due to perceived increase in trip costs and disproportionate cost sharing of new capacity to commercial vehicle companies



Lane Configuration Core Concepts – Segment 1, Segment 2, and Segment 3

Criteria Category	<u>Common Factors in Benefits and Impacts Among Concepts</u>	<u>Differences in Benefits and Impacts Among Concepts</u>
Incident Management	<ul style="list-style-type: none"> • Supplemental Elements will be necessary to address project needs. Level 3 evaluation will bring Supplemental Elements together with Core Concepts • Provide space for safety during incidents • Reduce delays during incidents • Improve incident response times • Potential for crash reduction due to additional room 	<ul style="list-style-type: none"> • Access to express lanes (or elevated section or reversible lane) may dictate impact on incident management • Peak Period Shoulders would eliminate the use of a shoulder for incident management
Compatibility with Community Planning Goals	<ul style="list-style-type: none"> • Benefit economic development regionally and locally • Compatible with Castle Rock and Castle Pines goals and plans (Segment 2) – except all express lane concept 	<ul style="list-style-type: none"> • Stays within ROW or not – tied to conservation easement compatibility (Segment 1) or planned parks (Segment 2)
Environmental Impacts	<ul style="list-style-type: none"> • Will require some level of environmental mitigation. The environmental mitigation required will depend on the degree of impact 	<ul style="list-style-type: none"> • Higher impact for walls or signs, or introduction of new vertical elements • Lower impact for Truck Climbing Lane and Peak Period Shoulder • Mitigation likely due to sensitive environmental conditions



Segment 1: Add One General Purpose Lane Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional lane increases the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability. 	<ul style="list-style-type: none"> Would provide additional capacity but may not result in reliable travel times during high volume periods when the highway becomes congested.
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lane would increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. New travel lane would provide more capacity to serve trips on I-25, which would reduce travel pressure and volumes on local roads and alternate routes. New lane would likely not impact existing transit operations but would not preclude future transit options. New lane would not provide any additional support for non-vehicular travel choices. Not consistent with CDOT's goals for adding capacity and managing congestion. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. Freight travel could be improved and conflicts reduced by having additional travel lane. 	



Segment 1: Add One General Purpose Lane Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity A three-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Likely to stay within ROW and be assessed in an EA. 	<ul style="list-style-type: none"> Retaining walls associated with widening would introduce new vertical element. Biological and aquatic Resources impacts to Plum Creek and its tributaries would likely require mitigation.



Segment 1: Add One Express Lane Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) 	<ul style="list-style-type: none"> Additional travel lane could reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lanes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across three lanes may be limited due to restriction in use of Express Lane. Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improve assessm event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lane would be operated to promote consistent speeds and reliable travel times. Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending on level of use of Express Lanes. 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time. Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane. New travel lane could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lane which could reduce travel pressure and volumes on local roads and alternate routes. Consistent with CDOT's goals for adding capacity and managing congestion. 	



Segment 1: Add One Express Lane Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New express lane(s) may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. New express lane(s) does not preclude future transit options. New express lane(s) may provide support for car/van pooling. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A three-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Likely to stay within CDOT's ROW and be assessed. 	<ul style="list-style-type: none"> Mitigation may be needed due to sensitive environmental conditions in surrounding area. Retaining walls, signs, and gantries associated with Express Lane would introduce new vertical element.



Segment 1: Add One General Purpose Lane and One Express Lane Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) 	<ul style="list-style-type: none"> Additional GP and Express travel lanes would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional GP travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional GP lane increases the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. Additional GP lane increases the weaving length and number of lanes to weave across at interchanges for Express lane drivers. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lane would be operated to promote consistent speeds and reliable travel times. Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Would provide additional capacity and the additional express lane will provide reliable travel times during high volume periods when the highway becomes congested. 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time. Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane. New travel lanes could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes. Consistent with CDOT's goals for adding capacity and managing congestion in urban corridors. 	



Segment 1: Add One General Purpose Lane and One Express Lane Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New express lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. New express lane does not preclude future transit options. New express lane may provide support for car/van pooling. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one general purpose lane and one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A four-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay with CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Outside widening increases potential for in compatibility with Conservation Easements. Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. While potentially being able to be constructed within existing CDOT ROW, culvert widening and bridge reconstruction would impact sensitive environmental resources concentrated along Plum Creek and its tributaries, and may impact the historic and recreational resources adjacent to I-25. 	<ul style="list-style-type: none"> Mitigation likely to be need do to sensitive environmental conditions in surrounding area. Retaining walls, signs, and gantries associated with Express Lane would introduce new vertical element. Widening to the outside would bring traffic closer to the campground and residential areas in the Monument area. Outside widening would increase potential for impacts to Plum Creek floodplain, riparian areas, and potentially eligible historic properties bordering the corridor.



Segment 1: Add Two General Purpose Lanes Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lanes would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lanes would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional travel lanes increase the potential for crashes related to lane changing that occur when drivers to the left and right attempt to maneuver into the same lane at the same time. Additional travel lanes increase the weaving length and number of lanes to weave across at interchanges. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability. 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lanes would increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. New travel lanes would provide more capacity to serve trips on I-25, which would reduce travel pressure and volumes on local roads and alternate routes. New lane would likely not impact existing transit operations but would not preclude future transit options. New lanes would not provide any additional support for non-vehicular travel choices. Not consistent with CDOT's goals for adding capacity and managing congestion. Operations and maintenance work are improved because additional lanes improve work zone flexibility. 	



Segment 1: Add Two General Purpose Lanes Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> Additional lanes increase the maintenance/plowing burden. Freight travel could be improved and conflicts reduced by having additional travel lanes. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding two general purpose lanes in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity A four-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Outside widening increases potential for incompatibility with conservation easements. Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. While potentially being able to be constructed within existing CDOT ROW, culvert widening and bridge reconstruction would impact sensitive environmental resources concentrated along Plum Creek and its tributaries,, and may impact the historic and recreational resources adjacent to I-25. 	<ul style="list-style-type: none"> Mitigation likely to be needed due to sensitive environmental conditions in surrounding area. Retaining walls would introduce new vertical element. Widening to the outside would bring traffic closer to the campground and residential areas in the Monument area. Outside widening would increase potential for impacts to Plum Creek floodplain, riparian areas, and potentially eligible historic properties bordering the corridor.



Segment 1: Add Two Express Lanes Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lanes could reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lanes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. Additional travel lanes increase the weaving length and number of lanes to weave across at interchanges. 	<ul style="list-style-type: none"> Maneuverability and crash reduction across four lanes may be limited due to restriction in use of Express Lanes. Opportunity to address specific locations for localized safety improvements in combination with this element. <input type="checkbox"/> Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lanes would be operated to promote consistent speeds and reliable travel times. Additional lanes would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending on level of use of Express Lanes. 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. Additional travel lanes provides improved travel options for drivers that choose to pay for a reliable travel time. Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lanes. New travel lanes could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lanes which could reduce travel pressure and volumes on local roads and alternate routes. Consistent with CDOT's goals for adding capacity and managing congestion. 	



Segment 1: Add Two Express Lanes Each Direction (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. New express lanes do not preclude future transit options. New express lanes may provide support for car/van pooling. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding two Express Lanes in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially less congested lanes with more shoulder room to utilize to travel to incidents. A four-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Outside widening increases potential for incompatibility with Conservation Easements. Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. While potentially being able to be constructed within existing CDOT ROW, culvert widening and bridge reconstruction would impact sensitive environmental resources concentrated along Plum Creek and its tributaries, and may impact the historic and recreational resources adjacent to I-25. 	<ul style="list-style-type: none"> Mitigation likely to be needed due to sensitive environmental conditions in surrounding area. Retaining walls, signs, and gantries associated with Express Lane would introduce new vertical element. Widening to the outside would bring traffic closer to the campground and residential areas in the Monument area. Outside widening would increase potential for impacts to Plum Creek floodplain, riparian areas, and potentially eligible historic properties bordering the corridor.



Segment 1: Add Auxiliary Lanes between Tomah exit and Larkspur exit

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Allows acceleration/deceleration to occur outside of the mainline travel lanes. This will promote more homogeneous travel speeds in the through lanes that will reduce turbulence, which has been demonstrated to improve safety. Additional lane would increase maneuverability when an incident occurs adjacent to the auxiliary lanes. 	
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> New auxiliary lane would partially reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. Provides additional queuing capacity for traffic exiting to Renaissance Festival 	<ul style="list-style-type: none"> Would be a more viable option with options that add additional capacity by either GP or managed lanes.
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New auxiliary lane would add local capacity to I-25, increasing the appeal for local trips and potentially removing some traffic from the local road network. New lane would likely not impact existing transit operations but would not preclude future transit options and could provide support for future local bus routing. New lane would not provide any additional support for non-vehicular travel choices. New lane is not consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> New auxiliary lanes would improve incident response times in this segment and potentially further if the improvements are tied to improvements to Spruce Mountain Road. The auxiliary lanes would provide better access to emergency detour and alternate routes during all incident types. 	<ul style="list-style-type: none">
Compatibility with Community Planning Goals			



Segment 1: Add Auxiliary Lanes between Tomah exit and Larkspur exit

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Compatible. Impact to Bear Dance Conservation Easement west of I-25 could likely be avoided by constructing auxiliary lane within CDOT ROW. Would not significantly improve frontage access to neighborhoods adjacent to the Conservation Easement, which is currently provided by Bear Dance Drive, Old Territorial Road, and Perry Park Avenue. Conceptually would not preclude planned local transportation projects 	<ul style="list-style-type: none"> Columbine Open Space/Fee Title Area present along east side of I-25 south of Tomah Road. Bear Dance Golf Course is west of I-25 and separated by Bear Dance Drive, the BNSF rail line, and the existing Frontage Road. Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Auxiliary lane west of the highway could likely be constructed within CDOT existing ROW. Mitigation likely needed for biological and aquatic resources impacts associated with Plum Creek tributaries 	<ul style="list-style-type: none"> Mitigation for Plum Creek tributary crossing impacts would likely be needed. Preble's meadow jumping mouse occupied range present along all the Plum Creek tributaries between Upper Lake Gulch Road and Tomah Road. J A Ranch, potentially eligible historic property located east of I-25, south of Tomah Road.



Segment 1: Add Southbound Truck Climbing Lane(s) or Passing Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Separate lanes for slower moving vehicles would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Reduces the potential for rear-end and sideswipe same direction crashes involving slow-moving trucks. 	
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> New SB truck climbing or passing lane would partially reduce traffic delay from slow moving vehicles passing another slow moving vehicle. Additional capacity would partially decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability. 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New SB truck climbing or passing lane would add capacity to I-25. New SB truck climbing or passing lane would likely not impact existing transit operations but would not preclude future transit options and could provide support for future local bus routing. New SB truck climbing or passing lane would not provide any additional support for non-vehicular travel choices. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding southbound truck climbing or passing lanes has the potential to reduce crashes by minimizing slow moving vehicle impacts on traffic flow It would improve incident response times for southbound incidents and provide for more space to safely respond to incidents where it is located. 	
Compatibility with Community Planning Goals			



Segment 1: Add Southbound Truck Climbing Lane(s) or Passing Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Compatible. Coordination with Conservation Easement and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Truck lane would provide improved movement of freight. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Adjacent Greenland Ranch is Conservation and Fee Title Area. Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely to stay within CDOT ROW and qualify for CE. 	<ul style="list-style-type: none"> Greenland Ranch straddles I-25 through the Monument Hill area. Plum Creek major tributaries are located east of the northbound side of I-25 through Monument Hill area. Gantries and signage associated with truck lane would introduce new vertical element in the Gap. Likely to be supported by CMCA .



Segment 1: Add New Elevated Travelway Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lane would be operated to promote consistent speeds and reliable travel times. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending on level of use of Express Lanes. Elevated roadway would be more prone to icing 	<ul style="list-style-type: none"> Assume Travel way is an Express Lane More predictable travel times could improve driver behavior and promote a safer and more pleasant travel experience.
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time. Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane. New travel lane could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes. Diversion to local roads could persist if drivers choose not to use Express Lane. Consistent with CDOT's goals for adding capacity and managing congestion. Restricted width limits flexibility for mobility during incident management. 	<ul style="list-style-type: none"> Assume Travel way is an Express Lane Maneuverability and crash reductions across three lanes may be limited due to restriction in use of Express Lane.



Segment 1: Add New Elevated Travelway Each Direction (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A three-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Compatible with conservation easements from a ROW standpoint; not compatible from a land use standpoint. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects 	<ul style="list-style-type: none"> Elevated travelway would border open space and conservation easements Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would constrain Crystal Valley interchange construction
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Although likely able to be completed within existing ROW, an elevated travelway would significantly change the visual character of the corridor. Support structures for facility could act as a wildlife boundary, further disrupting large animal movement 	<ul style="list-style-type: none"> Cantilever or viaduct-type of structure would introduce significant new vertical element to the area, potentially obstructing views of and from the road.



Segment 1: Add Express Lane and Convert Existing General Purpose Lanes to Express Lanes (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional lane increases the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as the express lanes would be operated to promote more consistent speeds and reliable travel times. Additional lanes would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures. 	<ul style="list-style-type: none"> Would require legislative action at state and federal level
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> Imposing a toll on all general purpose lanes will likely encourage more traffic to divert to local roads. Repurposed express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. Repurposed express lanes do not preclude future transit options. Repurposed express lanes may provide support for car/van pooling. New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident. New travel lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time. Consistent with CDOT's goals for adding capacity and managing congestion. New express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. 	



Segment 1: Add Express Lane and Convert Existing General Purpose Lanes to Express Lanes (three lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lanes and converting one general purpose lane to an Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially less congested lanes with more shoulder room to utilize to travel to incidents. A three-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW. Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Development and access requests (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Likely to stay withing ROW and be assessed in an EA 	<ul style="list-style-type: none"> Mitigation may be need due to sensitive environmental area. Retaining walls, signs, and gantries associated with Express Lanes would introduce new vertical element. Conversion of existing general purpose lane to Express Lane could generate public support or criticism



Segment 1: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Should be implemented with speed harmonization or reduced speed limits to promote homogeneous travel speeds, which has been demonstrated to improve safety. May impact safety of first responders at incident scene. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as the shoulder lane would be operated to promote more consistent speeds and reliable travel times during peak periods. Off-peak and short-duration congestion would continue to impact travel times. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending on level of use of Express Lanes. 	<ul style="list-style-type: none"> Assume the peak period shoulder would be a express lane.
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New shoulder lane would draw some traffic from general purpose lanes, increasing the appeal for local trips and potentially removing some traffic from the local road network. New shoulder lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. New shoulder lane does not preclude future transit options. New shoulder lane would not provide any additional support for non-vehicular travel choices. New shoulder lane is consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors. New shoulder lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident. New shoulder lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream. New shoulder lanes provides improved travel options for drivers that choose to pay for a reliable travel time. Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lanes. 	<ul style="list-style-type: none"> Assume the peak period shoulder would be a express lane. Mobility benefits are when Peak Period Shoulder is operational.



Segment 1: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New shoulder lane could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lanes which could reduce travel pressure and volumes on local roads and alternate routes. New shoulder lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE. Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding/convert peak period shoulders in each direction to I-25 would enhance its reliability during peak times. Because the shoulder is being utilized for travel, it is not available for incident management activities. Responders may need to close the affected lane and the adjacent lane to safely deal with incidents, minimizing its effectiveness at improving incident management activities. It may improve incident response times by providing more capacity upstream of the incident. 	<ul style="list-style-type: none"> The design and operation of the peak period shoulder lane has a large impact on its usefulness for incident management and reduction in delays.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Coordination with conservation easements and restrictions would be required but likely compatible if improvements stay within CDOT's ROW Improvements to mobility and reliability would benefit economic development regionally and locally. Conceptually would not preclude planned local transportation projects. 	<ul style="list-style-type: none"> Development and access request (e.g., Jellystone Campground expansion) would continue to be reviewed per CDOT's normal procedures. Conceptually would not preclude Crystal Valley interchange construction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely stay within ROW and qualify for CE 	<ul style="list-style-type: none"> Mitigation may be needed due to sensitive environmental conditions in surrounding area. Retaining walls, signs, and gantries associated with Express lane would introduce new vertical element.



Segment 2: Add One General Purpose Lane Each Direction south of Founders Parkway (continuous four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional lane increases the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane would increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream New travel lane would provide more capacity to serve trips on I-25, which would reduce travel pressure and volumes on local roads and alternate routes New lane would likely not impact existing transit operations but would not preclude future transit options New lane would not provide any additional support for non-vehicular travel choices Not consistent with CDOT's goals for adding capacity and managing congestion Operations and maintenance work are improved because an additional route improves work zone flexibility Additional road increases the maintenance/plowing burden Freight travel could be improved and conflicts reduced by having additional route choice 	



Segment 2: Add One General Purpose Lane Each Direction south of Founders Parkway (continuous four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one general purpose lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity A four-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none">
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned mixed use areas in Castle Rock south of Founders Parkway. Compatible with Castle Rock transportation goals to develop an efficient roadway network to allow for ease of movement through the Town. Conceptually does not preclude local transportation 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> IFair. Could likely be constructed within existing CDOT ROW and be assessed in an EA. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area. 	<ul style="list-style-type: none"> Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. \ Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway



Segment 2: Add One Express Lane Each Direction South of Founders Parkway (continuous four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane could reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lanes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across three lanes may be limited due to restriction in use of Express Lane. Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as express lane would be operated to promote more consistent speeds and reliable travel times Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of express lanes 	<ul style="list-style-type: none">
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane New travel lane could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lane which could reduce travel pressure and volumes on local roads and alternate routes Consistent with CDOT's goals for adding capacity and managing congestion. New express lane(s) may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE New express lane(s) does not preclude future transit options 	<ul style="list-style-type: none">



Segment 2: Add One Express Lane Each Direction South of Founders Parkway (continuous four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New express lane(s) may provide support for car/van pooling Operations and maintenance work are improved because additional lanes improve work zone flexibility Additional lanes increase the maintenance/plowing burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A four-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned mixed use areas in Caste Rock south of Founders Parkway. Compatible with Castle Rock transportation goals to develop an efficient roadway network to allow for ease of movement through the Town. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Could likely be constructed within existing CDOT ROW and be assessed in an EA. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area. 	<ul style="list-style-type: none"> Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway. Signs and gantries associated with Express Lane would introduce new vertical element.



Segment 2: Add Two General Purpose Lanes Each Direction South of Founders Parkway; Add One General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lanes would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Additional lanes increase the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. Crashes may persist in southbound direction at boundary between Segments 1 and 2 in lane-reduction influence area. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Additional lane would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lanes would increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream New travel lanes would provide more capacity to serve trips on I-25, which would reduce travel pressure and volumes on local roads and alternate routes New lane would likely not impact existing transit operations but would not preclude future transit options New lanes would not provide any additional support for non-vehicular travel choices Not consistent with CDOT's goals for adding capacity and managing congestion Operations and maintenance work are improved because additional lanes improve work zone flexibility. Additional lanes increase the maintenance/plowing burden 	



Segment 2: Add Two General Purpose Lanes Each Direction South of Founders Parkway; Add One General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> Freight travel could be improved and conflicts reduced by having additional travel lanes. 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding two general purpose lanes (one north of Founder's Parkway) for a continuous five lane section in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity A five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> An increase in weaving type crashes may offset the crash benefit
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned residential and mixed use areas in Castle Rock and Castle Pines. Compatible with Castle Rock and Castle Pines transportation goals to improve travel reliability and support mixed use development. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area. Conceptually does not preclude construction of Crystal Valley Road interchange or reconstruction of the Happy Canyon Road interchange.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Unlikely to be constructed within existing CDOT ROW with increased potential for significant impacts. Class of action likely an EA or EIS. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area and Newlin Gulch tributaries. 	<ul style="list-style-type: none"> Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south and north of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway. Sensitive noise land uses bordering I-25 through Castle Pines



Segment 2: Add One General Purpose Lane and One Express Lane Each Direction South of Founders Parkway; Add One Express Lane or General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lanes would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lane. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. Additional lanes increase the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. Crashes may persist in southbound direction at boundary between Segments 1 and 2 in lane-reduction influence area. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across four lanes may be limited due to restriction in use of Express Lane. Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as express lane would be operated to promote more consistent speeds and reliable travel times Additional lanes would reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures Would provide additional capacity and the additional lanes will provide reliable travel times during high volume periods when the highway becomes congested. 	<ul style="list-style-type: none">
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane 	<ul style="list-style-type: none">



Segment 2: Add One General Purpose Lane and One Express Lane Each Direction South of Founders Parkway; Add One Express Lane or General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New travel lanes could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes Consistent with CDOT's goals for adding capacity and managing congestion in urban corridors. New express lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE New travel lanes do not preclude future transit options New express lane may provide support for car/van pooling Operations and maintenance work are improved because 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one general purpose lane and one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management. An increase in weaving crashes may offset the crash benefit
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned residential and mixed use areas in Castle Rock and Castle Pines. Compatible with Castle Rock and Castle Pines transportation goals to improve travel reliability and support mixed use development. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area. Conceptually does not preclude construction of Crystal Valley Road interchange or reconstruction of the Happy Canyon Road interchange.
Environmental Impacts			



Segment 2: Add One General Purpose Lane and One Express Lane Each Direction South of Founders Parkway; Add One Express Lane or General Purpose Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Unlikely to be constructed within existing CDOT ROW with increased potential for significant impacts. Class of action likely an EA or EIS. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area and Newlin Gulch tributaries. 	<ul style="list-style-type: none"> Preble’s meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT’s ROW in Segment 2 south and north of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway. Sensitive noise land uses bordering I-25 through Castle Pines. Increased signage and gantryways associated with Express Lanes would increase presence of vertical elements throughout Segment 2.



Segment 2: Add Two Express Lanes Each Direction South of Founders Parkway; Add One Express Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lanes could reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lanes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. Additional lanes increase the potential for crashes related to lane changing that occur when drivers in the left and right lanes attempt to maneuver into the center lane at the same time. Crashes may persist in southbound direction at boundary between Segments 1 and 2 in lane-reduction influence area. Additional travel lanes would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across three lanes may be limited due to restriction in use of Express Lane.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as express lanes would be operated to promote more consistent speeds and reliable travel times Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of express lanes 	<ul style="list-style-type: none">
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) 	<ul style="list-style-type: none"> New travel lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lanes 	<ul style="list-style-type: none">

Segment 2: Add Two Express Lanes Each Direction South of Founders Parkway; Add One Express Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
	<ul style="list-style-type: none"> Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lanes could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lanes which could reduce travel pressure and volumes on local roads and alternate routes Consistent with CDOT's goals for adding capacity and managing congestion New express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTA New express lanes do not preclude future transit options New express lanes may provide support for car/van pooling Operations and maintenance work are improved because additional lanes improve work zone flexibility Additional lanes increase the maintenance/plowing burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Implementing two Express Lanes in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially less congested lanes with more shoulder room to utilize to travel to incidents. A five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management. An increase in weaving type crashes may offset the crash benefit
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned residential and mixed use areas in Castle Rock and Castle Pines. Compatible with Castle Rock and Castle Pines transportation goals to improve travel reliability and support mixed use development. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area. Conceptually does not preclude construction of Crystal Valley Road interchange or reconstruction of the Happy Canyon Road interchange.
Environmental Impacts			



Segment 2: Add Two Express Lanes Each Direction South of Founders Parkway; Add One Express Lane Each Direction North of Founders Parkway (continuous five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Unlikely to be constructed within existing CDOT ROW with increased potential for significant impacts. Class of action likely an EA or EIS. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area and Newlin Gulch tributaries. 	<ul style="list-style-type: none"> Preble’s meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT’s ROW in Segment 2 south and north of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway. Sensitive noise land uses bordering I-25 through Castle Pines. Increased signage and gantryways associated with Express Lanes would increase presence of vertical elements throughout Segment 2

Segment 2: Add One Reversible Lane

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<p>Safety</p> <ul style="list-style-type: none"> • Reduce crashes • Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> • Potential for crash reduction (qualitative) • Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> • Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. • Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. • Introduces a weaving section where lane merges into GP lanes, which could increase turbulence and potential for rear-end and sideswipe same direction crashes at the merge influence area. 	<ul style="list-style-type: none"> • Opportunity to address specific locations for localized safety improvements in combination with this element.
<p>Reliability</p> <ul style="list-style-type: none"> • Improve travel times • Improve predictability of travel times • Reduce delays on I-25 	<ul style="list-style-type: none"> • Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> • Travel reliability would be improved as reversible lane would be operated to promote more consistent speeds and reliable travel times in one direction only • Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability in one direction only • Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of reversible lanes 	<ul style="list-style-type: none"> •
<p>Mobility</p> <ul style="list-style-type: none"> • Provide additional travel options • Serves regional trip demand on I-25 • Reduces I-25 traffic diversions to local roads • Promotes multimodal options • Supports CDOT network goals for operations • Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> • Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) • Ability to improve existing transit operations (qualitative) • Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> ○ Bus Rapid Transit (BRT) ○ Commuter Rail ○ High-Speed Rail (ICS) • Provides additional travel choices (options for multiple modes and travel choices) • Compatible with CDOT operational strategies for I-25 • Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> • New reversible lane would provide additional space for incident response and allow more through traffic to maneuver around the incident • New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream • Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time • Freight travel could be improved and conflicts reduced if other drivers choose to use reversible lane • New travel lane could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes 	<ul style="list-style-type: none"> • Assumes reversible lane has managed/special use restrictions (i.e., HOV and transit only)

Segment 2: Add One Reversible Lane

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> • Diversion to local roads could persist if drivers choose not to use managed reversible lane • Consistent with CDOT's goals for adding capacity and managing congestion • Restricted width limits flexibility for mobility during incident management 	
Incident Management			
<ul style="list-style-type: none"> • Reduce delays related to incident management • Improve safety for drivers and responders during incidents • Improve driver communication / coordination • Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> • Improved time to respond to incidents (qualitative) • Potential for crash reduction (qualitative) • Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> • Adding a reversible lane would not affect incident response times significantly due to the need to control the lane with fixed barriers and gates thus reducing access opportunities. • There would be the possibility of using the reversible lane to move more traffic in the affected direction, however this would only be useful during Major or Extended incidents due to the time for changeover of the lane. • If the lane is configured in the direction affected by the incident it would help reduce driver delay and could improve incident response times. • Some additional width in the transition areas of the reversible lane and entries/exits may be available to use as safety or staging areas for incident management 	<ul style="list-style-type: none"> • This improvement could cause complications for first responders if the incident is in the reversible lane but they respond to the general purpose lanes.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> • Compatible with Conservation Easements • Does not preclude community land use or transportation goals / projects in Master Plans • Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> • Compatible with Conservation Easements • Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) • Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> • No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. • Could promote economic development for planned mixed use areas in Caste Rock south of Founders Parkway. • Compatible with Castle Rock transportation goals to develop an efficient roadway network to allow for ease of movement through the Town. • Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> • Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area.
Environmental Impacts			
<ul style="list-style-type: none"> • Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> • Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> • Good. Likely constructible within CDOT's existing ROW and qualify for a CE with minimal mitigation. 	<ul style="list-style-type: none"> • Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. • Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south of Founders Parkway. • Plum Creek regulatory floodway crossing just north of Plum Creek Parkway.



Segment 2: Add One Reversible Lane

Criteria	Level 2 Performance Measure	Result	Comments / Notes
			<ul style="list-style-type: none">Signs and gantries associated with reversible lane would introduce new vertical element.

Segment 2: Add New Elevated Travelway Each Direction (four/five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lane would be operated to promote consistent speeds and reliable travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of Express Lane Elevated roadway would be more prone to icing 	<ul style="list-style-type: none">
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane New travel lane could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes Diversion to local roads could persist if drivers choose not to use Express Lane Consistent with CDOT's goals for adding capacity and managing congestion Restricted width limits flexibility for mobility during incident management 	<ul style="list-style-type: none"> Assumes travel-way has managed/special use restrictions (i.e., HOV and transit only)

Segment 2: Add New Elevated Travelway Each Direction (four/five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A four or five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management. An increase in weaving crashes may offset reductions in other crash types
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Compatible with economic development goals of Castle Rock and Castle Pines; incompatible with land use goal to maintain rural/small town character. 	<ul style="list-style-type: none"> Conceptually may constrain Happy Canyon Road interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Compatible with economic development goals of Castle Rock and Castle Pines; incompatible with land use goal to maintain rural/small town character. 	<ul style="list-style-type: none"> Cantilever or viaduct-type of structure would introduce significant new vertical element to the area, potentially obstructing views of and from the road. Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway.

Segment 2: Convert One Existing Lane to Express Lane (three/four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<p>Safety</p> <ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Depending upon level of Express Lane use, congestion may increase in GP lanes, which would increase congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across two/three lanes may be limited due to restriction in use of Express Lane. Opportunity to address specific locations for localized safety improvements in combination with this element.
<p>Reliability</p> <ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved in Express Lane as it would be operated to promote consistent speeds and reliable travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of Express Lane Peak period travel reliability and travel times may degrade in general purpose lanes as express lane usage will likely be lower than previous use as a general purpose lane 	
<p>Mobility</p> <ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> Repurposing one general purpose lane as an Express Lane may encourage more traffic to divert to local roads Consistent with CDOT's goals for adding capacity and managing congestion Express Lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE Express Lane does not preclude future transit options Express Lane may provide support for car/van pooling Additional Express Lane technology will increase the maintenance burden 	

Segment 2: Convert One Existing Lane to Express Lane (three/four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Converting one general purpose lane to an Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would improve incident response times by providing a potentially less congested lane with more shoulder room to utilize to travel to incidents. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lane to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Compatible with Castle Rock and Castle Pines transportation goals to develop an efficient roadway network to allow for ease of movement through the I-25 corridor. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually does not preclude Happy Canyon Road interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely to qualify for CE. Mitigation minimal or absent 	<ul style="list-style-type: none"> Gantries and signage associated with Express Lane would increase number of vertical elements in Segment 2. Minimal or absent widening would greatly reduce potential for impacts to aquatic and biological resources.



Segment 2: Convert All Existing Lanes to Express Lanes (three/four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> No impact to safety, other than spot specific safety improvements. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved in Express Lanes as they would be operated to promote more consistent speeds and reliable travel times 	<ul style="list-style-type: none">
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> Imposing a toll on all general purpose lanes will likely encourage more traffic to divert to local roads Repurposed express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE Repurposed express lanes do not preclude future transit options Repurposed express lanes may provide support for car/van pooling Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time Consistent with CDOT's goals for adding capacity and managing congestion Additional Express Lanes technology will increase the maintenance burden 	<ul style="list-style-type: none">
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> If all lanes were tolled, it is assumed that traffic would divert to another roadway system, thus reducing overall traffic and improving incident response times. This alternative could increase the number of incidents occurring on the local road system. 	<ul style="list-style-type: none">
Compatibility with Community Planning Goals			

Segment 2: Convert All Existing Lanes to Express Lanes (three/four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Incompatible with local transportation plans as I-25 traffic would be pushed onto the local roadway network, specifically in Castle Rock and Castle Pines. Conceptually may constrain or preclude local transportation projects not anticipated or designed to handle interstate traffic. 	<ul style="list-style-type: none"> Conceptually would not preclude Happy Canyon Road interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Converting all lanes likely to push a higher-than-existing-volume and type of traffic onto the local roadway network. Class of action anticipated to be an EIS. 	<ul style="list-style-type: none"> Anticipated strong public controversy and high degree of secondary impacts to surrounding local roadway network and adjacent sensitive resources. Gantries and signage associated with Express Lanes would increase number of vertical elements in Segment 2.

Segment 2: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Should be implemented with speed harmonization or reduced speed limits to promote homogeneous travel speeds, which has been demonstrated to improve safety. May impact safety of first responders at incident scene. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as the shoulder lane would be operated to promote more consistent speeds and reliable travel times during peak periods Off-peak and short-duration congestion would continue to impact travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of managed shoulder lanes 	<ul style="list-style-type: none"> Assume the peak period shoulder would be a express lane
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New shoulder lane would draw some traffic from general purpose lanes, increasing the appeal for local trips and potentially removing some traffic from the local road network New shoulder lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE New shoulder lane does not preclude future transit options New shoulder lane would not provide any additional support for non-vehicular travel choices New shoulder lane is consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors New shoulder lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident New shoulder lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream New shoulder lanes provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use managed shoulder lanes 	<ul style="list-style-type: none"> Assumes peak period shoulder lane is a managed lane (i.e., tolled, HOV, transit, etc) similar to I-70 PPSL

Segment 2: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<p>Incident Management</p> <ul style="list-style-type: none"> • Reduce delays related to incident management • Improve safety for drivers and responders during incidents • Improve driver communication / coordination • Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> • Improved time to respond to incidents (qualitative) • Potential for crash reduction (qualitative) • Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> • Adding/convert peak period shoulders in each direction to I-25 would enhance its reliability during peak times. • Because the shoulder is being utilized for travel, it is not available for incident management activities. Responders may need to close the affected lane and the adjacent lane to safely deal with incidents, minimizing its effectiveness at improving incident management activities. • It may improve incident response times by providing more capacity upstream of the incident. 	<ul style="list-style-type: none"> • The design and operation of the peak period shoulder lane has a large impact on its usefulness for incident management and reduction in delays.
<p>Compatibility with Community Planning Goals</p> <ul style="list-style-type: none"> • Compatible with Conservation Easements • Does not preclude community land use or transportation goals / projects in Master Plans • Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> • Compatible with Conservation Easements • Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) • Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> • No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. • Compatible with Castle Rock and Castle Pines transportation goals to develop an efficient roadway network to allow for ease of movement through the I-25 corridor. • Conceptually does not preclude local transportation projects. • Could promote economic development for planned residential and mixed use areas in Castle Rock and Castle Pines. 	<ul style="list-style-type: none"> • Conceptually does not preclude Happy Canyon Road interchange reconstruction. • Shoulder widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area
<p>Environmental Impacts</p> <ul style="list-style-type: none"> • Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> • Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> • Good. Could likely be constructed within existing CDOT ROW and qualify as a CE. 	<ul style="list-style-type: none"> • Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. • Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south of Founders Parkway. • Plum Creek regulatory floodway crossing just north of Plum Creek Parkway. • Signs and gantries associated with Peak Period Shoulder would introduce new vertical element.

Segment 2: Add Auxiliary Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Allows acceleration/deceleration to occur outside of the mainline travel lanes. This will promote more homogeneous travel speeds in the through lanes that will reduce turbulence, which has been demonstrated to improve safety. Additional lane would increase maneuverability when an incident occurs adjacent to the auxiliary lanes. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element. Improved shoulders are a key element for safety in all segment-long alternatives.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> New auxiliary lane would partially reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures Additional capacity would partially decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability 	<ul style="list-style-type: none"> Would be a more viable option with options that add additional capacity by either GP or managed lanes
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New auxiliary lane would add local capacity to I-25, increasing the appeal for local trips and potentially removing some traffic from the local road network New lane would likely not impact existing transit operations but would not preclude future transit options and could provide support for future local bus routing New lane would not provide any additional support for non-vehicular travel choices New lane is not consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors Operations and maintenance work are improved because additional lanes improve work zone flexibility Additional lanes increase the maintenance/plowing burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> New auxiliary lanes would improve incident response times in this segment. The auxiliary lanes would provide better access to emergency detour and alternate routes during all incident types. 	
Compatibility with Community Planning Goals			

Segment 2: Add Auxiliary Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No conservation easements present; however, existing and planned parks and are adjacent to I-25 south of Founders within Castle Rock. Could promote economic development for planned residential and mixed use areas in Castle Rock and Castle Pines. Compatible with Castle Rock and Castle Pines transportation goals to improve travel reliability and support mixed use development. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Widening toward Plum Creek in Castle Rock would immediately encounter the Plum Creek floodplain/floodway area. Conceptually may constrain reconstruction of the Happy Canyon Road interchange.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. Unlikely to be constructed within existing CDOT ROW Class of action likely an EA or EIS. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area and Newlin Gulch tributaries 	<ul style="list-style-type: none"> Preble's meadow jumping mouse habitat along west side of I-25 from Plum Creek Parkway to north of Wolfersberger Road interchanges. Potential eligible historic properties on both sides of CDOT's ROW in Segment 2 south and north of Founders Parkway. Plum Creek regulatory floodway crossing just north of Plum Creek Parkway.



Segment 3: Add One Express Lane Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane could reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Congestion and resulting congestion-related crashes in existing GP lanes may persist depending upon level of use of Express Lanes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as express lane would be operated to promote more consistent speeds and reliable travel times Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of express lane 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane New travel lane could provide more capacity to serve trips on I-25 if other drivers choose to use Express Lane which could reduce travel pressure and volumes on local roads and alternate routes Consistent with CDOT's goals for adding capacity and managing congestion 	



Segment 3: Add One Express Lane Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New express lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE New express lane does not preclude future transit options New express lane may provide support for car/van pooling Operations and maintenance work are improved because additional lane improves work zone flexibility Additional lane increase the maintenance/plowing burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management. An increase in weaving crashes may offset the crash benefit
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually would not preclude Lincoln Avenue interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. May not be able to be constructed within existing CDOT ROW. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area. 	<ul style="list-style-type: none"> ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: Add One Reversible Lane

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Additional travel lane would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Introduces a weaving section where lane merges into GP lanes, which could increase turbulence and potential for rear-end and sideswipe same direction crashes at the merge influence area. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as reversible lane would be operated to promote more consistent speeds and reliable travel times in one direction only Additional capacity would decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability in one direction only Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of reversible lanes 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New reversible lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use reversible lane New travel lane could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes Diversion to local roads could persist if drivers choose not to use managed reversible lane Consistent with CDOT's goals for adding capacity and managing congestion 	<ul style="list-style-type: none"> Assumes reversible lane has managed/special use restrictions (i.e., HOV and transit only)



Segment 3: Add One Reversible Lane

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> Restricted width limits flexibility for mobility during incident management 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding a reversible lane would not affect incident response times significantly due to the need to control the lane with fixed barriers and gates thus reducing access opportunities. There would be the possibility of using the reversible lane to move more traffic in the affected direction, however this would only be useful during Major or Extended incidents due to the time for changeover of the lane. If the lane is configured in the direction affected by the incident it would help reduce driver delay and could improve incident response times. Some additional width in the transition areas of the reversible lane and entries/exits may be available to use as safety or staging areas for incident management 	<ul style="list-style-type: none"> This improvement could cause complications for first responders if the incident is in the reversible lane but they respond to the general purpose lanes.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually would not preclude Lincoln Avenue interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Fair. May not be able to be constructed within existing CDOT ROW. Mitigation likely from impacts to biological, aquatic, and floodplain resources concentrated within the adjacent Plum Creek riparian area. 	<ul style="list-style-type: none"> ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: New Elevated Travelway Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Additional travel lane would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as Express Lane would be operated to promote consistent speeds and reliable travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of Express Lane Elevated roadway would be more prone to icing 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New travel lane would provide additional space for incident response and allow more through traffic to maneuver around the incident New travel lane could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream Additional travel lane provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use Express Lane New travel lane could provide more capacity to serve trips on I-25, which could reduce travel pressure and volumes on local roads and alternate routes Diversion to local roads could persist if drivers choose not to use Express Lane Consistent with CDOT's goals for adding capacity and managing congestion Restricted width limits flexibility for mobility during incident management 	<ul style="list-style-type: none"> Assumes travel-way has managed/special use restrictions (i.e., HOV and transit only)



Segment 3: New Elevated Travelway Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management <ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding one Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would reduce delays for drivers during all incident types It would improve incident response times by providing more capacity and potentially a less congested lane with more shoulder room to utilize to travel to incidents. A five-lane section has the potential for crash reduction due to additional room for slower moving vehicles. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lanes to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management. An increase in weaving crashes may offset other crash type reductions
Compatibility with Community Planning Goals <ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually may constrain interchange reconstruction.
Environmental Impacts <ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Likely significant environmental impacts and public controversy. Class of action anticipated to be an EIS. 	<ul style="list-style-type: none"> Cantilever or viaduct-type of structure would introduce significant new vertical element to the area. Potential eligible historic properties in Happy Canyon Area ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: Convert One Existing Lane to Express Lane (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Depending upon level of Express Lane use, congestion may increase in GP lanes, which would increase congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Differing travel speeds between a higher-speed Express Lane and lower-speed GP lanes during high volume periods could present safety concerns. 	<ul style="list-style-type: none"> Maneuverability and crash reductions across four lanes may be limited due to restriction in use of Express Lane. Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved in Express Lane as it would be operated to promote consistent speeds and reliable travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of Express Lane Peak period travel reliability and travel times may degrade in general purpose lanes as express lane usage will likely be lower than previous use as a general purpose lane 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> Repurposing one general purpose lane as an Express Lane may encourage more traffic to divert to local roads Consistent with CDOT's goals for adding capacity and managing congestion Express Lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE Express Lane does not preclude future transit options Express Lane may provide support for car/van pooling Additional Express Lane technology will increase the maintenance burden 	



Segment 3: Convert One Existing Lane to Express Lane (four lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Converting one general purpose lane to an Express Lane in each direction to I-25 would enhance its reliability and provide additional space for incident response safety while impacting throughput less significantly during incident response. It would improve incident response times by providing a potentially less congested lane with more shoulder room to utilize to travel to incidents. 	<ul style="list-style-type: none"> The improvements to incident response times from this alternative are dependent on how access to/from the Express Lane is managed. If emergency vehicles can easily cross from Express Lane to General Purpose Lanes, this improvement is maximized. Design and management practices for the Express Lane will impact its usefulness for incident management.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually would not preclude Lincoln Avenue interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely to qualify for CE. 	<ul style="list-style-type: none"> ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: Add New Truck Climbing Lane SB at Surrey Ridge

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Separate lane for slower moving vehicles would increase maneuverability overall and promote more homogeneous travel speeds that will reduce turbulence, which has been demonstrated to improve safety. Reduces the potential for rear-end and sideswipe same direction crashes involving slow-moving truck. 	
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> New SB truck climbing or passing lane would partially reduce traffic delay from slow moving vehicles passing another slow moving vehicle Additional capacity would partially decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability 	<ul style="list-style-type: none"> Assumes 1-3 miles of climbing lane only, between and independent from RidgeGate and Castle Pines Pkwy
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New SB truck climbing or passing lane would add capacity to I-25 New SB truck climbing or passing lane would likely not impact existing transit operations but would not preclude future transit options and could provide support for future local bus routing New SB truck climbing or passing lane would not provide any additional support for non-vehicular travel choices Operations and maintenance work are improved because additional lane improves work zone flexibility Additional lane increases the maintenance/plowing burden 	<ul style="list-style-type: none"> Assumes 1-3 miles of climbing lane only, between and independent from RidgeGate and Castle Pines Pkwy
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding southbound truck climbing or passing lanes has the potential to reduce crashes by minimizing slow moving vehicle impacts on traffic flow It would improve incident response times for southbound incidents and provide for more space to safely respond to incidents where it is located. 	
Compatibility with Community Planning Goals			



Segment 3: Add New Truck Climbing Lane SB at Surrey Ridge

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Compatible with local transportation goals. Conceptually does not preclude local transportation projects. Truck lane would provide improved movement of freight. 	<ul style="list-style-type: none"> May constrain interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely to stay within CDOT ROW and qualify for CE. 	<ul style="list-style-type: none"> Greenland Ranch straddles I-25 through the Monument Hill area. Plum Creek major tributaries are located east of the northbound side of I-25 through Monument Hill area. Gantries and signage associated with truck lane would introduce new vertical element in Segment 3. Likely to be supported by CMCA ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: Add Auxiliary Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Allows acceleration/deceleration to occur outside of the mainline travel lanes. This will promote more homogeneous travel speeds in the through lanes that will reduce turbulence, which has been demonstrated to improve safety. Additional lane would increase maneuverability when an incident occurs adjacent to the auxiliary lanes. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> New auxiliary lane would partially reduce traffic delay from non-recurring events such as crashes, disabled vehicles, and other shoulder closures Additional capacity would partially decrease the impacts on travel time from congestion, special events, and incidents, improving trip time reliability 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New auxiliary lane would add local capacity to I-25, increasing the appeal for local trips and potentially removing some traffic from the local road network New auxiliary lane would likely not impact existing transit operations but would not preclude future transit options and could provide support for future local bus routing New auxiliary lane would not provide any additional support for non-vehicular travel choices New auxiliary lane is not consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors Operations and maintenance work are improved because additional lane improves work zone flexibility Additional lane increases the maintenance/plowing burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> New auxiliary lanes would improve incident response times in this segment. The auxiliary lanes would provide better access to emergency detour and alternate routes during all incident types. 	
Compatibility with Community Planning Goals			



Segment 3: Add Auxiliary Lane(s)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation projects. 	<ul style="list-style-type: none"> Conceptually would not preclude interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Likely to qualify for CE. 	<ul style="list-style-type: none"> ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25



Segment 3: Convert All Existing Lanes to Express Lanes Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> No impact to safety, other than spot specific safety improvements. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved in Express Lanes as they would be operated to promote more consistent speeds and reliable travel times 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> Imposing a toll on all general purpose lanes will likely encourage more traffic to divert to local roads Repurposed express lanes may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE Repurposed express lanes do not preclude future transit options Repurposed express lanes may provide support for car/van pooling Additional travel lanes provide improved travel options for drivers that choose to pay for a reliable travel time Consistent with CDOT's goals for adding capacity and managing congestion Additional Express Lanes technology will increase the maintenance burden 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> If all lanes were tolled, it is assumed that traffic would divert to another roadway system, thus reducing overall traffic and improving incident response times. This alternative could increase the number of incidents occurring on the local road system. 	
Compatibility with Community Planning Goals			



Segment 3: Convert All Existing Lanes to Express Lanes Each Direction (five lanes)

Criteria	Level 2 Performance Measure	Result	Comments / Notes
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> Incompatible with local transportation plans as I-25 traffic would be pushed onto the local roadway network, specifically in Castle Rock and Castle Pines. Conceptually may constrain or preclude local transportation projects not anticipated or designed to handle interstate traffic. 	<ul style="list-style-type: none"> Conceptually does not preclude interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Poor. Converting all lanes likely to push a higher-than-existing-volume and Interstate-level type of traffic onto the local roadway network. Class of action anticipated to be an EIS. 	<ul style="list-style-type: none"> Anticipated strong public controversy and high degree of secondary impacts to surrounding local roadway network and adjacent sensitive resources. Gantries and signage associated with Express Lanes would increase number of vertical elements in Segment 3.



Segment 3: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
Safety			
<ul style="list-style-type: none"> Reduce crashes Improve infrastructure/address deficiencies 	<ul style="list-style-type: none"> Potential for crash reduction (qualitative) Potential to address safety concerns overall (qualitative assessment of improvements at specific locations of LOSS III or IV) 	<ul style="list-style-type: none"> Would reduce congestion and congestion-related crashes, such as rear-end, sideswipe same direction, and secondary crashes. Should be implemented with speed harmonization or reduced speed limits to promote homogeneous travel speeds, which has been demonstrated to improve safety. May impact safety of first responders at incident scene. 	<ul style="list-style-type: none"> Opportunity to address specific locations for localized safety improvements in combination with this element.
Reliability			
<ul style="list-style-type: none"> Improve travel times Improve predictability of travel times Reduce delays on I-25 	<ul style="list-style-type: none"> Improvement in non-recurring congestion (qualitative assessment of incident management, weather, special event management) 	<ul style="list-style-type: none"> Travel reliability would be improved as the shoulder lane would be operated to promote more consistent speeds and reliable travel times during peak periods Off-peak and short-duration congestion would continue to impact travel times Congestion and resulting congestion-related crashes in existing general purpose lanes may persist depending on level of use of managed shoulder lanes 	
Mobility			
<ul style="list-style-type: none"> Provide additional travel options Serves regional trip demand on I-25 Reduces I-25 traffic diversions to local roads Promotes multimodal options Supports CDOT network goals for operations Supports CDOT network goals for asset management 	<ul style="list-style-type: none"> Discourages cut-through traffic on local roads (qualitative; improves I-25 operations) Ability to improve existing transit operations (qualitative) Ability to not preclude / support new transit options (potential ROW conflicts) <ul style="list-style-type: none"> Bus Rapid Transit (BRT) Commuter Rail High-Speed Rail (ICS) Provides additional travel choices (options for multiple modes and travel choices) Compatible with CDOT operational strategies for I-25 Compatible with CDOT asset management goals 	<ul style="list-style-type: none"> New shoulder lane would draw some traffic from general purpose lanes, increasing the appeal for local trips and potentially removing some traffic from the local road network New shoulder lane may provide travel time and reliability benefits to intercity transit, pending agreement with HPTE New shoulder lane does not preclude future transit options New shoulder lane would not provide any additional support for non-vehicular travel choices New shoulder lane is consistent with CDOT's goals for adding capacity and managing future congestion in urban corridors New shoulder lanes would provide additional space for incident response and allow more through traffic to maneuver around the incident New shoulder lanes could increase maneuverability overall, provide more passing opportunities, and reduce turbulence in the traffic stream 	<ul style="list-style-type: none"> Assumes peak period shoulder lane would be a managed facility (i.e., tolled, HOV, transit, etc), similar to I-70 PPSL



Segment 3: Add/Convert Peak Period Shoulder

Criteria	Level 2 Performance Measure	Result	Comments / Notes
		<ul style="list-style-type: none"> New shoulder lanes provides improved travel options for drivers that choose to pay for a reliable travel time Freight travel could be improved and conflicts reduced if other drivers choose to use managed shoulder lanes 	
Incident Management			
<ul style="list-style-type: none"> Reduce delays related to incident management Improve safety for drivers and responders during incidents Improve driver communication / coordination Improve emergency responder communication / coordination 	<ul style="list-style-type: none"> Improved time to respond to incidents (qualitative) Potential for crash reduction (qualitative) Ability to provide emergency detours / alternate routes (qualitative) 	<ul style="list-style-type: none"> Adding/convert peak period shoulders in each direction to I-25 would enhance its reliability during peak times. Because the shoulder is being utilized for travel, it is not available for incident management activities. Responders may need to close the affected lane and the adjacent lane to safely deal with incidents, minimizing its effectiveness at improving incident management activities. It may improve incident response times by providing more capacity upstream of the incident. 	<ul style="list-style-type: none"> The design and operation of the peak period shoulder lane has a large impact on its usefulness for incident management and reduction in delays.
Compatibility with Community Planning Goals			
<ul style="list-style-type: none"> Compatible with Conservation Easements Does not preclude community land use or transportation goals / projects in Master Plans Capitalizes on opportunities for economic development / attracting businesses 	<ul style="list-style-type: none"> Compatible with Conservation Easements Conceptually does not preclude planned local transportation projects (e.g., Happy Canyon interchange reconstruction, Crystal Valley / Douglas Lanes new interchange) (qualitative) Attractive to economic development (qualitative) 	<ul style="list-style-type: none"> No Conservation Easements adjacent to I-25 in Segment 3 Could promote economic development for planned mixed use areas in RidgeGate and Lone Tree areas. Compatible with local transportation goals. Conceptually does not preclude local transportation 	<ul style="list-style-type: none"> Conceptually would not preclude Lincoln Avenue interchange reconstruction.
Environmental Impacts			
<ul style="list-style-type: none"> Relative environmental effects / level of significance – fits a category of action that has been demonstrated to have limited environmental impacts (CE), potential for significant effects (EA), likely significant effects (EIS) 	<ul style="list-style-type: none"> Good (CE or limited), Fair (EA or some effects but mitigated), Poor (EIS or hard to mitigate and/or significant) 	<ul style="list-style-type: none"> Good. Could likely qualify as a CE. 	<ul style="list-style-type: none"> ROW becomes constrained north of Lincoln Avenue Glendale Farms Open Space adjacent to I-25 Signs and gantries associated with Peak Period Shoulder would introduce new vertical element to Segment 3.

Appendix A2
Safety Analysis Technical
Memorandum

Acronyms and Abbreviations

AADT	average annual daily traffic
CDOT	Colorado Department of Transportation
IHSDM	Interactive Highway Safety Design Model
LOSS	Level of Service of Safety
MP	milepost
OTIS	Online Transportation Information System
PEL	Planning and Environmental Linkage
SH	State Highway

I-25 South Gap EA: Safety Analysis

PREPARED FOR: Colorado Department of Transportation
COPY TO: CDOT Regions 1 and 2
PREPARED BY: Jacqueline Dowds Bennett, CH2M
DATE: April 19, 2018
REVISION NO.: 0

1. Study Area

A comprehensive analysis of the 2011 through 2015 safety history for the I-25 corridor between Monument and South Denver is documented in the *Safety Assessment Report – SH25A Planning & Environmental Linkage (PEL) Study - MP 157.70 – MP 194.31*, CDOT, July 2017 (Attachment 1 to this technical memorandum). An analysis of 2011 through 2015 crash records for the Gap section of this corridor was conducted in support of the Early Action project. The Gap section of this corridor extends between milepost (MP) 161 (just north of the SH 105 overpass) and MP 179 (approximate location of the transition between the four-lane and six-lane sections in the northbound direction, south of the Plum Creek Parkway interchange).

2. Existing Level of Service of Safety

Figure 1 shows the results of the Level of Service of Safety (LOSS) analysis for total crashes. LOSS in the III category suggests moderate to high potential to reduce crashes by implementing strategies targeted at the specific crash issues at the location. LOSS IV suggests high potential. Since no sections exhibit an LOSS below LOSS III, the LOSS analysis suggests moderate to high potential to reduce crashes along the whole length of the Gap segment.

The **Urban four lane** section is between the south end of corridor and the County Line Road interchange. Several transition points and severe topography increase the potential for conflict and turbulence in the traffic stream. The truck weigh station and its entrance/exit points are in this segment. The topography and resultant weather patterns in this area are likely more similar to conditions encountered for rural four-lane interstates. However, when comparing this dataset of adverse weather and wet roadway crashes to the statewide normatives for the rural four-lane interstates, the proportions are still significantly higher, suggesting the turbulence in the traffic stream likely plays a role in crash causation during weather events. The crashes in dark-lighted conditions occurred in a proportion that is twice what is expected and in a proportion much lower than expected for dark-unlighted conditions.

At 32.4 percent, the percentage of total crashes that resulted in a fatality or injury within this Urban 4 Lane section is slightly higher than the statewide normative of 28.5 percent.

The **Rural four lane** section very closely matches the statewide normatives in all categories. Thus, weather and roadway surface conditions did not play a role in crash causation in a greater proportion than expected north of the County Line Road interchange. At 34.3 percent, the percentage of total crashes that resulted in a fatality or injury within this Rural 4 Lane section is slightly higher than the statewide normative of 32.4 percent.

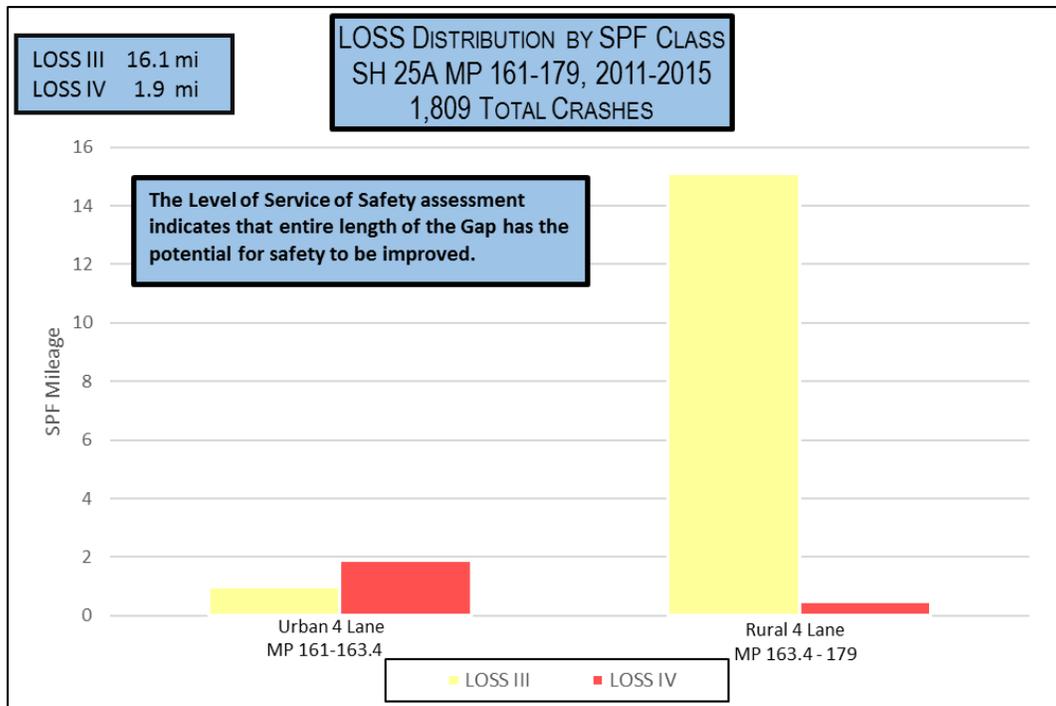


Figure 1. Level of Service of Safety Distribution by Safety Performance Function Class

3. Existing Study Area Crash Patterns

Colorado Department Transportation (CDOT) crash data between January 1, 2011 and December 31, 2015 were analyzed to identify crash patterns, causal factors and concentrations within the Gap study area. These crash records indicate that a total of 1,809 crashes were reported during this 5-year time period for the mainline, ramps, and ramp terminal intersections. Of this total, 34 percent (619 crashes) resulted in injuries and less than 1 percent (five crashes) resulted in fatalities. Property damage only was the result for the other 1,185 crashes. The crashes were generally evenly distributed between the northbound and southbound directions (51 percent occurred in the northbound direction and 49 percent in the southbound direction).

Single vehicle crashes accounted for 40 percent of the total corridor crashes. For the urban four-lane section between MP 161 - 163.4 that is near Monument, the proportion of single vehicle crashes is higher than expected when compared to similar facilities throughout the state. Along the same lines, the proportion of crashes that occurred off the road is higher than expected since single-vehicle crashes result when a driver leaves the travelway. Likewise, the proportion of single-vehicle and off-road crashes in the rural four-lane section of the Gap is higher than expected.

Some of the same contributing factors to multiple-vehicle crashes may also contribute to single vehicle crashes in the sense that some of these crashes may occur as drivers overcorrect after realizing that they are changing lanes into another vehicle or as drivers attempt to avoid a rear-end collision and subsequently hit concrete barrier or guardrail. Since turbulence is not a typical contributing factor to single-vehicle crashes, this can help explain the higher proportion of single-vehicle crashes in this corridor.

A. Crash Type Distribution

Figure 2 shows the distribution of crashes by crash type. The most common crash type was rear-end, followed by fixed object and sideswipe/same-direction. Rear-end and sideswipe/same-direction crashes can be indicative of turbulence in the traffic stream due to volume demand, transitions between two- and three-lane sections, interchange influence areas, speed limit changes, and topography (such as, slower moving vehicles on steep grades).

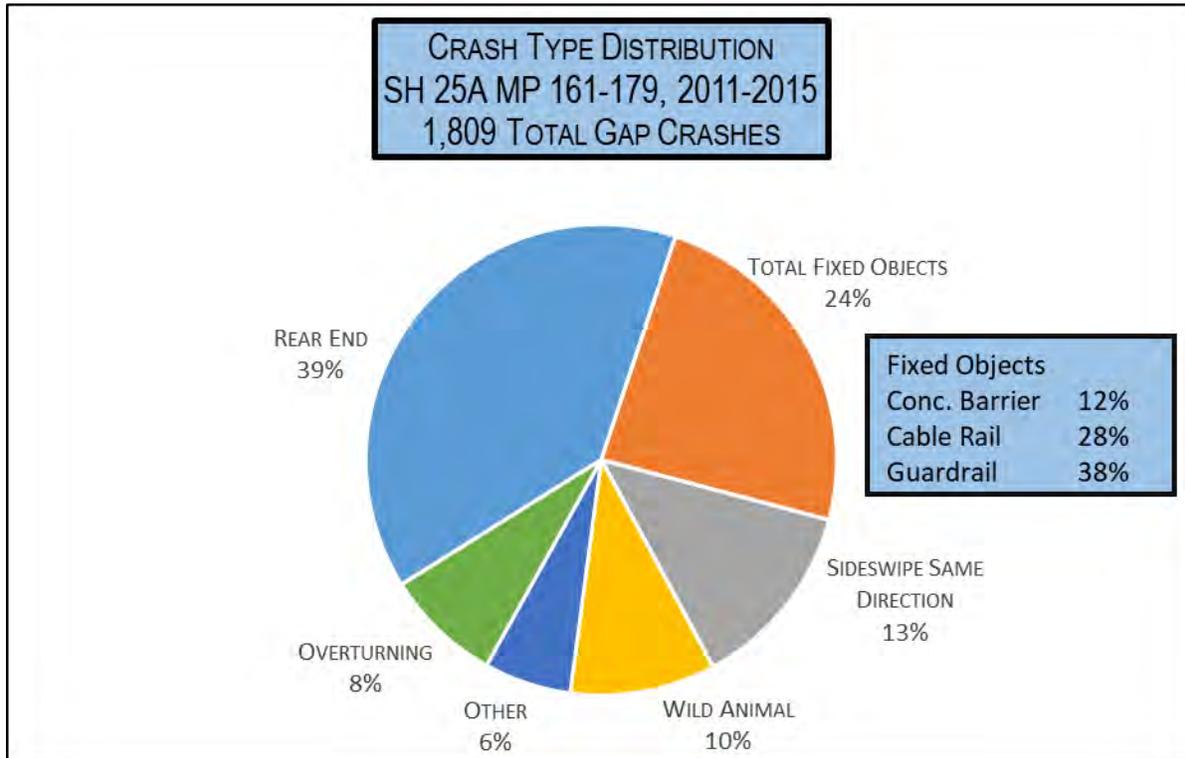


Figure 2. Crash Distribution by Type

1. Rear-end and Sideswipe/Same-Direction Crashes

Figure 3 shows the rear-end and sideswipe/same-direction crashes by milepost. These types of collisions occurred more frequently at MPs 161, 163, 167, 172, and 175. While these 5 miles represent 28 percent of the Gap mileage, the crashes that occurred along them represent 34 percent of the Gap total for these crash types. This suggests that rear-end and sideswipe/same-direction crashes occur more often than expected within these 5 miles. The frequency of these crash types peaks within interchanges and at transition points where the number of lanes is increasing or decreasing. Additional turbulence in the traffic stream that leads to an increase in these crash types is likely the result of these influencing factors: mile 161 is near the weigh station where an additional lane is introduced in the northbound direction; the northbound third lane drops and there is an interchange within mile 163; the grade changes and there is an interchange near mile 167; there is an interchange and horizontal curve near mile 172; and there is a horizontal curve within mile 175.

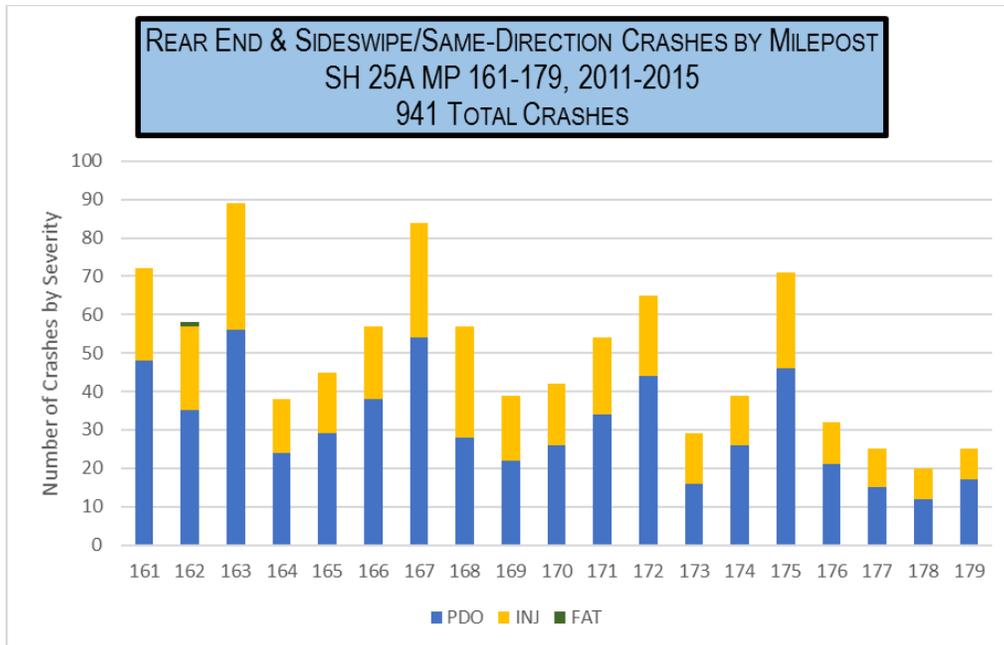


Figure 3. Rear-end and Sideswipe/Same-Direction Crashes by Milepost

2. Fixed-Object Crashes

Figure 4 shows the fixed-object crashes by milepost. These types of collisions occurred more frequently at MPs 161-163, 167, 171-172, 175, and 178-179. Several of these locations coincide with the high-frequency locations for the rear-end and sideswipe/same-direction crashes. As previously discussed, these single vehicle crashes may occur as a result of turbulence in the traffic stream. Drivers may overcorrect after realizing that they are changing lanes into another vehicle or attempt to avoid a rear-end collision and subsequently hit concrete barrier or guardrail that is too close to the roadway due to narrow shoulders. Horizontal alignment changes in combination with shoulders that do not provide adequate recovery room when a driver drifts out of the travel lane may also contribute to drivers running off the road and striking fixed objects. There are horizontal curves within or adjacent to MPs 162, 167, 171, 172, and 175.

3. Wild Animal Crashes

Figure 5 shows the wild animal crashes by milepost. The crash data set includes 183 records with wild animal as the crash type between 2011 and 2015. Wild animal collisions occurred along the entire length of the study area, but appear to be concentrated at mileposts 161-162, 171-174, and 176. Thus, 58 percent of the wild animal collisions occurred within these miles that represent one-third of the Gap. As expected, most (80 percent) occurred in low-light (dawn/dusk/darkness) driving conditions. The majority of these crashes (90 percent) resulted in a property damage only severity level for the vehicle occupants.

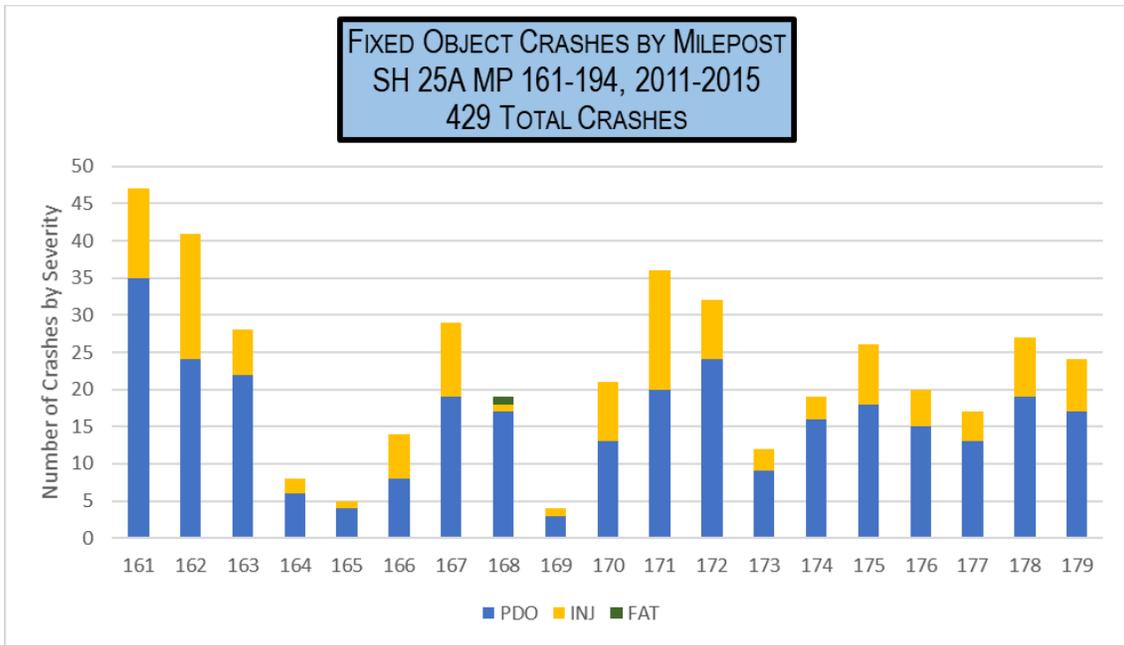


Figure 4. Fixed Object Crashes by Milepost

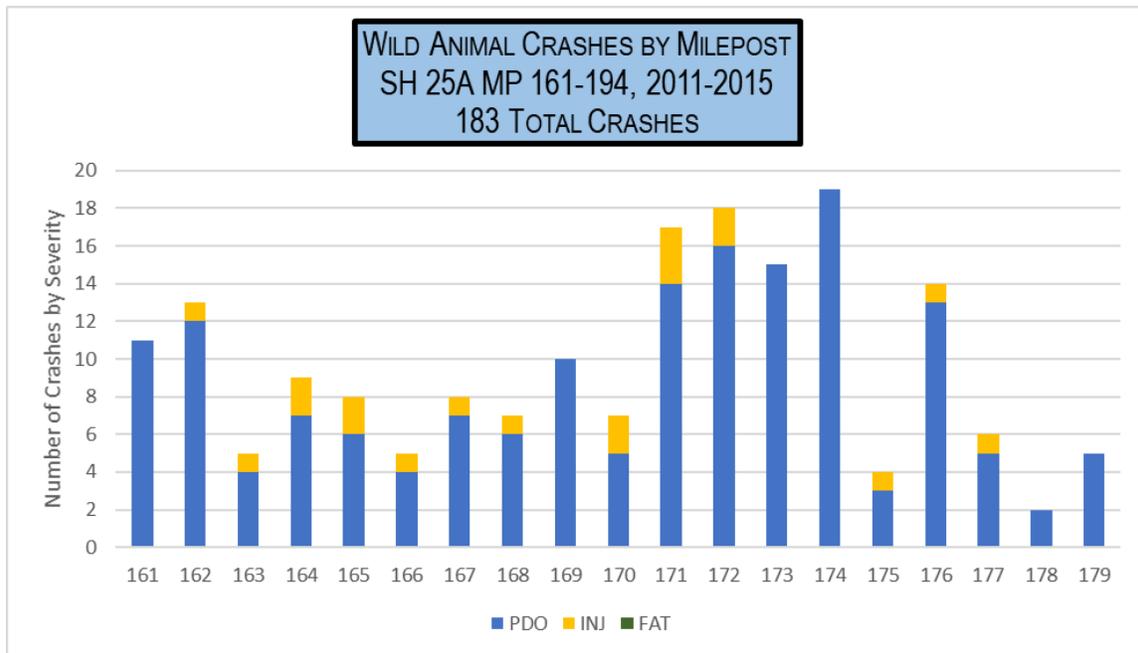


Figure 5. Wild Animal Crashes by Milepost

B. Crash Circumstances

The following list highlights the pertinent characteristics of the crash data set. Some of these characteristics suggest causal factors that contribute to the crash patterns and types.

- Facility Type:** The majority of crashes (96 percent) occurred on the mainline. Another 2 percent occurred on the frontage roads. The remainder occurred on the ramps and the intersections between the ramps/crossroads and frontage roads/crossroads.

- **Time of Day:** The highest proportions of crashes occurred between the hours of 11 AM and 5 PM, with the single highest hour between 3 and 4 PM.
- **Day of Week:** The highest proportion of crashes occurred on Saturdays, followed by Fridays. Approximately 43 percent of the crashes occurred on the weekdays of Monday through Thursday.
- **Month of Year:** Although the distribution among the May to November months is fairly even, the highest proportions of crashes occur in the months of June-July-August. These are also the highest volume months.
- **Multiple Crashes per Day:** Saturday had the most number of days with multiple crashes on the same day. Over the 5-year study period, there were 15 Saturdays that each had 5 or more crashes on a single day. The Sunday data followed a pattern very similar to Saturday and indicates that there were 12 Sundays that each had 5 or more crashes on a single day.
- **Lighting:** 34 percent of the crashes occurred in dawn/dusk, dark-lighted or dark-unlighted conditions.
- **Roadway Surface:** 22 percent of the crashes occurred when the roadway surface was wet, icy or snowy.
- **Weather Conditions:** 16 percent of the crashes occurred during a weather event such as snow, rain or hail.
- **Reported Speed:** Most of the crashes occurred when the speed of the primary vehicle was below the posted speed limit of 75 miles per hour. This suggests that factors were present that forced a reduction in operating speed. These factors could have been turbulence in the traffic stream or volume demand or roadway/weather conditions.
- **Rear-end and Sideswipe/Same-Direction Crashes:** The daily pattern for timing is similar to all crashes with the spikes occurring in the 11 AM and 3 PM hours. Very few of these crash types occur in the early morning hours, which is presumably the lowest volume time period.
- **Truck and Bus Crashes:** A total of 135 truck and bus crashes occurred in the Gap during the 5-year study timeframe. The most common crash type was sideswipe/same-direction (45 percent) followed by rear-end (27 percent). Three-fourths of these sideswipe/same-direction crashes occurred when the truck driver was changing lanes or passing.
- **Fatal Crashes:** Two of the five fatal crashes involved a driver under the influence of alcohol. Large trucks were involved in two of the fatal crashes. All occurred on dry roads in fair weather, and all but one occurred in low-light conditions. Excessive speed does not appear to be a contributing factor in most of these crashes.

4. Expected Future Safety Conditions

The following provides a summary of the expected safety conditions in the Gap area (MP 161 through 179) in 2040. The safety impacts of transportation decisions can be quantified using the predictive methodology detailed in Part C of the Highway Safety Manual. This methodology combines crash, roadway, and traffic volume data to predict the number of crashes on a facility before and after implementation of improvements, enabling comparisons among proposed build alternatives and the No Build alternative. Predictive analyses can also be used to estimate impacts of future traffic volumes and to determine if a design feature or proposed improvement will have a safety benefit. These

impacts/benefits can be compared against other considerations such as cost, right of way and environmental impacts when analyzing and selecting alternatives.

A. Crash Predictive Modeling Methodology

The crash predictive methodology was implemented with the Interactive Highway Safety Design Model (IHSDM) software to evaluate at a high-level the effects of lane additions and increased shoulder widths for the I-25 mainline and ramp merge/diverge areas. The models focus exclusively on the I-25 mainline and, thus, do not incorporate parallel State Highway (SH) routes such as SH 83, SH 85 and SH 105 or the frontage roads north of the Tomah Road interchange. The models simulate typical weekday traffic conditions that occur outside of the summer peak volume season.

The IHSDM models estimate the expected average annual crash frequency for the year 2017 existing condition and year 2040 alternatives. The expected average annual crash frequency is obtained by combining the predicted long-term average crash frequency with the observed crash frequency through use of the Empirical Bayes Analysis process and site-specific crash data. Predicted average crash frequency results from adjusting known crash frequencies for similar facilities using the geometric design (lane widths, shoulder widths, shoulder types, roadside hazard rating, sideslopes), traffic control features, and traffic volumes of the subject site. Observed frequency (the documented annual average number of crashes for the subject site) has some uncertainty because the time period may include one year with an unusually high number of crashes that can artificially inflate the yearly average. Combining the two average frequencies (predicted and observed) yields a more reliable average annual estimate.

B. Year 2017 Existing Conditions Model

1. Model Construction

The first model was built to represent existing conditions in the year 2017 for the entire corridor. Freeway evaluation models are constructed in IHSDM with geometric, volume, and crash data for homogeneous segments along the corridor. The segments are divided into freeway mainline sites, speed change entrance ramp sites, and speed change exit ramp sites. The existing conditions model consists of 33 freeway mainline sites, 30 speed change entrance sites, and 21 speed change exit sites. The Castle Rock Parkway interchange is not included in the existing conditions model because it was not fully operational during all three of the years that coincide with the crash data (2013, 2014, and 2015).

The average annual daily traffic (AADT) volumes for the mainline and ramps were input for the years 2013, 2014, 2015, and 2017. The 2013, 2014, and 2015 AADT for the mainline was obtained from CDOT's Online Transportation Information System (OTIS). The 2017 AADT was obtained from the weekday counts conducted in May 2017. Likewise, the proportion of high volume factor (proportion of AADT during hours when the mainline volume exceeds 1000 vehicles per hour per lane) was calculated from these May 2017 weekday counts. Ramp volumes were derived by applying the change in adjacent mainline volume between the given year and 2017 to the counted 2017 ramp volume. The software extrapolates the 2016 AADT volumes from the volumes input for the other four years.

The algorithms developed for use in the model are based on freeway facilities and crash histories from numerous sites across the country. Observed crash data was input so an Empirical Bayes analysis could be conducted to align the model more closely to the corridor. A total of 2,670 crash records for the years 2013 through 2015 were input into the model – 2,435 for the freeway segment sites (730 fatal/injury and 1,705 property damage), 196 for the speed change entrance ramp sites (32 fatal/injury and 164 property damage), and 39 for the speed change exit ramp sites (16 fatal/injury and 23 property damage). The 2,670 crash records were selected from the crash data set used in the overall project

analysis specifically because they occurred on the mainline and in the merge/diverge areas that are the subject of this modeling effort.

Ramp crash data were not input into the model because this effort is not focused on predicting crashes that occur on the entrance or exit ramps that are not related to merge and diverge maneuvers (i.e., crashes at the ramp terminal intersection with the crossroad). It is not always possible to determine from the crash data if a crash coded as a “ramp crash” occurred as a result of merging or diverging maneuvers at an interchange or occurred further up the ramp and is not related to the merging/diverging maneuvers. Therefore, some crash data records that were excluded from the model data input (may also be merge/diverge type crashes).

The 5-year evaluation period is 2013 through 2017, which is derived by the model to include all the years from the first year of crash data through the analysis year.

C. Comparison of Results to Observed Crashes

After revising the model according to the peer review comments, the existing conditions evaluation was conducted with the IHSDM software. The evaluation results were compared to the observed crashes (CDOT crash data) to assess the ability of the model to simulate the study corridor conditions. The model predicts the expected annual average crash frequency for the evaluation period and the distribution among crash types. The distribution among crash types is of interest when comparing to the observed crashes to assess how the facility functions compared to other similar facilities. Appendix A contains the IHSDM output report for the existing condition model.

The model predicts a total of 4,791 crashes for the 5-year evaluation period. This equates to an annual average frequency of 958 crashes for the freeway mainline, entrance ramp, and exit ramp sites. This annual average equates to 2,874 crashes (1,963 segment, 811 entrance ramp, and 100 exit ramp) for the 3-year crash data period. In comparison, there were 2,670 observed crashes (2,435 segment, 196 entrance ramp, and 39 exit ramp) input into the model for this same 3-year period, which is approximately 7 percent fewer crashes than expected per IHSDM. While the number of crashes expected per the model is relatively close to the observed number of crashes, the primary difference is the crash distribution between mainline and speed change sites. Nearly 24 percent more crashes were observed along the mainline sites in three years than expected per the model. Also, IHSDM expects more crashes to occur because of merging and diverging maneuvers in the speed change sites than were actually observed per the crash records. This suggests that mainline crashes outside of interchange influence areas occur more often than expected per the IHSDM methodology, and are likely due to the congestion that has been documented anecdotally and through travel time studies. This result is magnified specifically within the Gap area where the model predicts approximately 37 percent fewer crashes than observed for the freeway mainline sites during the 3-year crash data period. Overall, the model predicts that 30 percent of crashes will be fatal/injury and 70 percent will be property damage, which is similar to the distribution of the observed crashes (29 percent fatal/injury and 71 percent property damage).

As Table 1 shows, the model predicts a different crash distribution among mainline crash types than were observed. Specifically, the model predicts a higher proportion of multiple vehicle collisions and, subsequently, a lower proportion of single vehicle collisions than were observed. Notably, the expected proportion of rear-end collisions is nearly 9 percent higher than observed whereas the expected proportion of fixed object collisions is nearly 7 percent lower than observed. This result aligns with a previous evaluation of the crash data and existing conditions along the corridor which identified that single vehicle, fixed object crashes occur more often in this corridor than expected when compared to similar facilities across Colorado. It has been hypothesized that this issue is due to congestion, narrow

shoulders, and barrier immediately adjacent to the shoulder. The other notable difference is for animal collisions. Even though the model evaluation was conducted with a modified Crash Distribution data set that reflected the observed proportion of animal collisions, the model still expects a significantly lower proportion of this crash type. However, this difference is not surprising as this type of collision is known to be overrepresented along the entire length of this corridor when compared to similar facilities across Colorado.

The proportions for the less common multiple vehicle crash types exhibit greater variance between the model and observed; however, these small crash frequencies have minimal impact on the total crash predictions. Furthermore, these discrepancies could be due to differences in how the Colorado crash data is coded as compared to the data used to develop the IHSDM algorithms (particularly the “other” crash type).

For the speed change entrance sites, the distribution between single and multiple vehicle crashes is similar. However, the model predicts nearly twice the proportion of fatal/injury crashes than were observed. Likewise, the distributions of the specific crash types vary between the expected and observed. The crash severity distribution for the speed change exit sites is closer between the model and observed, but not an exact match. Fewer types of crashes were observed than predicted by the model, leading to a difference between the multiple and single-vehicle collision proportions. The model expects 63 percent of the exit site crashes to involve multiple vehicles as compared to the observed value of 77 percent.

The Gap is a commuting corridor in a rural environment with severe topographical terrain and, therefore, does not necessarily behave as a typical rural interstate. Thus, inclusion of the historical crash data for incorporation into an Empirical Bayes analysis was necessary to align the model more closely to the corridor operating characteristics. Although not exact, the resultant existing conditions model is in general a fair representation of the documented crash experience within the corridor. This existing conditions model is a reasonable base from which to create the models for the 2040 alternatives. The 18-mile portion of the corridor model that represents the Gap was extracted for use in applying the crash predictive methodology to determine expected annual average crashes in 2040.

Table 1. Crash Type Distribution Comparison of 2017 Expected to Observed Crashes

Crash Type	Crash Percentage (%)					
	SEGMENTS		ENTRANCE		EXIT	
	Expected	Observed	Expected	Observed	Expected	Observed
Animal Collision						
F+I	0.03%	0.94%	0.00%	0.51%	0.00%	0.00%
PDO	0.84%	5.46%	0.35%	5.61%	2.20%	2.56%
Total	0.87%	6.41%	0.35%	6.12%	2.20%	2.56%
Fixed Object Collision						
F+I	4.38%	6.61%	6.74%	1.53%	9.83%	5.13%
PDO	11.33%	16.34%	13.84%	22.96%	12.05%	15.38%
Total	15.70%	22.96%	20.58%	24.49%	21.88%	20.51%
Collision with Other Object						
F+I	0.29%	0.29%	0.62%	0.00%	1.11%	0.00%
PDO	2.22%	1.23%	3.48%	1.02%	2.65%	0.00%
Total	2.51%	1.52%	4.11%	1.02%	3.77%	0.00%
Other Single-Vehicle Collision						
F+I	1.75%	2.38%	3.36%	1.02%	5.32%	2.56%
PDO	2.07%	2.63%	2.60%	2.55%	2.82%	0.00%
Total	3.81%	5.01%	5.96%	3.57%	8.14%	2.56%
Collision with Parked Vehicle						
F+I	0.12%	0.37%	0.33%	0.00%	0.00%	0.00%
PDO	0.06%	0.37%	0.12%	1.02%	0.57%	0.00%
Total	0.18%	0.74%	0.45%	1.02%	0.57%	0.00%
Total Single Vehicle Crashes						
F+I	6.56%	7.76%	11.06%	2.55%	16.26%	5.13%
PDO	16.51%	19.06%	20.38%	30.10%	20.28%	17.95%
Total	23.07%	26.82%	31.44%	32.65%	36.55%	23.08%
Right-Angle Collision						
F+I	0.86%	0.04%	0.76%	0.51%	0.62%	0.00%
PDO	1.15%	0.00%	1.01%	0.00%	0.24%	0.00%
Total	2.01%	0.04%	1.76%	0.51%	0.86%	0.00%
Head-on Collision						
F+I	0.25%	0.00%	0.33%	0.00%	0.10%	0.00%
PDO	0.14%	0.08%	0.16%	0.00%	0.04%	0.00%
Total	0.38%	0.08%	0.49%	0.00%	0.14%	0.00%
Other Multiple-Vehicle Collision						
F+I	0.88%	2.67%	0.46%	0.51%	0.32%	0.00%
PDO	2.07%	3.57%	1.78%	3.57%	0.62%	0.00%
Total	2.95%	6.24%	2.24%	4.08%	0.94%	0.00%
Rear-end Collision						
F+I	16.01%	14.87%	14.82%	7.14%	23.18%	28.21%
PDO	35.06%	27.23%	28.16%	28.06%	21.51%	35.90%
Total	51.07%	42.09%	42.98%	35.20%	44.68%	64.10%
Sideswipe, Same Direction Collision						
F+I	4.42%	3.45%	4.14%	4.59%	5.90%	5.13%
PDO	16.10%	15.03%	16.94%	21.94%	10.94%	5.13%
Total	20.52%	18.48%	21.08%	26.53%	16.84%	10.26%
Total Multiple Vehicle Crashes						
F+I	22.42%	21.11%	20.51%	9.18%	30.10%	35.90%
PDO	54.51%	46.20%	48.05%	58.67%	33.35%	41.03%
Total	76.93%	67.31%	68.56%	67.86%	63.45%	76.92%
Total Highway Segment or Speed Change Crashes						
F+I	28.98%	29.98%	31.57%	16.33%	46.37%	41.03%
PDO	71.02%	70.02%	68.43%	83.67%	53.63%	58.97%
Total	100.00%	100.00%	100.00%	100.00%	100.00%	100.00%

5. 2040 No Build Alternative Network Assumptions

The No Build alternative assumes I-25 maintains the current four-lane cross section through the Gap. The Crystal Valley Parkway interchange was added to the existing conditions model reflect the 2040 roadway network (the Crystal Valley Parkway is a planned interchange). The segmentation was revised accordingly and the relative crash data moved to the new segments. The observed crash data is historical and, therefore, remains constant in the future models. The evaluation period increases to the horizon year for the future models. Thus, the evaluation period is 28 years to include all the years from the first year of crash data (2013) through the analysis year (2040). The ramp and mainline volumes along with the proportion of high volume factor were revised to reflect the travel demand model forecasts. The AADT volume forecasts were obtained from results of the travel demand modeling and post-processing effort.

6. 2040 Build Alternative Network Assumptions

The model for the Build alternative starts with the No Build model and incorporates the following improvements:

- Added one, 12-foot wide general-purpose lane.
- Increased outside shoulder width and revised offset distances to outside barriers (the full typical section width is 12 feet; the width varies between 10 and 11 feet south of County Line Road and is between 7 and 8 feet through the southbound transition between existing and proposed).
- Increased inside shoulder width (the full typical section width is 15 feet; the width varies between 7 and 12 feet south of County Line Road and is 12 feet through the southbound transition between existing and proposed sections).
- Modified effective median width relative to revised inside shoulder widths (34 feet for the full typical section).
- Decreased length of median barrier to account for additional emergency turnaround/crossover locations.
- Modified length of speed change segments and resultant mainline segmentation to match proposed ramp designs; deleted two speed change entrance segments because the proposed acceleration length exceeds the 1600-foot length definition of a speed change lane.
- Three grade-separated wildlife crossings. The industry-accepted crash predictive methodology does not have the capability to account for this safety strategy, so this analysis may not accurately predict this crash type.

The 2040 AADT volume forecasts were obtained from results of the travel demand modeling and post-processing effort.

7. Comparison of Crash Predictions for the Alternatives

Two build alternatives are proposed that each add one lane per direction. The difference between the two alternatives is reflected in usage of the additional lane: one alternative proposes a third general-purpose lane while the other proposes a managed express lane. A review of current research literature indicates that there is not yet a conclusive trend in crash data that permits the development of crash modification factors specific to the operation of express lanes. Therefore, the industry-accepted crash

predictive methodology does not yet have the capability to account for how express lane operations differ from those for general-purpose lanes. The transportation industry generally accepts that additional capacity which provides for homogeneous traffic flow provides a safety benefit. Therefore, one build alternative will represent the safety effects of one additional general-purpose lane per direction.

Although a few data inputs varied between the No Build and build alternative, the primary difference in predicted crashes and crash type distribution can be attributed to forecasted volume demand (AADT) and resultant density in each lane, and length of speed change sites. The Build alternative attracts more daily volume demand than the No Build alternative. The travel demand forecasting effort suggests that the demand to travel between the north and south ends of this corridor is similar between the No Build and Build alternatives. In the No Build alternative, however, some of the demand volume diverts to parallel routes (SH 83 and SH 105) due to lack of capacity on I-25. Additional capacity for the I-25 mainline provided by the Build alternative draws this demand back to the higher-classification interstate facility. The travel demand forecasts predict that the AADT volumes will decrease approximately 24 percent on SH 83 and approximately 38 percent on SH 105 with the Build alternative; this volume will remain on I-25 because of the extra capacity. The potential for a crash to occur generally increases with increased exposure, or volume. It is likely that crashes occur on the parallel routes involving the diverted vehicles, particularly given that these routes are lower-classification facilities that do not have the same capacity or design standards as an interstate. Although crash predictive modeling was not performed for these parallel facilities, it is possible that the sum of these crashes and those predicted for the No Build alternative would equal or exceed the predicted crashes for the Build alternative.

The necessity to lengthen the ramp acceleration and deceleration distances within existing right-of-way resulted in most of the speed change sites being parallel to the general-purpose lanes for most of their length. The longer distances provide the opportunity for drivers to execute most of their speed change in the ramp lane and not in the general-purpose lanes. Less turbulence in the mainline traffic flow results in fewer freeway site crashes, particularly for entrance sites associated with on-ramps. The crash reductions due to the improved design of the speed change sites as compared to the No Build alternative help to offset the increased exposure and potential for crashes with the Build alternative.

A. Expected Annual Crash Frequency

Table 2 summarizes the expected annual crashes for each alternative in 2040. It is important to note that the crash frequencies shown in Table 2 are to be used only for relative comparisons between alternatives and are not to be interpreted as exact predictors of future crash experience. To simplify the results discussion, the AADTs shown in Table 2 reflect a weighted-average AADT for the length of the Gap and the crash values represent the sum of the expected crashes for the freeway segments plus the entrance/exit speed change sites. The proportions of crash severity are similar between the alternatives. The results predict that approximately one-third of the crashes will result in fatalities or injuries for each alternative, which matches the historical crash records for the years between 2011 and 2015.

Even though the Build alternative provides a safety benefit over the No Build alternative by implementing several improvements (for example, inside/outside shoulder widths and acceleration/deceleration lane lengths), the AADT increases enough such that the result is a higher expected crash frequency for the Build alternative. These forecast differences reflect the attractiveness of each alternative to the traveling public. Again, it is likely that crashes would occur on parallel routes due to the volume diversion from I-25; when added to the No Build predicted crashes, the sum could equal or exceed the annual prediction for the Build alternative. The AADT forecasts suggest that approximately 8,900 daily vehicles will remain on I-25 rather than divert to SH 83 and 105 with the Build

alternative; the sum of the diverted volume and the mainline volume for the No Build alternative (113,700 AADT) is within one percent of the forecast for the Build alternative.

Table 2. Expected Annual Crashes in 2040 by Severity

Alternative	2040 AADT ^a	2040 Expected Annual Total Crashes			
		Total	Fatal	Injury	Property Damage
2040 No Build	113,700	602	5	196	401
2040 Build	123,200	629	4	198	427

Source: CH2M, February 2018

^a AADT = weight average for the length of the alternative.

Note:

AADT = average annual daily traffic

B. Expected Crash Type Distribution

The expected 2040 crash type distribution is similar between the two alternatives. For example, a proportion of 27 percent for single-vehicle collisions is expected for the No Build alternative and a proportion of 33 percent for the Build alternative. These results suggest the Gap would align more similarly to an urban 6-lane freeway facility in Colorado for which the statewide norms suggest a 31 percent proportion for single vehicle collisions. These are both less than the existing proportion of 40 percent. This result is not surprising for the Build alternative because the wider shoulders would provide the opportunity for drivers to redirect prior to striking barrier or guardrail adjacent to the shoulder. Also, the reduced congestion provided by the additional capacity is likely to result in a more typical pattern for run-off-road single-vehicle collisions in which congestion is not a contributing factor. The predicted crash type distribution is similar for the most common historical crash type recorded in the Gap (rear-end). The 2011 through 2015 crash records indicate the proportion of rear-end collisions was 39 percent whereas the predicted proportion in the year 2040 is 43 percent for the No Build alternative and 39 percent for the Build alternative.

8. Strategies to Improve Safety

Improving safety for vehicular travel is an identified need for the Gap. Crashes and other incidents regularly compromise traffic flow and travel time along this 18-mile stretch of I-25. The Build alternative includes several design features that will have a positive benefit from a safety perspective. The No Build alternative which does not deploy these safety strategies is not anticipated to provide the same potential for crash reduction.

9. Safety Features for the Build Alternative

The design features included in the Build alternative to reduce the potential for collisions to occur are:

- Increased Capacity.** Both build alternatives include an additional travel lane. This lane will eliminate the bottlenecks (areas where traffic slows down and queues form) that frequently occur at the locations where the cross section transitions from three to two lanes and smooth the resultant turbulence in the traffic stream. A consistent number of lanes will reduce weaving and lane changing maneuvers, and reduce the need for the acceleration/deceleration that accompanies the approach to and navigation through these transition areas. According to published research and the state of the practice, it is a generally accepted principle that a homogeneous traffic stream is safer and reduces the potential for crashes to occur. In the northbound direction, the additional lane

eliminates the existing left-lane drop prior to the start of the Gap section near milepost 161 that is a documented contributing factor to crashes.

- **Shoulder Width.** In general, the shoulders will be widened to 12 feet on the outside and 16 feet on the inside. The existing shoulder width will be maintained at locations where existing structures that are not being reconstructed do not permit the additional width. The existing narrow shoulders contribute to fixed object crashes in the Gap. Published research suggests that increasing shoulder widths to greater than 9 feet can reduce crashes by approximately 20 percent on rural roadways. Widening the shoulders will provide several benefits as follows:
 - There will be more recovery room for errant drivers that have left the travel lane to correct their vehicle's path prior to departing the roadside or striking a fixed object. Likewise, drivers will have adequate width to veer into the shoulder to prevent a rear-end or sideswipe/same-direction collision.
 - Barriers such as guard rail and bridge rail will be offset further from the edge of the travel way. Errant drivers will have more opportunity to recover from veering out of the travel lane prior to striking a fixed object. Nearly 75 percent of the fixed object crashes in the Gap during the 2011 to 2015 timeframe were collisions with guard rail, concrete barrier or cable rail.
 - Incident management will be improved, which will reduce the secondary crashes that occur when traffic flow has been disrupted by the initial incident. Responders will be able to reach an incident scene more quickly by using shoulders to navigate around traffic. Disabled vehicles can be moved out of the travel lanes and onto the shoulders. Responders will have more room to work on the roadside and reduce their exposure to moving traffic. These additional capabilities will shorten the time required to restore traffic flow to pre-incident levels. Current practice indicates the chance of a secondary incident occurring increases by 2.8 percent for each minute the primary incident impacts traffic flow. Harmonious traffic flow reduces the potential for crashes to occur, particularly multi-vehicle collisions.
 - Enforcement capabilities will be enhanced. Officers will have sufficient space to park their vehicles on the roadside to conduct observations, and to pull drivers over that are violating laws. Widths of 8 feet or greater are typically required to permit these activities. The additional width will reduce their exposure to moving traffic as they interact with drivers on the roadside. The reductions in speed differentials and aggressive driving that are likely to result will reduce the potential for collisions.
- **High visibility signs, pavement markings and delineators.** The design includes retroreflective pavement markings and signs which will improve driver guidance and navigation capabilities in low-light and low-visibility conditions. Positive guidance that helps drivers to stay in their travel lane reduces the potential for run-off-road collisions and sideswipe/same-direction collisions, particularly in horizontal curves. Published research suggests that retroreflective warning signs through curves can reduce crashes by approximately 25 percent on rural roadways. Delineators along the roadside further assist with defining the edge of travel way. During the 2011 to 2015 timeframe, one-third of the crashes in the Gap occurred during low-light driving conditions.
- **Interchange lighting.** Partial interchange lighting will be provided at the interchanges in the Gap to illuminate roadway locations where there is turbulence in the traffic stream from vehicles entering/exiting the I-25 mainline. Better visibility of vehicles that are weaving or accelerating/decelerating will provide drivers additional time to react to the turbulence and reduce the potential for crashes to occur. Published research suggests that installing lighting at

interchanges can reduce nighttime crashes by approximately 25 percent. In addition, continuous freeway lighting will be provided south of the County Line Road interchange.

- **Ramp acceleration and deceleration lanes.** The interchange ramps and adjacent acceleration/deceleration lanes will be lengthened to a 70-mile per hour design speed to enable drivers of entering or exiting vehicles to change speeds primarily outside of the mainline lanes. This will reduce the need for drivers on the mainline to adjust their speed to accommodate slower vehicles, which will lead to reduced turbulence and lower potential for crashes. Crash issues are typically more prevalent with vehicles entering the mainline from on-ramps because acceleration typically requires more time and distance than deceleration. As Table 2 shows, a 23 percent reduction in crashes is predicted for the build alternatives in the on-ramp speed change areas.
- **Wildlife crossings.** Four grade-separated wildlife crossings will be provided under I-25. These crossings should reduce the number of animals on/near the freeway which will reduce the potential for wild animal collisions and other collision types that result when drivers react to the presence of an animal in or adjacent to the travel lanes. Several publications have been written about experimental wildlife crossings. Although there is not enough data to develop crash modification factors, these studies do indicate that grade-separated wildlife crossings reduce vehicle collisions with wildlife in the immediate vicinity of the crossings. The five underpasses and two overpasses that CDOT has installed along SH 9 have thus far resulted in a reduction of wildlife-related crashes by nearly 90 percent.
- **Maintenance and incident management.** Strategies associated with these two elements of freeway operations are documented separately from this safety documentation. Efforts to improve these activities will improve the driving surfaces and promote more homogeneous traffic flow, with the result being a reduction in the potential for crashes and improved safety.

10. Safety Features for the Managed Lane

The design for the express lane incorporates additional features to support operations and reduce the potential for crashes between express lane and general-purpose lane drivers. Some of these features have been incorporated in direct response to eleven of the recommendations provided in the I-25 North Metro Managed Lanes Road Safety Audit Report as well as design elements proposed in the Twin Tunnels Environmental Assessment. These design features are:

- **Access.** The design limits access to the express lane at designated locations rather than allowing continuous access. This concentrates the weaving sections, and the potential for turbulence and multiple-vehicle collisions that accompany them, to a few locations rather than along the length of the Gap. The openings are located to provide adequate weaving distance to/from interchange ramps. Furthermore, the access locations permit only one movement (ingress or egress) to reduce the number of conflict points.

To minimize the possibility of drivers inadvertently being trapped in the express lane, drivers will have to make a lane change maneuver to enter the express lane. The freeway lanes will be gradually shifted laterally to accomplish a left-side introduction of the express lane.

There is a potential for turbulence to occur in the traffic stream if drivers are temporarily indecisive about whether or not to use the express lane. Such indecisiveness could result in a reduction in operating speed or weaving maneuvers made at the last minute, both of which would cause disruption to traffic flow. Clear, concise advance signing and increasing familiarity with the system should adequately inform drivers and minimize these instances. Also, the concept of operations

proposes to operate the express lane with dynamic, congestion-based pricing such that there will always be a fee to drive in the express lane. Maintaining the same mode of operations at all times (as opposed to operating the express lane for free sometimes and for a fee at other times) should improve driver expectancy and reduce indecisiveness by regular users of the facility.

- **Buffer zone.** A buffer zone will separate the express lane and left general-purpose lane in restricted access locations. This zone was designed in response to issues that have occurred with the I-25 express lane north of Denver that has a 2-foot buffer. Field observations and data suggest that crashes are occurring when general-purpose lane drivers choose to cross the double white lines to enter the express lane in a restricted area. The severity of the rear-end and sideswipe/same-direction collisions that sometimes result from these maneuvers can be increased due to the speed differentials in the two lanes.

This design doubles the width to a 4-foot buffer zone to separate the express lane and left general-purpose lane. Two, 8-inch solid white lines will emphasize the restriction that drivers should not cross the double white lines. A continuous rumble strip in the middle of these two channelizing lines should further encourage compliance with not crossing the double white lines. The increased complexity of navigating a 4-foot separation and rumble strip may discourage drivers from using the express lane as a passing lane. The noise and vibration produced by crossing the rumble strip will also serve to alert drivers in either of these lanes that they have inadvertently veered out of their lane, and allow them to correct their path before striking another vehicle.

The width of the express lane is 11 feet. The presence of the buffer zone between the express and left general-purpose lane should mitigate any driver discomfort with a narrower lane.

- **Signing.** Regulatory signing will explain the restrictions for use of the lane and the prohibition of crossing the double white lines. Advance guide signing will alert drivers to upcoming opportunities to enter or exit the express lane. The design locates these signs to provide sufficient time and distance for express lane drivers to react and change lanes as necessary and for general-purpose lane drivers to be prepared for turbulence in the traffic stream as other drivers conduct weaving maneuvers.
- **Enforcement bumpouts.** The design includes refuge areas in the median to facilitate enforcement activities. These bumpouts use concrete barriers to cordon off space for officers to park their vehicles in a location that is protected from the traffic flow. From their vehicles, officers can watch for illegal crossing of the double white lines or HOV violations. A secondary benefit is that drivers may align their travel speed more closely with the posted speed limit when a law enforcement vehicle is visible. These areas could also potentially be used as a refuge area to store disabled vehicles out of the mainline lanes.
- **Public Education.** Public education campaigns will be deployed to educate the public about the operations of the express lanes, the proper and legal use of them, and to encourage compliance with the express lane restrictions.

11. CMF Citations

Shoulder Width

Park, J., M. Abdel-Aty, and C. Lee. 2014. *Exploration and Comparison of Crash Modification Factors for Multiple Treatments on Rural Multilane Roadways*. Accident Analysis and Prevention, Vol. 70, (2014) pp. 167-177.

Neudorff, L., Jenior, P., Dowling, R., and Nevers, B. 2016. *Use of Narrow Lanes and Narrow Shoulders on Freeways: A Primer on Experiences, Current Practice, and Implementation Considerations*. Report No. FHWA-HOP-16-060. Federal Highway Administration: Washington, D.C. July.

Lighting

Hovey, P.W. and Chowdhury, M. 2005. *Development of Crash Reduction Factors*. 14801(0), Ohio Department of Transport.

Signs

Srinivasan, R., Baek, J., Carter, D., Persaud, B., Lyon, C. Eccles, K., Gross, F., Lefler, N. 2009. *Safety Evaluation of Improved Curve Delineation*. Report No. FHWA-HRT-09-045. Federal Highway Administration: Washington, D.C.

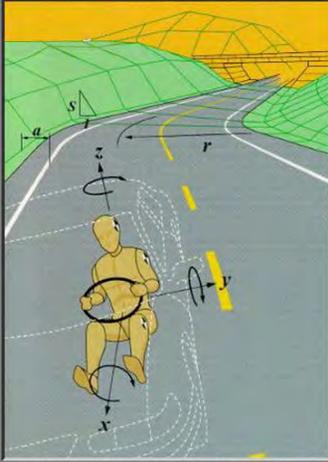
Wildlife Crossings

Patricia Cramer, Robert Hamlin. FHWA MT-17-003/8194. *Evaluation of Wildlife Crossing Structures on US 93 in Montana's Bitterroot Valley*.

Hoffman, Shannon M. 2018. "Animals are Using Colorado's Wildlife Crossings, Reducing Collisions, CDOT Says." *The Denver Post*. February 3.

Attachment 1
Safety Assessment Report – SH25A
Planning & Environmental Linkage
(PEL) Study - MP 157.70 – MP 194.31,
CDOT, July 2017

CDOT Safety Engineering
and Analysis Group



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Transportation Engineering
and Science

Safety Assessment Report

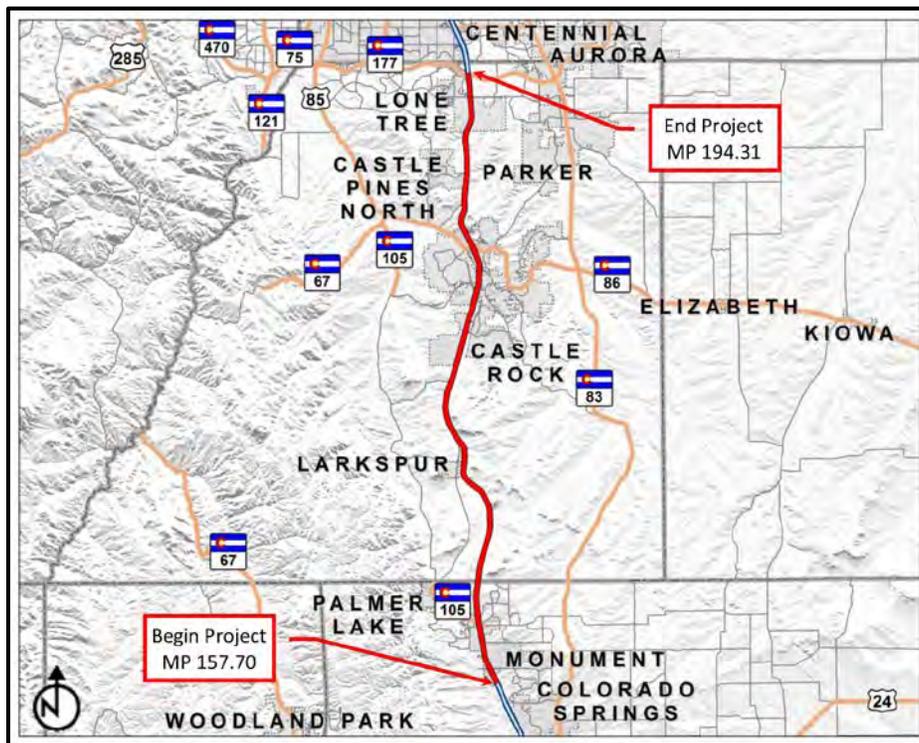
SH25A MP 157.70 - MP 194.31

Planning & Environmental Linkage (PEL) Study

July 2017

Prepared for: The Colorado Department of Transportation
HQ Safety and Traffic Engineering Branch
4201 E. Arkansas Ave, 3rd Floor
Denver, Colorado 80222

Prepared by: Muller Engineering Company

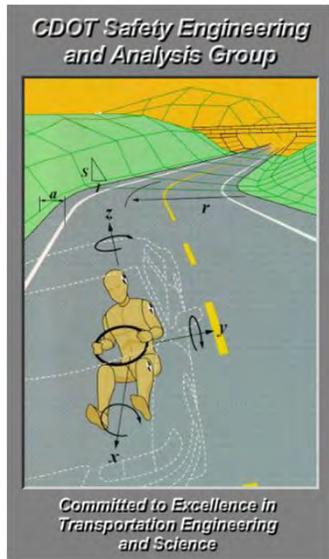


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Notwithstanding any other provision of law, reports, surveys, schedules, lists, or data compiled or collected for the purpose of identifying, evaluating, or planning safety enhancement of potential accident sites, hazardous roadway conditions, or railway-highway crossings, pursuant to sections 130, 144, and 152 of this title or for the purpose of developing any highway safety construction improvement project which may be implemented utilizing Federal-aid highway funds shall not be subjected to discovery or admitted into evidence in a Federal or State court proceeding or considered for other purposes in any action for damages arising from any occurrence at a location mentioned or addressed in such reports, surveys, schedules, lists or data.

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A Statement of Philosophy

The efficient and responsible investment of resources in addressing safety problems is a difficult task. Since crashes occur on all highways in use, it is inappropriate to say of any highway that it is safe. However, it is correct to say that highways can be built to be safer or less safe. Road safety is a matter of degree. When making decisions effecting road safety, it is critical to understand that the expenditure of limited available funds on improvements in places where it prevents few injuries and saves few lives can mean that injuries will occur and lives will be lost by not spending them in places where more accidents could have been prevented.¹ It is CDOT's objective to maximize accident reduction within the limitations of available budgets by making road safety improvements at locations where it does the most good or prevents the most accidents.

Introduction

The primary intent of the Planning and Environmental Linkages (PEL) study is to identify the safety and operational needs along State Highway 25 (SH 25, Interstate 25 or I-25) (Mainline Only), between Baptist Road Interchange (MP 157.70) and the south half of C-470/E-470 Interchange (MP 194.31). In conjunction with the PEL project, an opportunity exists for the detection of safety problems and the implementation of selected improvements at locations where it is justified by crash experience.

The scope of this report is as follows:

- Assess the magnitude and nature of the safety problem within the project limits;
- Relate crash causality to roadway geometrics, roadside features, traffic control devices, traffic operations, driver behavior, and vehicle type;
- Suggest cost effective counter measures to address identified problems; and
- Provide guidance on how to maximize accident reduction within the scope of a PEL study.

This report is based on the comprehensive analysis of five years of crash history and video log review. The Region is advised to verify through field survey, the information included in this report regarding physical features and roadside characteristics in the study area.

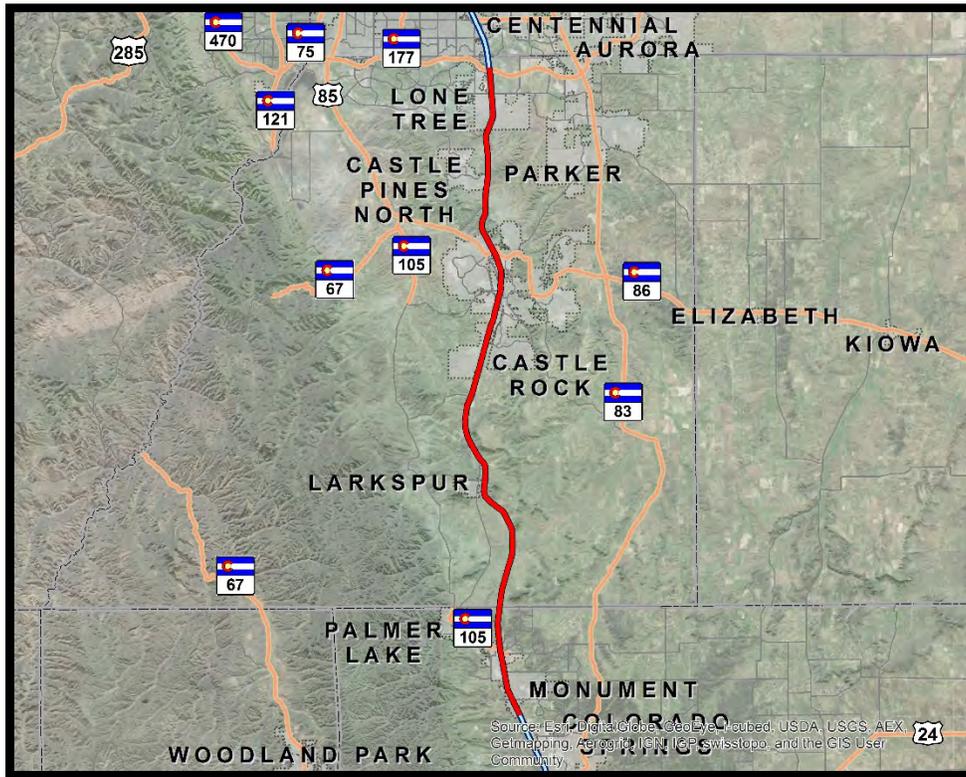
¹ Hauer, E., (1999) Safety Review of Highway 407: Confronting Two Myths. TRB

Site Location and Conditions

This safety assessment addresses mainline I-25 in El Paso County, between MP 157.70 to MP 163.32; and Douglas County, between MP 163.32 and 194.31. The included distance is approximately 36.61 miles. This segment of I-25 is classified as a rural and urban divided interstate; where it is urban from the southern limit to County Line Road (MP 163.32), and MP 179.00 to C-470/E-470; and rural from County Line Road to MP 178.99. Travel lanes are 12 feet wide throughout the study corridor. Travel lanes and shoulders are made of asphalt from the southern limit to approximately 1.5 miles south of Plum Creek Parkway (MP 179.12). Concrete pavement is utilized for travel lanes and shoulders north of this transition to the northern limit of the study area. The 2014 average daily traffic (ADT) on I-25 varies from 63,000 to 162,000 vehicles per day (VPD) with truck traffic ranging from 5.2 to 10.7 percent.

An aerial view of the study corridor is provided on **Figure 1**.

Figure 1
Vicinity Map



The following observations of the I-25 study corridor were based on a review of aerial photography, the CDOT video, and the 2014 CORIS data:

- Through Lanes Quantity:
 - 4 lanes from MP 160.81 (SH 105) to MP 179.17.
 - 6 lanes from MP 157.70 (Baptist Road) to MP 160.80 (SH 105), and MP 179.18 to MP 184.58 (Founders Parkway),
 - 8 lanes from MP 184.59 to MP 194.31 (C-470/E-470).
- Posted Speed Limit:
 - 75 mph from MP 157.70 to MP 180.50 (Plum Creek Parkway), and MP 187.81 to MP 192.50 (Lincoln Avenue).
 - 65 mph from MP 180.51 to MP 187.80, and MP 192.51 to MP 194.31 (C-470/E-470).
- Median Type / Width:
 - Depressed Median / 30-ft wide from MP 157.70 to MP 159.88.
 - Level Median with Concrete Barrier Separation from MP 159.89 to MP 163.29.
 - Depressed Median / 20-ft wide from MP 163.30 to MP 163.31 (County Line Road).
 - Depressed Median / 32-ft wide from MP 163.32 to MP 165.99.
 - Depressed Median / 22-ft wide from MP 166.00 to MP 167.45 (Greenland Road).
 - Depressed Median / 15-ft wide from MP 167.46 to MP 169.51.
 - Depressed Median / 32-ft wide from MP 169.52 to MP 171.81 (SH 18).
 - Level Median with Concrete Barrier Separation from 171.82 to MP 172.30 (Spruce Mountain Road).
 - Depressed Median / 20-ft wide from MP 172.31 to MP 172.99.
 - Depressed Median / 27-ft wide from MP 173.00 to MP 173.59.
 - Depressed Median / 32-ft wide from MP 173.60 to MP 173.78 (Sky View Lane).
 - Depressed Median / 40-ft wide from MP 173.79 to MP 174.99.
 - Depressed Median / 52-ft wide from MP 175.00 to MP 179.11.
 - Level Median with Concrete Barrier Separation from MP 179.12 to MP 194.31.
- Rumble Strips:
 - Inconsistent, but present in intervals between MP 157.70 and MP 164.00.
 - Generally present between MP 164.00 and MP 179.00.
 - Rumble strips are not present where guardrail is immediately adjacent to the shoulder, and where auxiliary lanes are present.
 - Not observed from MP 179.00 to MP 194.31 (concrete pavement).

Crash History and Study Analysis

Crash History

The crash history for the five-year study period of January 1, 2011 through December 31, 2015 was examined to locate crash cluster and identify collision causes. Within the study period, 5537 crashes were reported along I-25 between MP 157.70 and MP 194.31. Of these crashes, there were 1484 injury collisions, and 15 fatal collisions; 2201 injured and 15 killed, overall. **Table 1** summarizes the crash totals for I-25 over the five-year study period.

Table 1
Crash History of I-25 from MP 157.70 to MP 194.31, by Year

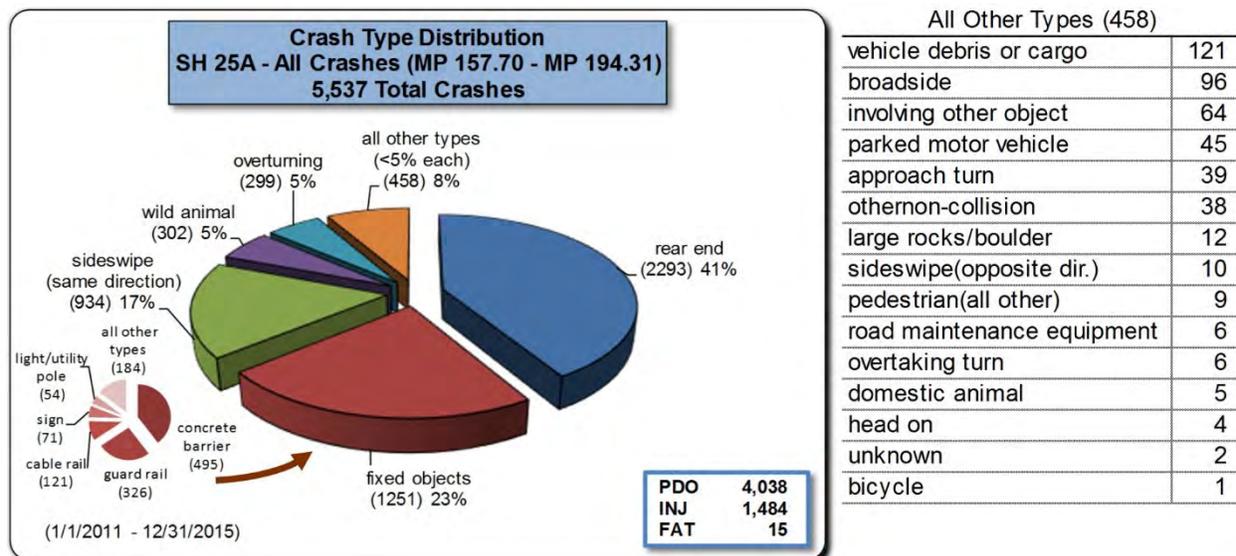
Year	Crashes				Persons	
	PDO*	Injury	Fatal	Total	Injured	Killed
1/1/2011 to 12/31/2011	638	242	2	882	334	2
1/1/2012 to 12/31/2012	674	255	3	932	371	3
1/1/2013 to 12/31/2013	784	252	6	1042	379	6
1/1/2014 to 12/31/2014	926	341	1	1268	517	1
1/1/2015 to 12/31/2015	1016	394	3	1413	394	3
Total	4038	1484	15	5537	2201	15
Average/Yr	807.6	296.8	3.0	1107.4	440.2	3.0

*PDO - Property Damage Only

In general, the number of total crashes increased, annually, during the five-year study period, and the number of injury crashes increased significantly from 2013 to 2014 by 89. The frequency of crashes reported in 2015 was approximately 60% higher than in 2011.

Figure 2 shows the breakdown of crashes, by type, for the 36.61-mile study corridor. *Rear-end* type crashes were predominant (41%), followed by crashes involving fixed objects (23%), and *sideswipe (same direction)* (17%).

Figure 2
I-25 Crash Distribution, by Type



The complete listing and detailed crash summary sheets for the entire I-25 study corridor are provided in the **Appendix**.

Fatal Crash History

There were 15 fatal crashes along the I-25 study corridor, resulting in 15 fatalities. Six (6) of the 15 fatal crashes were *overturning* type crashes. Three (3) of the six (6) *overturning* type crashes occurred between the Baptist Road interchange (Exit 158) and the County Line Road interchange (Exit 163); of which alcohol (DUI) was listed as a contributing factor in two (2) of the three (3) crashes. The remaining three (3) *overturning* type crashes were scattered between MP 176.00 and MP 191.00; of which all three (3) occurred during dark-unlighted conditions.

Three (3) of the 15 fatal crashes were *pedestrian* type crashes; of which all three (3) crashes occurred during dark-unlighted conditions, and alcohol (DUI) was a contributing factor in two (2) of the three (3) crashes. There were no notable locations reported for these fatal crashes as they were located between MP 176.00 and MP 193.10.

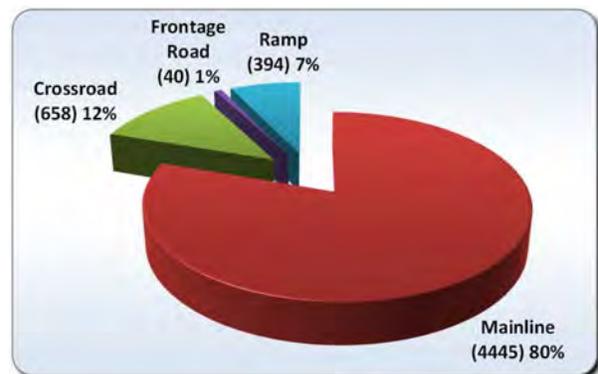
The fatal crashes will be discussed in greater detail in following sections of this report.

Crash Locations

The majority of the crashes along the I-25 study corridor occurred on mainline I-25 (4445 of 5537, 80%). The remaining crashes occurred on the crossroads or at one of the ramp terminal intersections (658 of 5537, 12%), along one of the ramps connecting to/from I-25 (394 of 5537, 7%), and along frontage roads (40 of 5537, 1%). This breakdown is shown in **Figure 3**.

The crash history specific to mainline I-25 traffic will be discussed separately from the ramp and crossroad-related crashes in the following sections of this report.

Figure 3
Crashes by Location



Mainline I-25 Crashes

Mainline crashes, those not attributed to ramps or crossroads, make up the majority of the crashes on the study section. It should be noted that mainline crashes include accidents associated with on-ramp and off-ramp acceleration and deceleration lanes beyond the gore of the ramp (where there is a physical separation of pavement between ramp and mainline I-25).

Crash History

The crash history for the period of January 1, 2011 through December 31, 2015 was examined to locate crash clusters and identify collision causes. Within the study period, 4,445 crashes were reported along I-25 between MP 157.70 and MP 194.31. Of these crashes, there were 1,280 injury collisions and 13 fatal collisions; 1921 injured and 13 killed, overall. **Table 2** summarizes the crash totals for mainline I-25 over the five-year study period.

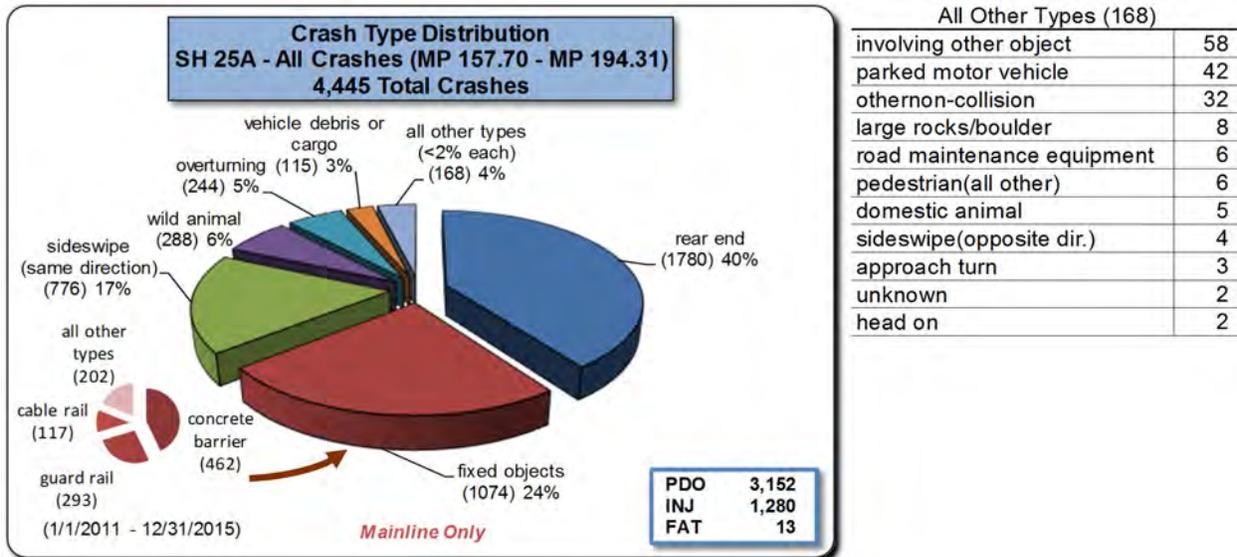
Table 2
Crash History of Mainline I-25 from MP 157.70 to MP 194.31 by Year

Year	Crashes				Persons	
	PDO*	Injury	Fatal	Total	Injured	Killed
1/1/2011 to 12/31/2011	492	205	2	699	288	2
1/1/2012 to 12/31/2012	520	209	2	731	307	2
1/1/2013 to 12/31/2013	620	229	6	855	349	6
1/1/2014 to 12/31/2014	701	289	1	991	441	1
1/1/2015 to 12/31/2015	819	348	2	1169	536	2
Total	3152	1280	13	4445	1921	13
Average/Yr	630.4	256.0	2.6	889.0	384.2	2.6

*PDO – Property Damage Only

Figure 4 shows the breakdown of mainline I-25 crashes, by type, for the 36.61-mile study corridor. *Rear-end* crash types were predominant (40%), followed by crashes involving fixed objects (24%), *sideswipe (same direction)* (17%), *wild animal* (6%), and *overturning* (5%). The rate of all of these crash types, except *rear-end*, were higher than the statewide average for similar facilities.

Figure 4
I-25 Mainline Crash Distribution by Type



The complete listing and detailed crash summary sheets for the mainline study section of I-25 are provided in the **Appendix**.

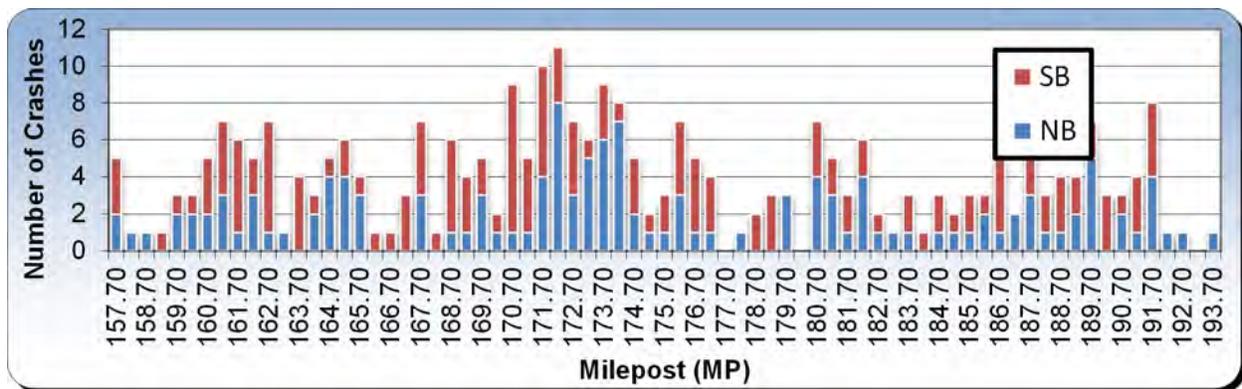
Fatal Crash History

There were 13 fatal crashes along the mainline I-25 study section, resulting in 13 fatalities. There was no notable concentration nor pattern to these fatal crashes, except that alcohol was a factor in six (6) of the 13 crashes, and 11 of 13 occurred during dark-unlighted or dark-lighted conditions. *Overturning* was the most common crash type (4 of 13), followed by *pedestrian* (3 of 13).

Wild Animal Crashes

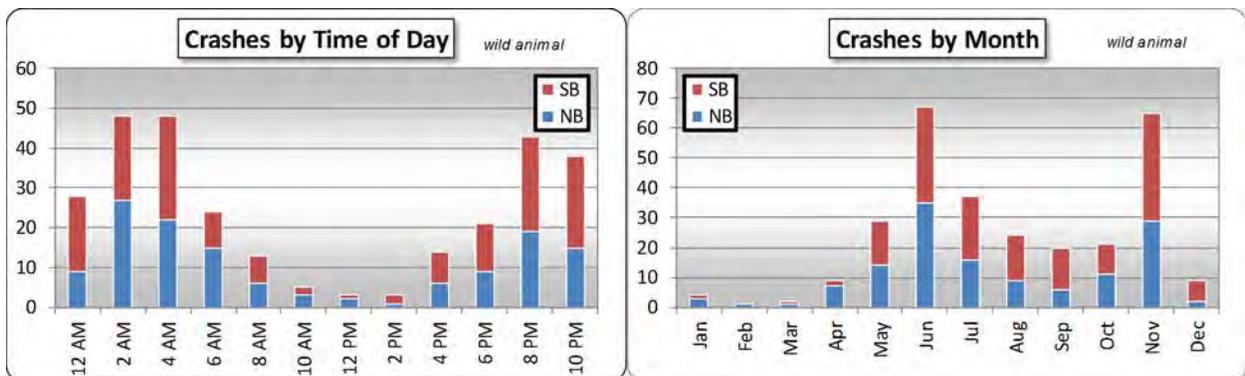
The frequency of *wild animal* crashes was higher than the statewide average rate for similar facilities. **Figure 5** shows that these crashes were spread throughout the I-25 mainline corridor with a few concentrations. A little more than half of the crashes involved southbound vehicles (154 of 288, 53%).

Figure 5
Wild Animal Crashes by Milepost



A review of the crash history indicated that the *wild animal* crashes were mainly property damage only (PDO) and generally involved deer (225 of 288). The majority of crashes occurred at night (210 of 288). The frequency of *wild animal* crashes was higher in the summer months, although November experienced very high frequencies during the five-year study period. **Figure 6** shows the distribution of *wild animal* crashes by time of day, and by month, in separate graphs.

Figure 6
Wild Animal Crashes by Time and Date

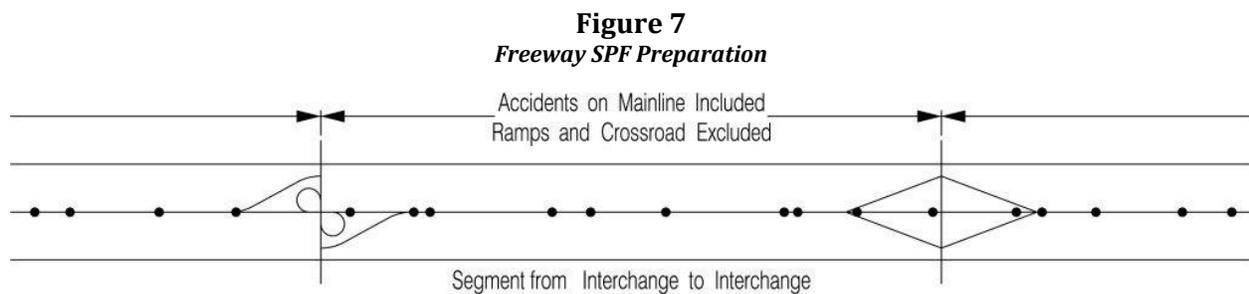


According to the CDOT Strip Maps and the video log, there are no Deer Warning Signs (W11-3) anywhere on I-25 between MP 157.00 and MP 194.75. The Region should consider installing a Deer Warning Sign posted near MP 171.50 in the northbound direction, and near MP 172.50 in the southbound direction, with a “Next 3 Miles” supplemental plaque (W16-4P) to warn motorists of possible wild animals crossing I-25. Also consider supplementing existing VMS signs throughout the corridor with Wild Animal warning messages during peak hours and months.

Safety Performance Function

The assessment of the magnitude of safety problems is refined through the use of Safety Performance Functions (SPF). The SPF reflects the complex relationship between traffic exposure measured in Average Daily Traffic (ADT), and crash count measured in crashes per year. The SPF model provide an estimate of the normal or expected crash frequency and severity for a range of ADT among similar facilities. Two kinds of SPF's were calibrated. The first addresses the total number of crashes, and the second addresses crashes involving an injury or fatality, allowing the assessment of the magnitude of the safety problem from the frequency and severity standpoint.

All of the dataset preparation was performed using the Colorado Department of Transportation (CDOT) crash databases. Crash history for each facility was prepared using the most recent five years of available crash data. The ADT for each roadway segment for each of the five years was entered into the same dataset. **Figure 7** illustrate how the datasets were prepared for rural and urban interstates.



Development of the SPF lends itself to the conceptual formulation of the Level of Service of Safety (LOSS). The concept of level of service uses qualitative measures that characterize safety of an intersection in reference to its expected performance. If the level of safety predicted by the SPF will represent a normal or expected number of crashes at a specific level of ADT, selected percentiles within the frequency distribution can be stratified to represent specific levels of safety.

- LOSS I – Below 20th Percentile
Indicates a low potential for crash reduction.
- LOSS II – 20th Percentile to Mean
Indicates a low to moderate potential for crash reduction.
- LOSS III – Mean to 80th Percentile
Indicates a moderate to high potential for crash reduction.
- LOSS IV – Above 80th Percentile
Indicates a high potential for crash reductions.

LOSS reflects how the roadway or intersection is performing in regard to its expected crash frequency at a specific level of ADT (major and minor). It does not, however, provide any information related to the nature of the safety problem itself. If a safety problem is present, LOSS will only describe its magnitude from the frequency and severity standpoints. The nature of the problem is determined through diagnostic analysis using direct diagnostic and pattern recognition techniques discussed later in this assessment.

Mainline Crash Analysis Segments

Crash history within the study period and section for I-25 has been split into 18 segments. **Table 3** lists the analysis segments by milepost range.

Table 3
Mainline I-25 Study Segments

ID	MP Range	Segment Description	Lanes	AADT	Rural/ Urban	LOSS ²	Number of Crashes ¹			
							PDO	INJ	FAT	TOT
1	157.70 – 158.19	Begin Study Section to Baptist Rd	6	91,000	Urban	*	75	35	1	111
2	158.20 – 160.75	Baptist Road to SH 105	6	80,000	Urban	III/IV	195	102	0	297
3	160.76 – 163.81	SH 105 to County Line Rd	4	63,000	Urban	III/IV	265	141	1	407
4	163.82 – 166.96	County Line Rd to Greenland Rd	4	63,000	Rural	III/III	161	97	1	259
5	166.97 – 167.95	Greenland Rd	4	63,000	Rural	IV/IV	85	43	0	128
6	167.96 – 171.32	Greenland Rd to Upper Lake Gulch Rd	4	63,000	Rural	III/III	169	108	1	278
7	171.33 – 172.81	Upper Lake Gulch Rd to Spruce Mtn. Rd	4	63,000	Rural	IV/IV	134	68	0	202
8	172.82 – 173.03	Spruce Mountain Rd	4	65,000	Rural	*	16	4	0	20
9	173.04 – 174.29	Sky View Ln	4	65,000	Rural	II/II	59	22	0	81
10	174.30 – 178.99	Sky View Ln to MP 179.00	4	68,000	Rural	II/II	227	107	2	336
11	179.00 – 180.87	MP 179.00 to Plum Creek Pkwy	6	68,000	Urban	III/III	91	36	1	128
12	180.88 – 181.84	Plum Creek Pkwy to Wolfensberger Rd	6	79,000	Urban	II/III	47	28	0	75
13	181.85 – 184.20	Wolfensberger Rd to Founders Pkwy (SH 85 and SH 86B)	6	96,000	Urban	II/II	135	53	1	189
14	184.21 – 186.93	Founders Pkwy (SH 85B and SH 86B) to Happy Canyon Rd	8	110,000	Urban	II/II	182	77	1	260
15	186.94 – 188.48	Happy Canyon Rd to Castle Pines Pkwy	8	110,000	Urban	II/II	121	57	0	178
16	188.49 – 192.07	Castle Pines Pkwy to Ridgeway Pkwy	8	114,000	Urban	III/III	488	189	3	680
17	192.08 – 192.98	Ridgeway Pkwy to Lincoln Ave	8	130,000	Urban	IV/II	249	44	0	293
18	192.99 – 194.31	Lincoln Ave to C-470/E-470	8	162,000	Urban	IV/II	453	69	1	523
Total							3152	1280	13	4445
Average/Year							630.4	256.0	2.6	889.0

¹PDO – Property Damage Only, INJ – Injury, FAT – Fatal, TOT – Total

²Level of Service of Safety: Total/Severe

* Segment length insufficient for SPF analysis

Segment 1 – Begin Study Section to Baptist Rd (MP 157.70 – MP 158.19)

There were 111 crashes during the five-year study period along this segment of I-25, of which 35 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

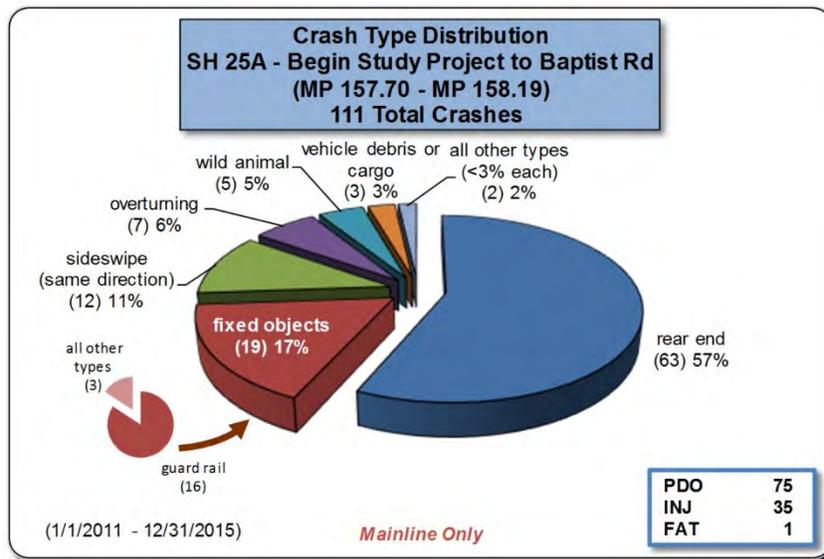


- This segment of mainline I-25 is a 6-lane urban freeway with a 30-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders and 10-ft wide outside shoulders.
- The terrain is rolling with a gentle northbound upward slope.
- There is a bridge structure over Jackson Creek (MP 157.70); guard rail was present on the outside shoulders adjacent to this structure.

Crash History

Figure 8 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (57%), followed by crashes involving fixed objects (17%) and *sideswipe (same direction)* type crashes (11%).

Figure 8
Segment 1 Crash Distribution



All Other Types (2)	
pedestrian(all other)	1
large rocks/boulder	1

Construction Timeline

The Baptist Road interchange was reconstructed during the five-year study period, as indicated by the increase in *rear-end*, *guard rail* and other various crash types between 2012 and 2014. As such, no recommendations for improvements are made at this time.

Fatal Crash

There was one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The *guard rail* type crash occurred during wintry road conditions in January 2013. A vehicle traveling southbound in the left lane entered the left gravel shoulder and impacted the

guard rail. The driver overcorrected and caused the vehicle to enter a clockwise rotation before leaving the roadway off-right and overturning approximately two times while traveling down an embankment. The vehicle was occupied twice, both driver and passenger were intoxicated beyond the legal limit and the passenger had suspected drug impairment. As a result of the crash, the driver sustained serious injuries while the passenger expired on scene. As the crash occurred during the reconstruction of the Baptist Road interchange no recommendations for improvements can be made at this time.

Segment 2 – Baptist Rd to SH 105A (MP 158.20 – MP 160.75)

There were 297 crashes during the five-year study period along this segment of I-25, of which 102 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

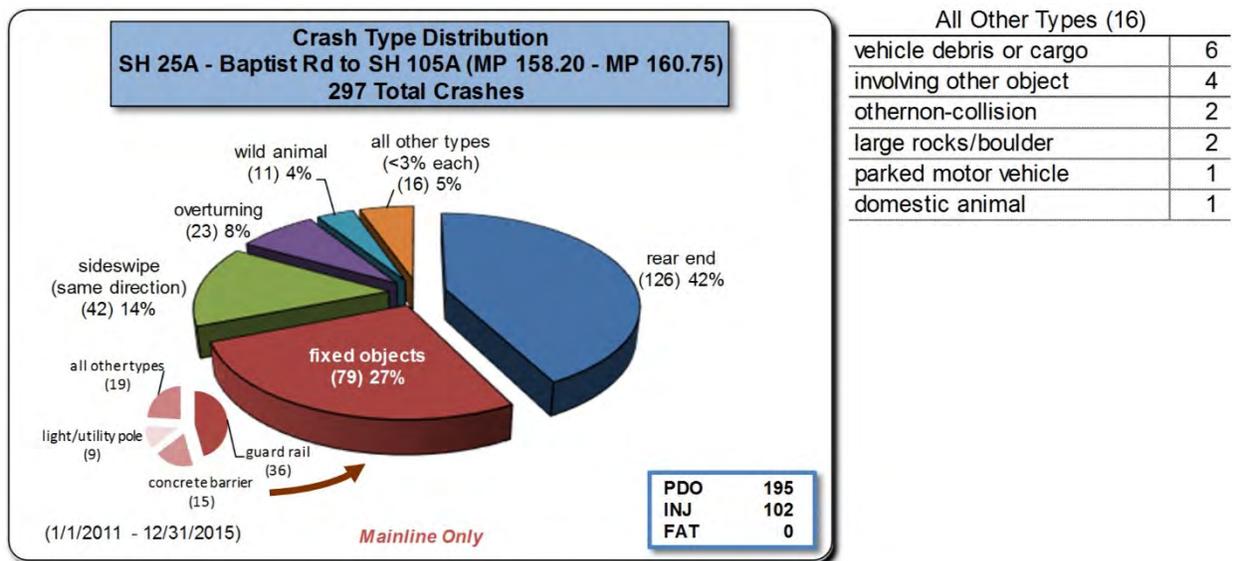


- This segment of I-25 is a 6-lane asphalt urban freeway with 30-ft wide depressed median, 4-ft wide inside shoulders, 10-ft wide outside shoulders from MP 158.20 to MP 160.16. From MP 160.16 to MP 160.75, there is a concrete barrier in the level median with 10-ft wide inside shoulders, 12-ft wide travel lanes, and 6-ft wide outside shoulders.
- The terrain is rolling with a gentle northbound upward slope.
- There are bridge structures over Teachout Creek (MP 159.34) and Dirty Woman Creek (MP 160.58); guard rail is present on the outside shoulders adjacent to these structures.

Crash History

Figure 9 provides a graphical representation of the mainline crash types for this location. Rear-end crashes were predominant (42%), followed by crashes involving fixed objects (27%) and sideswipe (same direction) type crashes (14%).

Figure 9
Segment 2 Crash Distribution



Run-Off-Road Collisions

The proportion of crashes in this segment that involved a vehicle running off the road was 102 of 297. In approximately a third of the incidents (36 of 102) the vehicle impacted a *guard rail*. Wintry road conditions were contributing factors in most of the *guard rail* crashes (21 of 36). The Region should consider utilizing the existing variable message signs at MP 159.80 to warn drivers of deteriorating roadway conditions.

Segment 3 – SH 105 to County Line Rd (MP 160.76 – MP 163.81)

There were 407 crashes during the five-year study period along this segment of I-25, of which 141 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

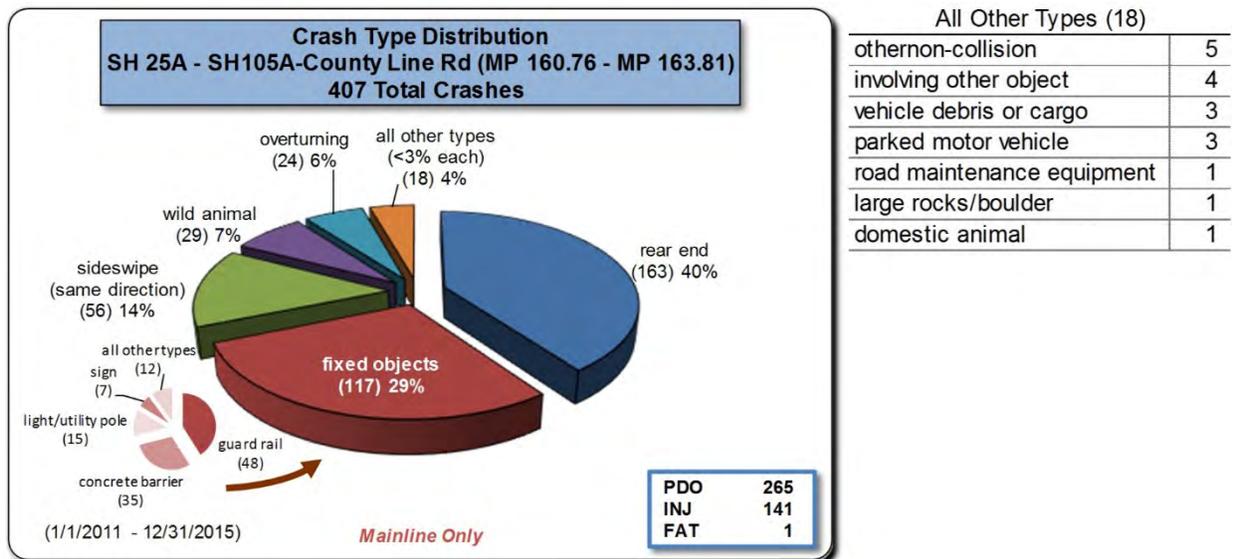


- This segment of I-25 is a 4-lane urban freeway, with a 20-ft wide concrete median, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 6-ft to 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound upgrade slope.
- Highest elevation point is 7343 ft. at MP 163.00.

Crash History

Figure 10 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (40%), followed by crashes involving fixed objects (29%) and *sideswipe (same direction)* type crashes (14%).

Figure 10
Segment 3 Crash Distribution



Fatal Crash

There was one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25 listed from CDOT crash data. The Colorado Department of Revenue Motor Vehicle Division Electronic Reporting System has no record of the crash. However, it is known that this was a *sideswipe (same direction)* type crash involving a motorcycle with no apparent causal factor.

Rear-End Crashes

There was a high proportion of rear-end crashes in this segment; many occurred between the SH 105 interchange and the Monument Weigh Station (48 of 163). The northbound and southbound weigh stations are located on an approximate average grade of 3.5-percent (northbound positive grade, southbound negative grade). The region should consider relocating the weigh station to an area not on-grade or proximate to an existing interchange without penalizing truck travel time unreasonably. Additional consideration should be made to preserve lane continuity by widening the roadway to provide three lanes in each direction. Roadway widening through this segment would likely reduce the frequency of other crash types, such as *sideswipe (same direction)* type crashes.

Run-off-Road Crashes

There was a high proportion of run-off-road type crashes; this included a higher than expected frequency of *guard rail* crashes (48 of 139). Wintry road conditions were a contributing factor in many of these crashes (17 of 48). The Region should consider utilizing the existing variable message sign at MP 162.95 to advise drivers of deteriorating roadway conditions. Additionally, consideration should be given to pretreat the road surface to prevent icy/snow accumulation before wintry conditions.

Light/Utility Pole Crashes

There was a high proportion of *light/utility* pole crashes in this segment (15 of 407). Outside shoulder widths are currently approximately 6-ft wide, with the light poles approximately five feet beyond the edge of the shoulder pavement. The reduced shoulder width limits the recovery area before striking a light pole (currently within the clear zone). The Region should consider installing a standard-width shoulder from SH 105 (approximate MP 160.5) to approximately MP 162.25. If additional pavement for a right shoulder cannot be provided, then guard rail should be considered for this segment.

Wet Roadway Crashes

There was a concentration of crashes involving *wet roadway* conditions in the southbound direction in the area of the SH 105 interchange; over half of the wet roadway crashes (28 of 53) occurred between MP 160.80 and MP 161.30. These crashes were predominately *concrete barrier* collisions involving vehicles traveling off of the left-side of the road (16 of 28). The Region should consider a review of the drainage profile, super-elevation, and culvert conditions to ensure proper drainage and inlet capacity and help reduce the frequency of *wet roadway* crashes.

Segment 4 – County Line Rd to Greenland Rd (MP 163.82 – MP 166.96)

There were 259 crashes during the five-year study period along this segment of I-25, of which 97 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

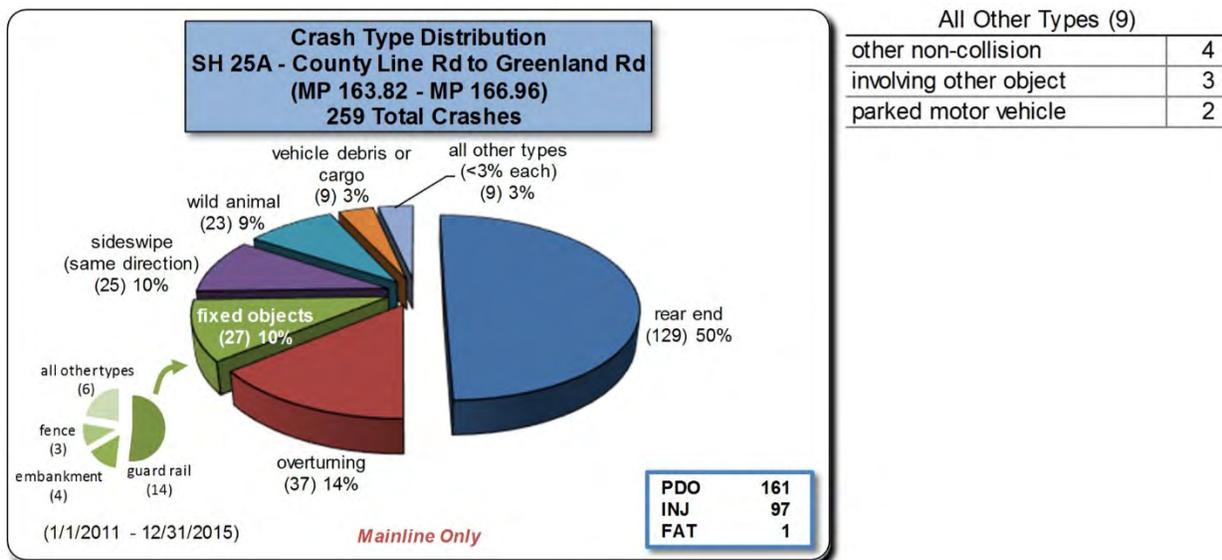


- This segment of I-25 is a 4-lane rural freeway, with a 32-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 11 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (50%), followed by *overturning* crashes (14%) and *fixed object* type crashes (10%).

Figure 11
Segment 4 Crash Distribution



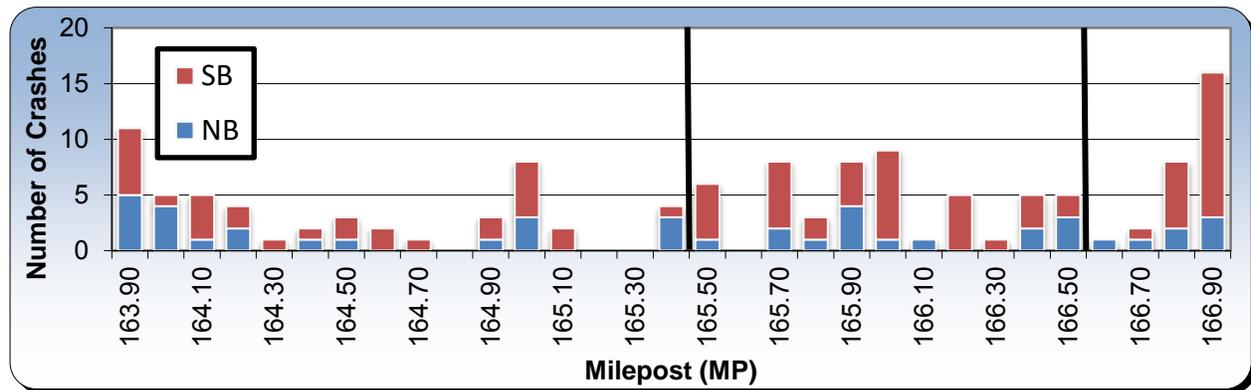
Fatal Crash

There was one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The *overturning* type crash occurred in daylight on dry roadway conditions in August 2013. A fully loaded concrete cement truck was traveling northbound in the right lane. The right tire on the tag axle suffered a catastrophic failure causing the truck to enter the left lane. The driver applied the brakes and steered to the right. This action caused the truck to begin rotating clockwise striking a roadside delineator with the rear of the truck. The forward outside dual tire then failed causing the truck to overturn. The cab struck the median embankment before coming to a rest on its left side. The driver required extrication and was found expired on scene. Due to the cause of the crash, mechanical failure, and the vehicle type, no specific recommendations or improvements can be made at this time.

Rear-End Crashes

There was a high proportion of *rear-end* crashes in this segment (129 of 259). A concentration of these crashes occurred southbound between MP 165.50 and MP 166.50 (36 of 86) and are shown in **Figure 12**.

Figure 12
Rear-End Crashes by Location



The southbound uphill grade between MP 165.50 and MP 166.50 is approximately 2.5-percent. Many of the *rear-end* crashes were the result of vehicles in the left-lane colliding with vehicles changing lanes from the slower right lane, or to the sudden change in speeds in the left lane as a result of such maneuvers. The Region should consider adding a southbound general purpose lane to allow slower vehicles (i.e. trucks, recreational vehicles, etc.) to use as a climbing lane.

An additional southbound *rear-end* crash concentration occurred near MP 166.8 and is related to the Greenland Road interchange. Details and recommendations for that concentration of crashes is discussed in Segment 5.

Segment 5 – Greenland Rd (MP 166.97 – MP 167.95)

There were 128 crashes during the five-year study period along this segment of I-25, of which 43 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

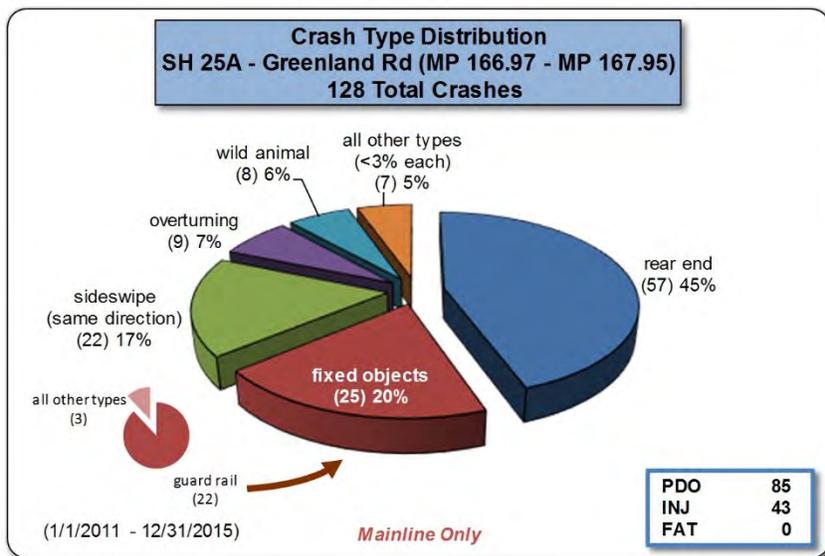
- This segment of I-25 is a 4-lane rural freeway, with a 15-ft to 22-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.
- There are bridge structures over East Plum Creek (MP 167.30) and Greenland Road (MP 167.46); guard rail is present on the inside and outside shoulders adjacent to these structures.



Crash History

Figure 13 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (45%), followed by *fixed object* crashes (20%) and *sideswipe (same direction)* crashes (17%).

Figure 13
Segment 5 Crash Distribution



All Other Types (7)	
vehicle debris or cargo	3
approach turn	2
sideswipe(opposite dir.)	1
involving other object	1

Run-off-Road Crashes

There was a high proportion of run-off-road type crashes (33 of 128) in this segment. Notably there was a higher than expected frequency of *guard rail* crashes (22 of 33) for this type of interstate when compared with similar facilities. Snowy roads (4 of 22) and wet roads (5 of 22) were contributing factors in approximately half of the *guard rail* crashes. Consideration should be given to using the existing variable message sign at MP 167.00 to warn drivers of deteriorating roadway conditions. Consideration should be given to pretreat the roadway surface to prevent icy/snow accumulation before wintery conditions.

Rear-End and Sideswipe (Same Direction) Crashes

There was a high concentration of *rear-end* (57 of 128) and *sideswipe (same direction)* (22 of 128) crashes. These two crash types represent approximately 60-percent of the total crashes in this segment and were concentrated between MP 167.00 and MP 167.20, near the southbound Greenland Road on-ramp. Crashes occasionally (10 of 79) occur during dark-unlighted conditions. The Region should consider a review of the interchange geometry and sight distances. These measures may also reduce the frequency of other crash types.

Segment 6 – Greenland Rd to Upper Lake Gulch Rd (MP 167.96 – MP 171.31)

There were 278 crashes during the five-year study period along this segment of I-25, of which 108 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

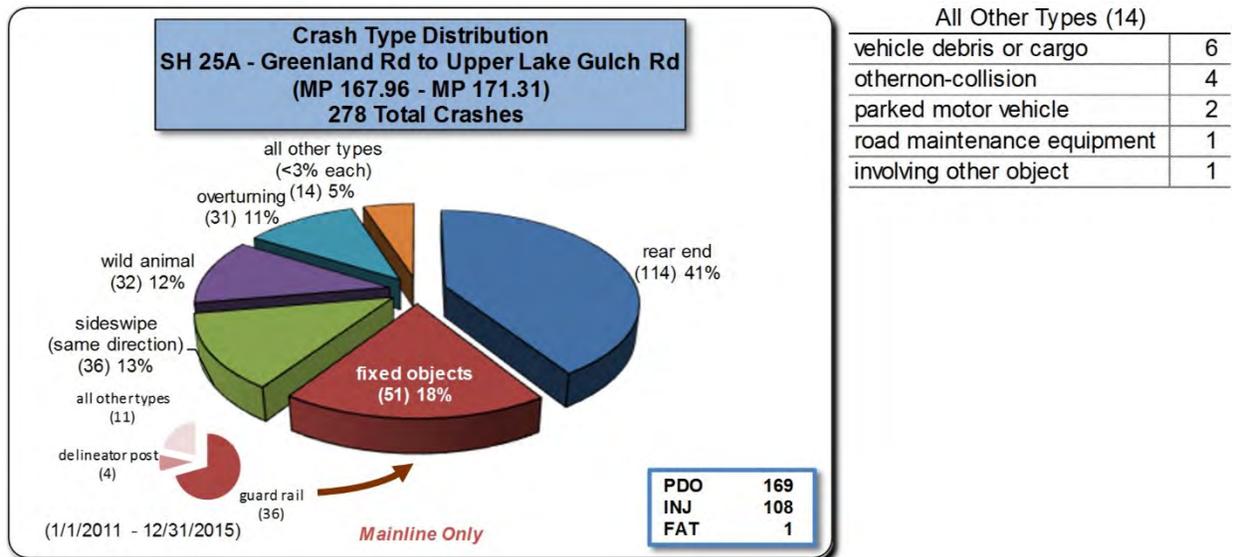


- This segment of I-25 is a 4-lane rural freeway, with a 15-ft to 32-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 14 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (41%), followed by *fixed object* crashes (18%) and *sideswipe (same direction)* type crashes (13%).

Figure 14
Segment 6 Crash Distribution



Fatal Crash

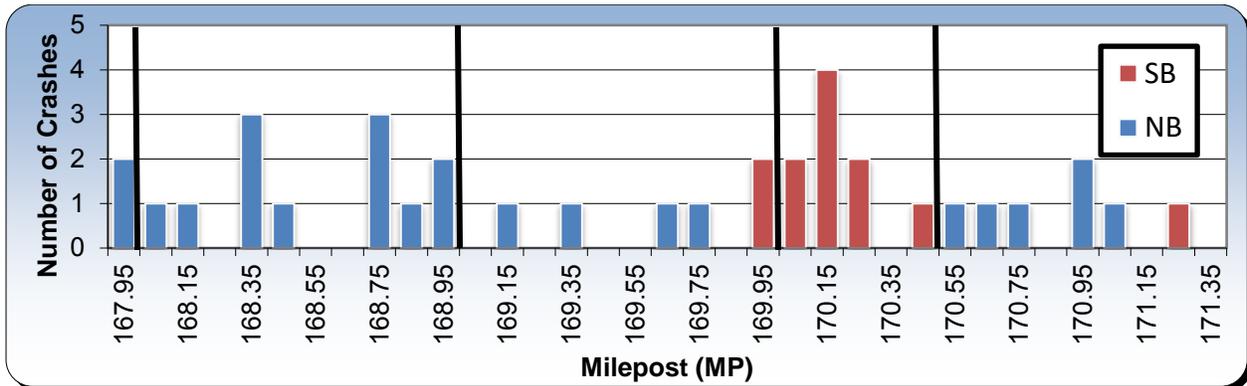
There was a one (1) crash during the five-year study period which resulted in a fatality along this segment. The *guard rail* type crash occurred around midnight with dry roadway conditions in August 2013. A northbound passenger vehicle drifted off the left side of the roadway, striking the median guard rail. The vehicle then traveled off the right side of the roadway for unknown reasons traveling for approximately 200 feet before colliding with a tree 60 feet from the edge of pavement. The driver was found expired on scene.

Since the fatal crash occurred due to unknown reasons no specific recommendations can be made to address the fixed object collision at this time.

Run-off-Road Crashes

There was a higher proportion of run-off-road type crashes (81 of 278) in this segment; including a higher than expected frequency of *guard rail* crashes (36 of 81). *Guard rail* crashes are directional with northbound *guard rail* crashes (14 of 36) concentrated between MP 168.00 and MP 169.00. Southbound *guard rail* crashes (11 of 36) are concentrated between MP 170.00 and MP 170.50. *Guard rail* crashes by location are provided as **Figure 15**.

Figure 15
Guard Rail Crashes by Location



Both *guard rail* crash concentrations are located on left-hand curves and do not show a propensity to dark-unlighted or daylight conditions. The Region should consider replacing existing guard rail reflectors and repairing or upgrading the existing guard rail where it is damaged or does not meet current standards to reduce the severity of a crash when it does occur. The Region should also consider installing strip delineators on all guard rail to improve its visibility.

Sideswipe (Same Direction) Crashes

There was a higher proportion of *sideswipe (same direction)* crashes (36 of 278) in this segment. The *sideswipe (same direction)* crashes had concentrations in the same locations as the *guard rail* crashes; northbound near MP 169.00 (7 of 19) and southbound near MP 170.00 (10 of 17). Both concentrations occur on the same left-hand curves as the *guard rail* crashes. Approximately a third of *sideswipe (same direction)* occurred during dark-unlighted conditions (11 of 36). The Region should consider restriping these locations with high visibility lane markings to improve lane recognition. Additionally, luminaires should be considered to better illuminate the roadway. The Region should also consider adding chevrons to the southbound left-hand curve near MP 170.50 to alert motorists of an upcoming curve.

Segment 7 - Upper Lake Gulch Rd. to Spruce Mtn. Rd. (MP 171.32 - MP 172.81)

There were 202 crashes during the five-year study period along this segment of I-25, of which 68 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

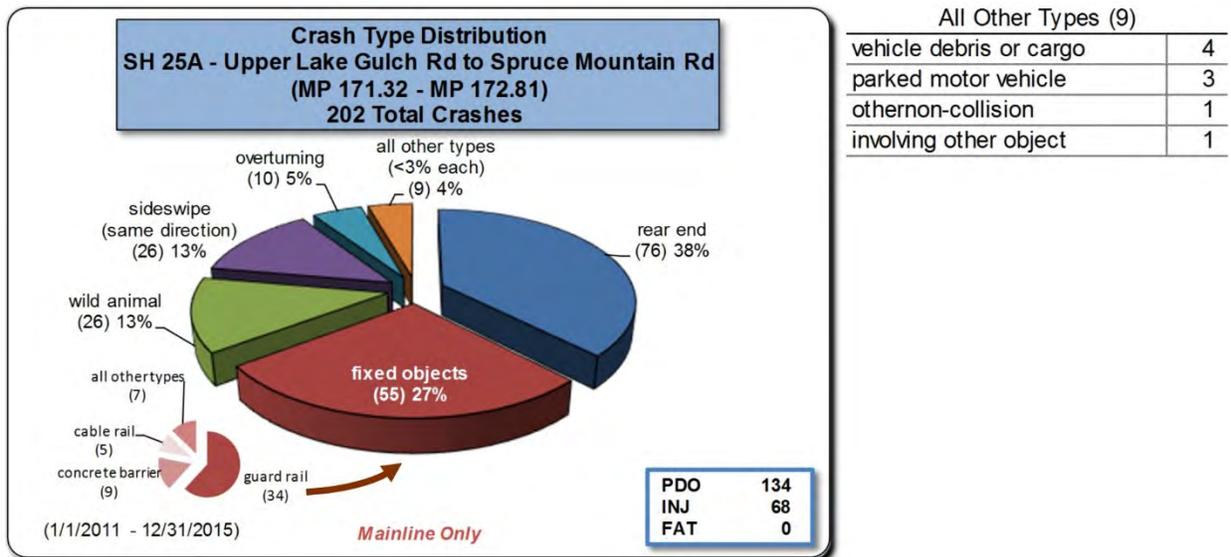


- This segment of I-25 is a 4-lane rural freeway, with 12-ft wide travel lanes and 10-ft wide outside shoulders.
- MP 171.32 - MP 171.82, inside shoulders are 4-ft wide, and the median is 32-ft wide and depressed.
- MP 171.82 - MP 172.31, inside shoulders are 18-ft wide, the median is a 10-ft wide raised curb.
- MP 172.31 - MP 172.81, inside shoulders are 4-ft wide, and median is 20-ft wide and depressed.
- There are bridge structures located over Upper Lake Gulch Road (MP 171.31), a BNSF/UP railroad track (MP 172.02) and East Plum Creek (MP 172.19); guard rail is present on the inside and outside shoulders adjacent to these structures, except for concrete barrier on the inside shoulders of the adjacent structure over East Plum Creek.

Crash History

Figure 16 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (38%), followed by *fixed object* crashes (27%) and *wild animal* crashes (13%).

Figure 16
Segment 7 Crash Distribution



Run-off-Road Crashes

There was a higher proportion of run-off-road type crashes (63 of 202) in this segment; including a higher than expected frequency of *guard rail* crashes (34 of 63). The majority of *guard rail* crashes were concentrated northbound between MP 171.80 and MP 172.20 (14 of 21). The concentration occurs on a left-hand curve with a 2-percent downhill grade. The Region should consider replacing existing guard rail reflectors and repairing or upgrading the existing guard rail where it is damaged or does not meet current standards to reduce the severity of a guard rail crash when it does occur. Consideration should also be given to installing strip delineators on all guard rail to improve its visibility during dark-unlighted or other low visibility conditions. Also, the Region should consider placing a suggested speed limit on the advanced curve warning sign at approximately MP 171.50. Additionally, chevrons may be installed at the beginning of the curve at MP 171.70. In general, any visual queue to prepare the motorist for an upcoming curve may reduce the frequency of other crash types (i.e. *rear-end* or *sideswipe (same direction)*).

Wild Animal Crashes

There was a higher proportion of *wild animal* crashes (26 of 202) in this segment. Most crashes occurred during dark-unlighted conditions (17 of 26). The region should consider focusing wild animal mitigation efforts, including but not limited to, fencing, over/under-pass, to this segment due to the high occurrence of wild animal crashes at 3.5 per mile per year.

Segment 8 – Spruce Mountain Rd (MP 172.82 – MP 173.03)

There were 20 crashes during the five-year study period along this segment of I-25, of which four (4) resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

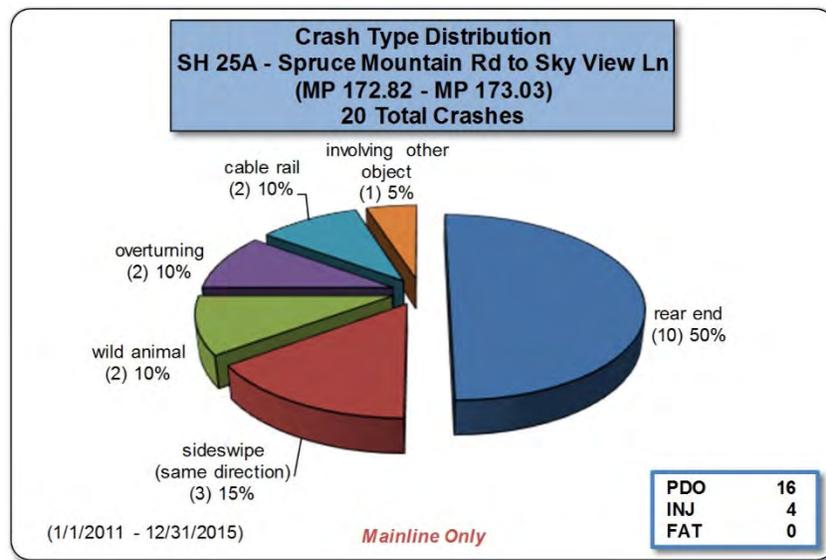


- This segment of I-25 is a 4-lane rural freeway with a 27-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 17 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (50%), followed by *sideswipe (same direction)* crashes (15%) and *wild animal* crashes (10%).

Figure 17
Segment 8 Crash Distribution



No significant crash patterns were noted in this segment.

Segment 9 – Sky View Ln (MP 173.04 – MP 174.29)

There were 81 crashes during the five-year study period along this segment of I-25, of which 22 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

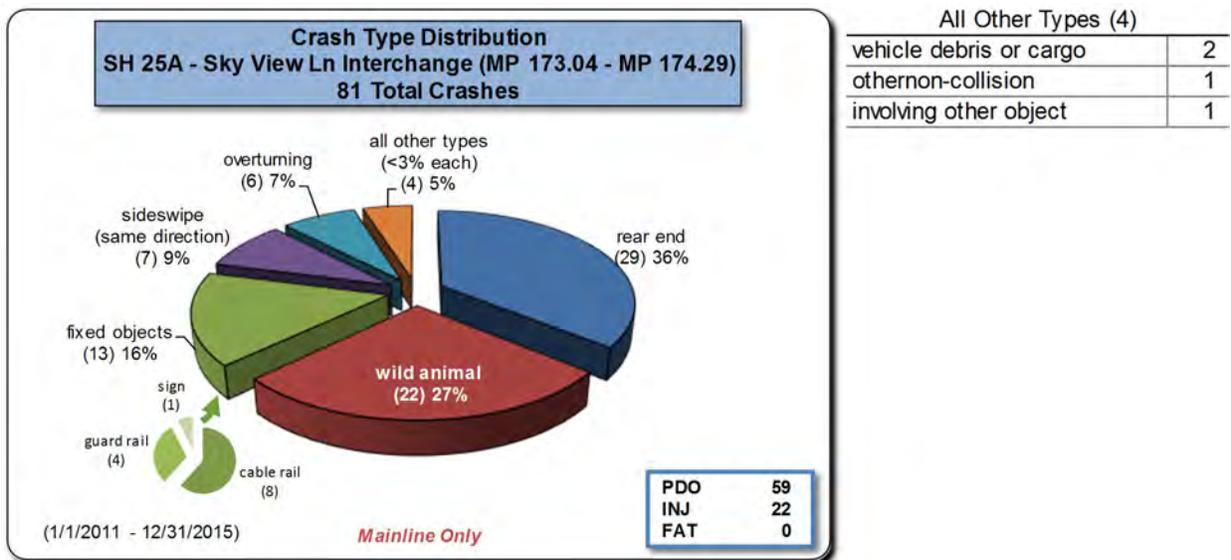


- This segment of I-25 is a 4-lane rural freeway with a 40-ft wide depressed median, 12-ft wide travel lanes, 4-ft wide inside shoulders and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 18 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (36%), followed by *wild animal* crashes (27%) and *fixed object* crashes (16%).

Figure 18
Segment 9 Crash Distribution



Rear-End Crashes

There was a high concentration of rear-end crashes in the northbound direction near MP 174.20, approximately 0.20 miles south of the northbound off-ramp to Sky View Lane (11 of 29). Approximately a third of these occurred during dark-unlighted conditions (4 of 11). Roadway condition was not a contributing factor to the *rear-end* crashes. The Region should consider increasing the northbound off-ramp deceleration lane to reduce the likelihood of decelerating motorists in the through lanes of traffic. Consideration should be made to add lighting for the interchange to illuminate the lane lines and off-ramp. Additionally, consider restriping with high visibility lane markings.

Wild Animal Crashes

There was a higher proportion of run-off-road type crashes (22 of 81) in this segment. Most crashes occurred during dark-unlighted conditions (14 of 22). The region should consider focusing wild animal mitigation efforts, including but not limited to, fencing, over/under-pass, to this segment due to the high occurrence of wild animal crashes at 3.5 per mile per year.

Segment 10 - Sky View Ln to MP 179.00 (MP 174.30 - MP 178.99)

There were 336 crashes during the five-year study period along this segment of I-25, of which 107 resulted in injury, and two (2) resulted in fatalities. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

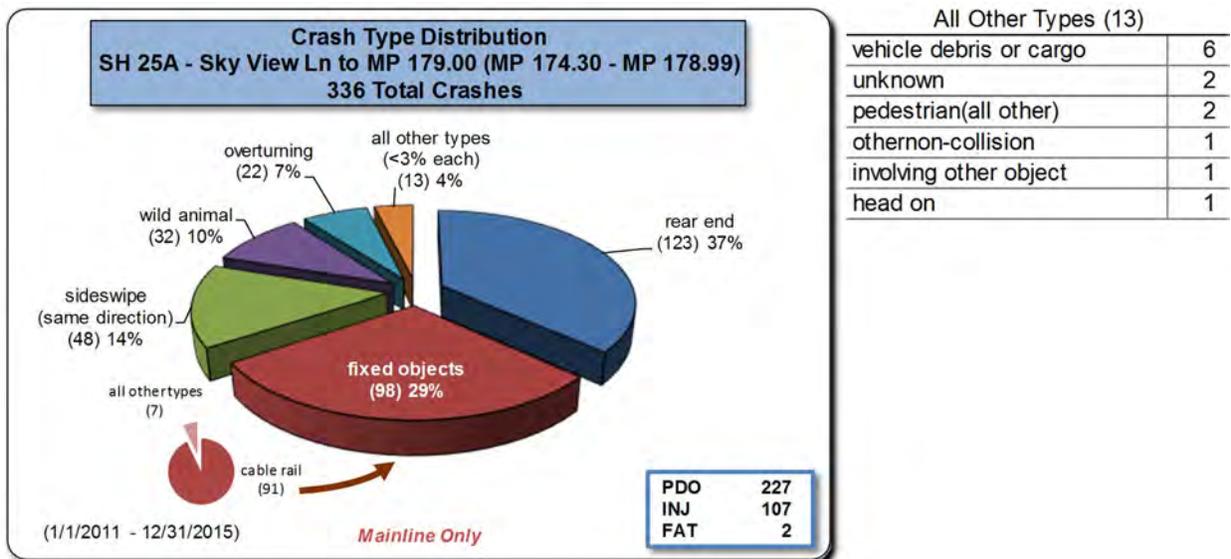


- This segment of I-25 is a 4-lane rural freeway with 12-ft wide travel lanes, 4-ft wide inside shoulders, 10-ft wide outside shoulders, and cable rail in the depressed median and along the back edge of the outside shoulders.
- From MP 174.30 to MP 175.00, the median is 40-ft wide and depressed.
- From MP 175.00 to MP 179.00, the median is 52-ft wide and depressed.

Crash History

Figure 19 provides a graphical representation of the mainline crash types for this location. Rear-end crashes were predominant (37%), followed by fixed object crashes (29%) and sideswipe (same direction) crashes (14%).

Figure 19
Segment 10 Crash Distribution



Fatal Crashes

There were a two (2) crashes during the five-year study period which resulted in a fatality along this segment of I-25.

The first fatal crash was an *overturning* crash occurring on dry roadway conditions in June 2013. A northbound passenger vehicle experienced a catastrophic tire failure. The driver lost control of the vehicle which began rotating clockwise before overturning approximately three times. An unrestrained passenger was ejected and expired on scene. The driver and a second passenger experienced injuries as a result of the crash. Due to the cause of the crash, mechanical failure, and the limited occurrence, no specific recommendations for improvements are made at this time.

The second fatal crash was a *pedestrian* type crash occurring on dry roadway conditions in November 2015. A uniformed Colorado State Patrol trooper was on foot directing traffic from a previous crash. The at-fault vehicle failed to merge and/or give way to the trooper. The trooper was struck by the vehicle and expired on scene. It was later determined the driver of the at-fault vehicle was under the influence of alcohol (DUI). Though no recommendations are made at this time, it should be noted that there was a similar Colorado State Patrol fatality in this vicinity on northbound I-25 in November 2016.

Run-off-Road Crashes

There was high proportion of run-off-road type crashes (119 of 336); including a higher than expected frequency of *cable rail* crashes (91 of 119). Approximately 25-percent of the *cable rail* crashes were concentrated between MP 178.40 and MP 179.00 (20 of 91). Wet roadway conditions were contributing factors in half of those crashes (10 of 20). Dark-unlighted roadway conditions were contributing factors in approximately half of the *cable rail* crashes as well (11 of 20). However, there was no correlation between wet and dark-unlighted roadway conditions. The Region should consider a review of the drainage profile along this segment in order to ensure proper drainage and inlet capacity. Additionally, consider restriping with high visibility lane markings. It should be noted that the increase in crashes from 2014-2015 may be related to construction congestion near Spruce Mountain Road.

Segment 11 – MP 179.00 to Plum Creek Pkwy (MP 179.00 – 180.87)

There were 128 crashes during the five-year study period along this segment of I-25, of which 36 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

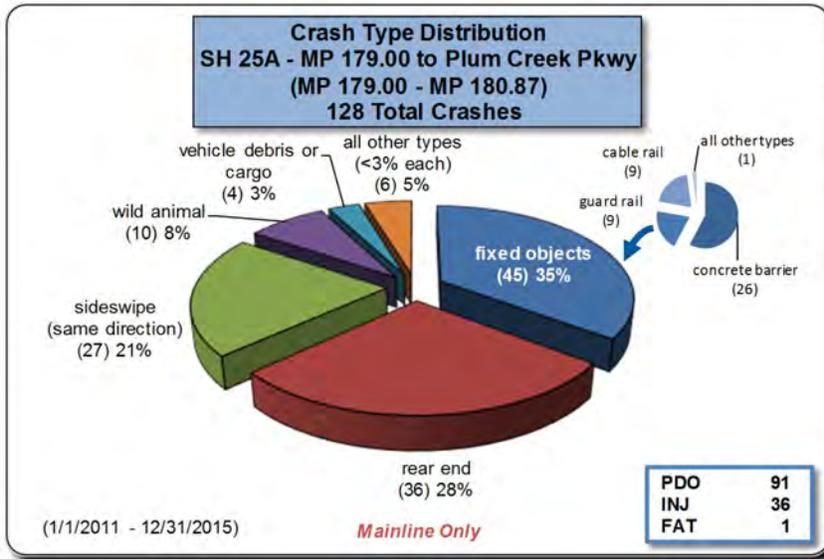
- This segment of I-25 is a 6-lane urban freeway with a 10-ft wide concrete median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.



Crash History

Figure 20 provides a graphical representation of the mainline crash types for this location. *Fixed object* crashes were predominant (35%), followed by *rear-end* crashes (28%) and *sideswipe (same direction)* crashes (21%).

Figure 20
Segment 11 Crash Distribution



All Other Types (6)	
parked motor vehicle	2
road maintenance equipment	1
pedestrian(all other)	1
other non-collision	1
approach turn	1

Fatal Crash

There was a one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The *pedestrian* crash occurred in dark-unlighted conditions on dry roads in July 2012. A pedestrian was attempting to cross the northbound lanes of I-25 and was struck by several vehicles. The pedestrian expired on scene. This was the only instance of a *pedestrian* related crash in the segment, no specific recommendations are made at this time.

Run-off-Road Crashes

There was high proportion of run-off-road type crashes (44 of 128); including a higher than expected frequency of *concrete barrier* crashes (25 of 44). Snowy road (9 of 25) and wet road (5 of 25) were the predominate factors. Most crashes were off-road-left (20 of 25). Consideration should be given to using the existing variable message signs at MP 178.80 to warn drivers of deteriorating roadway conditions. The Region should consider a review of the drainage profile along this segment (especially the northbound and southbound left most lane) in order to ensure proper drainage and inlet capacity. Additionally, consideration should be given to pretreat the road surface to prevent icy/snow accumulation before wintery conditions.

Segment 12 – Plum Creek Pkwy to Wolfensberger Rd (MP 180.88 to 181.84)

There were 75 crashes during the five-year study period along this segment of I-25, of which 28 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

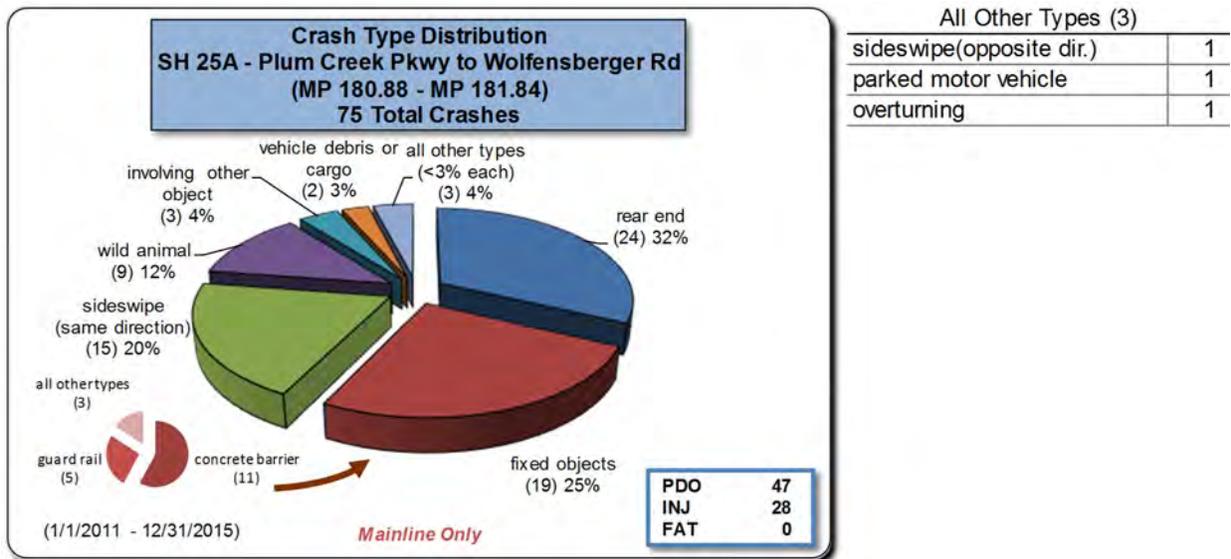


- This segment of I-25 is a 6-lane urban freeway with a 10-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 21 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (32%), followed by *fixed object* crashes (25%) and *sideswipe (same direction)* crashes (20%).

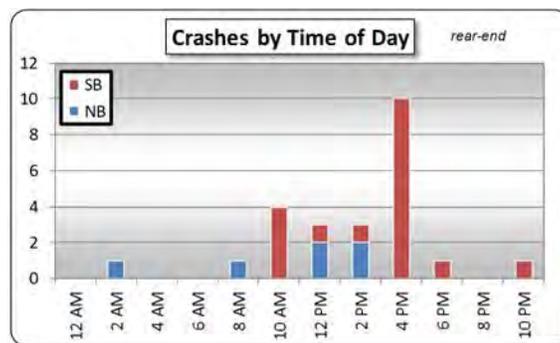
Figure 21
Segment 12 Crash Distribution



Rear-End Crashes

There was high proportion of *rear-end* crashes (24 of 75); including a concentration of southbound crashes near MP 181.00 (14 of 24). Many of these *rear-end* crashes (10 of 24) occurred due to stopped or slowing traffic through the midday time period (4:00 p.m.). **Figure 22** shows the *rear-end* crashes by time of day through this segment.

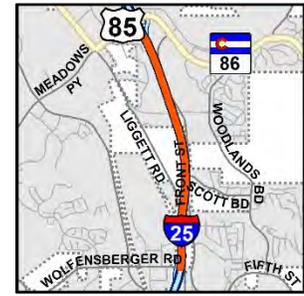
Figure 22
Rear-End Crashes by Time of Day



Measures taken to reduce congestion along the corridor should help reduce the frequency of *rear-end* collisions in this area.

Segment 13 – Wolfensberger Rd to Founders Pkwy (MP 181.85 – MP 184.20)

There were 189 crashes during the five-year study period along this segment of I-25, of which 53 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

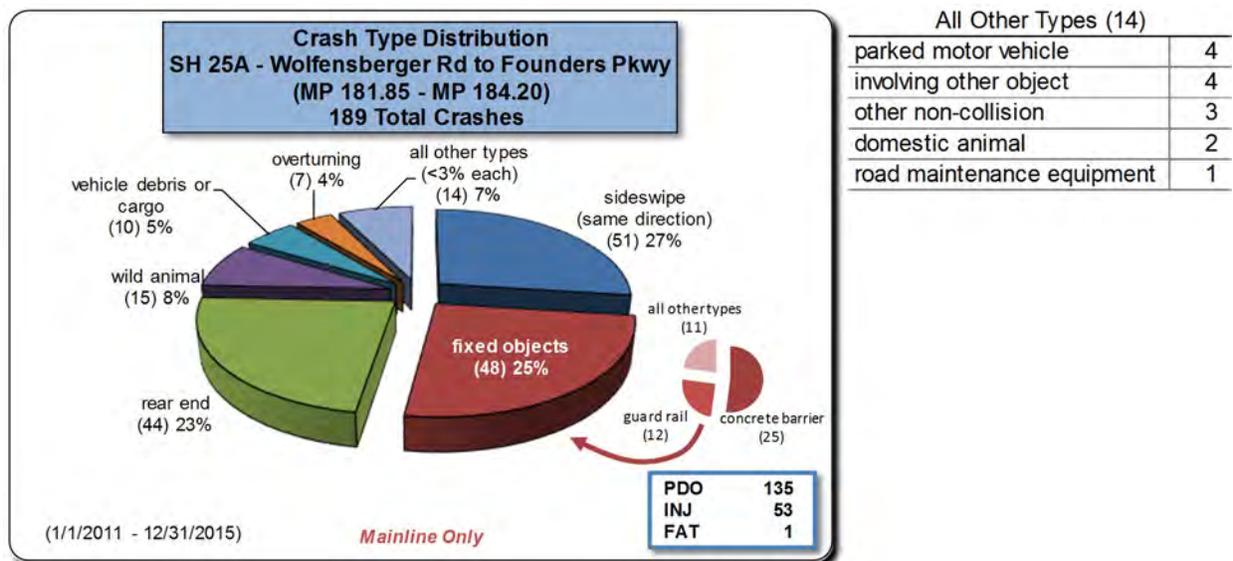


- This segment of I-25 is a 6-lane urban freeway with a 20-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 4-ft wide inside shoulders, and 5-ft to 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound upgrade slope

Crash History

Figure 23 provides a graphical representation of the mainline crash types for this location. *Sideswipe (same direction)* crashes were predominant (27%), followed by *fixed object* crashes (25%) and *rear-end* crashes (23%).

Figure 23
Segment 13 Crash Distribution



Fatal Crash

There was a one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The *pedestrian* crash occurred in dark-unlighted conditions on dry roads in February 2012. A pedestrian was outside a parked vehicle and was struck by a northbound traveling vehicle. It was later determined that the driver of the vehicle at-fault was under the influence of alcohol (DUI). The pedestrian expired on scene. This was the only instance of a *pedestrian* related crash in the segment, no specific recommendations are made at this time.

Concrete Barrier Crashes

There was high proportion of *concrete barrier* crashes (25 of 189) in this segment. Most crashes occurred due to wintry (7 of 25) or wet (9 of 25) roadway conditions. Most of these crashes were single-vehicle crashes originating from the left through lane traveling near the posted speed limit. This suggests that the vehicles were traveling at excessive speeds given the conditions at the time. The general recommendation for variable speed limits during inclement weather may help reduce the frequency of collisions observed in this segment. Additionally, a review of the drainage profile and culvert conditions along this segment should be considered (especially the inside northbound lane in the area of the Wolfensberger Road interchange) in order to ensure proper drainage and inlet capacity.

Segment 14 – Founders Pkwy to Happy Canyon Rd (MP 184.21 to MP 186.93)

There were 260 crashes during the five-year study period along this segment of I-25, of which 77 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

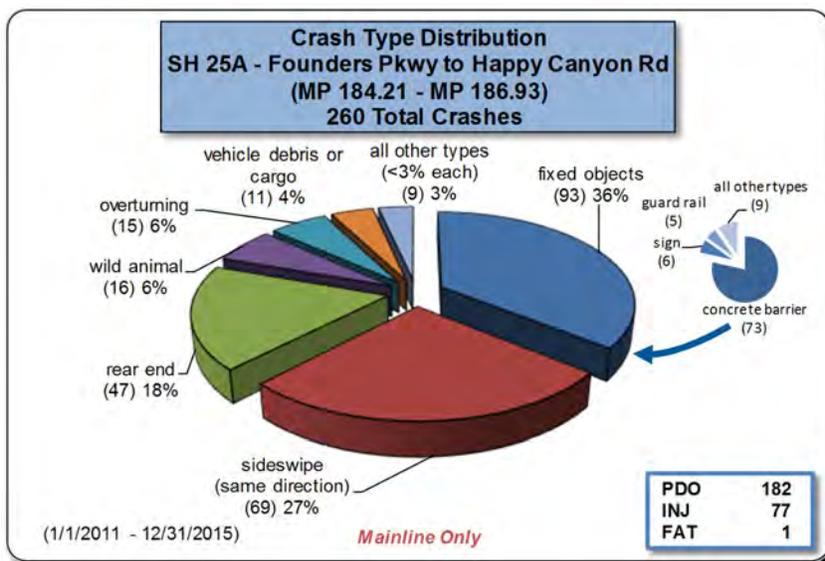


- This segment of I-25 is an 8-lane urban freeway with a 10-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 4-ft inside shoulders and 6-ft to 12-ft wide outside shoulders.
- The terrain is rolling with a general northbound upgrade slope.

Crash History

Figure 24 provides a graphical representation of the mainline crash types for this location. *Fixed object* crashes were predominant (36%), followed by *sideswipe (same direction)* crashes (27%) and *rear-end* crashes (18%).

Figure 24
Segment 14 Crash Distribution



All Other Types (9)	
involving other object	5
parked motor vehicle	2
road maintenance equipment	1
large rocks/boulder	1

Fatal Crash

There was one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The concrete barrier crash occurred in dark-unlighted conditions on dry roads in May 2014. A southbound passenger vehicle exited a right-hand curve, losing control for an unknown reason. The vehicle struck the inside concrete barrier before crossing all southbound lanes and striking the outside concrete barrier. Both driver and passenger suffered injuries as a result of the crash. The driver expired due to injuries at a local hospital. Specific recommendations can be found in the *Concrete Barrier Crash* section.

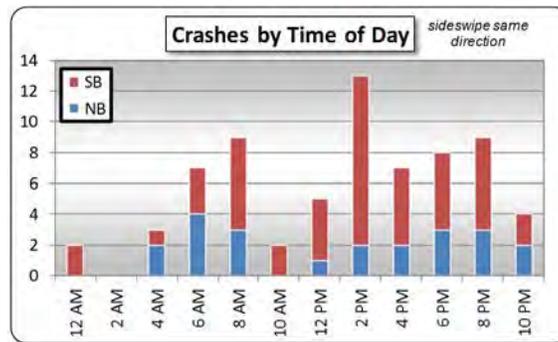
Concrete Barrier Crashes

There was high proportion of *concrete barrier* crashes (73 of 260) in this segment. Concentrations of southbound *concrete barrier* crashes occurred at the right-hand curve south of the Happy Canyon Road interchange (MP 186.00 to MP 186.50). In addition to crashes occurring due to wintry road conditions, a pattern of crashes during dark-unlighted conditions occurred involving drivers traveling off-left from the outside lane, this includes the fatal crash. The Region should consider extending the lighting south of the Happy Canyon Road interchange, as the horizontal curve is located near the transition between lighted and unlighted conditions. In addition, targeted improvements to curve delineation (high visibility striping and barrier reflections) should be considered in this area.

Sideswipe (Same Direction) Crashes

There was a pattern of *sideswipe (same direction)* crashes (69 of 260), along with various other crash types, that occurred between 2:00 p.m. and 4:00 p.m. with a significant proportion of crashing involving wintry (18 of 69) or wet (6 of 69) roadway conditions. **Figure 25** shows the *sideswipe (same direction)* crashes by time of day.

Figure 25
Sideswipe (Same Direction) by Time of Day



The Region should consider adjusting the road maintenance schedules during adverse weather conditions (if necessary) to service this segment of roadway prior to the observed crash patterns (prior to the PM peak period).

Segment 15 – Happy Canyon Rd to Castle Pines Pkwy (MP 186.94 – MP 188.48)

There were 178 crashes during the five-year study period along this segment of I-25, of which 57 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

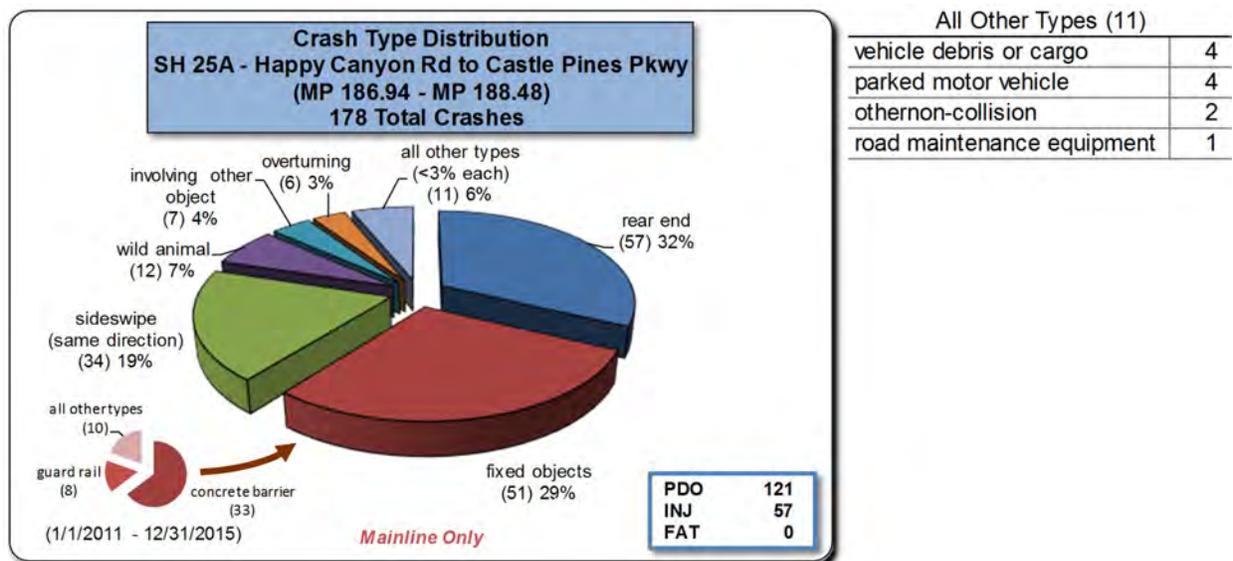


- This segment of I-25 is an 8-lane urban freeway with a 16-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 6-ft to 8-ft wide inside shoulders and 5-ft to 10-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 26 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (32%), followed by *fixed object* crashes (29%) and *sideswipe (same direction)* crashes (19%).

Figure 26
Segment 15 Crash Distribution



Concrete Barrier Crashes

There was a high proportion of run-off-road type crashes in this segment (61 of 178) when compared to similar facilities. Most of the run-off-road crashes involved the concrete barrier off the left-side of the roadway (28 of 33). Wintry road conditions were a contributing factor in many of the concrete barrier crashes (15 of 28). Poor lighting may be another potential contributing factor. There was a concentration of such crashes south of the Castle Pines Parkway interchange, around the crest of the hill in which vehicles attempting a lane change/driving too fast for conditions lost control, on-road, and spun or slid into the median barrier. The general recommendation for variable speed limits during inclement weather may help reduce the frequency of collisions observed in this segment.

Segment 16 – Castle Pines Pkwy to Ridgeway Pkwy (MP 188.49 – MP 192.07)

There were 680 crashes during the five-year study period along this segment of I-25, of which 189 resulted in injury, and three (3) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

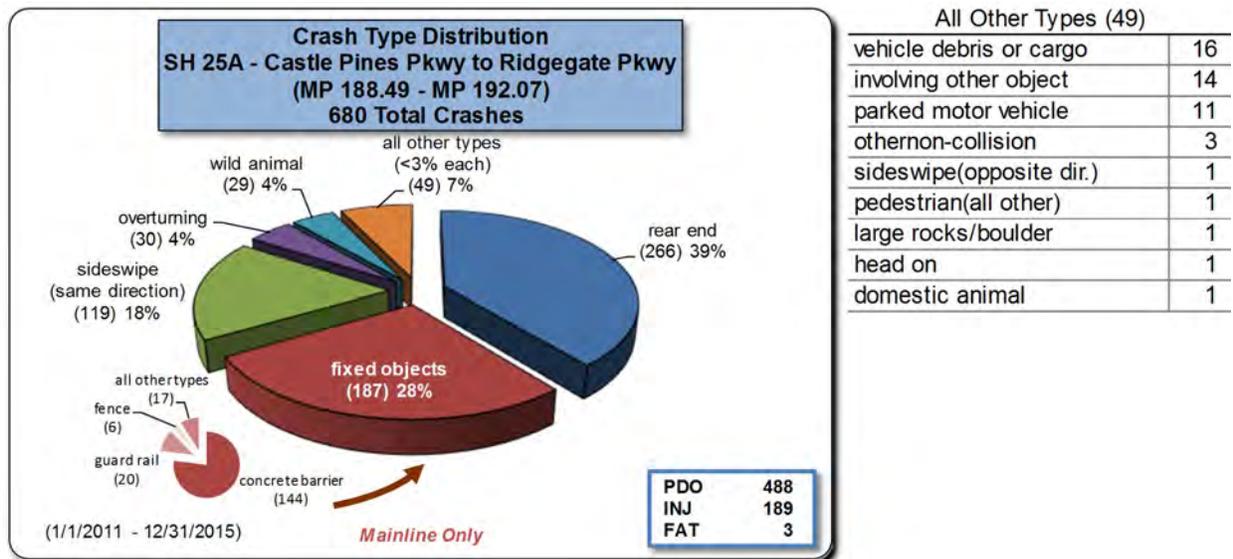


- This segment of I-25 is an 8-lane urban freeway with a 14-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 12-ft wide travel lanes, 6-ft wide inside shoulders and 9-ft to 15-ft wide outside shoulders.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 27 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (39%), followed by *fixed object* crashes (28%) and *sideswipe (same direction)* crashes (18%).

Figure 27
Segment 16 Crash Distribution



Fatal Crashes

There were three (3) crashes during the five-year study period which resulted in a fatality along this segment of I-25.

The first fatal crash was a *sideswipe (same direction)* crash occurring on wet, dark-unlighted roadway conditions in March 2011. A northbound passenger vehicle was attempting to make a lane change and struck a second vehicle. The at-fault vehicle then overturned multiple times. The at-fault driver expired on scene. The driver of the second vehicle had minor injuries. *Sideswipe (same direction)* crashes are within the expected frequency when compared with similar facilities. The wet roadway crashes exceeded the expected frequency when compared with similar facilities. Wet roadway

crashes accounted for approximately 15-percent of total crashes and will be discussed in detail in the *Wet Roadway Crashes* section.

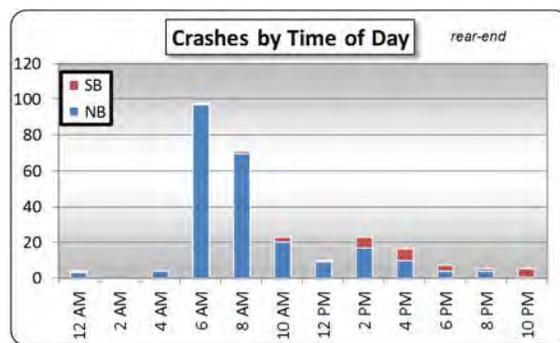
The second fatal crash was a *sideswipe (same direction)* crash occurring on dry, dark-unlighted roadway conditions in April 2011. A truck with flatbed trailer was parked partially blocking the southbound left lane and inside shoulder due to a mechanical failure. A southbound passenger vehicle sideswiped the trailer, removing the driver side door and ejecting the driver. The driver was then stuck by a second southbound passenger vehicle and found expired on scene. *Sideswipe (same direction)* crashes are within the expected frequency when compared with similar facilities. No recommendations for improvements can be made at this time.

The third fatal crash was an *overturning* crash occurring on dry, dark-unlighted roadway conditions in February 2013. A northbound passenger vehicle was traveling at a high rate of speed and attempted to make a lane change, overcorrecting for a slower traveling vehicle. The at-fault vehicle began rotating clockwise before overturning approximately three times. The driver was ejected from the vehicle and found expired on scene. The primary cause of the crash is speed related (traveling in excess of the posted speed limit), as such there are no recommendations for improvements at this time.

Rear-End Crashes

While the *rear-end* crash frequency (266 of 680) is within the expected frequency when compared with similar facilities, it is worth noting that 90-percent (239 of 266) of the *rear-end* crashes occurred in the northbound direction, concentrated in the AM peak period (167 of 239). **Figure 28** shows *rear-end* crashes by time of day. At this time, no specific recommendations are suggested.

Figure 28
Rear-End Crashes by Time of Day



Wet Roadway Crashes

There was a high proportion of wet roadway crashes (108 of 680, 16%) noted at two locations, one was northbound near MP 189.00 through the left-hand curve, the second was southbound near MP 190.50 through the left-hand curve. These locations correlated with concentrations of several crash types, including *concrete barrier* and other run-off-road type crashes. Vehicles in the left-most lane tended to lose control on-road and travel off-left to collide with the median barrier or into other vehicles on the roadway. The Region should consider a review of the drainage profile, super-elevation, and culvert conditions to ensure proper drainage and inlet capacity and help reduce the frequency of run-off-road crashes.

Segment 17 – Ridgeway Pkwy to Lincoln Ave (MP 192.08 – MP 192.98)

There were 293 crashes during the five-year study period along this segment of I-25, of which 44 resulted in injury, and none resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

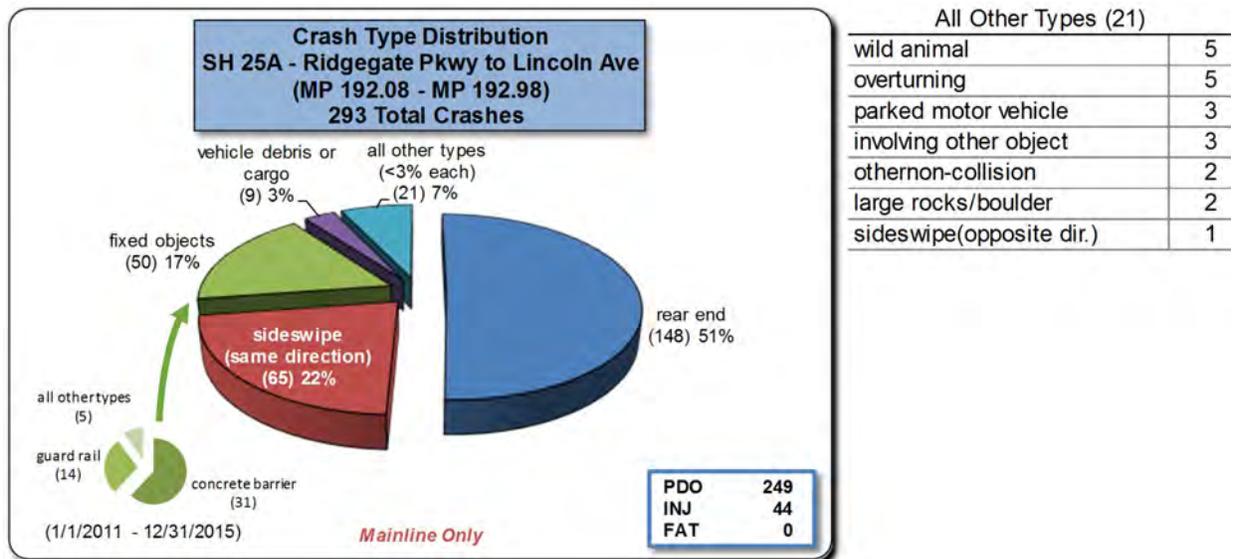


- This segment of I-25 is an 8-lane urban freeway with a 14-ft wide median with a concrete barrier separating the northbound and southbound inside shoulders, 6-ft wide inside shoulders, 5-ft to 10-ft wide outside shoulders and 12-ft wide travel lanes.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 29 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (51%), followed by *sideswipe (same direction)* crashes (22%) and *fixed object* crashes (17%).

Figure 29
Segment 17 Crash Distribution



Sideswipe (Same Direction) Crashes

Other than crashes that are typically associated with congested roadway conditions, casual factors were difficult to isolate due to the changing traffic conditions at the Ridgeway Parkway interchange and construction activity near Lincoln Avenue.

Segment 18 – Lincoln Ave to C-470/E-470 (MP 192.99 – MP 194.31)

There were 523 crashes during the five-year study period along this segment of I-25, of which 69 resulted in injury, and one (1) resulted in a fatality. The following observations specific to this segment were made based on a review of aerial photography, the CDOT video log, and field review:

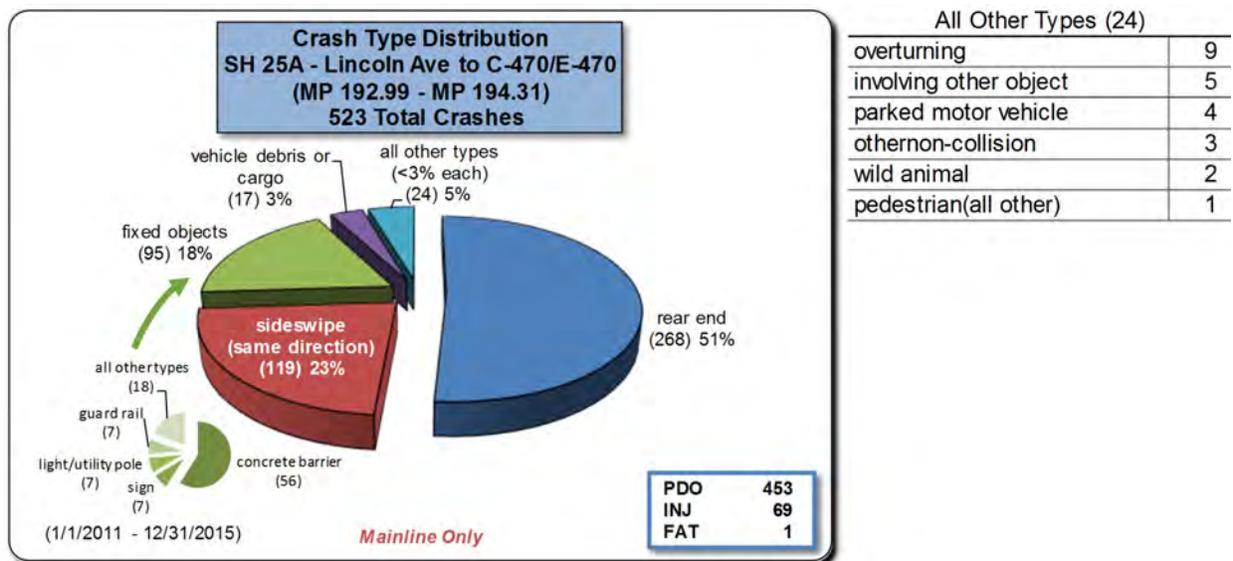


- This segment of I-25 is an 8-lane urban freeway with a 10-ft wide concrete median with 4-ft wide inside shoulders, 10-ft wide outside shoulders and 12-ft wide travel lanes.
- The terrain is rolling with a general northbound downgrade slope.

Crash History

Figure 30 provides a graphical representation of the mainline crash types for this location. *Rear-end* crashes were predominant (51%), followed by *sideswipe (same direction)* crashes (23%) and *fixed object* crashes (18%).

Figure 30
Segment 18 Crash Distribution



Fatal Crash

There was a one (1) crash during the five-year study period which resulted in a fatality along this segment of I-25. The pedestrian crash occurred in dark-unlighted conditions on dry roads in February 2013. A westbound pedestrian attempted to cross the interstate. A northbound passenger vehicle struck the pedestrian. The pedestrian was found expired on scene. Given the nature of the crash and the infrequency of the crash type in this segment, no recommendations or improvements are provided at this time.

Sideswipe (Same Direction) Crash

There was a pattern of *sideswipe (same direction)* type crashes in this segment (119 of 523) concentrated near the Lincoln Avenue interchange. However, there are no recommendations for improvement due to the recent construction efforts along this segment that are expected to alter the traffic patterns in the vicinity.

Ramp / Crossroad Crashes

Crashes that were attributed to ramps and crossroads (including ramp terminal intersections) accounted for 19% of the total crashes (1052 of 5537). **Table 4** lists the location, configuration and number of crashes for each interchange along the study section.

Table 4:
Ramp/Crossroad Related Crashes, by Location

MP	Interchange	Configuration	Number of Crashes			
			PDO*	Injury	Fatal	Total
158.2	Exit 158 – Baptist Road	Diamond	31	8	0	39
160.76	Exit 161 – SH 105A	Partial Cloverleaf	117	46	2	165
163.32	Exit 163 – County Line Road	Diamond	5	5	0	10
167.46	Exit 167 – Greenland Road	Diamond	4	1	0	5
171.82	Exit 172 – Upper Lake Gulch Road	Diamond	3	1	0	4
172.31	Exit 173 – Spruce Mountain Road	Partial Y	1	0	0	1
173.79	Exit 174 – Sky View Lane	Diamond	3	2	0	5
180.88	Exit 181 – Plum Creek Parkway	Diamond	38	13	0	51
181.85	Exit 182 – Wolfensberger Road	Diamond	56	10	0	66
184.21	Exit 184 – SH 85B / SH 86B	Partial Cloverleaf	216	42	0	258
186.94	Exit 187 – Happy Canyon Road	Diamond	5	1	0	6
188.49	Exit 188 – Castle Pines Parkway	Partial Cloverleaf	24	9	0	33
192.08	Exit 192 – Ridgegate Parkway	Partial Cloverleaf	68	8	0	76
192.99	Exit 193 – Lincoln Avenue	Partial Cloverleaf	234	29	0	263
194.31	Exit 194 – C-470 / E-470 (South Half)	Directional	55	15	0	70
Total			860	190	2	1052

*Property Damage Only

Note that there were two (2) crashes that resulted in fatalities, and both crashes occurred at the Exit 161 interchange. Details of the two (2) fatal crashes are described later in the Exit 161 – SH 105A Interchange subsection.

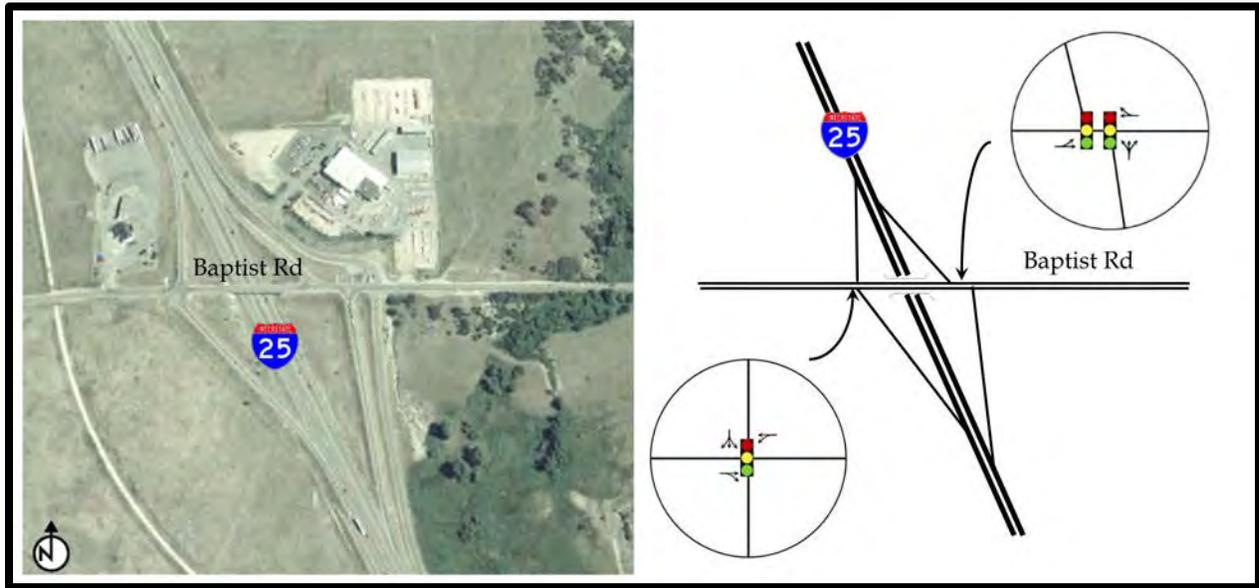
The Baptist Road, Plum Creek Parkway and Lincoln Avenue interchanges have been reconstructed in recent years. Further detailed information is described in the Baptist Road, Plum Creek Parkway and Lincoln Avenue interchange subsections.

Interchange Analysis Discussions

Exit 158 – Baptist Road Interchange (MP 158.20)

There were 39 crashes during the five-year study period at this interchange. The Baptist Road interchange with I-25 is a conventional diamond interchange, formerly with one-lane on-ramps and off-ramps with signalized ramp terminal intersections. The interchange was reconstructed in 2012; which involved replacing the bridge structure and eliminated the skew in the northbound I-25 ramp terminal intersection with Baptist Road. The previous alignment of this interchange is shown on **Figure 31**.

Figure 31:
Exit 158 - Baptist Road (previous configuration)



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad crosses over I-25 at this location. The ramps are on an upgrade, while mainline I-25 remains relatively level.
- The northbound I-25 off-ramp is a one-lane exit which widens to two-lanes after approximately 400 feet; the southbound off-ramp is similar and widens to two-lanes after approximately 700 feet.
- There is a channelized right-turn lane which splits off of the northbound I-25 off-ramp and crossroad prior to the ramp terminal intersection, removing almost all right-turn movements from the signalized intersection.
- The northbound I-25 on-ramp is two-lanes, which narrows to one-lane approximately 760 feet prior to the I-25 merge. The southbound on-ramp is three-lanes, which narrows to two-lanes approximately 940 feet prior to the I-25 merge; and the on-ramp narrows to one-lane at approximately 230 feet prior to the I-25 merge.

The current configuration of the Baptist Road interchange is shown in **Figure 32**.

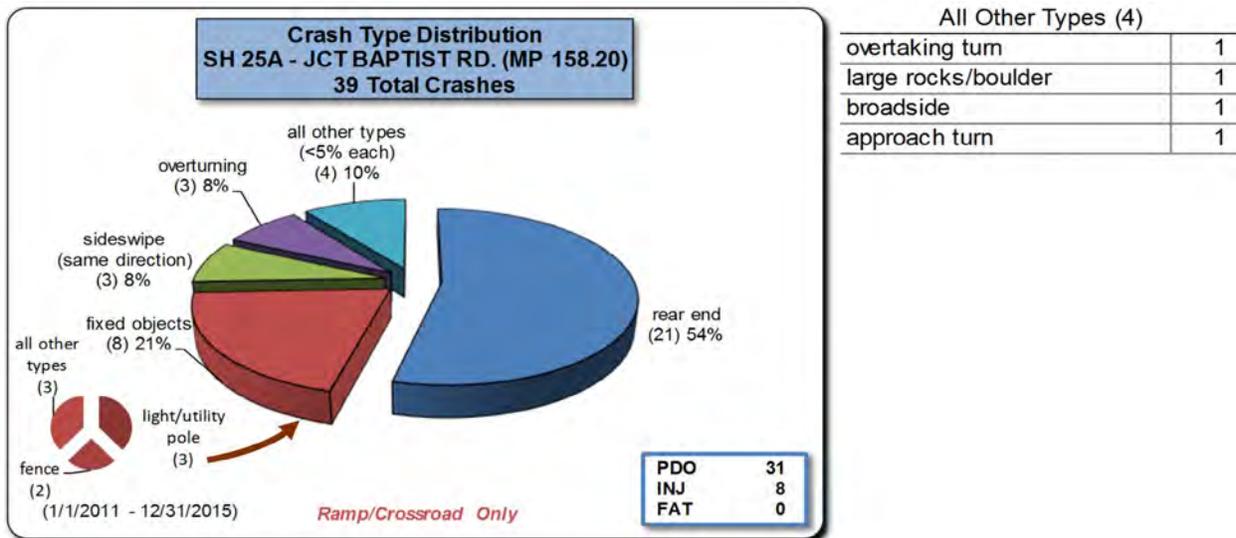
Figure 32:
Exit 158 - Baptist Road (current configuration)



Crash History

Figure 33 provides a graphical representation of crash types for this interchange. *Rear-end* crashes were predominant (54%), followed by crashes involving fixed objects (21%), *sideswipe (same direction)* and *overturning* crashes (8%, each).

Figure 33:
Exit 158 - Baptist Road Crash Distribution

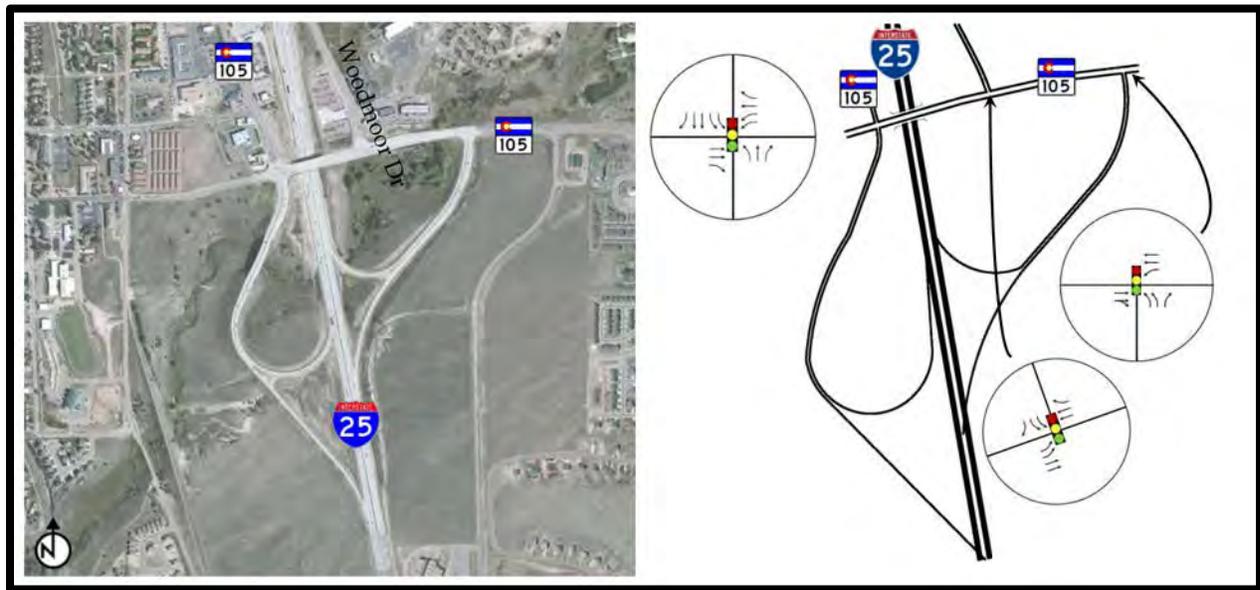


Due to limited amount of post-construction data available at this interchange, no recommendations for improvement are made at this time. This information was reported for potential use in before-after analysis at this interchange.

Exit 161 - SH 105A Interchange (MP 160.76)

There were 165 crashes during the five-year study period at this interchange. The SH 105A interchange with I-25 is a partial cloverleaf, with the on-ramps and off-ramps located on the south side of SH 105A, and signalized ramp terminal intersections. An aerial view of the interchange, along with the associated ramp terminal geometry, is shown in **Figure 34**.

Figure 34:
Exit 161 - SH 105 Interchange



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes over I-25 at this location; the ramps are on an upgrade, while mainline I-25 remains relatively level.
- The northbound I-25 off-ramp is a one-lane exit which widens to two-lanes after approximately 1,280 feet; the southbound off-ramp is similar and widens to two-lanes after approximately 1,760 feet.
- The southbound I-25 on-ramp is a two-lane entrance which narrows to one-lane approximately 3,000 feet prior to the I-25 merge.
- There are two channelized left-turn lanes and one right-turn lane which splits off of the northbound off-ramp prior to the ramp terminal intersection.
- The north and east leg of the southbound ramp/SH105A signalized intersection is SH 105A.
- Between I-25 and the northbound ramp/SH 105 signalized intersection is Woodmoor Drive/SH 105A signalized T-intersection, which has double left-turn lanes on the north and west leg, and a right-turn lane on the north and east leg.

Crash History

Figure 35 provides a graphical representation of crash types for this interchange. *Rear-end* crashes were predominant (49%); followed by crashes involving fixed objects (24%), *overturning* crashes (10%), and *sideswipe (same direction)* crashes (8%). Note that two (2) of the crashes resulted in fatalities.

Figure 35:
Exit 161 - SH 105A Crash Distribution

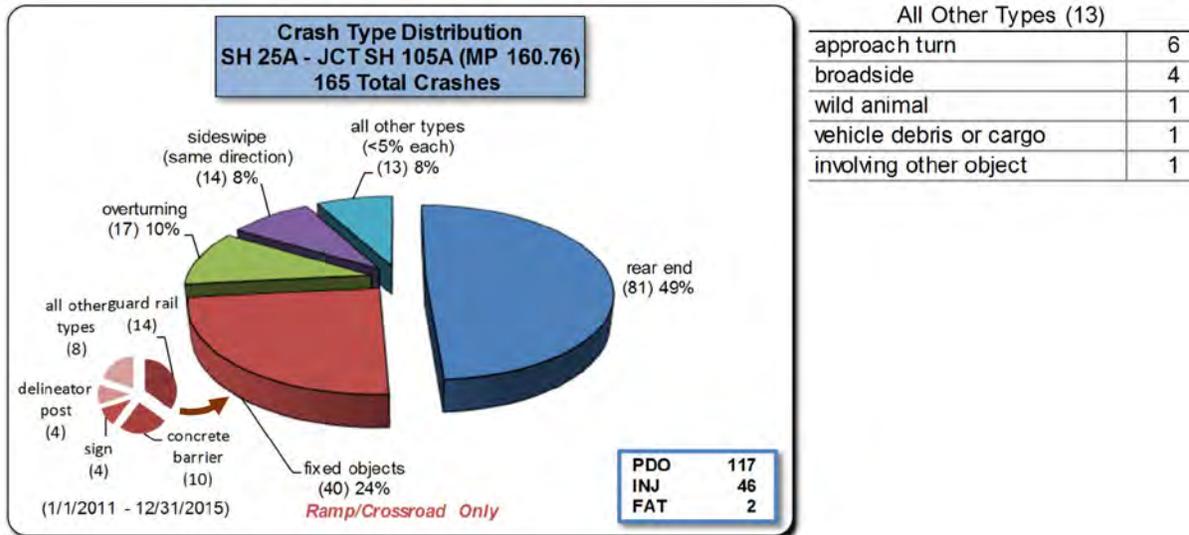
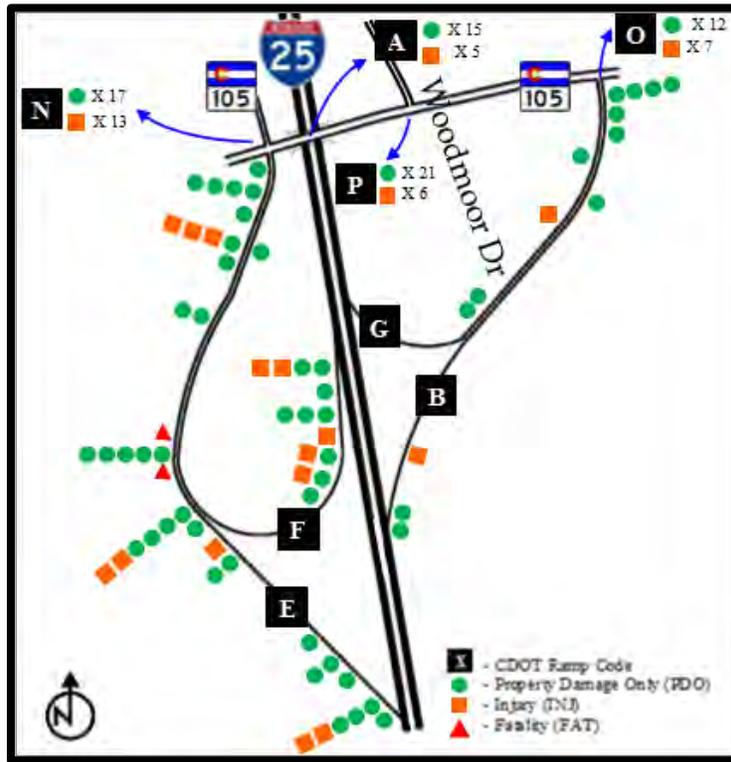


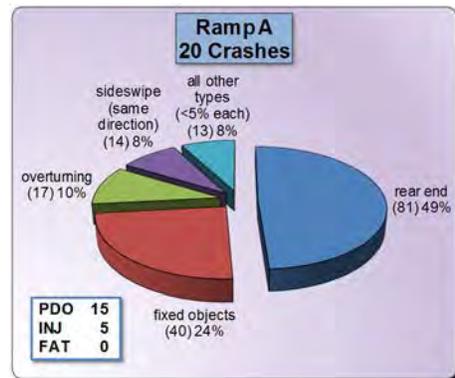
Figure 36 shows the approximate location, number, and severity of the ramp and crossroad crashes for the SH 105A interchange.

Figure 36:
Ramp/Crossroad Crashes



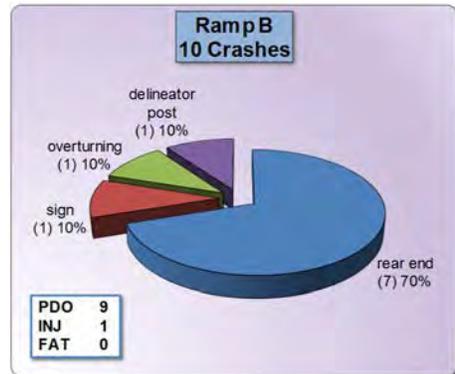
Ramp A (Crossroad)

There were 20 crashes on the crossroad (SH 105A) in the vicinity of the interchange which were not located at either ramp terminal intersection. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *rear-end* type (15 of 20), which were related to traffic congestion from the adjacent traffic signals; where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic. No suggestions for improvement specific to these crashes are recommended at this time.



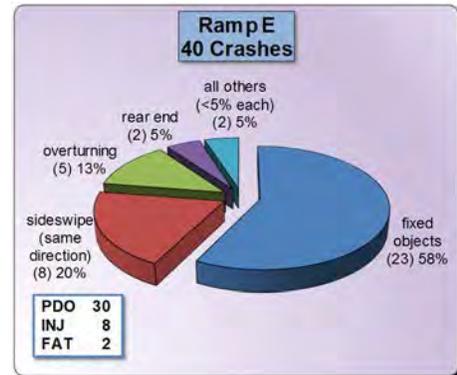
Ramp B

There were 10 crashes on the northbound I-25 off-ramp to SH 105A during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *rear-end* (7 of 10), which were related to traffic congestion from the ramp terminal intersection on SH 105A; where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic. No suggestions for improvement specific to these crashes are recommended at this time.



Ramp E

There were 40 crashes during the five-year study period along the southbound I-25 on-ramp from SH 105A. The breakdown of crashes, by type, is shown to the right. Both of this interchange's fatal crashes occurred on this ramp. Most of the crashes were crashes involving fixed objects (23 of 40); which were *guard rail* (12 of 40), *concrete barrier* (7 of 40), *delineator post* (2 of 40), *fence* and *embankment* (1 of 40, each). *Sideswipe (same direction)* (8 of 40), and *overturning* (5 of 40) were the next most common crash types.



Fatal Collisions

The two (2) fatal crashes were *overturning* crashes occurring between MP 160.49 and MP 160.52, where the ramp is a left-hand horizontal curve. Each crash involved a motorcycle traveling at 55 mph in the left-hand lane (prior to the lane reduction), attempting to pass vehicles in the right-hand lane approaching the left-hand curve. The motorcycles lost control and went off-road-right on dry pavement conditions. Alcohol was a factor in one (1) of the fatal crashes. The Region should inspect the superelevation rate of this left-hand horizontal curve, making sure the superelevation conforms to the AASHTO design criteria. Another suggestion to consider is to post an advance horizontal curve sign with an advisory speed between 35 mph and 45 mph (W1-2L) approximately 700 feet north of the PC of the horizontal curve, to warn motorists to drive at a slower traveling speed to navigate the horizontal curve safely without going off-road to the right side (see **Figure 37**). Also, make sure there is good skid resistance and drainage of the roadway surface.

Figure 37:
Ramp E with Proposed Sign

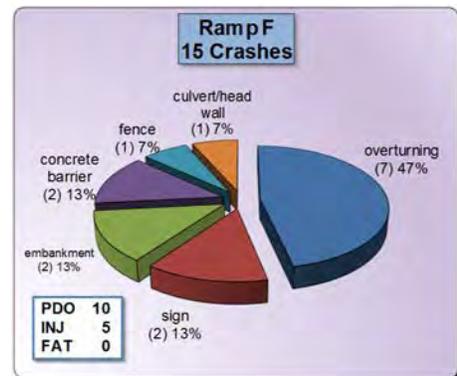


Nine (9) of the 12 *guard rail* crashes occurred off-road right, six (6) of these crashes occurred on snowy or icy road conditions, and eight (8) of these crashes occurred between MP 160.46 and

MP 160.50, where the ramp is a left-hand horizontal curve. The Region should intensify winter maintenance operations to minimize the ice and snow on this ramp.

Ramp F

There were 15 crashes during the five-year study period along the southbound I-25 off-ramp to SH 105A. The breakdown of crashes, by type, is shown to the right. Most of the crashes involved fixed objects (8 of 15), followed by *overturning* (7 of 15).



Fixed Object Collisions

Six (6) of the eight (8) fixed object crashes occurred off-road-left along a right-hand horizontal curve segment. A tired driver was a factor in two (2) of the six (6) crashes, and the vehicle was traveling at 45 mph or greater in four (4) of the eight (8) crashes despite a posted advisory ramp speed limit of 35 mph. The Region may want to consider adding flashing beacons above the advisory speed limit sign to remind motorists to slow down when entering the ramp to navigate the horizontal curve safely, increase law enforcement to reduce ramp traveling speed and check the superelevation of the horizontal curve to make sure it meets AASHTO’s design criteria.

Overturning Collisions

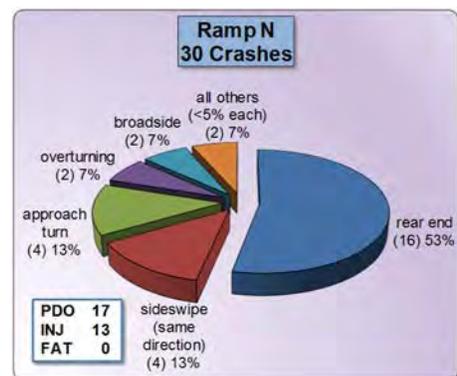
Six (6) of the seven (7) *overturning* crashes also occurred off-road-left along the right-hand horizontal curve segment, where four (4) of the crashes resulted in injuries. Road and lighting conditions were not factors in the *overturning* crashes as all of the crashes occurred on dry pavement, and six (6) of the crashes occurred during daylight. However, five (5) of the crashes involved vehicles traveling at 45 mph or greater. The Region may want to consider adding flashing beacons above the advisory speed limit sign to remind motorists to slow down when entering the ramp to navigate the horizontal curve safely, increase law enforcement to reduce ramp traveling speed, and check the superelevation of the horizontal curve to make sure it meets AASHTO’s design criteria.

Ramp G

There were four (4) crashes during the five-year study period along northbound I-25 on-ramp from SH 105A. Because of the low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were 30 crashes during the five-year study period at the signalized intersection of the southbound I-25 ramps with SH 105A. The breakdown of crashes, by type, is shown to the right. *Rear-end* crashes were predominant (16 of 30, 53%), followed by *sideswipe (same direction)* and *approach turn* (4 of 30, 13%, each).



Rear-End Collisions

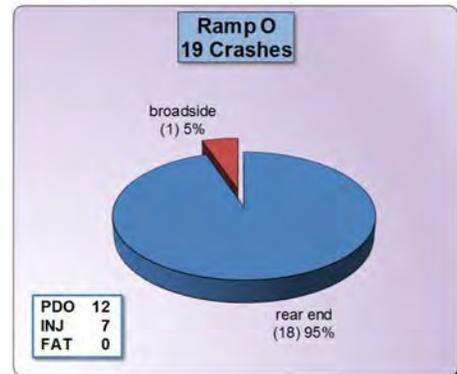
The rate of *rear-end* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved westbound vehicles where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic at this signalized intersection. The Region may want to confirm that all signal heads along the crossroad have 12-inch LED lenses, replacing any incandescent lenses as per current standards, to improve signal visibility which may help reduce the frequency of *rear-end* type crashes. In addition, the Region may want to consider investigating the signal timing and progression as a possible mitigation for *rear-end* crashes.

Approach Turn Collisions

The rate of *approach turn* type crashes was higher than the statewide average rate for similar intersections. The small number of these crashes tends to skew the rate of *approach turn* crashes. The crash listing shows that the collisions involved northbound or eastbound vehicles making a left-turn, striking a vehicle going straight in the opposite direction. However, there were fewer than one per year on any approach; as such no recommendations for improvements are made at this time.

Ramp O (Northbound Ramp Terminal Intersection)

There were 19 crashes during the five-year study period at the signalized intersection of the northbound I-25 ramps with SH 105A. The breakdown of crashes, by type, is shown to the right. *Rear-end* crashes were predominant (18 of 19, 95%), followed by *broadside* (1 of 19, 5%, each).



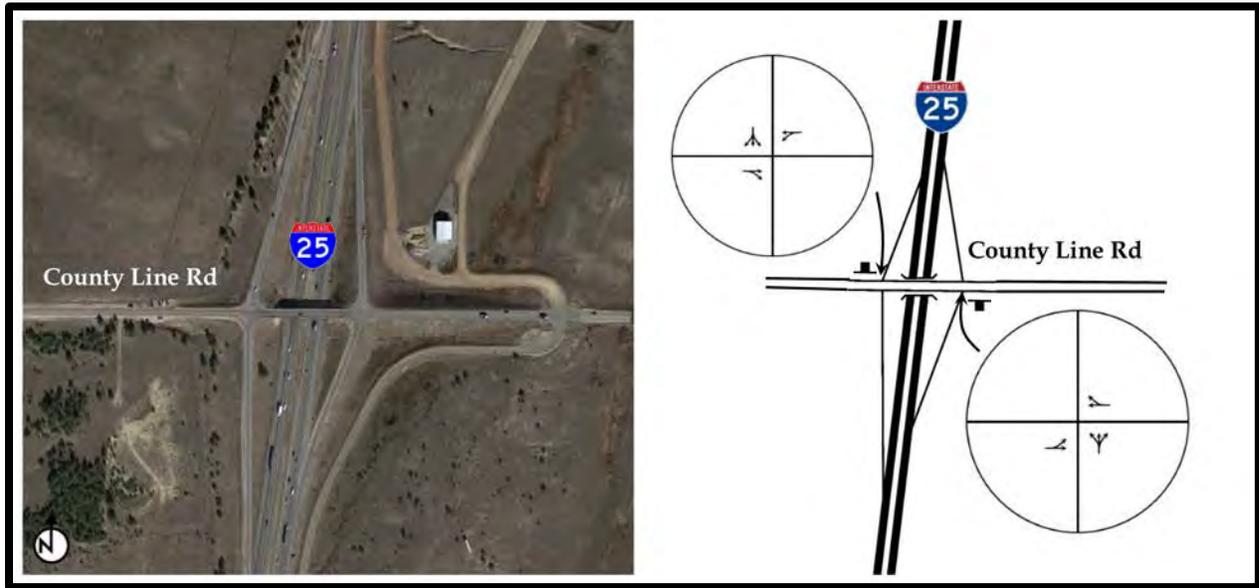
Rear-End Collisions

The rate of *rear-end* type crashes was higher than the statewide average rate for similar facilities. Most of these collisions involved northbound vehicles where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic at this signalized intersection. Lighting and pavement conditions do not appear to be factors as 15 of 18 *rear-end* crashes occurred during daylight, and 14 of 18 occurred on dry pavement conditions. The Region may also want to consider investigating the signal timing and progression as a possible mitigation for *rear-end* crashes.

Exit 163 - County Line Road (MP 163.32)

There were 10 crashes during the five-year study period at this interchange. The County Line Road interchange with I-25 is a conventional diamond interchange with one-lane on-ramps and off-ramps with unsignalized ramp terminal intersections. To the east of I-25, the crossroad is called Palmer Divide Avenue, and the crossroad to the west is called County Line Road. An aerial view of the interchange, along with the associated ramp terminal lane geometry, is shown on **Figure 38**.

Figure 38:
Exit 163 - County Line Road Interchange



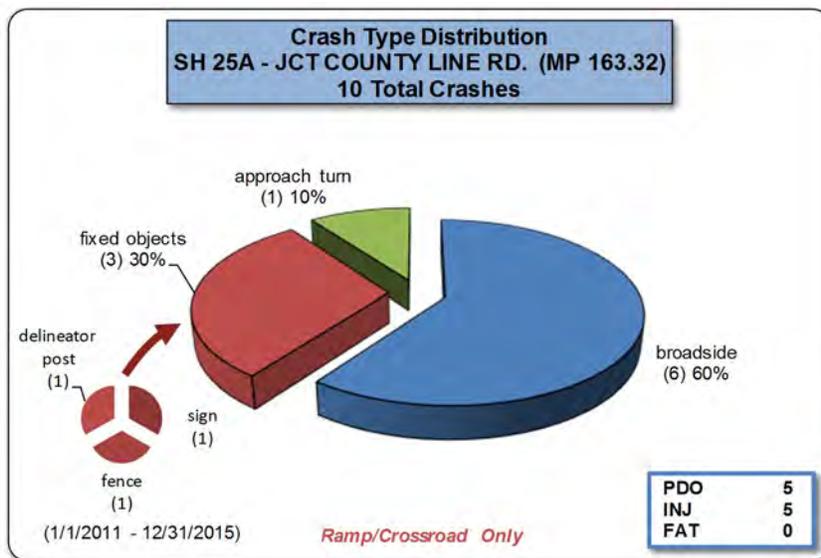
The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes over I-25 at this location; the ramps are on an upgrade, while mainline I-25 remains relatively level.
- The ramp terminal intersections are stop-controlled (unsignalized).

Crash History

Figure 39 provides a graphical representation of crash types for this interchange. *Broadside* crashes were predominant (60%); followed by crashes involving fixed objects (30%), and *approach turn* crashes (10%).

Figure 39:
Exit 163 - County Line Road Crash Distribution



Ramp B

There were two (2) crashes during the five-year study period along the northbound I-25 on-ramp from County Line Road. Because of the low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were only three (3) crashes during the five-year study period at the unsignalized intersection of the southbound I-25 ramps with County Line Road. Because of the very low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp O (Northbound Ramp Terminal Intersection)

There were only four (4) crashes during the five-year study period at the unsignalized intersection of the northbound I-25 ramps with County Line Road. Three (3) of the crashes were *broadside*, and one (1) was *approach-turn*. Distracted driving was a factor in two (2) of the three (3) *broadside* crashes. Because of the very low number of *broadside* crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp R (Frontage Road)

There was only one (1) crash during the five-year study period; no correctable crash pattern was identified.

Exit 167 - Greenland Road (MP 167.46)

There were only five (5) crashes during the five-year study period at this interchange. There were two (2) *sign* crashes; and one (1) each of *overturning*, *rear-end*, and *broadside* type crashes. Because of the small number of various crash types; no suggestions for improvements specific to this interchange are recommended at this time.

Exit 172 – Upper Lake Gulch Road (MP 171.82)

There were only four (4) crashes during the five-year study period at this interchange; two (2) were *overturning* crashes, and two (2) were crashes involving fixed objects (*delineator post* and *light/utility pole*). Because of the very small number of crashes, there were no correctable pattern to examine; therefore, no suggestions for improvements specific to this interchange are recommended at this time.

Exit 173 – Spruce Mountain Road (MP 172.31)

There was only one (1) crash during the five-year study period; no correctable crash pattern was identified.

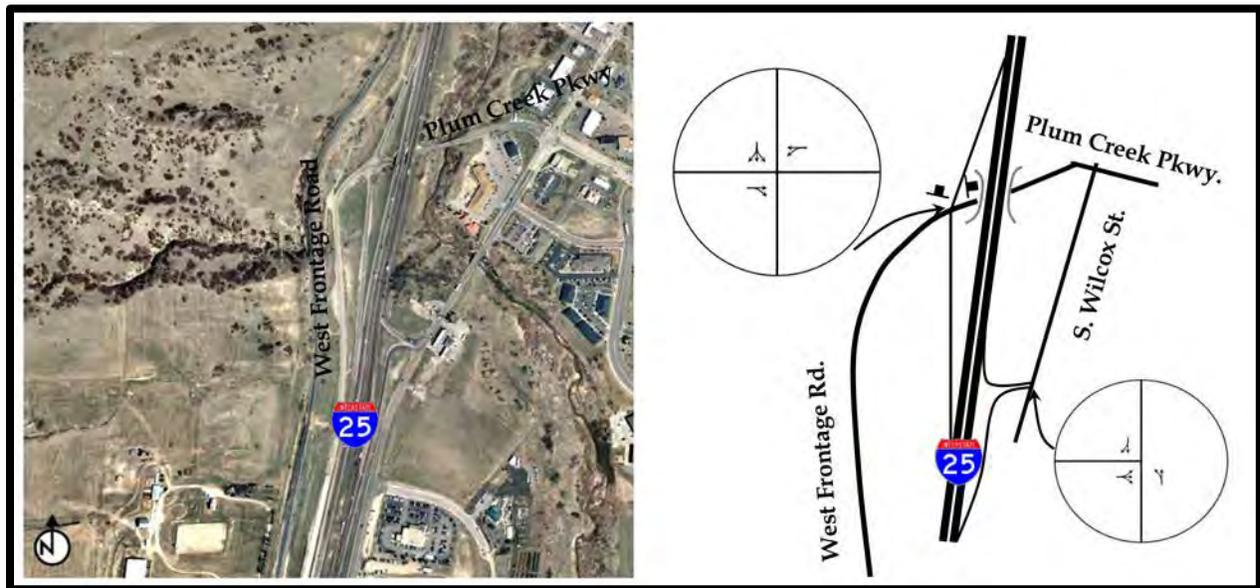
Exit 174 – Sky View Lane (MP 173.79)

There were only five (5) crashes during the five-year study period at this interchange; two (2) were crashes involving fixed objects (*sign* and *cable rail*), one (1) was *pedestrian*, one (1) was *other non-collision*, and one (1) was *broadside*. Because of the very small number of crashes; no suggestions for improvements specific to this interchange are recommended at this time.

Exit 181 – Plum Creek Parkway (MP 180.88)

There were 51 crashes during the five-year study period at this interchange. The Plum Creek Parkway interchange was reconstructed in 2011. The reconstruction replaced the bridge structure, realigned Plum Creek Parkway and the west frontage road, and converted the interchange to a conventional diamond configuration. The previous alignment of this interchange is shown on **Figure 40**.

Figure 40:
Exit 181 - Plum Creek Parkway (previous configuration)



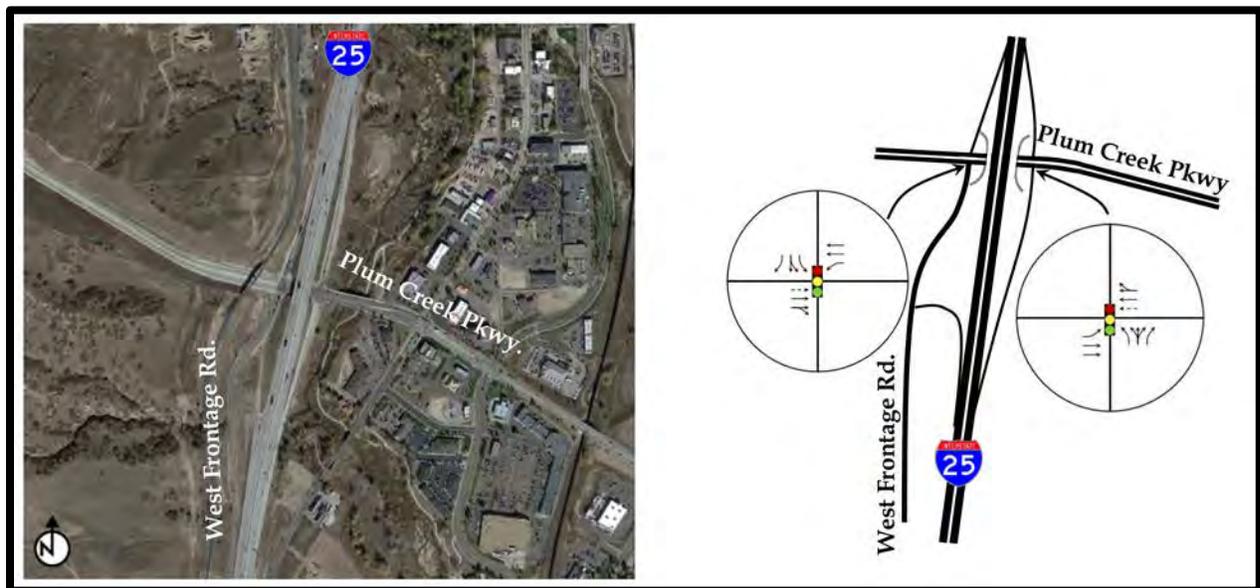
Plum Creek Parkway passes under I-25 at this interchange, and there is a railroad bridge west of the interchange. The former diamond interchange connected the northbound I-25 ramp terminals with south Wilcox Street, south of Plum Creek Parkway. The southbound I-25 ramp connected to Plum

Creek Parkway at a stop-controlled ramp terminal intersection. The following changes were made to the interchange:

- Plum Creek Parkway was realigned to pass perpendicularly under I-25.
- There is a stand-alone northbound I-25 off-ramp that is a one-lane exit that splits into multiple lanes at the approach to the signalized ramp terminal intersection.
- The southbound I-25 off-ramp is a two-lane exit that splits into three lanes at the approach to the signalized ramp terminal intersection.
- The former northbound I-25 on-ramp is eliminated from South Wilcox Street, and it is a two-lane on-ramp intersecting from Plum Creek Parkway and narrows to one-lane approximately 950 feet prior to the I-25 merge.
- West Frontage Road has been realigned to intersect with Plum Creek Parkway and the I-25 southbound off-ramp terminal as a signalized intersection.
- The southbound I-25 on-ramp is two-lanes from the realigned West Frontage Road, and the southbound I-25 on-ramp narrows to one-lane at the tangent segment of the ramp, approximately 600 feet prior to the I-25 merge.

The current configuration of the Plum Creek Parkway interchange is shown on **Figure 41**.

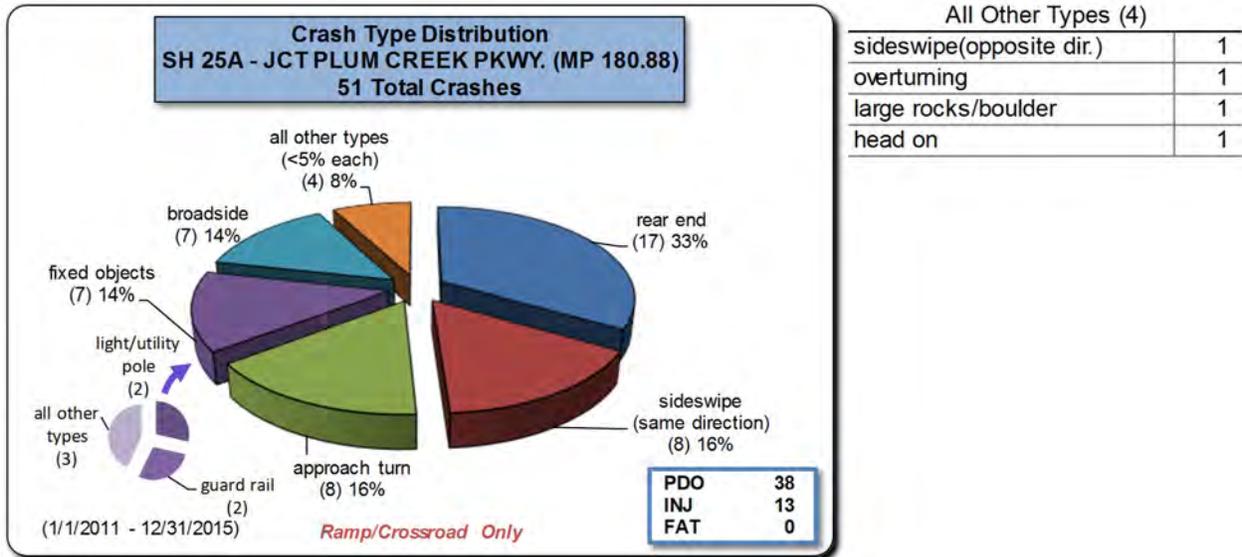
Figure 41:
Exit 181 - Plum Creek Pkwy. (current configuration)



Crash History

Figure 42 provides a graphical representation of crash types for this interchange. *Rear-end* type crashes were predominant (33%), followed by *sideswipe (same direction)* and *approach turn* (16%, each), and crashes involving fixed objects (14%).

Figure 42:
Exit 181 - Plum Creek Parkway Crash Distribution

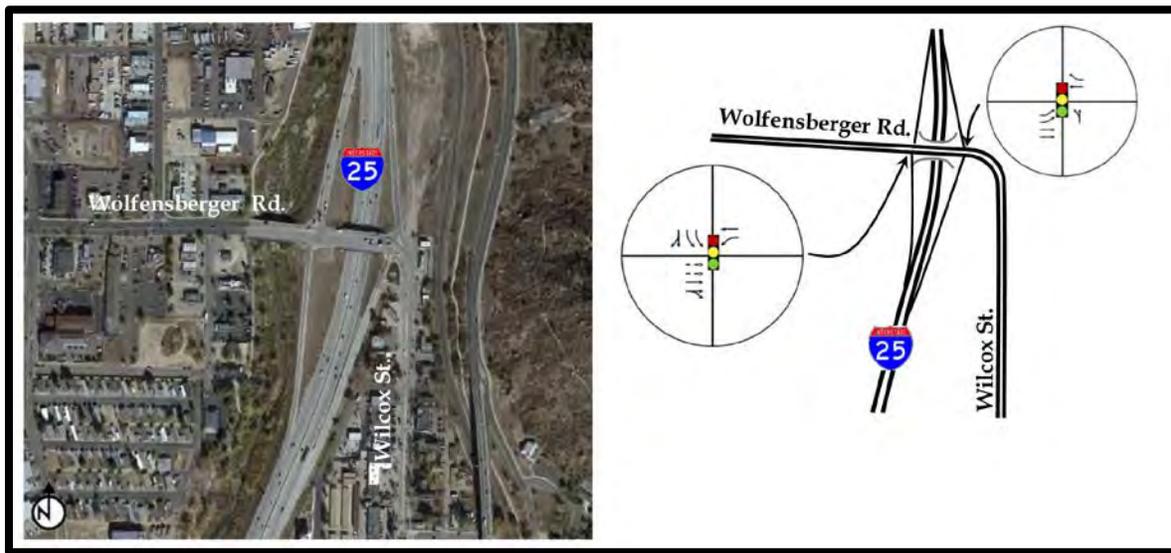


Due to the limited amount of post construction data available at this interchange, no recommendations for improvement are made at this time. This information was reported for potential use in before-after analysis at this interchange.

Exit 182 - Wilcox Street/Wolfensberger Road (MP 181.85)

There were 66 crashes during the five-year study period at this interchange. The Wilcox Street/Wolfensberger Road interchange with I-25 is a conventional diamond interchange, with one-lane on-ramps and two-lane off-ramps with signalized ramp terminal intersections. Wolfensberger Road begins east of the northbound I-25 off-ramp terminal and oriented in the north-south direction. An aerial view of the interchange, along with the associated ramp terminal lane geometry, is shown on **Figure 43**.

Figure 43:
Exit 182 - Wolfensberger Pkwy. Interchange



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes over I-25; the ramps are on an upgrade, while mainline I-25 remains relatively level.
- The channelized right-turn lane splits off of the northbound I-25 off-ramp and Wolfensberger Road, removing almost all right-turn movements from the signalized intersection.
- The southbound I-25 off-ramp widens to three-lanes (two left-turn lanes and one right-turn lane at the approach to the ramp terminal intersection).

Crash History

Figure 44 provides a graphical representation of crash types for this interchange. *Rear-end* type crashes were predominant (29%); followed by *approach turn* (26%), and *broadside* type crashes (24%).

Figure 44:
Exit 182 - Wolfensberger Rd Crash Distribution

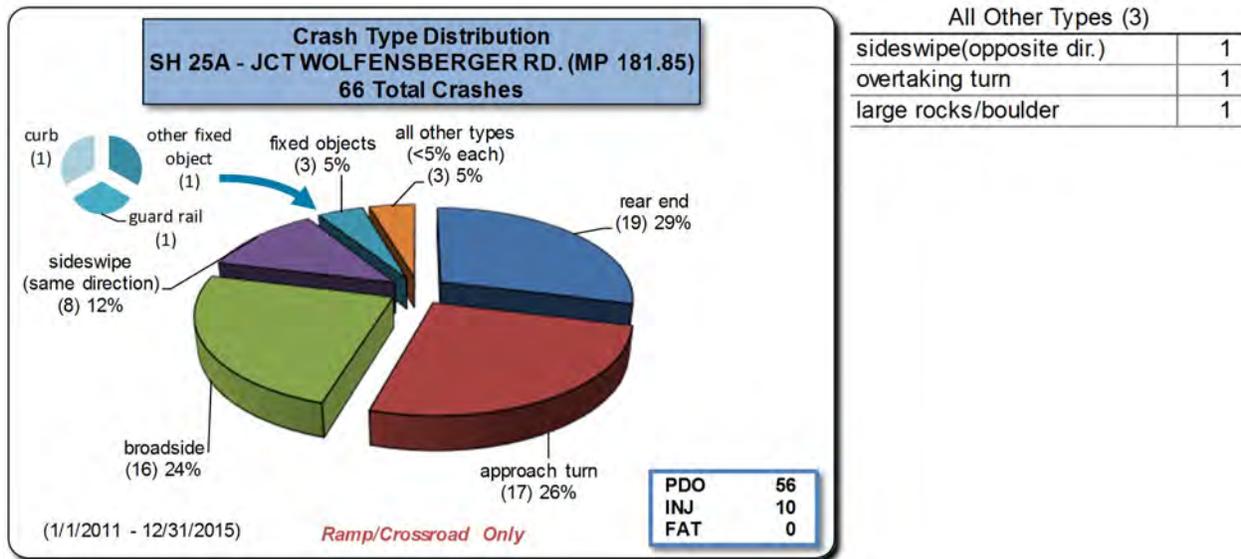
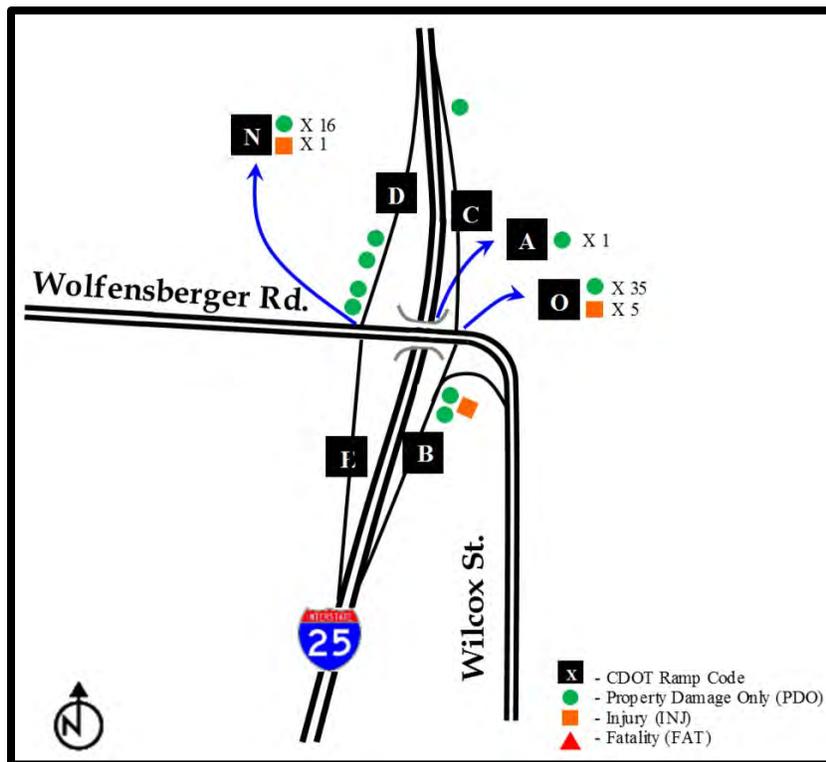


Figure 45 shows the approximate location, number, and severity of the ramp and crossroad crashes for the Wilcox Street/Wolfensberger Road interchange.

Figure 45:
Ramp/Crossroad Crashes



Ramp A (Crossroad)

There was only one (1) crash on the crossroad (Wolfensberger Road) during the five-year study period in the vicinity of the interchange which was not located at either ramp terminal intersection. Because of the very low number of crashes, a correctable crash pattern could not be identified; therefore, no suggestion for improvement specific to this ramp is recommended at this time.

Ramp B

There were three (3) crashes on the northbound I-25 off-ramp to Wolfensberger Road during the five-year study period. The three crashes were different crash types; *large boulder*, *other fixed object*, and *rear-end*. Distracted driving was a factor in the *rear-end* crash, and driver under the influence of alcohol was a factor in the *other fixed object* crash. A closer examination of the crash listing revealed no correctable crash pattern; therefore, no suggestion for improvement specific to this ramp is recommended at this time.

Ramp C

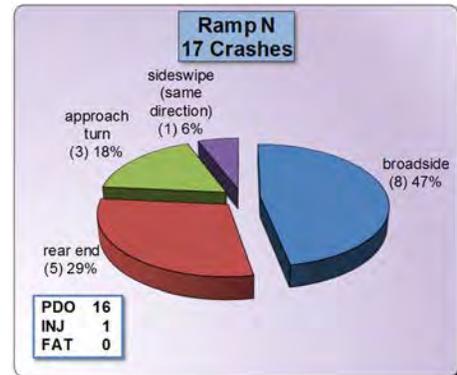
There was only one (1) crash on the northbound I-25 on-ramp from Wolfensberger Road during the five-year study period. Because of the low number of crashes; there are no suggestions for improvements specific to this ramp at this time.

Ramp D

There were four (4) crashes on the southbound I-25 off-ramp to Wolfensberger Road during the five-year study period; of which *rear-end* type was predominant (3 of 4). Driver under the influence of alcohol was a factor in one (1) of the three (3) crashes. Because of the low number of crashes; there are no suggestions for improvements specific to this location at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were 17 crashes at the signalized intersection of the southbound I-25 ramps with Wolfensberger Road during the five-year study period. The breakdown of crashes, by type, is shown on right. *Broadside* type crashes were predominant (8 of 17), followed by *rear-end* (5 of 17), and *approach turn* type crashes (3 of 17).

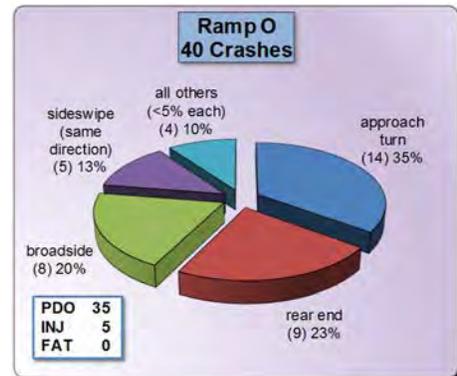


Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide rate when compared to similar intersections. Most of these crashes involved eastbound or westbound vehicles running a red light and striking a southbound vehicle attempting to make a left-turn onto eastbound Wolfensberger Road. None of the *broadside* crashes resulted in an injury or a fatality. If there is ample funding available, and if it hasn't been addressed yet, the Region should consider upgrading all signal heads to include 12-inch LED lenses (replacing incandescent bulbs, as necessary) and installing backplates with yellow borders to help increase visibility to reduce the number of *broadside* crashes. In addition, the Region may consider checking the clearance interval to reduce the probability of dilemma zone crashes.

Ramp O (Northbound Ramp Terminal Intersection)

There were 40 crashes during the five-year study period at the signalized intersection of the northbound I-25 ramps with Wolfensberger Road. The breakdown of crashes, by type, is shown to the right. *Approach turn* crashes were predominant (14 of 40), followed by *rear-end* and *broadside* type crashes (9 and 8 of 40, respectively).



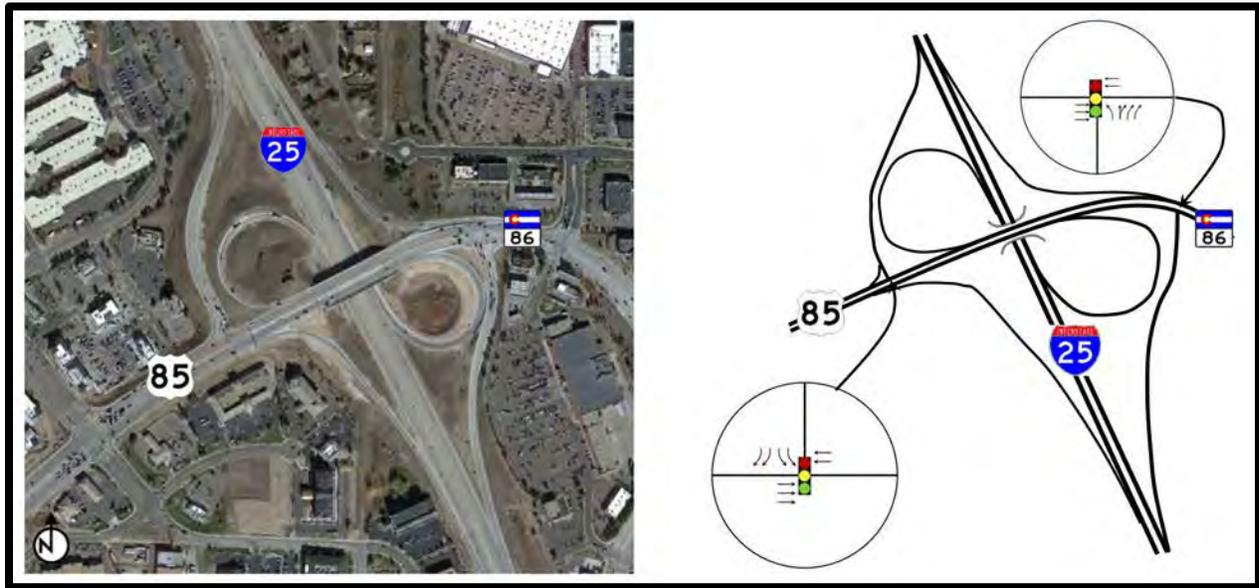
Approach Turn Collisions

The rate of *approach turn* type crashes was higher than expected when compared to similar intersections. Most of these crashes involved eastbound vehicles attempting to make a left-turn that failed to yield the right-of-way to an opposing vehicle during the permissive phase (protective/permissive left-turn signal phasing). It is unknown whether any crashes occurred during the yellow signal phase. The Region should consider changing the eastbound left-turn signal phasing from protective/permissive to protective/permissive with flashing yellow arrow to minimize the number of *approach turn* crashes.

Exit 184 - SH 85B/SH 86B (Founders Parkway) (MP 184.21)

There were 258 crashes during the five-year study period at this interchange. The SH 85B/SH 86B (Founders Parkway) interchange with I-25 is a partial cloverleaf, 4-quadrant interchange; with cloverleaf ramps from eastbound SH 85B to northbound I-25 and westbound SH 86B to southbound I-25, one-lane on-ramps and two-lane off-ramps with signalized ramp terminal intersections. An aerial view of the interchange, along with the associated ramp terminal lane geometry, is shown on **Figure 46**.

Figure 46:
Exit 184 - US 85/SH 86 Interchange



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes over I-25; the ramps are on an upgrade, while mainline I-25 remains relatively level.
- The northbound and southbound I-25 off-ramps are two-lane exits that split into multiple lanes at the approach to the ramp terminal intersection.
- The eastbound SH 85B off-ramp to northbound I-25 on-ramp cloverleaf is a two-lane exit which the left-hand lane merges onto northbound I-25, while the right-hand lane is added as a fourth travel lane onto northbound I-25.
- The southbound I-25 off-ramp is a two-lane exit; originating from a right auxiliary lane and a shared travel lane at the gore point.

Crash History

Figure 47 provides a graphical representation of crash types for this interchange. *Rear-end* crashes were predominant (56%); followed by *broadside* (16%), *sideswipe (same direction)* (12%), and crashes involving fixed objects (12%).

Figure 47:
Exit 184 – SH 85B/SH 86B Crash Distribution

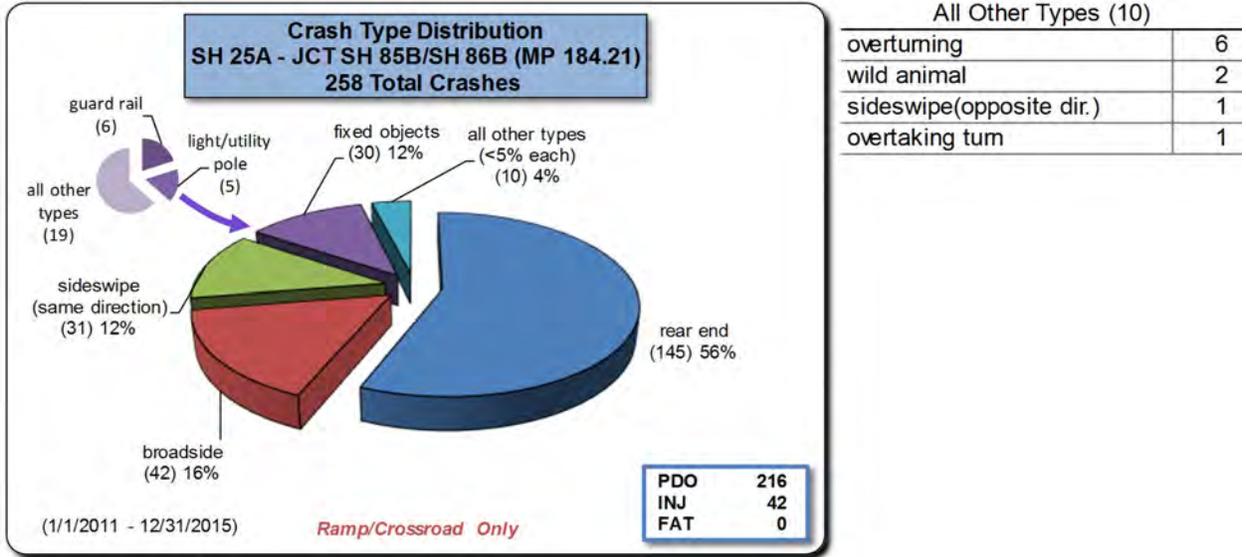
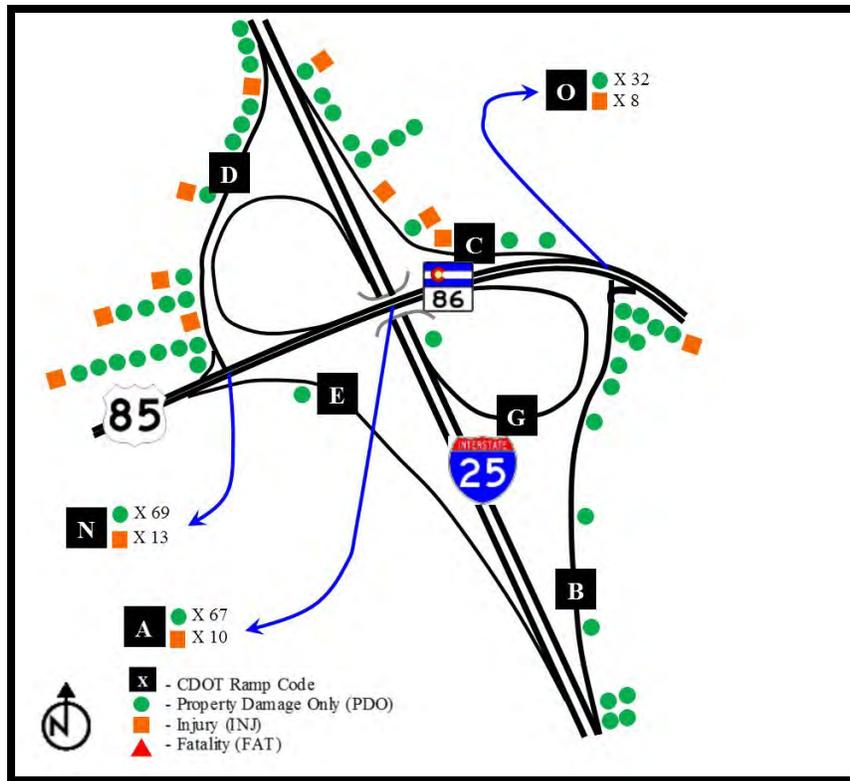


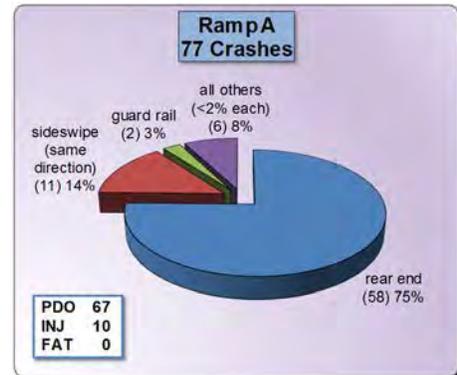
Figure 48 shows the approximate location, number, and severity of the ramp and crossroad crashes for the SH 85B/SH 86B interchange.

Figure 48:
Ramp/Crossroad Crashes



Ramp A (Crossroad)

There were 77 crashes on the crossroad (SH 85B/SH 86B) in the vicinity of the interchange, which were not located at either ramp terminal signalized intersection. The breakdown of crashes, by type, is shown to the right. A review of the crash history indicated that the majority of these crashes were *rear-end* (58 of 77), followed by *sideswipe (same direction)* (11 of 77).

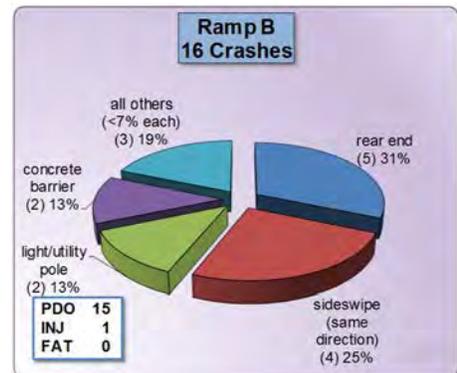


Rear-End Collisions

There was a high frequency of *rear-end* type crashes on the crossroads. Most of these collisions involved westbound vehicles where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic for the signalized ramp terminal intersections. Road and lighting conditions were not factors in the crashes as 51 of 58 *rear-end* crashes occurred on dry pavement, and 48 of 58 crashes occurred during daylight. In addition, distracted driving was a factor in 20 of 58 crashes. The Region may want to confirm that all signal heads at the ramp terminal intersections on the crossroads have 12-inch LED lenses, replacing any incandescent lenses as per current standards, to improve signal visibility which may help reduce the frequency of *rear-end* type crashes. In addition, the Region may want to consider investigating the signal timing and progression as a possible mitigation for *rear-end* crashes.

Ramp B

There were 16 crashes during the five-year study period along northbound I-25 on-ramp from SH 86B. The breakdown of crashes, by type, is shown to the right. A review of crash history indicated that *rear-end* crashes were predominant (5 of 16), followed by *sideswipe (same direction)* (4 of 16).

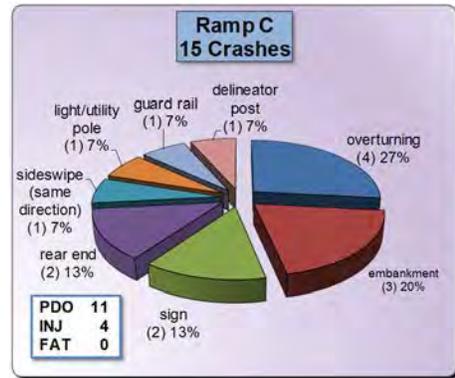


Rear-End Collisions

Most of the *rear-end* crashes involved northbound vehicles where the trailing motorist failed to stop for the leading vehicle, which slowed to stop with traffic at the downstream signalized ramp terminal intersection. Road and lighting conditions were not factors in the crashes as four (4) of five (5) *rear-end* crashes occurred on dry pavement, and all five (5) crashes occurred during daylight. In addition, distracted driving was a factor in one (1) of the *rear-end* crashes. The crashes were related to congestion from the adjacent traffic signals. No suggestions for improvements specific to these *rear-end* crashes are recommended at this time.

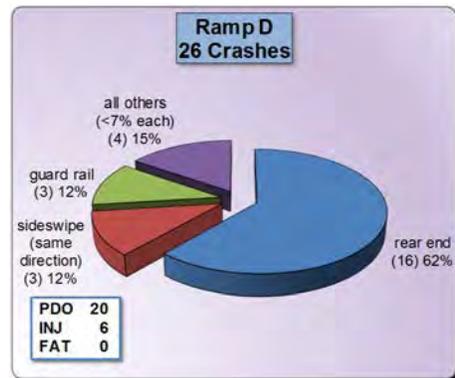
Ramp C

There were 15 crashes on the northbound I-25 on-ramp from SH 86B during the five-year study period. The breakdown of crashes, by type, is shown to the right. A review of the crash listing revealed that *overturning* crashes were predominant (4 of 15), followed by *embankment* (3 of 15), *rear-end* (2 of 15), and *sign* (2 of 15). A closer examination of the crash listing did not identify correctable crash patterns; therefore, there are no suggestions for improvements specific to the *rear-end* crashes on this ramp are recommended at this time.



Ramp D

There were 26 crashes on the southbound I-25 off-ramp to SH 85B during the five-year study period. The breakdown of crashes, by type, is shown to the right. *Rear-end* crash type was the most predominant (16 of 28); followed by *sideswipe (same direction)* and *guard rail* (3 of 26, each).



The *rear-end* crashes were related to congestion from the adjacent traffic signals. No suggestions for improvements specific to these *rear-end* crashes on this ramp are recommended at this time.

Ramp E

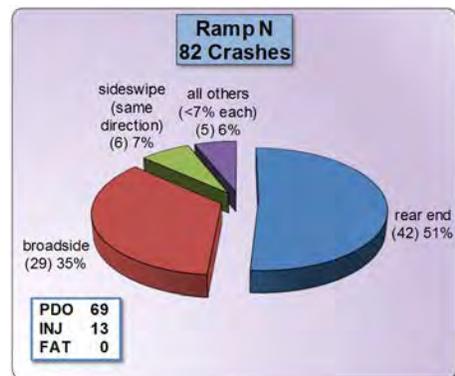
There was only one (1) crash on the southbound I-25 on-ramp during the five-year study period. Because of the very low number of crashes; there are no suggestions for improvements specific to this location at this time.

Ramp G

There was only one (1) crash on the northbound I-25 cloverleaf on-ramp during the five-year study period. Because of the very low number of crashes; there are no suggestions for improvements specific to this location at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were 82 crashes at the signalized intersection of the southbound I-25 off-ramp with SH 85B during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crash types were *rear-end* (42 of 82); followed by *broadside* (29 of 82), and *sideswipe (same direction)* (6 of 82).



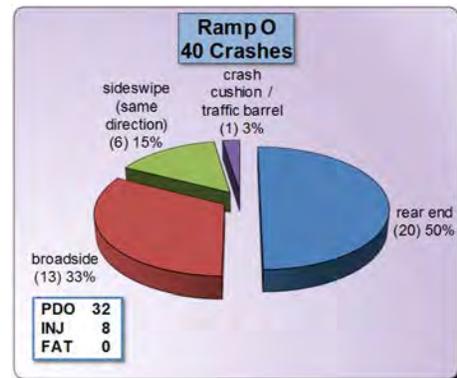
Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved westbound or eastbound vehicles running a red light, and striking a southbound vehicle. Road and lighting conditions were not factors in the crashes as 18 of 29 *broadside* crashes occurred on dry pavement, and 21 of 29 crashes occurred during daylight. The Region may want to confirm that all signal heads along the crossroad have 12-

inch LED lenses, replacing any incandescent lenses as per current standards, to improve signal visibility which may help reduce the frequency of *broadside* type crashes. In addition, the Region may consider checking the clearance interval to reduce the probability of dilemma zone crashes as a possible mitigation for *broadside* crashes.

Ramp O (Northbound Ramp Terminal Intersection)

There were 40 crashes at the signalized intersection of the northbound I-25 off-ramp with SH 86B during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crash types were *rear-end* (20 of 40); followed by *broadside* (13 of 40), and *sideswipe (same direction)* (6 of 40).



Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide average rate for similar intersections. Most of these crashes (10 of 13) involved westbound vehicles running a red light, and striking a northbound vehicle. Road and lighting conditions were not factors in these crashes as eight (8) of 10 *broadside* crashes occurred on dry pavement, and all 10 crashes occurred during daylight. The Region may want to confirm that all signal heads along the crossroad have 12-inch LED lenses, replacing any incandescent lenses as per current standards, to improve signal visibility which may mitigate the frequency of *broadside* type crashes. In addition, the Region may consider checking the clearance interval to reduce the probability of dilemma zone crashes as a possible mitigation for *broadside* crashes.

Sideswipe (Same Direction) Collisions

The rate of *sideswipe (same direction)* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved eastbound or northbound vehicles that either failed to stay in their designated lane, made unsafe lane changes, or made a northbound right-turn from the wrong lane. Lighting and pavement conditions were not a factor as five (5) of six (6) crashes occurred during daylight and on dry pavement. The Region may consider inspecting pavement marking along SH 85B, SH 86B and northbound I-25 off-ramp terminal at a greater frequency, make sure the pavement markings are visible (perhaps consider upgrading pavement marking), and meet the minimum standard retroreflectivity to mitigate the number of *sideswipe (same direction)* crashes.

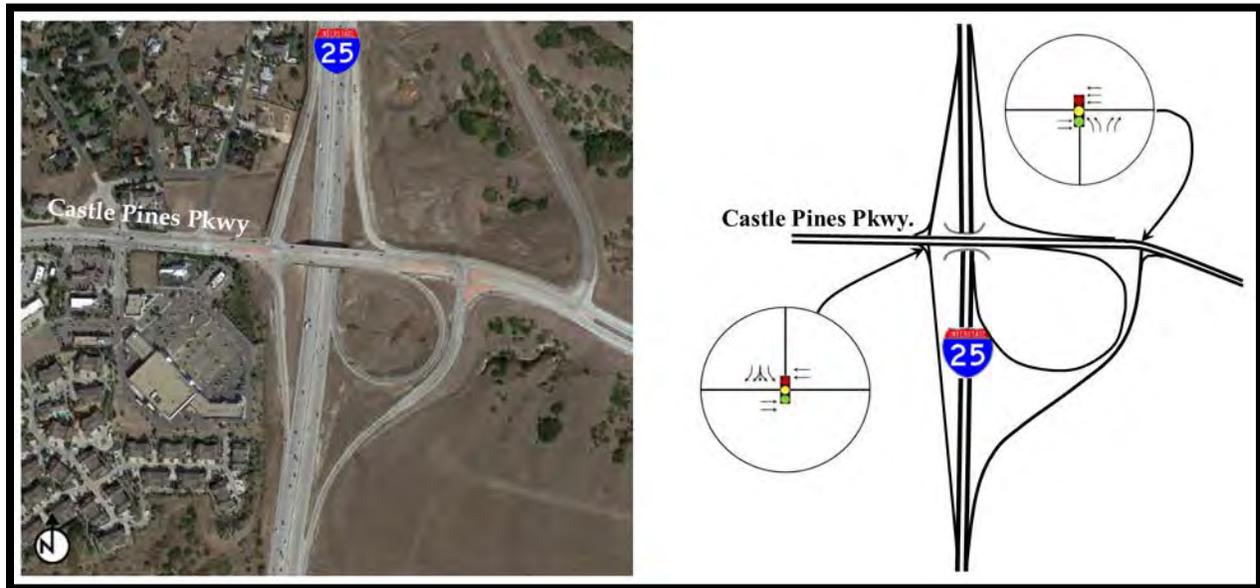
Exit 187 - Happy Canyon Road (MP 186.94)

There were only six (6) crashes during the five-year study period at this interchange; three (3) *broadside*, one (1) *sideswipe (same direction)*, one (1) *head on*, and one (1) *guard rail* type crash. Because of the very small number of crashes, there were no correctable pattern to examine; therefore, no suggestions for improvements specific to this interchange are recommended at this time.

Exit 188 – Castle Pines Parkway (MP 188.49)

There were 33 crashes during the five-year study period at this interchange. The Castle Pines Parkway interchange with I-25 is a partial cloverleaf, 4-quad interchange; with a cloverleaf ramp from eastbound Castle Pines Parkway to northbound I-25, one-lane off-ramps, two-lane on-ramps with signalized ramp terminal intersections and ramp metering signals. An aerial view of the interchange, along with the associated ramp terminal lane geometry, is shown on **Figure 49**.

Figure 49:
Exit 188 – Castle Pines Parkway Interchange



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes over I-25; the ramps are on an upgrade, while mainline I-25 remains relatively level.
- The northbound and southbound I-25 off-ramps are one-lane exits that split into multiple lanes at the approach to the ramp terminal intersection.
- Except for the cloverleaf ramp, the northbound on-ramps are two-lanes that narrow to one-lane approximately 1,000-feet prior to the I-25 merge; the southbound on-ramp is similar and narrows to one-lane approximately 750-feet from the merge.

Crash History

Figure 50 provides a graphical representation of crash types for this interchange. *Rear-end* crashes were predominant (45%); followed by *sideswipe (same direction)* (15%), and crashes involving fixed objects and *broadside* (12%, each).

Figure 50:
Exit 188 – Castle Pines Parkway Crash Distribution

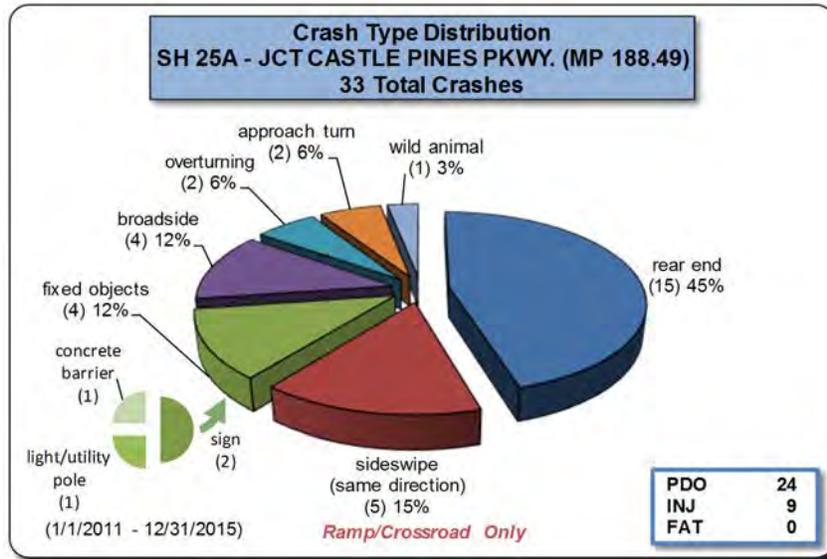
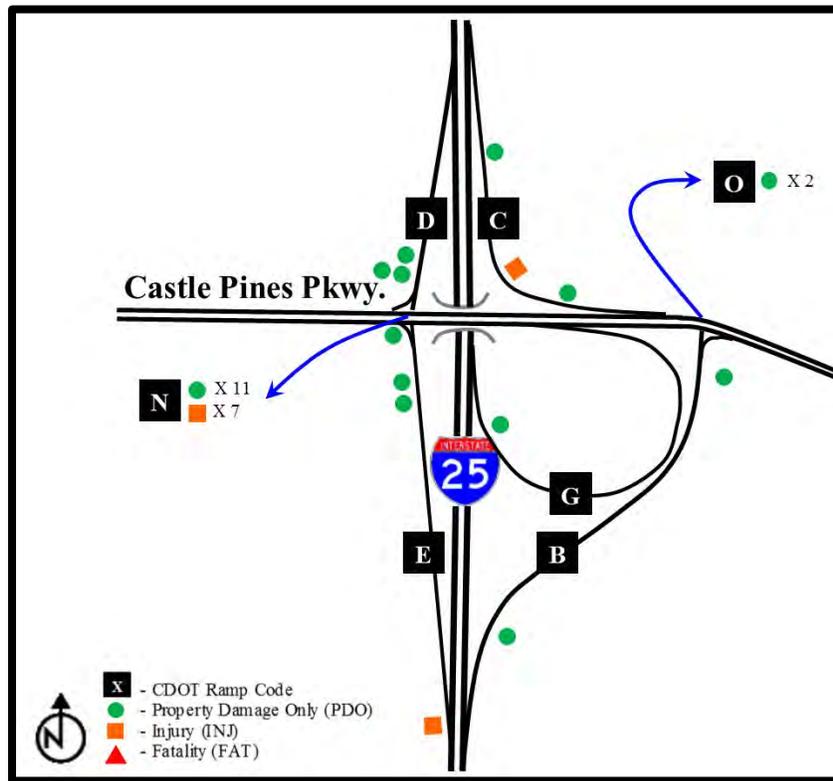


Figure 51 shows the approximate location, number, and severity of the ramp and crossroad crashes for the Castle Pines Parkway interchange.

Figure 51:
Crash Type Distribution



Ramp B

There were only two (2) crashes on the northbound I-25 off-ramp to Castle Pines Parkway. Because of the very low number of crashes; no suggestions for improvements specific to this location are made at this time.

Ramp C

There were only three (3) crashes on the northbound I-25 on-ramp from Castle Pines Parkway. A review of the crash history did not identify a correctable pattern; therefore, no suggestion for improvement specific to this ramp is recommended at this time.

Ramp D

There were three (3) crashes on the southbound off-ramp to Castle Pines Parkway. All three (3) crashes were *rear-end* type. The *rear-end* crashes were related to congestion from the adjacent traffic signals at the ramp terminal intersection. No suggestions for improvements specific to these *rear-end* crashes on this ramp are recommended at this time.

Ramp E

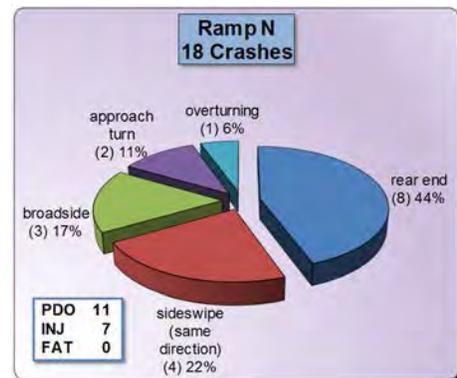
There were four (4) crashes on the southbound on-ramp from Castle Pines Parkway. A review of the crash history did not identify a correctable pattern; therefore, no suggestion for improvement specific to this ramp is recommended at this time.

Ramp G

There was only one (1) crash on the northbound cloverleaf on-ramp from eastbound Castle Pines Parkway. Because of the very low number of crashes; no suggestions for improvements specific to this location are made at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were 18 crashes during the five-year study period at the signalized intersection of the southbound I-25 ramps with Castle Pines Parkway during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crash types were *rear-end* (8 of 18); followed *sideswipe (same direction)* (4 of 18), and *broadside* (3 of 18).



Sideswipe (Same Direction Collisions)

The rate of *sideswipe (same direction)* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved southbound or eastbound vehicles changing lanes unsafely. Distracted driving was a factor in two (2) of the crashes, and driver illness was a factor in one (1) of the crashes. However, no correctable crash pattern was identified and there are no suggestions for improvements specific to this location at this time.

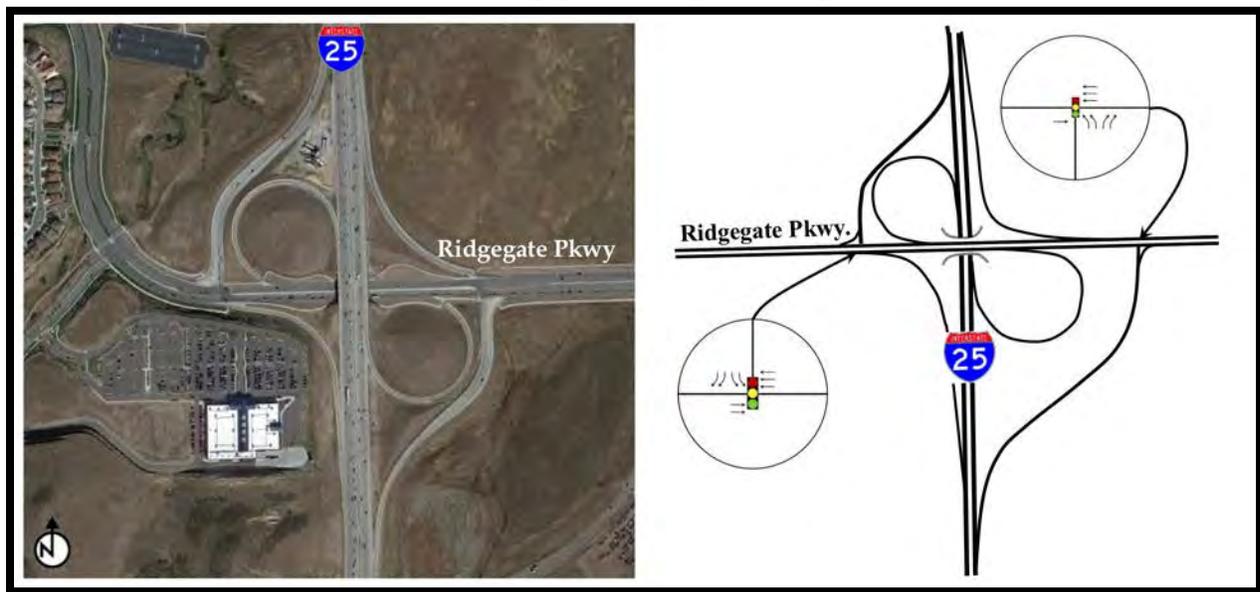
Ramp O (Northbound Ramp Terminal Intersection)

There were only two (2) crashes during the five-year study period at the signalized intersection of the northbound I-25 ramps with Castle Pines Parkway during the five-year study period. Because of the small number of crashes, no identifiable crash pattern could be detected; therefore, there are no suggestions for improvements specific to this location at this time.

Exit 192 – Ridgeway Parkway (MP 192.08)

There were 76 crashes during the five-year study period at this interchange. The Ridgeway Parkway interchange with I-25 is a partial cloverleaf, 4-quad interchange; with one-lane cloverleaf ramps from eastbound Ridgeway Parkway to northbound I-25 and westbound Ridgeway Parkway to southbound I-25, northbound I-25 one-lane off-ramp and southbound two-lane off-ramp with signalized ramp terminal intersections. In 2012, a ramp metering signal was installed on the northbound I-25 on-ramp from westbound Ridgeway Parkway (Ramp C). An aerial view of the interchange, along with the associated ramp terminal lane geometry, is shown on **Figure 52**.

Figure 52:
Exit 192 - Ridgeway Parkway Interchange



The following observations related to this interchange were made based on a review of aerial photos, the CDOT video log, and field review:

- The crossroad passes under I-25; the ramps are on a downgrade.
- The northbound I-25 off-ramp is a one lane exit that splits into multiple lanes at the approach to the ramp terminal intersection.
- The southbound I-25 off-ramp is a two-lane exit that splits into multiple lanes at the approach to the ramp terminal intersection.
- There are channelized right-turn lanes which split off of the I-25 off-ramps and crossroad prior to the ramp terminal intersection, removing almost all right-turn movements from the signalized intersections.

Crash History

Figure 53 provides a graphical representation of crash types for this interchange. *Rear-end* crashes were predominant (43%); followed by *sideswipe (same direction)* (22%), and crashes involving fixed objects (18%).

Figure 53:
Exit 192 - Ridgeway Parkway Crash Distribution

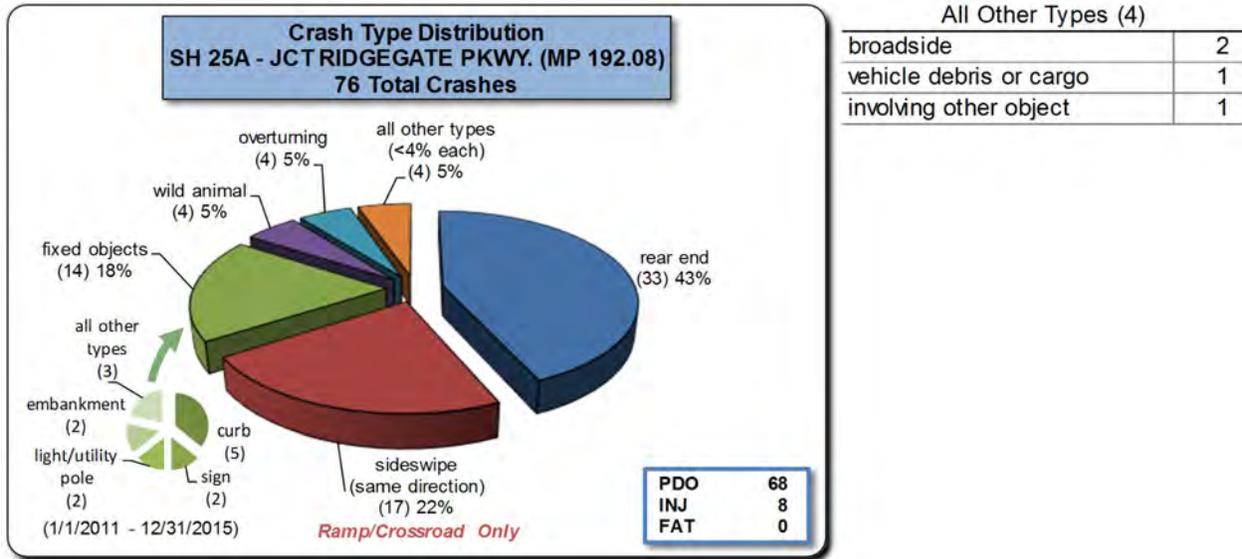
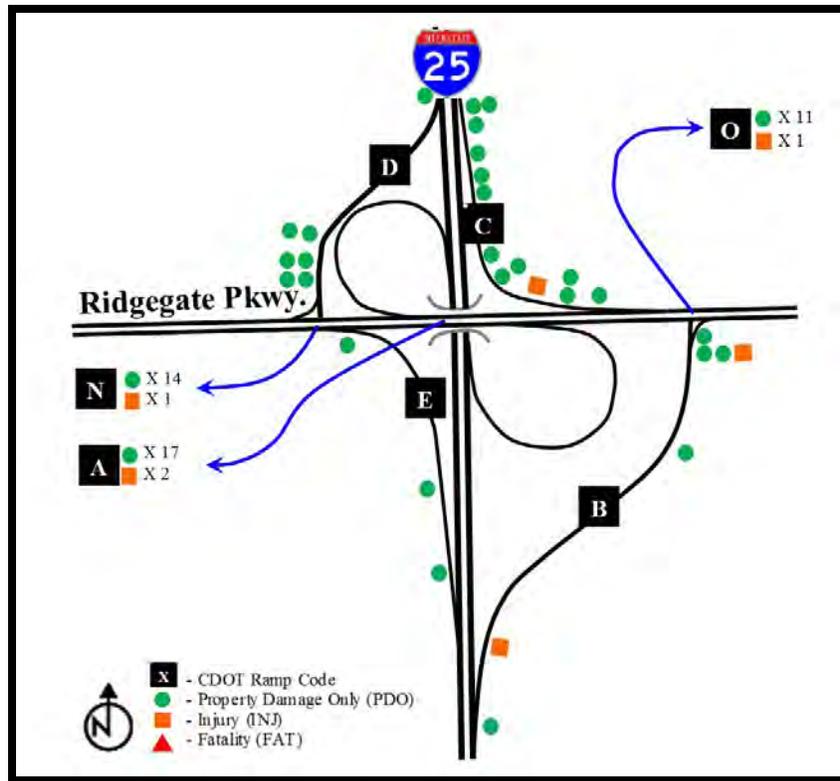


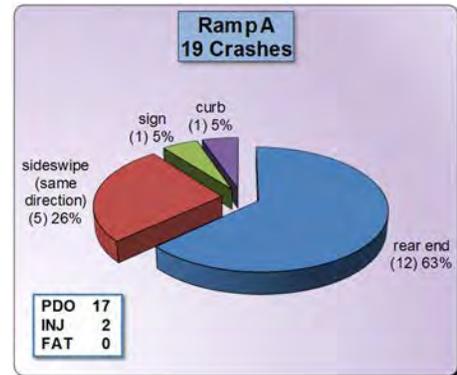
Figure 54 shows the approximate location, number, and severity of the ramp and crossroad crashes for the Ridgeway Parkway interchange.

Figure 54:
Ramp/Crossroad Crashes



Ramp A (Crossroad)

There were 19 crashes on the crossroad (Ridgegate Parkway) during the five-year study period in the vicinity of the interchange which were not located at either ramp terminal intersection. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *rear-end* (12 of 19), which were related to congestion from the adjacent traffic signals. No suggestions for improvement specific to these *rear-end* crashes are recommended at this time. *Sideswipe (same direction)* was the next most common crash type (5 of 20).



Sideswipe (Same Direction) Collisions

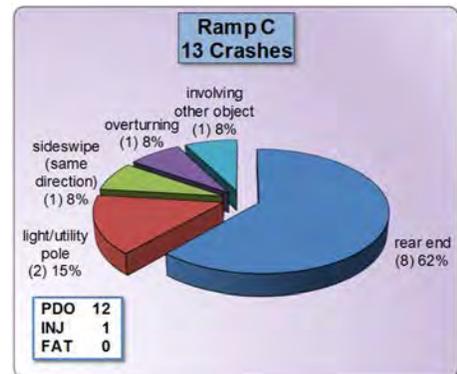
There were five (5) *sideswipe (same direction)* type crashes at this intersection. Most of these collisions involved eastbound vehicles changing lanes unsafely. Pavement and lighting conditions were not factors as the most crashes occurred on dry pavement and during daylight (4 of 5). The Region may consider inspecting pavement marking along eastbound Ridgegate Parkway at a greater frequency, make sure the pavement marking are visible (perhaps consider upgrading pavement marking), and meet the minimum standard retroreflectivity to mitigate the number of *sideswipe (same direction)* crashes.

Ramp B

There were six (6) crashes on the northbound I-25 off-ramp to Ridgegate Parkway during the five-year study period. Close examination of the crashes did not reveal a correctable crash pattern; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp C

There were 13 crashes on the northbound I-25 on-ramp from Ridgegate Parkway during the five-year study period. The breakdown of crashes, by type, is shown to the right. *Rear-end* was the most predominant (8 of 13), followed by crashes involving fixed objects (2 of 13). The *rear-end* crashes were related to congestion from the adjacent traffic signals at the ramp terminal intersection. No suggestions for improvements specific to these *rear-end* crashes on this ramp are recommended at this time.



Ramp D

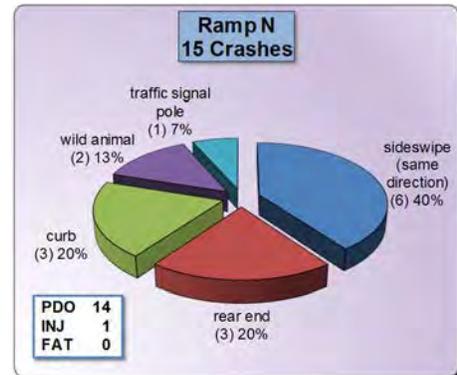
There were seven (7) crashes on the southbound I-25 off-ramp to Ridgegate Parkway during the five-year study period. Close examination of the crashes did not reveal an identifiable crash pattern; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp E

There were three (3) crashes on the southbound I-25 on-ramp from Ridgegate Parkway during the five-year study period. Because of the small number of crashes, no identifiable crash pattern could be detected; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp N (Southbound Ramp Terminal Intersection)

There were 15 crashes on the signalized intersection of the southbound I-25 ramps with Ridgeway Parkway during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crash types were *sideswipe (same direction)* (6 of 15), followed by *rear-end* and *curb/raised median* (3 of 15, each).



Sideswipe (Same Direction) Collisions

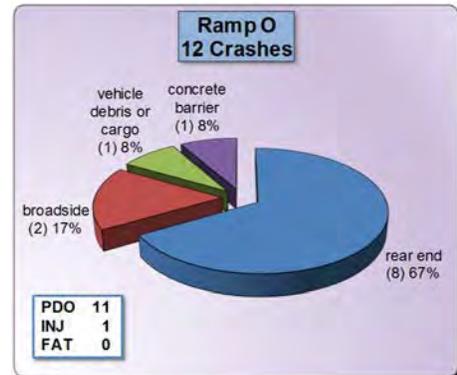
The rate of *sideswipe (same direction)* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved southbound sport utility vehicles and commercial semi-trucks, side-by-side, in the double left-turn lanes making left-turns onto eastbound Ridgeway Parkway, only to strike each other in their respective turning paths. Pavement and lighting conditions were not factors as the most crashes occurred on dry pavement and during daylight (4 of 6). The Region should examine the left-turn radii path from the southbound I-25 off-ramp, make necessary design adjustments to the turning radius to accommodate large trucks, and add the dotted white stripe extension pavement marking between the two (2) southbound left lanes (see MUTCD Figure 3B-13 and CDOT S-627-1 (Sheet 4 of 5) S-Standard Plan for examples) to help provide positive guidance to southbound left-turning motorists to maintain their designated left-turn to mitigate the number of *sideswipe (same direction)* crashes (see **Figure 55**).

Figure 55:
Ridgeway Pkwy/SB I-25 Ramp Terminal Intersection with Suggested Striping



Ramp O (Northbound Ramp Terminal Intersection)

There were 12 crashes at the signalized intersection of the northbound I-25 ramps with Ridgeway Parkway during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crash types were *rear-end* (8 of 12), followed by *broadside* (2 of 12).

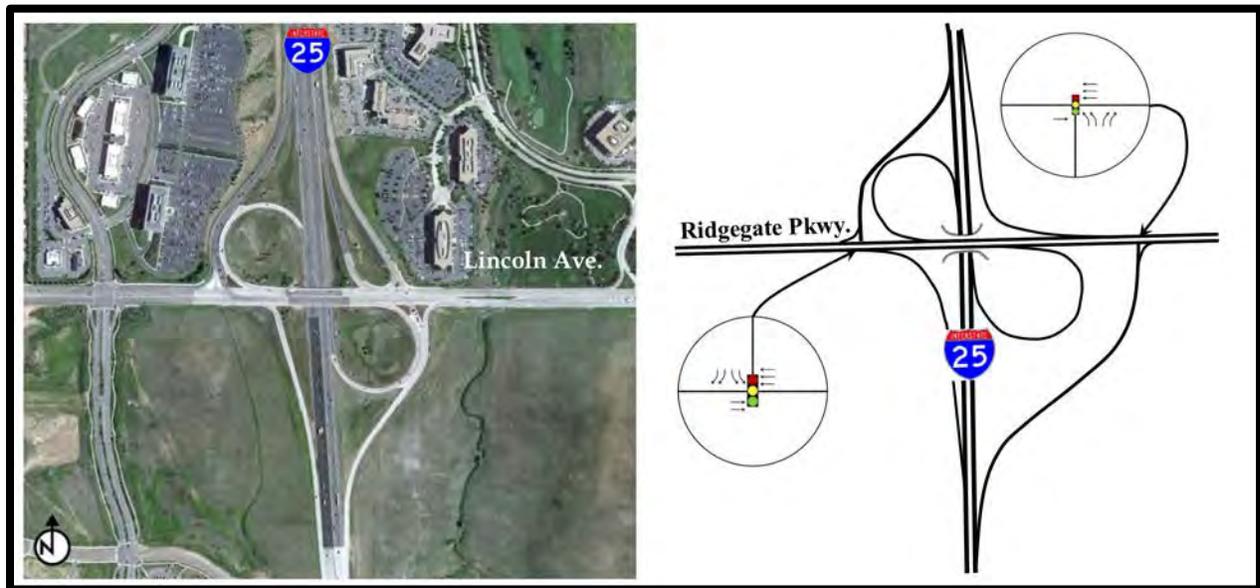


Lighting and pavement conditions were not factors in the *rear-end* crashes as seven (7) of eight (8) occurred during daylight, and six (6) of eight (8) occurred on dry pavement. The Region may want to confirm that all signal heads have 12-inch LED lenses, replacing any incandescent lenses as per current standards, to improve signal visibility which may help reduce the frequency of *rear-end* type crashes. In addition, the Region may want to consider investigating the signal timing and progression as a possible mitigation for *rear-end* crashes.

Exit 193 - Lincoln Avenue (MP 192.99)

There were 263 crashes during the five-year study period at this interchange. The Lincoln Avenue interchange with I-25 is a partial cloverleaf, 4-quad interchange and was altered between 2011 and 2012. The previous alignment of this interchange is shown on **Figure 56**.

Figure 56:
Exit 193 - Lincoln Avenue (previous configuration)

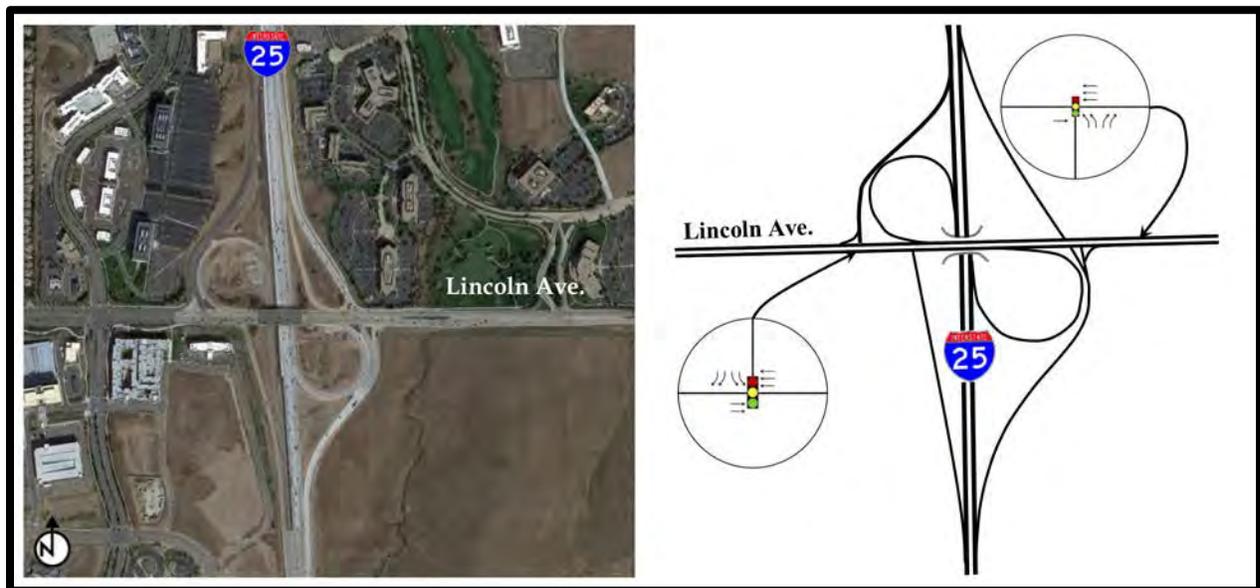


The following alterations were made to the interchange:

- Northbound I-25 off-ramp:
 - Added a second lane at the exit.
 - There are channelized right-turn lanes, which splits off of the ramps and crossroad prior to the ramp terminal intersection, removing almost all right-turn movements from the signalized intersection.
- Northbound I-25 cloverleaf on-ramp:
 - Added ramp metering signal.
 - Added a second lane at the exit from the crossroad.

The current configuration of the Lincoln Avenue interchange is shown on **Figure 57**.

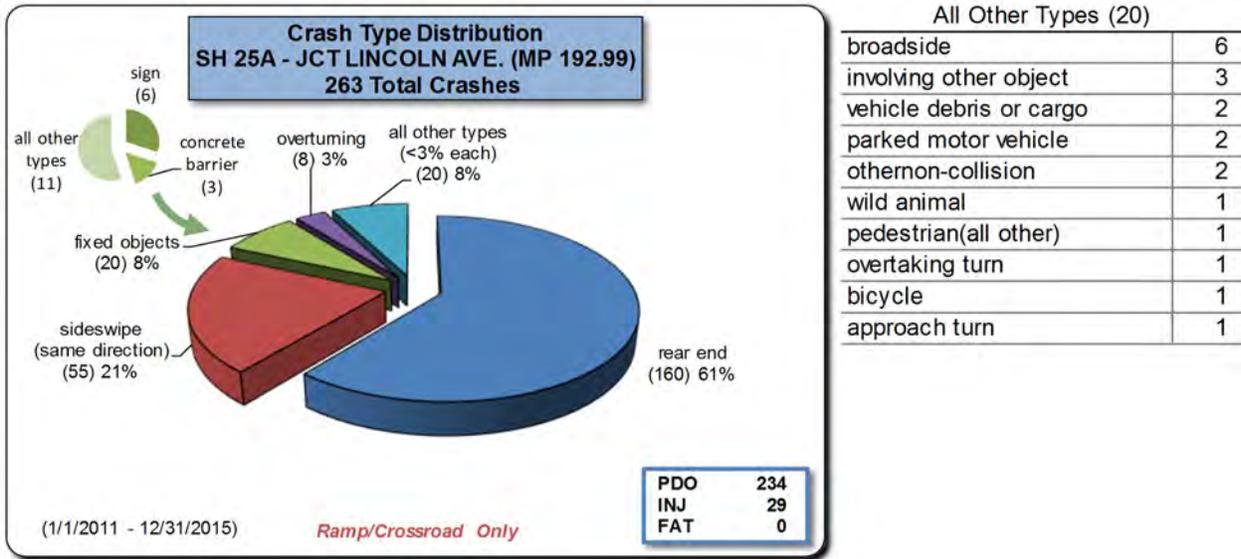
Figure 57:
Exit 193 - Lincoln Avenue (current configuration)



Crash History

The crash history is provided for the five-year study period and includes crashes under both interchange configurations. **Figure 58** provides the graphical representation of crashes, by type for the ramps and crossroads at this interchange.

Figure 58:
Exit 193 – Lincoln Avenue Crash Distribution

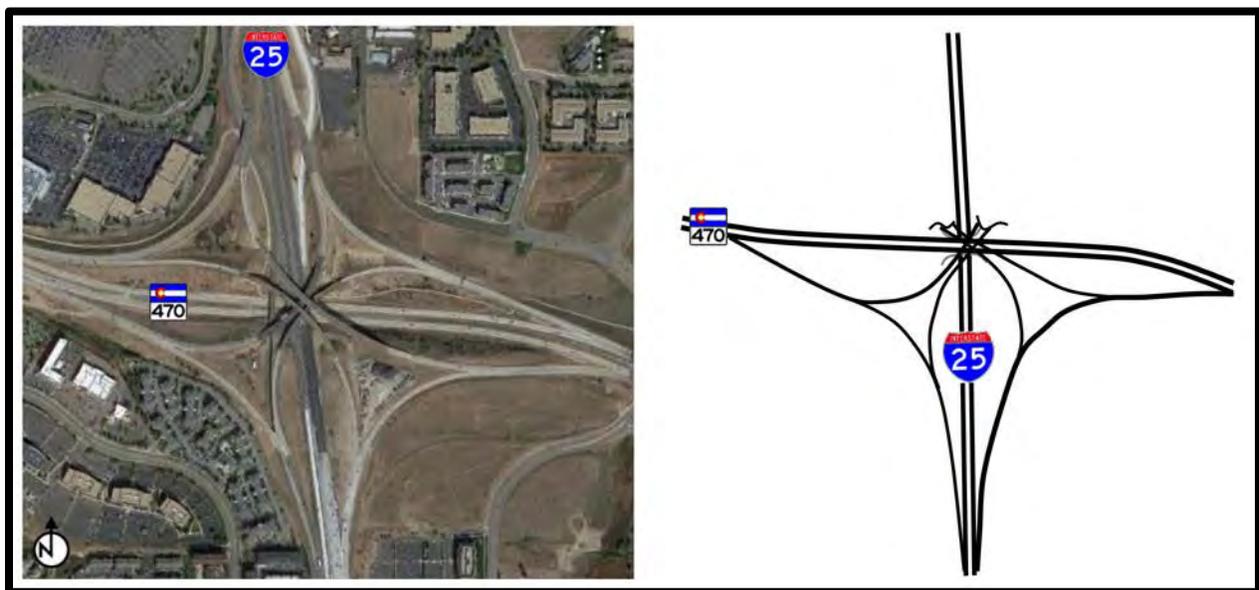


Due to limited amount of post-construction data available at this interchange, no recommendations for improvement are made at this time. This information was reported for potential use in before-after analysis at this interchange.

Exit 194 – C-470 / E-470 (South Half) (MP 194.31)

There were 70 crashes during the five-year study period at the south half of this four-way directional stack interchange. The C- 470 interchange with I-25 is directional with one-lane on- and off-ramps. An aerial view of the interchange is shown in **Figure 59**. The C-470/E-470 mainline crashes were not included in this section.

Figure 59:
Exit 194 - C-470/E-470 (South Half)



Crash History

Figure 60 provides a graphical representation of crash types for this interchange. Crashes involving fixed objects were predominant (41%), followed by rear-end (20%, each), sideswipe (same direction) (19%), and overturning (11%).

Figure 60:
C-470/E-470 (South Half) Crash Distribution

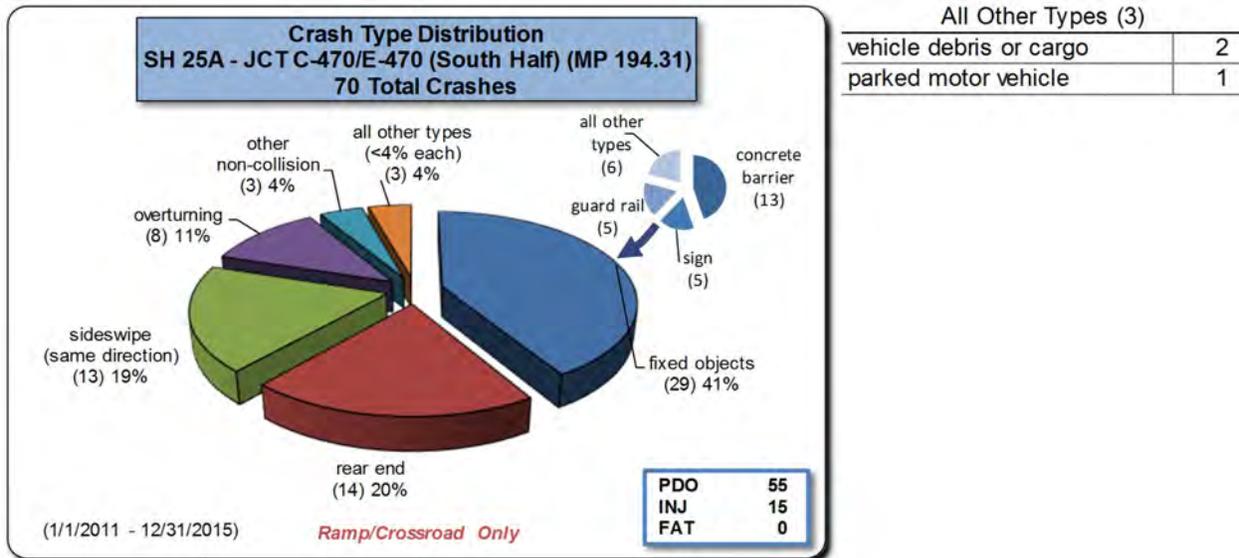
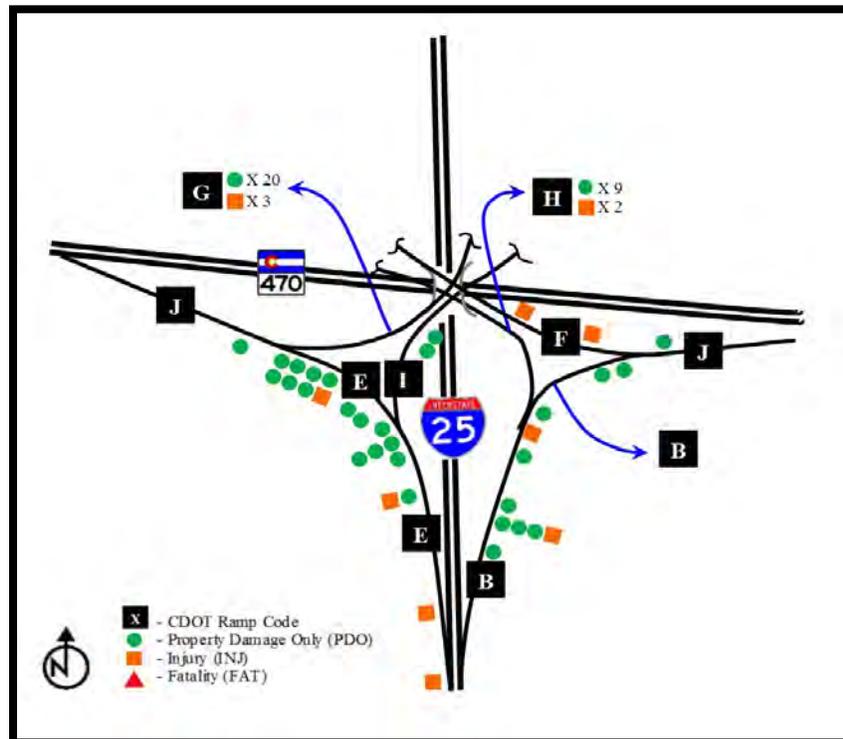


Figure 61 shows the approximate location, number, and severity of the ramp and crossroad crashes for the C-470/E-470 interchange.

Figure 61:
Ramp Crashes



Ramp B

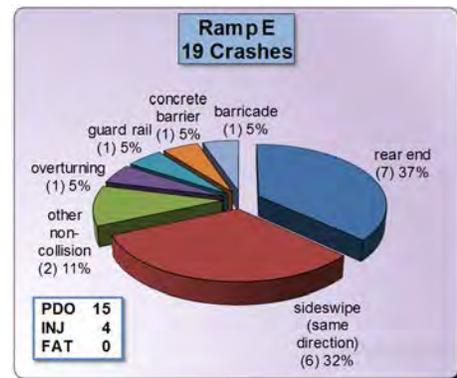
There were 11 crashes on the northbound I-25 off-ramp to eastbound E-470 during the five-year study period. There were three (3) *overturning*, two (2) *sign and crash cushion*, and one *rear-end, concrete barrier*, and *other non-collision* crashes. Close examination of the crash data indicated no correctable crash pattern was identified; therefore, no suggestions for improvements specific to this location are recommended at this time.

Ramp D

There was only one (1) crash on the southbound I-25 off-ramp to westbound C-470 during the five-year study period. Because there was only one (1) crash, no correctable crash pattern was identified; therefore, no suggestions for improvements specific to this location are recommended at this time.

Ramp E

There were 18 crashes on the southbound I-25 on-ramp from eastbound C-470 during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *rear-end* (7 of 18), followed by *sideswipe (same direction)* (6 of 18).



Rear-End Collisions

Most of the *rear-end* crashes involved trailing motorist failing to stop for the leading vehicle, which slowed in traffic on the on-ramp, approaching southbound I-25. One suggestion the Region may consider is adding a flashing beacon above the advisory ramp speed limit sign. Another suggestion to consider is provide two lanes on Ramp E from Ramp G to southbound I-25 to help avoid traffic slowdowns on Ramp E just prior to the southbound I-25 approach, which would result in fewer *rear-end* crashes.

Sideswipe (Same Direction) Collisions

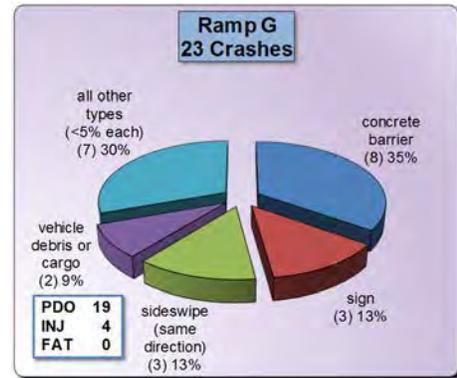
Most of the *sideswipe (same direction)* crashes involved vehicles on Ramp E making lane changes in anticipation to the two-lanes merging into one-lane just before merging onto southbound I-25, only to strike the vehicle in the adjacent lane. As mentioned previously as suggestions for reducing the number of rear-end crashes, the Region may want to consider continuing two-lanes on Ramp E from Ramp G to southbound I-25 to help reduce the number of *sideswipe (same direction)* crashes.

Ramp F

There were only two (2) crashes on the southbound I-25 off-ramp to eastbound E-470 during the five-year study period. Because of the very low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp G

There were 23 crashes on the northbound I-25 on-ramp from eastbound C-470 during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *concrete barrier* (8 of 23).

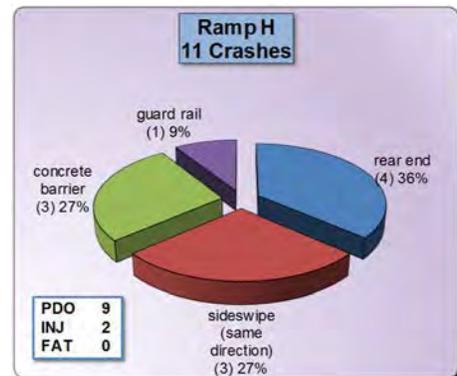


Concrete Barrier Collisions

Most of the *concrete barrier* crashes involved motorist losing control of their vehicles and striking the concrete barrier off-road left side on the left-hand horizontal curve segment of Ramp G. According to the crash listing, seven (7) of the eight (8) *concrete barrier* crashes occurred on icy pavement, and five (5) of eight (8) vehicles were traveling between 45 mph and 96 mph, which seems too fast for icy road conditions. Several suggestions the Region may consider are check the superelevation of Ramp G to make sure the cross section slope is correct for the design ramp speed, intensify winter maintenance operations on Ramp G, and check the drainage system to make sure there is proper storm water runoff, and make necessary corrections to help mitigate the number of *concrete barrier* crashes.

Ramp H

There were 11 crashes on the northbound I-25 off-ramp to westbound C-470 during the five-year study period. The breakdown of crashes, by type, is shown to the right. Most of the crashes were *rear-end* (4 of 11); followed by *sideswipe (same direction)* and *concrete barrier* (3 of 11, each).



Rear-End Collisions

The *rear-end* crashes involved trailing motorist failing to stop for the leading vehicle, which slowed in traffic on the off-ramp, approaching westbound C-470. The times of these crashes were scattered, between 7 am and 6 pm. Distracted driving and a hit-and-run incident were factors in three (3) of the four (4) *rear-end* crashes. If these crashes were eliminated from further analysis, only one (1) *rear-end* crash would remain; which would mean no correctable crash pattern can be identified, and no suggestions for improvements specific to *rear-end* crashes on Ramp H can be made at this time.

Sideswipe (Same Direction) Collisions

The *sideswipe (same direction)* crashes involved vehicles traveling between 65 mph and 90 mph and colliding with each other while making a lane change on a one-lane ramp between 12:30 pm and 4:15 pm. Since there are a low number of crashes and there appears to be different circumstances for these crashes, no correctable crash pattern can be identified; except make sure there is a posted advisory speed limit for this ramp.

Concrete Barrier Collisions

The *concrete barrier* crashes involved vehicles losing control and striking concrete barrier off-road right or left. According to the crash listings, the vehicles were traveling between 40 mph and 55 mph on snowy or icy road conditions. Several suggestions the Region may consider are check the

superelevation of Ramp H to make sure the cross section slope is correct for the design ramp speed, post an advisory speed limit sign for Ramp H, intensify winter maintenance operations on Ramp H, and check the drainage system to make sure there is proper storm water runoff, and make necessary corrections to help mitigate the number of *concrete barrier* crashes.

Ramp I

There were only two (2) crashes on the southbound I-25 on-ramp from westbound E-470 during the five-year study period. Because of the very low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Ramp J

There were only two (2) crashes on the eastbound C-470 off-ramp to I-25 during the five-year study period. Because of the very low number of crashes, no correctable crash pattern was identified; therefore, there are no suggestions for improvements specific to this location at this time.

Conclusion and Recommendations

These conclusions and recommendations are based on the analysis of five years of crash history. The Region is advised to verify through field survey, the observations made in this report regarding physical features, roadside characteristics and traffic control devices.

General Recommendations

The conditions along I-25 between MP 157.70 and MP 194.31 may to undergo a significant change following completion of the improvements proposed by the PEL. The following features typically associated with a roadway projects should be provided:

- Good skid resistance and drainage of the roadway surface,
- Adjustment, repair, and upgrade of existing guardrail to meet current standards,
- Elimination of pavement edge drop-offs (Safety Edge Application),
- Crown correction where required,
- Appropriate pavement markings, signing, and delineation,
- Appropriate advance warning signing of curves, and
- Replace all button reflectors and guardrail reflectors to ensure good nighttime and inclement weather (fog, snow, rain, etc.) delineation.

Mainline General Recommendations

Rear-End Collisions

1780 Total Crashes (1168 Injury)

Common Causal Factor: Downstream Congestion

- Improved skid resistance should generally help reduce the frequency of crashes along I-25.
- Improvements to alleviate congestion (bottleneck mitigation) should result in a reduction in peak period *rear-end* type crashes.
- Consider adding additional pavement to create a standard width left shoulder to provide an area for emergency deceleration.

Adverse Weather/Wintery Road Conditions

1223 Total Crashes (354 Injury, 1 Fatal)

Common Casual Factor: Poor roadway conditions

- Consider using variable speed limit signs along the corridor to harmonize traffic speeds during periods of adverse weather and wet roads.
- Improved road maintenance where patterns are present.

Wild Animal

288 Total Crashes (37 Injury)

Common Casual Factor: Dark-unlighted conditions

- Consider wildlife treatments (fencing, over/under-pass facilities, or other alternatives) where concentrations are noted.
- Consideration should also be given to supplementing the existing VMS signs throughout the corridor with Wild Animal warning messages during peak hours and months.

Segment 1 – Begin Study Section to Baptist Road (MP 157.70 – 158.19)

111 Total Crashes (35 Injury, 1 Fatal)

The frequency of *rear-end* type crashes (63 of 111) and *guard rail* type crashes (16 of 111) was higher than expected.

Construction Timeframe

The Baptist Road interchange was reconstructed during the five-year study period, as indicated by the increase in various crash types between 2012 and 2014. As such, no recommendations for improvements are made at this time.

Segment 2 – Baptist Road to SH 105A (MP 158.20 – 160.75)

297 Total Crashes (102 Injury)

The safety performance over the segment at a whole was worse than expected (LOSS III); the safety performance is much worse at each end of this segment, proximate to the interchanges. The frequency of run-off-road type crashes was higher than expected when compared to similar facilities statewide.

Run-off-Road Collisions (102 of 297)

There was a high proportion of run-off-road type crashes (34%); this included a higher than expected frequency of *guard rail* crashes (36 of 102). Wintry or wet roadway conditions were a contributing factor many of those crashes (21 of 36).

- While many of the crashes occurred in 2011-2013, when construction was in progress, the following recommendations are made should the observed patterns remain consistent.
 - Consideration should be given to using the existing variable message signs at MP 159.80 to warn drivers of worsened roadway conditions.

Segment 3 – SH 105A to County Line Road (MP 160.76 – 163.81)

407 Total Crashes (141 Injury, 1 Fatal)

The overall safety performance was consistently worse than expected (LOSS IV) through the study segment. The frequency of *rear-end* type crashes and run-off-road crashes, in particular, were higher than expected.

Rear-End Crashes (163 of 407)

There was a high proportion of *rear-end* crashes within the study segment; many occurred between the SH 105 interchange and the Monument Weigh Station (48 of 163).

- Consider relocating the Monument Weigh Station to an area not on-grade or proximate to an existing interchange without penalizing truck travel time unreasonably.
- If roadway widening is an option in this area, consider providing three lanes in each direction in such a way as preserves lane continuity.
 - This measure would likely reduce the frequency of other crash types, such as *sideswipe (same direction)* type crashes.

Run-off-Road Crashes (139 of 407)

There was a high proportion of run-off-road type crashes; this included a higher than expected frequency of *guard rail* crashes (48 of 139). Wintry road and adverse roadway conditions were a contributing factor many of those chases (17 of 48).

- Consideration should be given to using the existing variable message sign at MP 162.95 to warn drivers of worsened roadway conditions.
- Consideration should be given to pretreat the road surface to prevent icy/snow accumulation before wintry conditions.

Light/Utility Pole Crashes (15 of 407)

There was a high proportion of *light/utility* pole crashes in this segment. Current outside shoulder is approximately five feet wide, with the light poles approximately five feet from the edge of the shoulder pavement. The reduced shoulder width limits the recovery area before striking a light pole (within the clear zone).

- The Region should consider installing a standard-width right shoulder from the SH 105 interchange (approximate MP 160.5) to approximately MP 162.25. If additional pavement for a right shoulder cannot be accommodated, then guard rail should be considered along this segment.

Wet Roadway Crashes (53 of 407)

There was a concentration of crashes involving wet roadway conditions in the southbound direction in the vicinity of the SH 105 interchange between MP 160.80 and MP 161.30 (28 of 53). These were predominately *concrete barrier* collisions involving vehicles travelling off of the left-side of the road.

- Consider a review of the drainage profile, superelevation, and culvert conditions to ensure proper drainage and inlet capacity and help reduce the frequency of run-off-road crashes.

Segment 4 – County Line Road to Greenland Road (MP 163.82 – 166.96)

259 Total Crashes (97 Injury, 1 Fatal)

The overall safety performance was much higher than expected (LOSS III); higher at each end proximate to the interchanges. The frequency of *rear-end* type crashes and *wild animal* type crashes was higher than expected.

Rear-End Crashes (129 of 259)

Most of the crashes were in the southbound direction (86 of 129). A concentration of these occurred between MP 165.50 and MP 166.50 (36 of 86), many involving vehicles in the left-lane reacting to slower vehicles in the right-lane.

- Consider adding a southbound general purpose lane to allow slower vehicles (i.e. trucks to use as a climbing lane).

Segment 5 – Greenland Road Interchange (MP 166.97 – 167.95)

128 Total Crashes (43 Injury)

The overall safety performance was consistently worse than expected (LOSS IV) through the study segment. The frequency of *rear-end* crashes and *sideswipe (same direction)* type crashes, and *guard rail* type crashes was higher than expected.

Run-off-Road Crashes (33 of 128)

There was a high proportion of run-off-road type crashes; including a higher than expected frequency of *guard rail* crashes (22 of 33). Snowy roads (4 of 22) and Wet roads (5 of 22) were contributing factors in these crashes.

- Consideration should be given to using the existing variable message sign at MP 167.00 to warn drivers of worsened roadway conditions.
- Consideration should be given to pretreat the road surface to prevent icy/snow accumulation before wintery conditions.

Rear-End and Sideswipe (Same Direction) Crashes

There was a high concentration *rear-end* (57 of 128) and *sideswipe (same direction)* (22 of 128) crashes; including a higher than expected frequency in the southbound direction between MP 167.00 and 167.20, the area of the southbound Greenland Road on-ramp. Crashes occasionally occur in dark-unlighted conditions.

- The Region should consider a review of the interchange geometry and sight distances.
 - These measures should also reduce the frequency of other crash types.

Segment 6 – Greenland Road to Upper Lake Gulch Road (MP 167.96 – 171.31)

278 Total Crashes (108 Injury, 1 Fatal)

The overall safety performance was generally worse than expected (LOSS III); more concentrated near the interchanges. The frequency of run-off-road crashes and *sideswipe (same direction)* type crashes was higher than expected.

Run-off-Road Crashes (81 of 278)

There was a high proportion of run-off-road type crashes; including a higher than expected frequency of *guard rail* crashes (36 of 81). Crashes are directional and located on curves; northbound *guard rail* crashes (14 of 36) are located between MP 168.00 and MP 169.00, southbound *guard rail* crashes (11 of 36) are located between MP 170.00 and MP 170.50.

- Consider replacing existing guard rail reflectors and repairing or upgrading the existing guard rail where it is damaged or does not meet current standards.
- The Region should consider installing strip delineators on all guard rail to improve its visibility.

Sideswipe (Same Direction) Crashes (36 of 278)

There was a high concentration of *sideswipe (same direction)* crashes: northbound near MP 169.00 (7 of 19) and southbound near MP 170.00 (10 of 17). At both locations, a significant portion of the crashes occurred during dark-unlighted conditions.

- Consider restriping these locations with high visibility lane markings.
- The Region should consider adding luminaires to the area.
- Also, consider adding chevrons to the southbound left-hand curve near MP 170.50.

Segment 7 – Upper Lake Gulch Rd. and Spruce Mtn Rd. (MP 171.32 – 172.81)

202 Total Crashes (68 Injury)

The overall safety performance was much worse than expected (LOS IV) through the study segment. The frequency of several crash types, including run-off-road type crashes and *wild animal* type crashes.

Run-off-Road Type Crashes (63 of 202)

There was a high proportion of run-off-road type crashes; including a higher than expected frequency of *guard rail* crashes (34 of 63). The majority of the *guard rail* crashes occurred northbound between MP 171.80 and MP 172.20 (14 of 21). There is a considerable left-hand curve at this location which may contribute to the *guard rail* crashes.

- Consider replacing existing guard rail reflectors and repairing or upgrading the existing guard rail where it is damaged or does not meet current standards.
- The Region should consider installing strip delineators on all guard rail to improve its visibility.
- Also consider adding chevrons to the beginning of the curve at approximately MP 171.70 and placing a suggested speed limit on the advanced curve warning approximately MP 171.5.
 - Preparing motorists for an upcoming curve may reduce the frequency of other crash types such as *rear-end* and *side swipe (same direction)*.

Wild Animal Crashes (26 of 202)

There was a high proportion of *wild animal* crashes in this segment (26 of 202). Most crashes occurred during dark-unlighted conditions (17 of 26).

- Focus wild animal mitigation efforts (fencing, over/under-pass etc.) due to the high occurrence of wide animal crashes at 3.5 per mile per year.

Segment 8 – Spruce Mountain Road to Sky View Lane (MP 172.82 – 173.03)

20 Total Crashes (4 Injury)

No significant patterns noted.

Segment 9 – Sky View Lane Interchange (MP 173.04 – 174.29)

81 Total Crashes (22 Injury)

The overall safety performance was consistently better than expected (LOSS II) along this segment. The frequency of *rear-end* type crashes and *wild animal* type crashes was higher than expected when compared to similar intersections statewide.

Rear-End Collisions (29 of 81)

There was a high concentration *rear-end* crashes in the northbound direction near MP 174.2, approximately 0.20 miles south of the Sky View Lane northbound off-ramp (11 of 29). Approximately half of the crashes occurred during dark-unlighted conditions (4 of 11).

- Consider increasing the northbound off-ramp deceleration lane to reduce the likelihood of deceleration in the through lanes of traffic.
- The Region should consider adding lighting for the interchange to illuminate lane lines and off-ramp.
- Additionally, consider restriping with high visibility lane markings.

Wild Animal Crashes (22 of 81)

There was a high proportion of *wild animal* crashes in this segment (22 of 81). Most crashes occurred during dark-unlighted conditions (14 of 22).

- Focus wild animal mitigation efforts (fencing, over/under-pass etc.) due to the high occurrence of wide animal crashes at 3.5 per mile per year.

Segment 10 – Sky View Lane Interchange to MP 179.00 (MP 174.30 – 178.99)

336 Total Crashes (107 Injury, 2 Fatal)

The safety performance was generally better than expected (LOSS II); though most of the segment seems to be somewhat better than expected except in the vicinity of MP 175.50. The frequency of several crash types were higher than expected, including *cable rail* type crashes.

Run-off-Road Crashes (119 of 336)

There was a high proportion of run-off-road type crashes; including a higher than expected frequency of *cable rail* crashes (91 of 119). Collisions were evenly distributed between MP 178.40 and 179.00 (20 of 91). Wet roadway conditions were contributing factors in half these crashes (10 of 20). Dark-unlighted roadway was also a contributing factor in approximately half the crashes (11 of 20). There was no correlation between roadway condition and lighting.

- The Region should consider a review of the drainage profile along this segment in order to ensure proper drainage and inlet capacity.
- Consider restriping MP 178.40 to 179.00 with high visibility lane markings.
- Note: the increase in accidents in 2014 and 2015 may be from construction congestion near Spruce Mountain Road.

Segment 11 – MP 179.00 to Plum Creek Parkway (MP 179.00 – 180.87)

128 Total Crashes (36 Injury, 1 Fatal)

The safety performance was consistently worse than expected (LOSS III). The frequency of run-off-road type crashes, among others, was higher than expected.

Run-off-Road Crashes (44 of 128)

There was a high proportion of run-off-road type crashes; including a higher than expected frequency of *concrete barrier* crashes (25 of 44). Snowy road (9 of 25) and wet road (5 of 25) crashes were the predominate factor. Most crashes were off-road-left (20 of 25).

- Consideration should be given to using the existing variable message signs at MP 178.8 to warn drivers of worsened roadway conditions.
- Consider a review of the drainage profile along this segment (especially the northbound and southbound left most lane) in order to ensure proper drainage and inlet capacity.
- Consideration should be given to pretreat the road surface to prevent icy/snow accumulation before wintery conditions.

Segment 12 – Plum Creek Parkway to Wolfensberger Road (MP 180.88 – 181.84)

75 Total Crashes (28 Injury)

The overall safety performance was better than expected (LOSS II); closer to the average between MP 180.88 and MP 181.25, near the Plum Creek Parkway interchange. Overall, the proportion of most crash types was within the expected range when compared to similar facilities statewide.

Rear-End Collisions (24 of 75)

There was a concentration of southbound *rear-end* (14 of 24 *rear-end*) near MP 181. Many of these collisions occurred due to stopped or slowing traffic through the mid-day time period (10 a.m. to 3 p.m.).

- Measures taken to reduce congestion along the corridor should help reduce the frequency of *rear-end* collisions in this area.

Segment 13 – Wolfensberger Road to Founders Parkway (MP 181.85 – 184.20)

189 Total Crashes (53 Injury, 1 Fatal)

The safety performance was consistently better than expected (LOSS II) through this segment. The frequency of *sideswipe (same direction)* type crashes (51 of 189) and *concrete barrier* type crashes (25 of 189), in particular, were higher than expected.

Fatal Crash (Parked Motor Vehicle)

The fatal crash was attributed to a driver under the influence (DUI) that struck a vehicle that was stopped in the roadway with a passenger standing outside the vehicle. There were nine (9) crashes with DUI cited as a factor.

Concrete Barrier Collisions (25 of 189)

Most of the *concrete barrier* type crashes occurred due to wintry (7 of 25) or wet (9 of 25) roadway conditions. Most of these were single-vehicle crashes starting in the inside (left) lane of traffic; many with stated speeds at or near the posted speed limit suggesting that driving too fast for conditions as a contributing factor.

- The general recommendation for variable speed limits during inclement weather may help reduce the frequency of collisions observed in this segment.
- Consider a review of the drainage profile and culvert conditions along this segment (especially the inside northbound in the vicinity of the Wolfensberger Road interchange) in order to ensure proper drainage and inlet capacity.

Segment 14 – Founders Parkway to Happy Canyon Road (MP 184.21 – 186.93)

260 Total Crashes (77 Injury, 1 Fatal)

The safety performance was generally better than expected (LOSS II) through this segment. The frequency of *concrete barrier* type crashes (73 of 260) and *sideswipe (same direction)* type crashes (69 of 260), in particular, were higher than expected.

Concrete Barrier Collisions (73 of 260)

The majority of *concrete barrier* type crashes involved vehicles travelling running off the left-side of the road with a concentration in the southbound direction at the right-hand curve south of the Happy Canyon interchange (MP 186.00 to MP 186.50). In addition to crashes occurring due to wintry road

conditions, a pattern of crashes during dark-unlighted conditions occurred involving drivers travelling off-left from the outside lane. Several of these involved drivers under the influence (DUI), including the one (1) fatal collision within the segment.

- Consider extending the lighting south of the Happy Canyon Interchange, as the horizontal curve is located near the transition between lighted and unlighted conditions.
- Targeted improvements to curve delineation (high visibility striping and barrier reflectors) should be considered in this area.

Sideswipe (Same Direction) Collisions (69 of 260)

There was an unusual pattern of *sideswipe (same direction)* crashes, along with other assorted crash types, that occurred between 2 p.m. and 4 p.m. with a significant proportion of crashes involving wintry or wet roadway conditions.

- Consider adjusting the road maintenance schedules during adverse weather conditions (if necessary) to hit this segment of roadway prior to the observed crash patterns (prior to the PM peak period).

Segment 15 – Happy Canyon Road to Castle Pines Parkway (MP 186.94 – 188.48)

178 Total Crashes (57 Injury)

The safety performance was generally better than expected (LOSS II) through this segment. The frequency of run-off-road crashes (61 of 178) was higher than expected when compared to similar facilities.

Concrete Barrier Collisions (33 of 178)

Most of the 61 run-off-road crashes involved the *concrete barrier* off the left-side of the roadway (28 of 33); most with wintry road conditions (15 of 28). Poor lighting may be another potential contributing factor. There was a concentration of such crashes south of the Castle Pines interchange, around the crest of the hill in which vehicles attempting a lane change and/or driving too fast for conditions that lost control on-road and spun or slid into the median barrier.

- The general recommendation for variable speed limits during inclement weather may help reduce the frequency of collisions observed in this segment.

Segment 16 – Castle Pines Parkway to Ridgeway Parkway (MP 188.49 – 192.07)

680 Total Crashes (189 Injury, 3 Fatal)

The overall safety performance was worse than expected (LOSS III) through this segment. The frequency of run-off-road crashes (144 of 680) was higher than expected when compared to similar facilities.

Rear-End Crashes (266 of 680)

While within expected norms compared to similar facilities, it is worth noting that 90% (239 of 266) of the *rear-end* crashes occurred in the northbound direction, concentrated in the AM peak period (167 of 239). *Rear-end* crashes were more predominant in the northern portion of the study segment, coincident with the worsening LOSS.

Wet Roadway Crashes (108 of 680)

There were two patterns of wet roadway crashes: northbound near MP 189.00 through the left-hand curve, and southbound near MP 190.50 around the left-hand curve. These locations correlated with concentrations of several crash types, including *concrete barrier* and other run-off-road type crashes. Vehicles in the left-most lane tended to lose control on-road and travel off-left to collide with the median barrier or into other vehicles on the roadway.

- Consider a review of the drainage profile, superelevation, and culvert conditions to ensure proper drainage and inlet capacity and help reduce the frequency of run-off-road crashes.

Segment 17 – Ridgeway Parkway to Lincoln Avenue (MP 192.08 – 192.98)

293 Total Crashes (44 Injury)

The overall safety performance was much worse than expected (LOSS IV) through this segment. The frequency of *sideswipe (same direction)* type crashes was higher than expected when compared to similar facilities statewide.

Sideswipe (Same Direction) Collisions (65 of 293)

Other than crashes that are typically associated with congested roadway conditions, causal factors were difficult to isolate due to the changing traffic conditions at the Ridgeway Parkway interchange and construction activity near Lincoln Avenue.

Segment 18 – Lincoln Avenue to SH 470A (MP 192.99 – 194.31)

523 Total Crashes (69 Injury, 1 Fatal)

The overall safety performance was much worse than expected (LOSS IV) through this segment. The frequency of *sideswipe (same direction)* type crashes and fixed object crashes were higher than expected when compared to similar facilities statewide.

Fatal Crash (Pedestrian)

The one (1) fatal crash along this segment involved a pedestrian that entered the roadway from the east and was fatally struck by a northbound motorist. The nature of this collision does not suggest a viable recommendation for improvement.

Sideswipe (Same Direction) Collisions (119 of 523)

There was a pattern of crashes observed in the vicinity of the Lincoln Avenue interchange. However, there are no recommendations for improvement due to the recent construction efforts along this segment that is expected to alter the traffic patterns in the vicinity.

Ramp / Crossroad Recommendations

Exit 161 – SH 105A (MP 160.76) Ramp E

40 Total Crashes (8 Injury, 2 Fatal)

Fatal Crashes (Overturning)

Two fatal overturning crashes occurred between MP 160.49 and MP 160.52. Roadway geometry at this location is a left-hand horizontal curve. Both crashes involved a motorcycle traveling at 55 mph attempting to pass a slower moving vehicle in the right-hand lane.

- The Region should inspect the superelevation of the curve to verify the exiting condition is within current design standards.
- Consideration should be made to install an advance horizontal curve sign with an advisory speed between 35 and 45 mph (W1-2L) approximately 700 feet north of the point of curve (PC).

Guard Rail Crashes

9 of the 12 *guard rail* crashes occurred off-road-right. Half of these crashes occurred on snowy or icy road (6 of 12) and two-thirds occurred between MP 160.46 and MP 150.50 (8 of 12).

- The Region should consider intensifying winter maintenance operations to minimize the ice and snow on this ramp.

Exit 161 – SH 105A (MP 160.76) Ramp F

15 Total Crashes (5 Injury)

Fixed Object Collisions

6 of the 8 fixed object crashes occurred off-road-left along a horizontal curve segment. A tired driver was a factor in a third of crashes (2 of 6) and the at-fault vehicle was traveling at 45 mph or greater in half of the crashes (4 of 8). This speed exceeds the posted advisory ramp speed limit of 35 mph.

- The Region should consider adding flashing beacons above the advisory speed limit sign as a visual reminder to slow down.
- The Region may also consider an additional law enforcement presence to the area.
- Additionally, the superelevation of the horizontal curve should be reviewed to insure the ramp dimensions are within standard design criteria.

Overturning Collisions

6 of the 7 *overturning* type crashes occurred off-road-left along the right-hand horizontal curve segment. Injuries resulted in approximately half of those crashes (4 of 7). Excessive speeds were a factor in most of the crashes (5 of 7).

- The Region should consider adding flashing beacons above the advisory speed limit sign as a visual reminder to slow down.
- The Region may also consider an additional law enforcement presence to the area.
- Additionally, the superelevation of the horizontal curve should be reviewed to insure the ramp dimensions are within standard design criteria.

Exit 161 – SH 105A (MP 160.76) Ramp N

30 Total Crashes (13 Injury)

Rear-End Collisions

There was a high frequency of *rear-end* crashes at this intersection. Most crashes involved westbound vehicles where the trailing motorist failed to stop for the leading vehicle decelerating/stopping for the signalized intersection.

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.
- In addition, the Region should consider investigating signal timing and progression as possible mitigation for *rear-end* crashes.

Exit 182 – Wilcox Street/Wolfensberger Road (MP 181.85) Ramp N

17 Total Crashes (1 Injury)

Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide rate when compared to similar intersections. Most of these crashes involved eastbound or westbound vehicles running a red light and striking a southbound vehicle attempting to make a left-turn onto eastbound Wolfensberger Road.

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.
- The Region should consider installing backplates with yellow border to increase visibility of the signal heads.
- In addition, the Region should consider a review of the signal clearance intervals to reduce the probability of dilemma zone crashes.

Exit 182 – Wilcox Street/Wolfensberger Road (MP 181.85) Ramp O

40 Total Crashes (5 Injury)

Approach Turn Collisions

The rate of *approach turn* type crashes was higher than expected when compared to similar intersections. Most of these crashes involved eastbound vehicles' attempt to make a left-turn that failed to yield the right-of-way to an opposing vehicle during the permissive phase.

- The Region should consider changing the eastbound left-turn signal phasing from protective/permissive to protective/permissive with flashing yellow arrow.

Exit 184 – SH 85B/SH 86B (Founders Parkway) (MP 184.21) Ramp A

77 Total Crashes (10 Injury)

Rear-End Collisions

The rate of *rear-end* type crashes was higher than the statewide average rate for similar facilities. Most of these crashes involved westbound vehicles where the trailing motorist failed to stop for a leading vehicle slowing/stopped at a signalized intersection.

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.

Exit 184 – SH 85B/SH 86B (Founders Parkway) (MP 184.21) Ramp N

82 Total Crashes (13 Injury)

Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide average rate for similar intersections. Most of these crashes involved westbound or eastbound vehicles running a red light and striking a southbound vehicle.

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.
- In addition, the Region should consider a review of the signal clearance intervals to reduce the probability of dilemma zone crashes.

Exit 184 – SH 85B/SH 86B (Founders Parkway) (MP 184.21) Ramp O

40 Total Crashes (8 Injury)

Broadside Collisions

The rate of *broadside* type crashes was higher than the statewide average rate for similar intersections. Most of these crashes involved westbound or eastbound vehicles running a red light and striking a southbound vehicle (10 of 13).

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.
- In addition, the Region should consider a review of the signal clearance intervals to reduce the probability of dilemma zone crashes.

Sideswipe (Same Direction) Collisions

The rate of *sideswipe (same direction)* type crashes was higher than the statewide average rate for similar intersections. Most of these crashes involved eastbound or northbound vehicles that either failed to stay in their designed lane, made unsafe lane changes, or made a northbound right-turn from the wrong lane.

- The Region should consider inspecting pavement markings along SH 85B, SH 86B and northbound I-25 off-ramp terminal at a greater frequency, making sure the pavement markings are visible and meet the minimum retroreflectivity requirements.

Exit 192 – Ridgeway Parkway (MP 192.08) Ramp A

19 Total Crashes (2 Injury)

Sideswipe (Same Direction) Collisions

There was a high frequency of *sideswipe (same direction)* type crashes at this location. Most of these crashes involved eastbound vehicles changing lanes unsafely.

- The Region should consider inspecting pavement markings along eastbound Ridgeway Parkway at a greater frequency, making sure the pavement markings are visible and meet the minimum retroreflectivity requirements.

Exit 192 – Ridgeway Parkway (MP 192.08) Ramp N

14 Total Crashes (1 Injury)

Sideswipe (Same Direction) Collisions

The rate of *sideswipe (same direction)* type crashes was higher than the statewide average rate for similar intersections. Most of these collisions involved southbound sport utility vehicles and commercial semi-trucks, side-by-side, in the double left-turn lanes making left-turns onto eastbound Ridgeway Parkway, only to strike each other in their respective turning paths.

- The Region should examine the left-turn radii path from the southbound I-25 off-ramp, make necessary design adjustments to the turning radius to accommodate large trucks
- In addition, the Region should consider adding a dotted white stripe extension pavement marking between the two (2) southbound left lanes (see MUTCD Figure 3B-13 and CDOT S-627-1 (Sheet 4 of 5) S-Standard Plan for guidance.

Exit 192 – Ridgeway Parkway (MP 192.08) Ramp O

12 Total Crashes (1 Injury)

Rear-End Collisions

Most of the crashes at Ramp O were *rear-end* type crashes (8 of 12). This rate of *rear-end* type crashes was higher than the statewide average rate for similar intersections.

- The Region should confirm the use 12-inch LED signal heads for the off-ramp approach replacing incandescent lenses if found.
- In addition, the Region should consider investigating signal timing and progression as possible mitigation for *rear-end* crashes.

Exit 194 – C-470 / E-470 (South Half) (MP 194.31) Ramp E

19 Total Crashes (4 Injury)

Rear-End Collisions

Most of the *rear-end* crashes involved trailing motorist failing to stop for the leading vehicle, which slowed in traffic on the on-ramp, approaching southbound I-25.

- The Region should consider adding a flashing beacon above the advisory ramp speed limit sign.
- In addition, the Region should consider providing two lanes on Ramp E from Ramp G to southbound I-25 to help avoid traffic slowdowns on Ramp E just prior to the southbound I-25 approach.

Sideswipe (Same Direction) Collisions

Most of the *sideswipe (same direction)* crashes involved vehicles on Ramp E making lane changes in anticipation to the two-lanes merging into one-lane just before merging onto southbound I-25, only to strike the vehicle in the adjacent lane.

- The Region should consider providing two lanes on Ramp E from Ramp G to southbound I-25 to help avoid traffic slowdowns on Ramp E just prior to the southbound I-25 approach.

Exit 194 – C-470 / E-470 (South Half) (MP 194.31) Ramp G

23 Total Crashes (4 Injury)

Concrete Barrier Collisions

Most of the *concrete barrier* crashes involved motorist losing control of their vehicles and striking the concrete barrier off-road left side on the left-hand horizontal curve segment of the ramp. According to the crash listing, seven (7) of the eight (8) *concrete barrier* crashes occurred on icy pavement, and five (5) of eight (8) vehicles were traveling at an excessive speed for the conditions.

- The Region should consider verifying the superelevation and cross section slope of the ramp to verify the ramp satisfies current design criteria.
- In addition, the Region should intensify winter maintenance operations and verify proper operation of the drainage system.

Exit 194 – C-470 / E-470 (South Half) (MP 194.31) Ramp H

11 Total Crashes (2 Injury)

Most of the *concrete barrier* crashes involved motorist losing control of their vehicles and striking the concrete barrier off-road-left or off-road-right. At-fault vehicles were traveling at an excessive speed for the snowy or icy road conditions.

- The Region should consider verifying the superelevation and cross section slope of the ramp to verify the ramp satisfies current design criteria.
- In addition, the Region should intensify winter maintenance operations and verify proper operation of the drainage system.

Appendix

Detailed Summary of Crash History

- Overall Detailed Summary (January 1, 2011 to December 31, 2015)
- Individual Year General Summary
 - Year 1: 1/1/2011 to 12/31/2011
 - Year 2: 1/1/2012 to 12/31/2012
 - Year 3: 1/1/2013 to 12/31/2013
 - Year 4: 1/1/2014 to 12/31/2014
 - Year 5: 1/1/2015 to 12/31/2015

Strip Maps

Highway CORIS (Colorado Roadway Inventory System)

Accident Listing January 1, 2011 through December 31, 2015



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
Detailed Summary of Crashes Report

02/10/2017

Job #: 20170210104438

Location: 25A **Begin:** 157.70 **End:** 194.31 **From:** 01/01/2011 **To:** 12/31/2015

Severity

PDO:	3809	
INJ:	1424	2116 :Injured
FAT:	15	15 :Killed
Total:	5248	

Number of Vehicles

One Vehicle:	1774
Two Vehicles:	2783
Three or More:	691
Unknown:	0
Total:	5248

Location

On Road:	3752
Off Road Left:	769
Off Road Right:	722
Off Road at Tee:	4
Off in Median:	1
Unknown:	0
Total:	5248

Lighting Conditions

Daylight:	3505
Dawn or Dusk:	228
Dark - Lighted:	563
Dark - Unlighted:	952
Unknown:	0
Total:	5248

Weather Conditions

None:	4078
Rain:	384
Snow/Sleet/Hail:	717
Fog:	17
Dust:	1
Wind:	39
Unknown:	12
Total:	5248

Crash Rates

PDO:	0.71 *	* MVMT
INJ:	0.26 *	** 100 MVMT
FAT:	0.28 **	
Total:	0.98 *	

Crash Type

Overtuning:	294	Bridge Abutment:	0
Other Non Collision:	38	Column/Pier:	0
Pedestrians:	9	Culvert/Headwall:	14
Broadside:	52	Embankment:	38
Head On:	4	Curb:	6
Rear End:	2103	Delineator Post:	32
Sideswipe (Same):	905	Fence:	27
Sideswipe (Opposite):	9	Tree:	12
Approach Turn:	33	Large Boulders or Rocks:	12
Overtaking Turn:	5	Barricade:	10
Parked Motor Vehicle:	45	Wall/Building:	2
Railway Vehicle:	0	Crash Cushion:	19
Bicycle:	1	Mailbox:	0
Motorized Bicycle:	0	Other Fixed Object:	8
Domestic Animal:	5	Total Fixed Objects:	1251
Wild Animal:	302	Rocks in Roadway:	0
Light/Utility Pole:	53	Vehicle Cargo/Debris:	121
Traffic Signal Pole:	5	Road Maintenance Equipment:	6
Sign:	69	Involving Other Object:	63
Bridge Rail:	7	Total Other Objects:	190
Guard Rail:	322	Unknown:	2
Cable Rail:	121	Total:	5248
Concrete Barrier:	494		

Mainline/Ramps/Frontage Roads

Mainline:	4445	Frontage/Ramp Intersections
Crossroad (A):	125	M: 5 N: 139 O: 133 P: 2
Ramps		
B: 75 F: 17 J: 2		Left Frontage Rd (L): 32
C: 62 G: 38 K: 0		Rt Frontage Rd (R): 7
D: 71 H: 11 L: 32		HOV Lanes (V): 0
E: 75 I: 5		Unknown: 0
		Total: 5248

Road Description

At Intersection:	236
At Driveway Access:	1
Intersection Related:	77
Non Intersection:	4571
In Alley:	1
Roundabout:	0
Ramp:	362
Parking Lot:	0
Unknown:	0
Total:	5248

Road Conditions

Dry:	3799
Wet:	587
Muddy:	1
Snowy:	296
Icy:	374
Slushy:	120
Foreign Material:	3
With Road Treatment:	0
Dry w/Icy Road Treatment:	5
Wet w/Icy Road Treatment:	3
Snowy w/Icy Road Treatment:	13
Icy w/Icy Road Treatment:	15
Slushy w/Icy Road Treatment:	5
Unknown:	27
Total:	5248

ADT: 80,495 **Length:** 36.59



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
Detailed Summary of Crashes Report

02/10/2017

Job #: 20170210104438

Location: 25A **Begin: 157.70** **End: 194.31** **From: 01/01/2011** **To: 12/31/2015**

Vehicle Type	Veh 1	Veh 2	Veh 3
Passenger Car/Van:	2616	1557	283
Passenger Car/Van w/Trl:	12	1	2
Pickup Truck/Utility Van:	719	517	119
Pickup Truck/Utility Van w/Trl:	75	37	6
SUV:	1338	1078	229
SUV w/Trl:	18	6	1
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Bus > 15 People:	218	202	25
School Bus < 15 People:	2	4	0
Non School Bus < 15 People:	3	3	0
Motorhome:	8	5	0
Motorcycle:	84	23	4
Bicycle:	0	1	0
Motorized Bicycle:	0	0	0
Farm Equipment:	1	0	0
Hit and Run - Unknown:	136	27	16
Other:	18	11	5
Unknown:	0	2	1
Total:	5248	3474	691

Vehicle Movement	Veh 1	Veh 2	Veh 3
Going Straight:	2636	1332	189
Slowing:	420	912	151
Stopped in Traffic:	39	926	320
Making Right Turn:	41	20	0
Making Left Turn:	83	45	1
Making U-Turn:	8	0	0
Passing:	39	17	0
Backing:	21	0	0
Enter/Leave Parked Position:	9	2	0
Starting in Traffic:	0	0	0
Parked:	2	49	7
Changing Lanes:	655	46	3
Avoiding Object/Veh in Road:	118	67	6
Weaving:	154	2	0
Other:	1019	53	12
Unknown:	4	3	2
Total:	5248	3474	691

Contributing Factor	Veh 1	Veh 2	Veh 3
No Apparent Contributing Factor:	2695	3357	667
Asleep at the Wheel:	127	6	3
Illness:	30	0	0
Distracted by Passenger:	62	0	0
Driver Inexperience:	441	19	1
Driver Fatigue:	40	2	0
Driver Preoccupied:	684	10	7
Driver Unfamiliar with Area:	139	1	1
Driver Emotionally Upset:	9	2	0
Evading Law Enforcement Officer:	7	0	0
Physical Disability:	2	0	0
Unknown:	1012	77	12
Total:	5248	3474	691

Direction	Veh 1	Veh 2	Veh 3
North:	2633	1765	398
Northeast:	14	6	0
East:	217	173	24
Southeast:	4	3	0
South:	2257	1396	250
Southwest:	13	10	0
West:	105	116	19
Northwest:	5	4	0
Unknown:	0	1	0
Total:	5248	3474	691

Condition of Driver	Veh 1	Veh 2	Veh 3
No Impairment Suspected:	5020	3459	690
Alcohol Involved:	177	12	1
RX, Medication, or Drugs Involved:	24	2	0
Illegal Drugs Involved:	0	0	0
Alcohol and Drugs Involved:	27	1	0
Driver/Pedestrian not Observed:	0	0	0
Unknown:	0	0	0
Total:	5248	3474	691

ADT: 80,495 **Length: 36.59**



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/10/2017

Job #: 20170210104803

Location: 25A Begin: 157.70 End: 194.31 From: 01/01/2011 To: 12/31/2011

Severity	
PDO:	607
INJ:	228 317 :Injured
FAT:	2 2 :Killed
Total:	837

Number of Vehicles	
One Vehicle:	326
Two Vehicles:	419
Three or More:	92
Unknown:	0
Total:	837

Location	
On Road:	561
Off Road:	276
Unknown:	0
Total:	837

Mainline/Ramps/Frontage Rds	
Mainline:	699
Ramps:	131
Frontage/Ramp Intsx:	2
Frontage Roads:	7
HOV Lanes:	0
Unknown:	0
Total:	837

Lighting Conditions	
Daylight:	541
Dawn or Dusk:	45
Dark - Lighted:	92
Dark - Unlighted:	159
Unknown:	0
Total:	837

Crash Rates	
PDO:	0.59* * Per MVMT
INJ:	0.22* ** Per 100 MVMT
FAT:	0.19**
Total:	0.81*

Crash Type	
Overtuning:	60
Other Non Collision:	5
Pedestrians:	1
Broadside:	7
Head On:	0
Rear End:	290
Sideswipe Same:	140
Sideswipe Opposite:	0
Approach Turn:	1
Overtaking Turn:	0
Parked Motor Vehicle:	12
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	60
Fixed Objects:	228
Other Objects:	33
Unknown:	0
Total:	837

Weather Conditions	
None:	646
Rain:	53
Snow/Sleet/Hail:	121
Fog:	7
Dust:	0
Wind:	10
Unknown:	0
Total:	837

Road Conditions	
Dry:	592
Wet:	95
Muddy:	0
Snowy:	43
Icy:	81
Slushy:	18
Foreign Material:	0
With Road Treatment:	3
Unknown:	5
Total:	837

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	439	252	42
Passenger Car/Van w/Trailer:	1	0	0
Pickup Truck/Utility Van:	124	73	18
Pickup Truck/Utility Van w/Trailer:	15	5	2
SUV:	195	132	24
SUV w/Trailer:	7	1	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	28	30	1
School Bus < 15 People:	0	2	0
Non School Bus < 15 People:	1	0	0
Motorhome:	1	3	0
Motorcycle:	11	4	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	14	4	3
Other:	1	4	1
Unknown:	0	1	0
Total:	837	511	92

ADT: 76,980 Length: 36.60



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/10/2017

Job #: 20170210104908

Location: 25A Begin: 157.70 End: 194.31 From: 01/01/2012 To: 12/31/2012

Severity	
PDO:	638
INJ:	240 350 :Injured
FAT:	3 3 :Killed
Total:	881

Number of Vehicles	
One Vehicle:	342
Two Vehicles:	447
Three or More:	92
Unknown:	0
Total:	881

Location	
On Road:	600
Off Road:	281
Unknown:	0
Total:	881

Mainline/Ramps/Frontage Rds	
Mainline:	731
Ramps:	139
Frontage/Ramp Intsx:	0
Frontage Roads:	11
HOV Lanes:	0
Unknown:	0
Total:	881

Lighting Conditions	
Daylight:	549
Dawn or Dusk:	26
Dark - Lighted:	127
Dark - Unlighted:	179
Unknown:	0
Total:	881

Crash Rates	
PDO:	0.62* * Per MVMT
INJ:	0.23* ** Per 100 MVMT
FAT:	0.29**
Total:	0.85*

Crash Type	
Overtuning:	66
Other Non Collision:	7
Pedestrians:	2
Broadside:	12
Head On:	1
Rear End:	324
Sideswipe Same:	124
Sideswipe Opposite:	3
Approach Turn:	6
Overtaking Turn:	3
Parked Motor Vehicle:	8
Railway Vehicle:	0
Bicycles:	1
Domestic Animal:	2
Wild Animal:	57
Fixed Objects:	223
Other Objects:	42
Unknown:	0
Total:	881

Weather Conditions	
None:	683
Rain:	65
Snow/Sleet/Hail:	120
Fog:	1
Dust:	1
Wind:	11
Unknown:	0
Total:	881

Road Conditions	
Dry:	643
Wet:	89
Muddy:	1
Snowy:	56
Icy:	65
Slushy:	17
Foreign Material:	2
With Road Treatment:	6
Unknown:	2
Total:	881

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	451	247	40
Passenger Car/Van w/Trailer:	3	1	0
Pickup Truck/Utility Van:	100	71	13
Pickup Truck/Utility Van w/Trailer:	14	6	1
SUV:	225	160	30
SUV w/Trailer:	3	2	1
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	33	43	5
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	4	0	0
Motorcycle:	21	3	0
Bicycle:	0	1	0
Motorized Bicycle:	0	0	0
Farm Equipment:	1	0	0
Hit and Run - Unknown:	20	2	1
Other:	6	2	1
Unknown:	0	1	0
Total:	881	539	92

ADT: 77,342 Length: 36.60



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

02/10/2017

Job #: 20170210105017

Location: 25A Begin: 157.70 End: 194.31 From: 01/01/2013 To: 12/31/2013

Severity

PDO:	736	
INJ:	246	371 :Injured
FAT:	6	6 :Killed
Total:	988	

Number of Vehicles

One Vehicle:	345
Two Vehicles:	509
Three or More:	134
Unknown:	0
Total:	988

Location

On Road:	711
Off Road:	277
Unknown:	0
Total:	988

Mainline/Ramps/Frontage Rds

Mainline:	855
Ramps:	124
Frontage/Ramp Intsx:	0
Frontage Roads:	9
HOV Lanes:	0
Unknown:	0
Total:	988

Lighting Conditions

Daylight:	647
Dawn or Dusk:	53
Dark - Lighted:	123
Dark - Unlighted:	165
Unknown:	0
Total:	988

Crash Rates

PDO:	0.70*	* Per MVMT
INJ:	0.24*	** Per 100 MVMT
FAT:	0.57**	
Total:	0.95*	

Crash Type

Overtuning:	76
Other Non Collision:	3
Pedestrians:	1
Broadside:	7
Head On:	1
Rear End:	370
Sideswipe Same:	181
Sideswipe Opposite:	4
Approach Turn:	7
Overtaking Turn:	0
Parked Motor Vehicle:	7
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	2
Wild Animal:	64
Fixed Objects:	228
Other Objects:	37
Unknown:	0
Total:	988

Weather Conditions

None:	759
Rain:	73
Snow/Sleet/Hail:	152
Fog:	2
Dust:	0
Wind:	2
Unknown:	0
Total:	988

Road Conditions

Dry:	709
Wet:	104
Muddy:	0
Snowy:	71
Icy:	81
Slushy:	11
Foreign Material:	1
With Road Treatment:	2
Unknown:	9
Total:	988

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	493	291	55
Passenger Car/Van w/Trailer:	5	0	0
Pickup Truck/Utility Van:	155	106	27
Pickup Truck/Utility Van w/Trailer:	17	3	0
SUV:	231	187	42
SUV w/Trailer:	4	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	40	41	7
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	1	0
Motorhome:	0	1	0
Motorcycle:	17	4	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	23	8	2
Other:	3	1	0
Unknown:	0	0	0
Total:	988	643	134

ADT: 78,200 Length: 36.58



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/10/2017

Job #: 20170210105106

Location: 25A Begin: 157.70 End: 194.31 From: 01/01/2014 To: 12/31/2014

Severity	
PDO:	865
INJ:	328 497 :Injured
FAT:	1 1 :Killed
Total:	1194

Number of Vehicles	
One Vehicle:	363
Two Vehicles:	667
Three or More:	164
Unknown:	0
Total:	1194

Location	
On Road:	867
Off Road:	327
Unknown:	0
Total:	1194

Mainline/Ramps/Frontage Rds	
Mainline:	991
Ramps:	197
Frontage/Ramp Intsx:	3
Frontage Roads:	6
HOV Lanes:	0
Unknown:	0
Total:	1194

Lighting Conditions	
Daylight:	810
Dawn or Dusk:	51
Dark - Lighted:	113
Dark - Unlighted:	220
Unknown:	0
Total:	1194

Crash Rates	
PDO:	0.76* * Per MVMT
INJ:	0.29* ** Per 100 MVMT
FAT:	0.09**
Total:	1.05*

Crash Type	
Overtuning:	41
Other Non Collision:	6
Pedestrians:	1
Broadside:	17
Head On:	0
Rear End:	516
Sideswipe Same:	209
Sideswipe Opposite:	2
Approach Turn:	9
Overtaking Turn:	1
Parked Motor Vehicle:	8
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	63
Fixed Objects:	284
Other Objects:	37
Unknown:	0
Total:	1194

Weather Conditions	
None:	938
Rain:	82
Snow/Sleet/Hail:	161
Fog:	4
Dust:	0
Wind:	9
Unknown:	0
Total:	1194

Road Conditions	
Dry:	858
Wet:	131
Muddy:	0
Snowy:	61
Icy:	88
Slushy:	40
Foreign Material:	0
With Road Treatment:	5
Unknown:	11
Total:	1194

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	603	366	67
Passenger Car/Van w/Trailer:	1	0	1
Pickup Truck/Utility Van:	151	122	25
Pickup Truck/Utility Van w/Trailer:	16	11	2
SUV:	316	276	57
SUV w/Trailer:	1	1	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	54	36	4
School Bus < 15 People:	1	2	0
Non School Bus < 15 People:	0	1	0
Motorhome:	1	1	0
Motorcycle:	16	7	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	28	6	5
Other:	6	2	2
Unknown:	0	0	0
Total:	1194	831	164

ADT: 84,977 Length: 36.59



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/10/2017

Job #: 20170210105207

Location: 25A Begin: 157.70 End: 194.31 From: 01/01/2015 To: 12/31/2015

Severity	
PDO:	963
INJ:	382 581 :Injured
FAT:	3 3 :Killed
Total:	1348

Number of Vehicles	
One Vehicle:	398
Two Vehicles:	741
Three or More:	209
Unknown:	0
Total:	1348

Location	
On Road:	1013
Off Road:	335
Unknown:	0
Total:	1348

Mainline/Ramps/Frontage Rds	
Mainline:	1169
Ramps:	173
Frontage/Ramp Intsx:	2
Frontage Roads:	6
HOV Lanes:	0
Unknown:	0
Total:	1348

Lighting Conditions	
Daylight:	958
Dawn or Dusk:	53
Dark - Lighted:	108
Dark - Unlighted:	229
Unknown:	0
Total:	1348

Crash Rates	
PDO:	0.85* * Per MVMT
INJ:	0.34* ** Per 100 MVMT
FAT:	0.26**
Total:	1.19*

Crash Type	
Overtuning:	51
Other Non Collision:	17
Pedestrians:	4
Broadside:	9
Head On:	2
Rear End:	603
Sideswipe Same:	251
Sideswipe Opposite:	0
Approach Turn:	10
Overtaking Turn:	1
Parked Motor Vehicle:	10
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	1
Wild Animal:	58
Fixed Objects:	288
Other Objects:	41
Unknown:	2
Total:	1348

Weather Conditions	
None:	1052
Rain:	111
Snow/Sleet/Hail:	163
Fog:	3
Dust:	0
Wind:	7
Unknown:	12
Total:	1348

Road Conditions	
Dry:	997
Wet:	168
Muddy:	0
Snowy:	65
Icy:	59
Slushy:	34
Foreign Material:	0
With Road Treatment:	25
Unknown:	0
Total:	1348

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	630	401	79
Passenger Car/Van w/Trailer:	2	0	1
Pickup Truck/Utility Van:	189	145	36
Pickup Truck/Utility Van w/Trailer:	13	12	1
SUV:	371	323	76
SUV w/Trailer:	3	2	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	63	52	8
School Bus < 15 People:	1	0	0
Non School Bus < 15 People:	2	1	0
Motorhome:	2	0	0
Motorcycle:	19	5	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	51	7	5
Other:	2	2	1
Unknown:	0	0	1
Total:	1348	950	209

ADT: 84,977 Length: 36.59



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

01/04/2017

Job #: 20170104111859

Location: 25A Begin: 157.70 End: 158.19 From: 01/01/2011 To: 12/31/2015

SEGMENT 1 - Begin Study to Baptist Road (MAINLINE ONLY)

Severity

PDO: 75
 INJ: 35 52 :Injured
 FAT: 1 1 :Killed

Total: 111

Number of Vehicles

One Vehicle: 31
 Two Vehicles: 60
 Three or More: 20
 Unknown: 0

Total: 111

Location

On Road: 86
 Off Road: 25
 Unknown: 0

Total: 111

Mainline/Ramps/Frontage Rds

Mainline: 111
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 111

Lighting Conditions

Daylight: 82
 Dawn or Dusk: 4
 Dark - Lighted: 8
 Dark - Unlighted: 17
 Unknown: 0

Total: 111

Crash Rates

PDO: 1.02* * Per MVMT
 INJ: 0.48* ** Per 100 MVMT
 FAT: 1.37** **Total: 1.52***

Crash Type

Overtuning: 7
 Other Non Collision: 0
 Pedestrians: 1
 Broadside: 0
 Head On: 0
 Rear End: 63
 Sideswipe Same: 12
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 5
 Fixed Objects: 20
 Other Objects: 3
 Unknown: 0

Total: 111

Weather Conditions

None: 94
 Rain: 7
 Snow/Sleet/Hail: 8
 Fog: 1
 Dust: 0
 Wind: 1
 Unknown: 0

Total: 111

Road Conditions

Dry: 91
 Wet: 12
 Muddy: 0
 Snowy: 4
 Icy: 1
 Slushy: 3
 Foreign Material: 0
 With Road Treatment: 0
 Unknown: 0

Total: 111

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	52	34	8
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	16	11	4
Pickup Truck/Utility Van w/Trailer:	6	0	0
SUV:	29	27	6
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	4	4	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	1	0
Motorcycle:	4	1	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	0	1	0
Other:	0	1	0
Unknown:	0	0	0

Total: 111 80 20

ADT: 78,600 Length: 0.51



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104111801

Location: 25A Begin: 158.20 End: 160.75 From: 01/01/2011 To: 12/31/2015

SEGMENT 2 - Baptist Road to SH 105A (MAINLINE ONLY)

Severity	
PDO:	195
INJ:	102 149 :Injured
FAT:	0 0 :Killed
Total:	297

Number of Vehicles	
One Vehicle:	118
Two Vehicles:	137
Three or More:	42
Unknown:	0
Total:	297

Location	
On Road:	195
Off Road:	102
Unknown:	0
Total:	297

Mainline/Ramps/Frontage Rds	
Mainline:	297
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	297

Lighting Conditions	
Daylight:	209
Dawn or Dusk:	11
Dark - Lighted:	30
Dark - Unlighted:	47
Unknown:	0
Total:	297

Crash Rates	
PDO:	0.57* * Per MVMT
INJ:	0.30* ** Per 100 MVMT
FAT:	0.00**
Total:	0.87*

Crash Type	
Overtuning:	23
Other Non Collision:	2
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	126
Sideswipe Same:	42
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	1
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	1
Wild Animal:	11
Fixed Objects:	81
Other Objects:	10
Unknown:	0
Total:	297

Weather Conditions	
None:	207
Rain:	33
Snow/Sleet/Hail:	43
Fog:	2
Dust:	0
Wind:	10
Unknown:	2
Total:	297

Road Conditions	
Dry:	206
Wet:	42
Muddy:	0
Snowy:	11
Icy:	19
Slushy:	13
Foreign Material:	0
With Road Treatment:	6
Unknown:	0
Total:	297

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	155	82	14
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	50	29	7
Pickup Truck/Utility Van w/Trailer:	9	3	1
SUV:	68	57	18
SUV w/Trailer:	2	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	7	7	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	1	0	0
Motorcycle:	2	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	3	1	1
Other:	0	0	0
Unknown:	0	0	0
Total:	297	179	42

ADT: 74,200 Length: 2.53



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104111642

Location: 25A Begin: 160.76 End: 163.81 From: 01/01/2011 To: 12/31/2015

SEGMENT 3 - SH 105A to County Line Road (MAINLINE ONLY)

Severity

PDO: 265
 INJ: 141 229 :Injured
 FAT: 1 1 :Killed

Total: 407

Number of Vehicles

One Vehicle: 161
 Two Vehicles: 196
 Three or More: 50
 Unknown: 0

Total: 407

Location

On Road: 268
 Off Road: 139
 Unknown: 0

Total: 407

Mainline/Ramps/Frontage Rds

Mainline: 407
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 407

Lighting Conditions

Daylight: 266
 Dawn or Dusk: 15
 Dark - Lighted: 105
 Dark - Unlighted: 21
 Unknown: 0

Total: 407

Crash Rates

PDO: 0.79* * Per MVMT
 INJ: 0.42* ** Per 100 MVMT
 FAT: 0.30** **Total: 1.21***

Crash Type

Overtuning: 24
 Other Non Collision: 5
 Pedestrians: 0
 Broadside: 0
 Head On: 0
 Rear End: 163
 Sideswipe Same: 56
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 3
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 1
 Wild Animal: 29
 Fixed Objects: 118
 Other Objects: 8
 Unknown: 0

Total: 407

Weather Conditions

None: 283
 Rain: 44
 Snow/Sleet/Hail: 69
 Fog: 4
 Dust: 0
 Wind: 5
 Unknown: 2

Total: 407

Road Conditions

Dry: 271
 Wet: 53
 Muddy: 0
 Snowy: 19
 Icy: 52
 Slushy: 7
 Foreign Material: 0
 With Road Treatment: 5
 Unknown: 0

Total: 407

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	197	94	18
Passenger Car/Van w/Trailer:	2	0	0
Pickup Truck/Utility Van:	65	33	12
Pickup Truck/Utility Van w/Trailer:	11	7	1
SUV:	92	86	13
SUV w/Trailer:	6	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	18	15	4
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	1	0	0
Motorhome:	0	1	0
Motorcycle:	7	2	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	8	6	1
Other:	0	2	0
Unknown:	0	0	0

Total: 407 246 50

ADT: 59,461 Length: 3.10



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104111449

Location: 25A Begin: 163.82 End: 166.96 From: 01/01/2011 To: 12/31/2015

SEGMENT 4 - County Line Road to Greenland Road (MAINLINE ONLY)

Severity

PDO: 161
 INJ: 97 151 :Injured
 FAT: 1 1 :Killed

Total: 259

Number of Vehicles

One Vehicle: 88
 Two Vehicles: 134
 Three or More: 37
 Unknown: 0

Total: 259

Location

On Road: 200
 Off Road: 59
 Unknown: 0

Total: 259

Mainline/Ramps/Frontage Rds

Mainline: 259
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 259

Lighting Conditions

Daylight: 179
 Dawn or Dusk: 8
 Dark - Lighted: 6
 Dark - Unlighted: 66
 Unknown: 0

Total: 259

Crash Rates

PDO: 0.48* * Per MVMT
 INJ: 0.29* ** Per 100 MVMT
 FAT: 0.30** **Total: 0.77** *

Crash Type

Overtuning: 37
 Other Non Collision: 4
 Pedestrians: 0
 Broadside: 0
 Head On: 0
 Rear End: 129
 Sideswipe Same: 25
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 2
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 23
 Fixed Objects: 27
 Other Objects: 12
 Unknown: 0

Total: 259

Weather Conditions

None: 227
 Rain: 14
 Snow/Sleet/Hail: 12
 Fog: 0
 Dust: 0
 Wind: 6
 Unknown: 0

Total: 259

Road Conditions

Dry: 225
 Wet: 20
 Muddy: 0
 Snowy: 4
 Icy: 7
 Slushy: 3
 Foreign Material: 0
 With Road Treatment: 0
 Unknown: 0

Total: 259

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	134	63	18
Passenger Car/Van w/Trailer:	1	0	0
Pickup Truck/Utility Van:	25	27	4
Pickup Truck/Utility Van w/Trailer:	4	2	0
SUV:	62	64	15
SUV w/Trailer:	2	2	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	18	9	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	6	2	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	6	1	0
Other:	1	1	0
Unknown:	0	0	0

Total: 259 171 37

ADT: 61,000 Length: 3.02



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104111151

Location: 25A Begin: 166.97 End: 167.95 From: 01/01/2011 To: 12/31/2015

SEGMENT 5 - Greenland Road Interchange (MAINLINE ONLY)

Severity	
PDO:	85
INJ:	43 70 :Injured
FAT:	0 0 :Killed
Total:	128

Number of Vehicles	
One Vehicle:	38
Two Vehicles:	71
Three or More:	19
Unknown:	0
Total:	128

Location	
On Road:	95
Off Road:	33
Unknown:	0
Total:	128

Mainline/Ramps/Frontage Rds	
Mainline:	128
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	128

Lighting Conditions	
Daylight:	87
Dawn or Dusk:	8
Dark - Lighted:	2
Dark - Unlighted:	31
Unknown:	0
Total:	128

Crash Rates	
PDO:	0.72* * Per MVMT
INJ:	0.36* ** Per 100 MVMT
FAT:	0.00**
Total:	1.08*

Crash Type	
Overtuning:	9
Other Non Collision:	0
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	57
Sideswipe Same:	22
Sideswipe Opposite:	1
Approach Turn:	2
Overtaking Turn:	0
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	8
Fixed Objects:	25
Other Objects:	4
Unknown:	0
Total:	128

Weather Conditions	
None:	111
Rain:	8
Snow/Sleet/Hail:	8
Fog:	0
Dust:	0
Wind:	1
Unknown:	0
Total:	128

Road Conditions	
Dry:	108
Wet:	9
Muddy:	0
Snowy:	3
Icy:	7
Slushy:	0
Foreign Material:	0
With Road Treatment:	0
Unknown:	1
Total:	128

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	62	36	12
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	13	15	2
Pickup Truck/Utility Van w/Trailer:	3	0	1
SUV:	39	37	4
SUV w/Trailer:	2	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	4	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	1	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	4	0	0
Other:	0	0	0
Unknown:	0	0	0
Total:	128	90	19

ADT: 60,699 Length: 1.06



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104110833

Location: 25A Begin: 167.96 End: 171.31 From: 01/01/2011 To: 12/31/2015

SEGMENT 6 - Greenland Road to Upper Lake Gulch Road (MAINLINE ONLY)

Severity

PDO: 169
 INJ: 108 171 :Injured
 FAT: 1 1 :Killed

Total: 278

Number of Vehicles

One Vehicle: 108
 Two Vehicles: 123
 Three or More: 47
 Unknown: 0

Total: 278

Location

On Road: 197
 Off Road: 81
 Unknown: 0

Total: 278

Mainline/Ramps/Frontage Rds

Mainline: 278
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 278

Lighting Conditions

Daylight: 186
 Dawn or Dusk: 6
 Dark - Lighted: 5
 Dark - Unlighted: 81
 Unknown: 0

Total: 278

Crash Rates

PDO: 0.46* * Per MVMT
 INJ: 0.30* ** Per 100 MVMT
 FAT: 0.27** **Total: 0.76** *

Crash Type

Overtuning: 31
 Other Non Collision: 4
 Pedestrians: 0
 Broadside: 0
 Head On: 0
 Rear End: 114
 Sideswipe Same: 36
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 2
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 32
 Fixed Objects: 51
 Other Objects: 8
 Unknown: 0

Total: 278

Weather Conditions

None: 235
 Rain: 11
 Snow/Sleet/Hail: 29
 Fog: 0
 Dust: 0
 Wind: 2
 Unknown: 1

Total: 278

Road Conditions

Dry: 220
 Wet: 25
 Muddy: 0
 Snowy: 10
 Icy: 13
 Slushy: 5
 Foreign Material: 0
 With Road Treatment: 3
 Unknown: 2

Total: 278

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	138	72	21
Passenger Car/Van w/Trailer:	0	0	1
Pickup Truck/Utility Van:	49	26	11
Pickup Truck/Utility Van w/Trailer:	7	5	1
SUV:	64	55	8
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	10	8	2
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	3	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	5	3	3
Other:	2	0	0
Unknown:	0	1	0

Total: 278 170 47

ADT: 60,400 Length: 3.30



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104112201

Location: 25A Begin: 171.32 End: 172.81 From: 01/01/2011 To: 12/31/2015

SEGMENT 7 - Upper Lake Gulch Road to Spruce Mountain Road (MAINLINE ONLY)

Severity	
PDO:	134
INJ:	68 94 :Injured
FAT:	0 0 :Killed
Total:	202

Number of Vehicles	
One Vehicle:	79
Two Vehicles:	98
Three or More:	25
Unknown:	0
Total:	202

Location	
On Road:	139
Off Road:	63
Unknown:	0
Total:	202

Mainline/Ramps/Frontage Rds	
Mainline:	202
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	202

Lighting Conditions	
Daylight:	137
Dawn or Dusk:	7
Dark - Lighted:	1
Dark - Unlighted:	57
Unknown:	0
Total:	202

Crash Rates	
PDO:	0.79* * Per MVMT
INJ:	0.40* ** Per 100 MVMT
FAT:	0.00**
Total:	1.20*

Crash Type	
Overtuning:	10
Other Non Collision:	1
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	76
Sideswipe Same:	26
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	3
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	26
Fixed Objects:	55
Other Objects:	5
Unknown:	0
Total:	202

Weather Conditions	
None:	173
Rain:	7
Snow/Sleet/Hail:	21
Fog:	0
Dust:	0
Wind:	1
Unknown:	0
Total:	202

Road Conditions	
Dry:	150
Wet:	19
Muddy:	1
Snowy:	7
Icy:	17
Slushy:	6
Foreign Material:	0
With Road Treatment:	1
Unknown:	1
Total:	202

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	97	47	8
Passenger Car/Van w/Trailer:	0	0	1
Pickup Truck/Utility Van:	35	26	5
Pickup Truck/Utility Van w/Trailer:	2	0	0
SUV:	48	35	10
SUV w/Trailer:	1	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	11	12	1
School Bus < 15 People:	0	1	0
Non School Bus < 15 People:	0	0	0
Motorhome:	1	0	0
Motorcycle:	4	1	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	0	0
Other:	1	1	0
Unknown:	0	0	0
Total:	202	123	25

ADT: 60,435 Length: 1.53



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104112352

Location: 25A Begin: 172.82 End: 173.03 From: 01/01/2011 To: 12/31/2015

SEGMENT 8 - Spruce Mountain Road to Sky View Lane (MAINLINE ONLY)

Severity	
PDO:	16
INJ:	4 5 :Injured
FAT:	0 0 :Killed
Total:	20

Number of Vehicles	
One Vehicle:	7
Two Vehicles:	12
Three or More:	1
Unknown:	0
Total:	20

Location	
On Road:	16
Off Road:	4
Unknown:	0
Total:	20

Mainline/Ramps/Frontage Rds	
Mainline:	20
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	20

Lighting Conditions	
Daylight:	17
Dawn or Dusk:	0
Dark - Lighted:	0
Dark - Unlighted:	3
Unknown:	0
Total:	20

Crash Rates	
PDO:	0.72* * Per MVMT
INJ:	0.18* ** Per 100 MVMT
FAT:	0.00**
Total:	0.90*

Crash Type	
Overturning:	2
Other Non Collision:	0
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	10
Sideswipe Same:	3
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	2
Fixed Objects:	2
Other Objects:	1
Unknown:	0
Total:	20

Weather Conditions	
None:	17
Rain:	2
Snow/Sleet/Hail:	1
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	20

Road Conditions	
Dry:	15
Wet:	3
Muddy:	0
Snowy:	0
Icy:	0
Slushy:	0
Foreign Material:	0
With Road Treatment:	2
Unknown:	0
Total:	20

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	9	3	0
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	8	4	0
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	3	6	1
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	0	0	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	0	0	0
Other:	0	0	0
Unknown:	0	0	0
Total:	20	13	1

ADT: 61,800 Length: 0.19



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

01/04/2017

Job #: 20170104113025

Location: 25A Begin: 173.04 End: 174.29 From: 01/01/2011 To: 12/31/2015

SEGMENT 9 - Sky View Lane (MAINLINE ONLY)

Severity

PDO: 59
 INJ: 22 35 :Injured
 FAT: 0 0 :Killed

Total: 81

Number of Vehicles

One Vehicle: 42
 Two Vehicles: 30
 Three or More: 9
 Unknown: 0

Total: 81

Location

On Road: 62
 Off Road: 19
 Unknown: 0

Total: 81

Mainline/Ramps/Frontage Rds

Mainline: 81
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 81

Lighting Conditions

Daylight: 44
 Dawn or Dusk: 3
 Dark - Lighted: 4
 Dark - Unlighted: 30
 Unknown: 0

Total: 81

Crash Rates

PDO: 0.41* * Per MVMT
 INJ: 0.15* ** Per 100 MVMT
 FAT: 0.00** **Total: 0.57***

Crash Type

Overtuning: 6
 Other Non Collision: 1
 Pedestrians: 0
 Broadside: 0
 Head On: 0
 Rear End: 29
 Sideswipe Same: 7
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 22
 Fixed Objects: 13
 Other Objects: 3
 Unknown: 0

Total: 81

Weather Conditions

None: 74
 Rain: 2
 Snow/Sleet/Hail: 4
 Fog: 1
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 81

Road Conditions

Dry: 70
 Wet: 3
 Muddy: 0
 Snowy: 1
 Icy: 6
 Slushy: 0
 Foreign Material: 0
 With Road Treatment: 1
 Unknown: 0

Total: 81

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	37	22	6
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	5	6	2
Pickup Truck/Utility Van w/Trailer:	1	0	0
SUV:	31	10	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	5	1	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	0	1
Other:	0	0	0
Unknown:	0	0	0

Total: 81 39 9

ADT: 62,661 Length: 1.24



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

01/04/2017

Job #: 20170104113450

Location: 25A Begin: 174.30 End: 178.99 From: 01/01/2011 To: 12/31/2015

SEGMENT 10 - Sky View Lane to MP 179.00 (MAINLINE ONLY)

Severity

PDO: 227
 INJ: 107 169 :Injured
 FAT: 2 2 :Killed

Total: 336

Number of Vehicles

One Vehicle: 147
 Two Vehicles: 157
 Three or More: 32
 Unknown: 0

Total: 336

Location

On Road: 217
 Off Road: 119
 Unknown: 0

Total: 336

Mainline/Ramps/Frontage Rds

Mainline: 336
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 336

Lighting Conditions

Daylight: 221
 Dawn or Dusk: 6
 Dark - Lighted: 3
 Dark - Unlighted: 106
 Unknown: 0

Total: 336

Crash Rates

PDO: 0.42* * Per MVMT
 INJ: 0.20* ** Per 100 MVMT
 FAT: 0.37** **Total: 0.62***

Crash Type

Overtuning: 22
 Other Non Collision: 1
 Pedestrians: 2
 Broadside: 0
 Head On: 1
 Rear End: 123
 Sideswipe Same: 48
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 32
 Fixed Objects: 98
 Other Objects: 7
 Unknown: 2

Total: 336

Weather Conditions

None: 288
 Rain: 23
 Snow/Sleet/Hail: 21
 Fog: 2
 Dust: 0
 Wind: 0
 Unknown: 2

Total: 336

Road Conditions

Dry: 273
 Wet: 38
 Muddy: 0
 Snowy: 9
 Icy: 11
 Slushy: 1
 Foreign Material: 0
 With Road Treatment: 3
 Unknown: 1

Total: 336

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	202	87	16
Passenger Car/Van w/Trailer:	2	0	0
Pickup Truck/Utility Van:	46	25	3
Pickup Truck/Utility Van w/Trailer:	3	3	0
SUV:	64	56	11
SUV w/Trailer:	0	2	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	12	8	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	1	0	0
Motorcycle:	4	4	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	3	1
Other:	0	1	0
Unknown:	0	0	0

Total: 336 189 32

ADT: 64,000 Length: 4.65



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104113617

Location: 25A Begin: 179.00 End: 180.87 From: 01/01/2011 To: 12/31/2015

SEGMENT 11 - MP 179.00 to Plum Creek Parkway (MAINLINE ONLY)

Severity		
PDO:	91	
INJ:	36	54 :Injured
FAT:	1	1 :Killed
Total:	128	

Crash Type	
Overtuning:	0
Other Non Collision:	1
Pedestrians:	1
Broadside:	0
Head On:	0
Rear End:	36
Sideswipe Same:	27
Sideswipe Opposite:	0
Approach Turn:	1
Overtaking Turn:	0
Parked Motor Vehicle:	2
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	10
Fixed Objects:	45
Other Objects:	5
Unknown:	0
Total:	128

Weather Conditions	
None:	102
Rain:	9
Snow/Sleet/Hail:	17
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	128

Number of Vehicles	
One Vehicle:	49
Two Vehicles:	68
Three or More:	11
Unknown:	0
Total:	128

Road Conditions	
Dry:	91
Wet:	15
Muddy:	0
Snowy:	6
Icy:	12
Slushy:	3
Foreign Material:	0
With Road Treatment:	1
Unknown:	0
Total:	128

Location	
On Road:	84
Off Road:	44
Unknown:	0
Total:	128

Mainline/Ramps/Frontage Rds	
Mainline:	128
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	128

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	63	45	3
Passenger Car/Van w/Trailer:	2	0	0
Pickup Truck/Utility Van:	21	8	2
Pickup Truck/Utility Van w/Trailer:	1	3	0
SUV:	30	14	4
SUV w/Trailer:	1	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	7	6	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	1	1	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	1	2	0
Other:	1	0	1
Unknown:	0	0	0
Total:	128	79	11

Lighting Conditions	
Daylight:	86
Dawn or Dusk:	4
Dark - Lighted:	6
Dark - Unlighted:	32
Unknown:	0
Total:	128

Crash Rates	
PDO:	0.42* * Per MVMT
INJ:	0.16* ** Per 100 MVMT
FAT:	0.46**
Total:	0.58*

ADT: 64,000 Length: 1.87



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104113826

Location: 25A Begin: 180.88 End: 181.84 From: 01/01/2011 To: 12/31/2015

SEGMENT 12 - Plum Creek Parkway to Wolfensberger Road (MAINLINE ONLY)

Severity	
PDO:	47
INJ:	28 39 :Injured
FAT:	0 0 :Killed
Total:	75

Number of Vehicles	
One Vehicle:	31
Two Vehicles:	39
Three or More:	5
Unknown:	0
Total:	75

Location	
On Road:	56
Off Road:	19
Unknown:	0
Total:	75

Mainline/Ramps/Frontage Rds	
Mainline:	75
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	75

Lighting Conditions	
Daylight:	45
Dawn or Dusk:	3
Dark - Lighted:	15
Dark - Unlighted:	12
Unknown:	0
Total:	75

Crash Rates	
PDO:	0.37* * Per MVMT
INJ:	0.22* ** Per 100 MVMT
FAT:	0.00**
Total:	0.59*

Crash Type	
Overtuning:	1
Other Non Collision:	0
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	24
Sideswipe Same:	15
Sideswipe Opposite:	1
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	1
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	9
Fixed Objects:	19
Other Objects:	5
Unknown:	0
Total:	75

Weather Conditions	
None:	59
Rain:	6
Snow/Sleet/Hail:	9
Fog:	1
Dust:	0
Wind:	0
Unknown:	0
Total:	75

Road Conditions	
Dry:	55
Wet:	6
Muddy:	0
Snowy:	4
Icy:	9
Slushy:	1
Foreign Material:	0
With Road Treatment:	0
Unknown:	0
Total:	75

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	44	21	3
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	8	8	0
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	18	13	2
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	0	0
Other:	1	0	0
Unknown:	0	0	0
Total:	75	44	5

ADT: 72,800 Length: 0.96



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104114128

Location: 25A Begin: 181.85 End: 184.20 From: 01/01/2011 To: 12/31/2015

SEGMENT 13 - Wolfensberger Road to Founders Parkway (MAINLINE ONLY)

Severity	
PDO:	135
INJ:	53 82 :Injured
FAT:	1 1 :Killed
Total:	189

Number of Vehicles	
One Vehicle:	69
Two Vehicles:	99
Three or More:	21
Unknown:	0
Total:	189

Location	
On Road:	136
Off Road:	53
Unknown:	0
Total:	189

Mainline/Ramps/Frontage Rds	
Mainline:	189
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	189

Lighting Conditions	
Daylight:	118
Dawn or Dusk:	8
Dark - Lighted:	27
Dark - Unlighted:	36
Unknown:	0
Total:	189

Crash Rates	
PDO:	0.36* * Per MVMT
INJ:	0.14* ** Per 100 MVMT
FAT:	0.27**
Total:	0.50*

Crash Type	
Overtuning:	7
Other Non Collision:	3
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	44
Sideswipe Same:	51
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	4
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	2
Wild Animal:	15
Fixed Objects:	48
Other Objects:	15
Unknown:	0
Total:	189

Weather Conditions	
None:	145
Rain:	17
Snow/Sleet/Hail:	26
Fog:	0
Dust:	0
Wind:	1
Unknown:	0
Total:	189

Road Conditions	
Dry:	138
Wet:	24
Muddy:	0
Snowy:	9
Icy:	13
Slushy:	4
Foreign Material:	0
With Road Treatment:	1
Unknown:	0
Total:	189

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	91	59	10
Passenger Car/Van w/Trailer:	1	0	0
Pickup Truck/Utility Van:	34	20	4
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	46	23	6
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	12	13	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	2	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	3	2	0
Other:	2	1	1
Unknown:	0	0	0
Total:	189	120	21

ADT: 88,956 Length: 2.32



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104114250

Location: 25A Begin: 184.21 End: 186.93 From: 01/01/2011 To: 12/31/2015

SEGMENT 14 - Founders Parkway to Happy Canyon Road (MAINLINE ONLY)

Severity	
PDO:	182
INJ:	77 133 :Injured
FAT:	1 1 :Killed
Total:	260

Number of Vehicles	
One Vehicle:	123
Two Vehicles:	110
Three or More:	27
Unknown:	0
Total:	260

Location	
On Road:	157
Off Road:	103
Unknown:	0
Total:	260

Mainline/Ramps/Frontage Rds	
Mainline:	260
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	260

Lighting Conditions	
Daylight:	154
Dawn or Dusk:	11
Dark - Lighted:	25
Dark - Unlighted:	70
Unknown:	0
Total:	260

Crash Rates	
PDO:	0.33* * Per MVMT
INJ:	0.14* ** Per 100 MVMT
FAT:	0.18**
Total:	0.47*

Crash Type	
Overtuning:	15
Other Non Collision:	0
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	47
Sideswipe Same:	69
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	2
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	16
Fixed Objects:	94
Other Objects:	17
Unknown:	0
Total:	260

Weather Conditions	
None:	191
Rain:	13
Snow/Sleet/Hail:	53
Fog:	0
Dust:	0
Wind:	3
Unknown:	0
Total:	260

Road Conditions	
Dry:	171
Wet:	21
Muddy:	0
Snowy:	27
Icy:	26
Slushy:	14
Foreign Material:	0
With Road Treatment:	0
Unknown:	1
Total:	260

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	132	61	11
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	31	19	9
Pickup Truck/Utility Van w/Trailer:	1	1	0
SUV:	67	38	4
SUV w/Trailer:	1	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	15	17	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	1	0	0
Motorcycle:	4	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	1	0	0
Hit and Run - Unknown:	7	1	0
Other:	0	0	1
Unknown:	0	0	1
Total:	260	137	27

ADT: 111,400 Length: 2.73



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104114503

Location: 25A **Begin: 186.94** **End: 188.48** **From: 01/01/2011** **To: 12/31/2015**

SEGMENT 15 - Happy Canyon Road to Castle Pines Parkway (MAINLINE ONLY)

Severity	
PDO:	121
INJ:	57 80 :Injured
FAT:	0 0 :Killed
Total:	178

Crash Type	
Overtuning:	6
Other Non Collision:	2
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	57
Sideswipe Same:	34
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	4
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	12
Fixed Objects:	51
Other Objects:	12
Unknown:	0
Total:	178

Weather Conditions	
None:	127
Rain:	8
Snow/Sleet/Hail:	42
Fog:	0
Dust:	0
Wind:	1
Unknown:	0
Total:	178

Number of Vehicles	
One Vehicle:	70
Two Vehicles:	81
Three or More:	27
Unknown:	0
Total:	178

Road Conditions	
Dry:	110
Wet:	15
Muddy:	0
Snowy:	18
Icy:	20
Slushy:	12
Foreign Material:	0
With Road Treatment:	2
Unknown:	1
Total:	178

Location	
On Road:	117
Off Road:	61
Unknown:	0
Total:	178

Mainline/Ramps/Frontage Rds	
Mainline:	178
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	178

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	85	50	8
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	19	17	5
Pickup Truck/Utility Van w/Trailer:	1	1	0
SUV:	61	31	9
SUV w/Trailer:	1	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	6	7	3
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	1	1	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	3	0	0
Other:	1	1	1
Unknown:	0	0	0
Total:	178	108	27

Lighting Conditions	
Daylight:	92
Dawn or Dusk:	6
Dark - Lighted:	31
Dark - Unlighted:	49
Unknown:	0
Total:	178

Crash Rates	
PDO:	0.42* * Per MVMT
INJ:	0.20* ** Per 100 MVMT
FAT:	0.00**
Total:	0.61*

ADT: 104,000 Length: 1.53



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104114810

Location: 25A **Begin: 188.49** **End: 192.07** **From: 01/01/2011** **To: 12/31/2015**

SEGMENT 16 - Castle Pines Parkway to Ridgeway Parkway (MAINLINE ONLY)

Severity	
PDO:	488
INJ:	189 267 :Injured
FAT:	3 3 :Killed
Total:	680

Number of Vehicles	
One Vehicle:	221
Two Vehicles:	341
Three or More:	118
Unknown:	0
Total:	680

Location	
On Road:	470
Off Road:	210
Unknown:	0
Total:	680

Mainline/Ramps/Frontage Rds	
Mainline:	680
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	680

Lighting Conditions	
Daylight:	455
Dawn or Dusk:	28
Dark - Lighted:	35
Dark - Unlighted:	162
Unknown:	0
Total:	680

Crash Rates	
PDO:	0.65* * Per MVMT
INJ:	0.25* ** Per 100 MVMT
FAT:	0.40**
Total:	0.90*

Crash Type	
Overtuning:	30
Other Non Collision:	3
Pedestrians:	1
Broadside:	0
Head On:	1
Rear End:	266
Sideswipe Same:	119
Sideswipe Opposite:	1
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	11
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	1
Wild Animal:	29
Fixed Objects:	188
Other Objects:	30
Unknown:	0
Total:	680

Weather Conditions	
None:	457
Rain:	77
Snow/Sleet/Hail:	138
Fog:	4
Dust:	0
Wind:	3
Unknown:	1
Total:	680

Road Conditions	
Dry:	425
Wet:	108
Muddy:	0
Snowy:	60
Icy:	49
Slushy:	26
Foreign Material:	0
With Road Treatment:	2
Unknown:	10
Total:	680

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	333	205	40
Passenger Car/Van w/Trailer:	1	0	0
Pickup Truck/Utility Van:	90	66	19
Pickup Truck/Utility Van w/Trailer:	10	6	1
SUV:	181	140	48
SUV w/Trailer:	2	2	1
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	27	34	4
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	1	0	0
Motorhome:	0	1	0
Motorcycle:	13	3	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	21	0	4
Other:	1	1	1
Unknown:	0	1	0
Total:	680	459	118

ADT: 115,490 Length: 3.57



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104115010

Location: 25A **Begin: 192.08** **End: 192.98** **From: 01/01/2011** **To: 12/31/2015**

SEGMENT 17 - Ridgeway Parkway to Lincoln Avenue (MAINLINE ONLY)

Severity	
PDO:	249
INJ:	44 53 :Injured
FAT:	0 0 :Killed
Total:	293

Number of Vehicles	
One Vehicle:	61
Two Vehicles:	175
Three or More:	57
Unknown:	0
Total:	293

Location	
On Road:	235
Off Road:	58
Unknown:	0
Total:	293

Mainline/Ramps/Frontage Rds	
Mainline:	293
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	293

Lighting Conditions	
Daylight:	207
Dawn or Dusk:	20
Dark - Lighted:	41
Dark - Unlighted:	25
Unknown:	0
Total:	293

Crash Rates	
PDO:	1.28* * Per MVMT
INJ:	0.23* ** Per 100 MVMT
FAT:	0.00**
Total:	1.50*

Crash Type	
Overtuning:	5
Other Non Collision:	2
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	148
Sideswipe Same:	65
Sideswipe Opposite:	1
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	3
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	5
Fixed Objects:	52
Other Objects:	12
Unknown:	0
Total:	293

Weather Conditions	
None:	207
Rain:	30
Snow/Sleet/Hail:	54
Fog:	0
Dust:	0
Wind:	1
Unknown:	1
Total:	293

Road Conditions	
Dry:	186
Wet:	47
Muddy:	0
Snowy:	32
Icy:	22
Slushy:	4
Foreign Material:	0
With Road Treatment:	0
Unknown:	2
Total:	293

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	143	96	28
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	51	35	5
Pickup Truck/Utility Van w/Trailer:	4	3	1
SUV:	64	76	22
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	19	16	1
School Bus < 15 People:	1	1	0
Non School Bus < 15 People:	0	1	0
Motorhome:	0	1	0
Motorcycle:	1	2	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	9	1	0
Other:	1	0	0
Unknown:	0	0	0
Total:	293	232	57

ADT: 118,600 Length: 0.90



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

01/04/2017

Job #: 20170104115146

Location: 25A Begin: 192.99 End: 194.31 From: 01/01/2011 To: 12/31/2015

SEGMENT 18 - Lincoln Avenue to C-470/E-470 (MAINLINE ONLY)

Severity

PDO: 453
 INJ: 69 88 :Injured
 FAT: 1 1 :Killed

Total: 523

Number of Vehicles

One Vehicle: 104
 Two Vehicles: 341
 Three or More: 78
 Unknown: 0

Total: 523

Location

On Road: 425
 Off Road: 98
 Unknown: 0

Total: 523

Mainline/Ramps/Frontage Rds

Mainline: 523
 Ramps: 0
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 523

Lighting Conditions

Daylight: 349
 Dawn or Dusk: 38
 Dark - Lighted: 100
 Dark - Unlighted: 36
 Unknown: 0

Total: 523

Crash Rates

PDO: 1.20* * Per MVMT
 INJ: 0.18* ** Per 100 MVMT
 FAT: 0.26** **Total: 1.38***

Crash Type

Overtuning: 9
 Other Non Collision: 3
 Pedestrians: 1
 Broadside: 0
 Head On: 0
 Rear End: 268
 Sideswipe Same: 119
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 4
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 2
 Fixed Objects: 95
 Other Objects: 22
 Unknown: 0

Total: 523

Weather Conditions

None: 415
 Rain: 39
 Snow/Sleet/Hail: 63
 Fog: 1
 Dust: 1
 Wind: 1
 Unknown: 3

Total: 523

Road Conditions

Dry: 388
 Wet: 56
 Muddy: 0
 Snowy: 29
 Icy: 34
 Slushy: 8
 Foreign Material: 1
 With Road Treatment: 2
 Unknown: 5

Total: 523

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	248	199	32
Passenger Car/Van w/Trailer:	3	1	0
Pickup Truck/Utility Van:	66	56	16
Pickup Truck/Utility Van w/Trailer:	5	3	0
SUV:	136	133	22
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	24	21	4
School Bus < 15 People:	0	1	0
Non School Bus < 15 People:	0	1	0
Motorhome:	3	1	0
Motorcycle:	4	2	1
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	31	1	3
Other:	3	0	0
Unknown:	0	0	0

Total: 523 419 78

ADT: 157,400 Length: 1.31



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/02/2017

Job #: 20170202122330

Location: 25A **Begin: 157.70** **End: 158.50** **From: 01/01/2011** **To: 12/31/2015**

EXIT 158 - Baptist Road

Severity		
PDO:	31	
INJ:	8	10 :Injured
FAT:	0	0 :Killed
Total:	39	

Number of Vehicles	
One Vehicle:	12
Two Vehicles:	26
Three or More:	1
Unknown:	0
Total:	39

Location	
On Road:	28
Off Road:	11
Unknown:	0
Total:	39

Mainline/Ramps/Frontage Rds	
Mainline:	0
Ramps:	39
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	39

Lighting Conditions	
Daylight:	25
Dawn or Dusk:	0
Dark - Lighted:	9
Dark - Unlighted:	5
Unknown:	0
Total:	39

Crash Rates		
PDO:	0.24*	* Per MVMT
INJ:	0.06*	** Per 100 MVMT
FAT:	0.00**	
Total:	0.30*	

Crash Type	
Overtuning:	3
Other Non Collision:	0
Pedestrians:	0
Broadside:	1
Head On:	0
Rear End:	21
Sideswipe Same:	3
Sideswipe Opposite:	0
Approach Turn:	1
Overtaking Turn:	1
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	0
Fixed Objects:	9
Other Objects:	0
Unknown:	0
Total:	39

Weather Conditions	
None:	32
Rain:	2
Snow/Sleet/Hail:	5
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	39

Road Conditions	
Dry:	29
Wet:	3
Muddy:	0
Snowy:	0
Icy:	6
Slushy:	0
Foreign Material:	0
With Road Treatment:	1
Unknown:	0
Total:	39

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	20	12	0
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	5	5	0
Pickup Truck/Utility Van w/Trailer:	1	0	0
SUV:	9	8	1
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	0	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	3	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	1	0	0
Other:	0	0	0
Unknown:	0	0	0
Total:	39	27	1

ADT: 76,702 **Length: 0.91**



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

02/02/2017

Job #: 20170202121146

Location: 25A 105A Begin: 160.10 End: 160.80 From: 01/01/2011 To: 12/31/2015

EXIT 161 - SH 105A

Severity

PDO: 54
 INJ: 16 17 :Injured
 FAT: 2 2 :Killed

Total: 72

Number of Vehicles

One Vehicle: 52
 Two Vehicles: 20
 Three or More: 0
 Unknown: 0

Total: 72

Location

On Road: 24
 Off Road: 48
 Unknown: 0

Total: 72

Mainline/Ramps/Frontage Rds

Mainline: 0
 Ramps: 72
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 72

Lighting Conditions

Daylight: 45
 Dawn or Dusk: 10
 Dark - Lighted: 14
 Dark - Unlighted: 3
 Unknown: 0

Total: 72

Crash Rates

PDO: 0.57* * Per MVMT
 INJ: 0.17* ** Per 100 MVMT
 FAT: 2.11** **Total: 0.76***

Crash Type

Overtuning: 14
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 0
 Head On: 0
 Rear End: 11
 Sideswipe Same: 8
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 1
 Fixed Objects: 37
 Other Objects: 1
 Unknown: 0

Total: 72

Weather Conditions

None: 50
 Rain: 1
 Snow/Sleet/Hail: 19
 Fog: 0
 Dust: 0
 Wind: 2
 Unknown: 0

Total: 72

Road Conditions

Dry: 48
 Wet: 2
 Muddy: 0
 Snowy: 3
 Icy: 12
 Slushy: 1
 Foreign Material: 0
 With Road Treatment: 6
 Unknown: 0

Total: 72

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	30	10	0
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	6	2	0
Pickup Truck/Utility Van w/Trailer:	2	0	0
SUV:	17	5	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	15	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	0	1	0
Other:	0	0	0
Unknown:	0	0	0

Total: 72 20 0

ADT: 73,550 Length: 0.70



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/10/2017

Job #: 20170210102123

Location: 105A Begin: 4.89 End: 5.29 From:01/01/2011 To:12/31/2015

EXIT 161 - Crossroad and Ramp Terminal Intersections

Severity		
PDO:	63	
INJ:	30	46 :Injured
FAT:	0	0 :Killed
Total:	93	

Number of Vehicles	
One Vehicle:	7
Two Vehicles:	80
Three or More:	6
Unknown:	0
Total:	93

Location	
On Road:	89
Off Road:	4
Unknown:	0
Total:	93

Mainline/Ramps/Frontage Rds	
Mainline:	93
Ramps:	0
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	93

Lighting Conditions	
Daylight:	81
Dawn or Dusk:	2
Dark - Lighted:	9
Dark - Unlighted:	1
Unknown:	0
Total:	93

Crash Rates		
PDO:	5.74*	* Per MVMT
INJ:	2.73*	** Per 100 MVMT
FAT:	0.00**	
Total:	8.47 *	

Crash Type	
Overtuning:	3
Other Non Collision:	0
Pedestrians:	0
Broadside:	4
Head On:	0
Rear End:	70
Sideswipe Same:	6
Sideswipe Opposite:	0
Approach Turn:	6
Overtaking Turn:	0
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	0
Fixed Objects:	3
Other Objects:	1
Unknown:	0
Total:	93

Weather Conditions	
None:	79
Rain:	7
Snow/Sleet/Hail:	4
Fog:	0
Dust:	0
Wind:	1
Unknown:	2
Total:	93

Road Conditions	
Dry:	79
Wet:	9
Muddy:	0
Snowy:	2
Icy:	3
Slushy:	0
Foreign Material:	0
With Road Treatment:	0
Unknown:	0
Total:	93

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	44	32	2
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	12	13	1
Pickup Truck/Utility Van w/Trailer:	0	1	0
SUV:	29	39	3
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	3	0	0
School Bus < 15 People:	0	1	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	3	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	0	0	0
Other:	2	0	0
Unknown:	0	0	0
Total:	93	86	6

ADT: 17,789 Length: 0.33



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/02/2017

Job #: 20170202121425

Location: 25A **Begin: 180.30** **End: 181.30** **From: 01/01/2011** **To: 12/31/2015**

EXIT 181 - Plum Creek Parkway

Severity		
PDO:	38	
INJ:	13	23 :Injured
FAT:	0	0 :Killed
Total:	51	

Number of Vehicles	
One Vehicle:	8
Two Vehicles:	42
Three or More:	1
Unknown:	0
Total:	51

Location	
On Road:	43
Off Road:	8
Unknown:	0
Total:	51

Mainline/Ramps/Frontage Rds	
Mainline:	0
Ramps:	46
Frontage/Ramp Intsx:	4
Frontage Roads:	5
HOV Lanes:	0
Unknown:	0
Total:	51

Lighting Conditions	
Daylight:	39
Dawn or Dusk:	4
Dark - Lighted:	5
Dark - Unlighted:	3
Unknown:	0
Total:	51

Crash Rates		
PDO:	0.32*	* Per MVMT
INJ:	0.11*	** Per 100 MVMT
FAT:	0.00**	
Total:	0.43	*

Crash Type	
Overtuning:	1
Other Non Collision:	0
Pedestrians:	0
Broadside:	7
Head On:	1
Rear End:	17
Sideswipe Same:	8
Sideswipe Opposite:	1
Approach Turn:	8
Overtaking Turn:	0
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	0
Fixed Objects:	8
Other Objects:	0
Unknown:	0
Total:	51

Weather Conditions	
None:	44
Rain:	1
Snow/Sleet/Hail:	5
Fog:	1
Dust:	0
Wind:	0
Unknown:	0
Total:	51

Road Conditions	
Dry:	43
Wet:	2
Muddy:	0
Snowy:	4
Icy:	0
Slushy:	1
Foreign Material:	0
With Road Treatment:	1
Unknown:	0
Total:	51

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	23	21	1
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	6	6	0
Pickup Truck/Utility Van w/Trailer:	1	0	0
SUV:	15	13	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	3	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	4	0	0
Other:	0	0	0
Unknown:	0	0	0
Total:	51	43	1

ADT: 67,842 **Length: 0.96**



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

02/02/2017

Job #: 20170202121754

Location: 25A Begin: 181.60 End: 182.10 From: 01/01/2011 To: 12/31/2015

EXIT 182 - Wolfensberger Road

Severity

PDO: 56
 INJ: 10 15 :Injured
 FAT: 0 0 :Killed

Total: 66

Number of Vehicles

One Vehicle: 4
 Two Vehicles: 61
 Three or More: 1
 Unknown: 0

Total: 66

Location

On Road: 62
 Off Road: 4
 Unknown: 0

Total: 66

Mainline/Ramps/Frontage Rds

Mainline: 0
 Ramps: 66
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 66

Lighting Conditions

Daylight: 45
 Dawn or Dusk: 3
 Dark - Lighted: 14
 Dark - Unlighted: 4
 Unknown: 0

Total: 66

Crash Rates

PDO: 0.66* * Per MVMT
 INJ: 0.12* ** Per 100 MVMT
 FAT: 0.00** **Total: 0.78***

Crash Type

Overtuning: 0
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 16
 Head On: 0
 Rear End: 19
 Sideswipe Same: 8
 Sideswipe Opposite: 1
 Approach Turn: 17
 Overtaking Turn: 1
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 0
 Fixed Objects: 4
 Other Objects: 0
 Unknown: 0

Total: 66

Weather Conditions

None: 58
 Rain: 4
 Snow/Sleet/Hail: 4
 Fog: 0
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 66

Road Conditions

Dry: 56
 Wet: 5
 Muddy: 0
 Snowy: 2
 Icy: 1
 Slushy: 1
 Foreign Material: 1
 With Road Treatment: 0
 Unknown: 0

Total: 66

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	32	34	1
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	8	9	0
Pickup Truck/Utility Van w/Trailer:	1	0	0
SUV:	23	17	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	0	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	1	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	1	0	0
Other:	0	0	0
Unknown:	0	0	0

Total: 66 62 1

ADT: 79,818 Length: 0.58



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/24/2017

Job #: 20170224135123

Location: 25A **Begin: 183.80** **End: 184.80** **From:01/01/2011** **To:12/31/2015**

EXIT 184 - SH 85B / SH 86B (Founders Parkway)

Severity	
PDO:	50
INJ:	12 17 :Injured
FAT:	0 0 :Killed
Total:	62

Number of Vehicles	
One Vehicle:	27
Two Vehicles:	34
Three or More:	1
Unknown:	0
Total:	62

Location	
On Road:	38
Off Road:	24
Unknown:	0
Total:	62

Mainline/Ramps/Frontage Rds	
Mainline:	0
Ramps:	62
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	62

Lighting Conditions	
Daylight:	44
Dawn or Dusk:	0
Dark - Lighted:	11
Dark - Unlighted:	7
Unknown:	0
Total:	62

Crash Rates	
PDO:	0.25* * Per MVMT
INJ:	0.06* ** Per 100 MVMT
FAT:	0.00**
Total:	0.32*

Crash Type	
Overtuning:	4
Other Non Collision:	0
Pedestrians:	0
Broadside:	2
Head On:	0
Rear End:	25
Sideswipe Same:	8
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	0
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	2
Fixed Objects:	21
Other Objects:	0
Unknown:	0
Total:	62

Weather Conditions	
None:	49
Rain:	1
Snow/Sleet/Hail:	12
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	62

Road Conditions	
Dry:	43
Wet:	6
Muddy:	0
Snowy:	5
Icy:	5
Slushy:	1
Foreign Material:	0
With Road Treatment:	2
Unknown:	0
Total:	62

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	28	14	1
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	7	8	0
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	20	8	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	5	0
School Bus < 15 People:	1	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	3	0	0
Other:	1	0	0
Unknown:	0	0	0
Total:	62	35	1

ADT: 102,681 Length: 1.04



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/24/2017

Job #: 20170224135537

Location: 85B Begin: 184.67 End: 184.80 From: 01/01/2011 To: 12/31/2015

EXIT 184 - Crossroad and Ramp Terminal Intersections

Severity

PDO: 117
 INJ: 21 25 :Injured
 FAT: 0 0 :Killed

Total: 138

Number of Vehicles

One Vehicle: 5
 Two Vehicles: 117
 Three or More: 16
 Unknown: 0

Total: 138

Location

On Road: 132
 Off Road: 6
 Unknown: 0

Total: 138

Mainline/Ramps/Frontage Rds

Mainline: 137
 Ramps: 1
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 138

Lighting Conditions

Daylight: 114
 Dawn or Dusk: 2
 Dark - Lighted: 18
 Dark - Unlighted: 3
 Unknown: 1

Total: 138

Crash Rates

PDO: 14.79* * Per MVMT
 INJ: 2.65* ** Per 100 MVMT
 FAT: 0.00** **Total: 17.45***

Crash Type

Overtuning: 2
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 28
 Head On: 0
 Rear End: 86
 Sideswipe Same: 14
 Sideswipe Opposite: 1
 Approach Turn: 0
 Overtaking Turn: 1
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 0
 Fixed Objects: 6
 Other Objects: 0
 Unknown: 0

Total: 138

Weather Conditions

None: 114
 Rain: 8
 Snow/Sleet/Hail: 16
 Fog: 0
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 138

Road Conditions

Dry: 110
 Wet: 15
 Muddy: 0
 Snowy: 6
 Icy: 7
 Slushy: 0
 Foreign Material: 0
 With Road Treatment: 0
 Unknown: 0

Total: 138

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	65	63	8
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	23	15	3
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	47	52	5
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	1	1	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	1	1	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	1	0	0
Other:	0	1	0
Unknown:	0	0	0

Total: 138 133 16

ADT: 38,000 Length: 0.11



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/24/2017

Job #: 20170224135715

Location: 86B Begin: 100.00 End: 100.12 From: 01/01/2011 To: 12/31/2015

EXIT 184 - Crossroad and Ramp Terminal Intersections

Severity

PDO: 49
 INJ: 9 14 :Injured
 FAT: 0 0 :Killed

Total: 58

Number of Vehicles

One Vehicle: 3
 Two Vehicles: 47
 Three or More: 8
 Unknown: 0

Total: 58

Location

On Road: 55
 Off Road: 3
 Unknown: 0

Total: 58

Mainline/Ramps/Frontage Rds

Mainline: 57
 Ramps: 1
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 58

Lighting Conditions

Daylight: 46
 Dawn or Dusk: 5
 Dark - Lighted: 7
 Dark - Unlighted: 0
 Unknown: 0

Total: 58

Crash Rates

PDO: 5.11 * * Per MVMT
 INJ: 0.94 * ** Per 100 MVMT
 FAT: 0.00 ** **Total: 6.05 ***

Crash Type

Overtuning: 0
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 12
 Head On: 0
 Rear End: 34
 Sideswipe Same: 9
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 0
 Fixed Objects: 3
 Other Objects: 0
 Unknown: 0

Total: 58

Weather Conditions

None: 50
 Rain: 3
 Snow/Sleet/Hail: 5
 Fog: 0
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 58

Road Conditions

Dry: 46
 Wet: 8
 Muddy: 0
 Snowy: 3
 Icy: 1
 Slushy: 0
 Foreign Material: 0
 With Road Treatment: 0
 Unknown: 0

Total: 58

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	22	20	5
Passenger Car/Van w/Trailer:	1	0	0
Pickup Truck/Utility Van:	9	9	1
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	20	25	2
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	1	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	4	0	0
Other:	0	0	0
Unknown:	0	0	0

Total: 58 55 8

ADT: 41,000 Length: 0.12



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/02/2017

Job #: 20170202122437

Location: 25A **Begin: 188.10** **End: 188.90** **From: 01/01/2011** **To: 12/31/2015**

EXIT 188 - Castle Pines Parkway

Severity

PDO: 24
 INJ: 9 15 :Injured
 FAT: 0 0 :Killed

Total: 33

Number of Vehicles

One Vehicle: 6
 Two Vehicles: 27
 Three or More: 0
 Unknown: 0

Total: 33

Location

On Road: 28
 Off Road: 5
 Unknown: 0

Total: 33

Mainline/Ramps/Frontage Rds

Mainline: 0
 Ramps: 33
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 33

Lighting Conditions

Daylight: 26
 Dawn or Dusk: 1
 Dark - Lighted: 5
 Dark - Unlighted: 1
 Unknown: 0

Total: 33

Crash Rates

PDO: 0.14* * Per MVMT
 INJ: 0.05* ** Per 100 MVMT
 FAT: 0.00** **Total: 0.19***

Crash Type

Overtuning: 2
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 4
 Head On: 0
 Rear End: 15
 Sideswipe Same: 5
 Sideswipe Opposite: 0
 Approach Turn: 2
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 1
 Fixed Objects: 4
 Other Objects: 0
 Unknown: 0

Total: 33

Weather Conditions

None: 29
 Rain: 0
 Snow/Sleet/Hail: 4
 Fog: 0
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 33

Road Conditions

Dry: 24
 Wet: 2
 Muddy: 0
 Snowy: 2
 Icy: 3
 Slushy: 1
 Foreign Material: 0
 With Road Treatment: 1
 Unknown: 0

Total: 33

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	14	17	0
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	3	1	0
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	14	6	0
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	0	1	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	0	1	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	0	0
Other:	0	1	0
Unknown:	0	0	0

Total: 33 27 0

ADT: 114,017 Length: 0.82



Colorado Department of Transportation
 DiExSys™ Roadway Safety Systems
 General Summary of Crashes Report

02/02/2017

Job #: 20170202122905

Location: 25A Begin: 191.70 End: 192.45 From: 01/01/2011 To: 12/31/2015

EXIT 192 - Ridgeway Parkway

Severity

PDO: 68
 INJ: 8 12 :Injured
 FAT: 0 0 :Killed

Total: 76

Number of Vehicles

One Vehicle: 21
 Two Vehicles: 48
 Three or More: 7
 Unknown: 0

Total: 76

Location

On Road: 58
 Off Road: 18
 Unknown: 0

Total: 76

Mainline/Ramps/Frontage Rds

Mainline: 0
 Ramps: 76
 Frontage/Ramp Intsx: 0
 Frontage Roads: 0
 HOV Lanes: 0
 Unknown: 0

Total: 76

Lighting Conditions

Daylight: 48
 Dawn or Dusk: 3
 Dark - Lighted: 14
 Dark - Unlighted: 11
 Unknown: 0

Total: 76

Crash Rates

PDO: 0.43* * Per MVMT
 INJ: 0.05* ** Per 100 MVMT
 FAT: 0.00** **Total: 0.48***

Crash Type

Overtuning: 4
 Other Non Collision: 0
 Pedestrians: 0
 Broadside: 2
 Head On: 0
 Rear End: 33
 Sideswipe Same: 17
 Sideswipe Opposite: 0
 Approach Turn: 0
 Overtaking Turn: 0
 Parked Motor Vehicle: 0
 Railway Vehicle: 0
 Bicycles: 0
 Domestic Animal: 0
 Wild Animal: 4
 Fixed Objects: 14
 Other Objects: 2
 Unknown: 0

Total: 76

Weather Conditions

None: 67
 Rain: 2
 Snow/Sleet/Hail: 7
 Fog: 0
 Dust: 0
 Wind: 0
 Unknown: 0

Total: 76

Road Conditions

Dry: 58
 Wet: 8
 Muddy: 0
 Snowy: 3
 Icy: 6
 Slushy: 1
 Foreign Material: 0
 With Road Treatment: 0
 Unknown: 0

Total: 76

Vehicle Types

	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	39	30	3
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	7	6	2
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	24	15	2
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	3	2	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	1	1	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	2	1	0
Other:	0	0	0
Unknown:	0	0	0

Total: 76 55 7

ADT: 115,948 Length: 0.74



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/02/2017

Job #: 20170202123445

Location: 25A **Begin: 192.40** **End: 193.50** **From: 01/01/2011** **To: 12/31/2015**

EXIT 193 - Lincoln Avenue

Severity	
PDO:	234
INJ:	29 37 :Injured
FAT:	0 0 :Killed
Total:	263

Number of Vehicles	
One Vehicle:	27
Two Vehicles:	187
Three or More:	49
Unknown:	0
Total:	263

Location	
On Road:	241
Off Road:	22
Unknown:	0
Total:	263

Mainline/Ramps/Frontage Rds	
Mainline:	0
Ramps:	263
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	263

Lighting Conditions	
Daylight:	209
Dawn or Dusk:	16
Dark - Lighted:	32
Dark - Unlighted:	6
Unknown:	0
Total:	263

Crash Rates	
PDO:	0.86* * Per MVMT
INJ:	0.11* ** Per 100 MVMT
FAT:	0.00**
Total:	0.96*

Crash Type	
Overtuning:	8
Other Non Collision:	2
Pedestrians:	1
Broadside:	6
Head On:	0
Rear End:	160
Sideswipe Same:	55
Sideswipe Opposite:	0
Approach Turn:	1
Overtaking Turn:	1
Parked Motor Vehicle:	2
Railway Vehicle:	0
Bicycles:	1
Domestic Animal:	0
Wild Animal:	1
Fixed Objects:	20
Other Objects:	5
Unknown:	0
Total:	263

Weather Conditions	
None:	222
Rain:	18
Snow/Sleet/Hail:	23
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	263

Road Conditions	
Dry:	208
Wet:	30
Muddy:	0
Snowy:	14
Icy:	6
Slushy:	2
Foreign Material:	0
With Road Treatment:	0
Unknown:	3
Total:	263

Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	138	118	19
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	32	34	7
Pickup Truck/Utility Van w/Trailer:	0	0	0
SUV:	75	79	20
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	3	1	1
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	6	1	0
Bicycle:	0	1	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	8	2	2
Other:	1	0	0
Unknown:	0	0	0
Total:	263	236	49

ADT: 136,345 **Length: 1.09**



Colorado Department of Transportation
DiExSys™ Roadway Safety Systems
General Summary of Crashes Report

02/02/2017

Job #: 20170202124044

Location: 25A **Begin: 193.80** **End: 194.31** **From: 01/01/2011** **To: 12/31/2015**

EXIT 194 - C-470 / E-470

Severity		
PDO:	55	
INJ:	15	19 :Injured
FAT:	0	0 :Killed
Total:	70	

Number of Vehicles	
One Vehicle:	35
Two Vehicles:	31
Three or More:	4
Unknown:	0
Total:	70

Location	
On Road:	33
Off Road:	37
Unknown:	0
Total:	70

Mainline/Ramps/Frontage Rds	
Mainline:	0
Ramps:	70
Frontage/Ramp Intsx:	0
Frontage Roads:	0
HOV Lanes:	0
Unknown:	0
Total:	70

Lighting Conditions	
Daylight:	48
Dawn or Dusk:	4
Dark - Lighted:	11
Dark - Unlighted:	7
Unknown:	0
Total:	70

Crash Rates		
PDO:	0.37*	* Per MVMT
INJ:	0.10*	** Per 100 MVMT
FAT:	0.00**	
Total:	0.48	*

Crash Type	
Overtuning:	8
Other Non Collision:	3
Pedestrians:	0
Broadside:	0
Head On:	0
Rear End:	14
Sideswipe Same:	13
Sideswipe Opposite:	0
Approach Turn:	0
Overtaking Turn:	0
Parked Motor Vehicle:	1
Railway Vehicle:	0
Bicycles:	0
Domestic Animal:	0
Wild Animal:	0
Fixed Objects:	29
Other Objects:	2
Unknown:	0
Total:	70

Weather Conditions	
None:	53
Rain:	1
Snow/Sleet/Hail:	16
Fog:	0
Dust:	0
Wind:	0
Unknown:	0
Total:	70

Road Conditions	
Dry:	43
Wet:	5
Muddy:	0
Snowy:	7
Icy:	12
Slushy:	2
Foreign Material:	1
With Road Treatment:	0
Unknown:	0
Total:	70

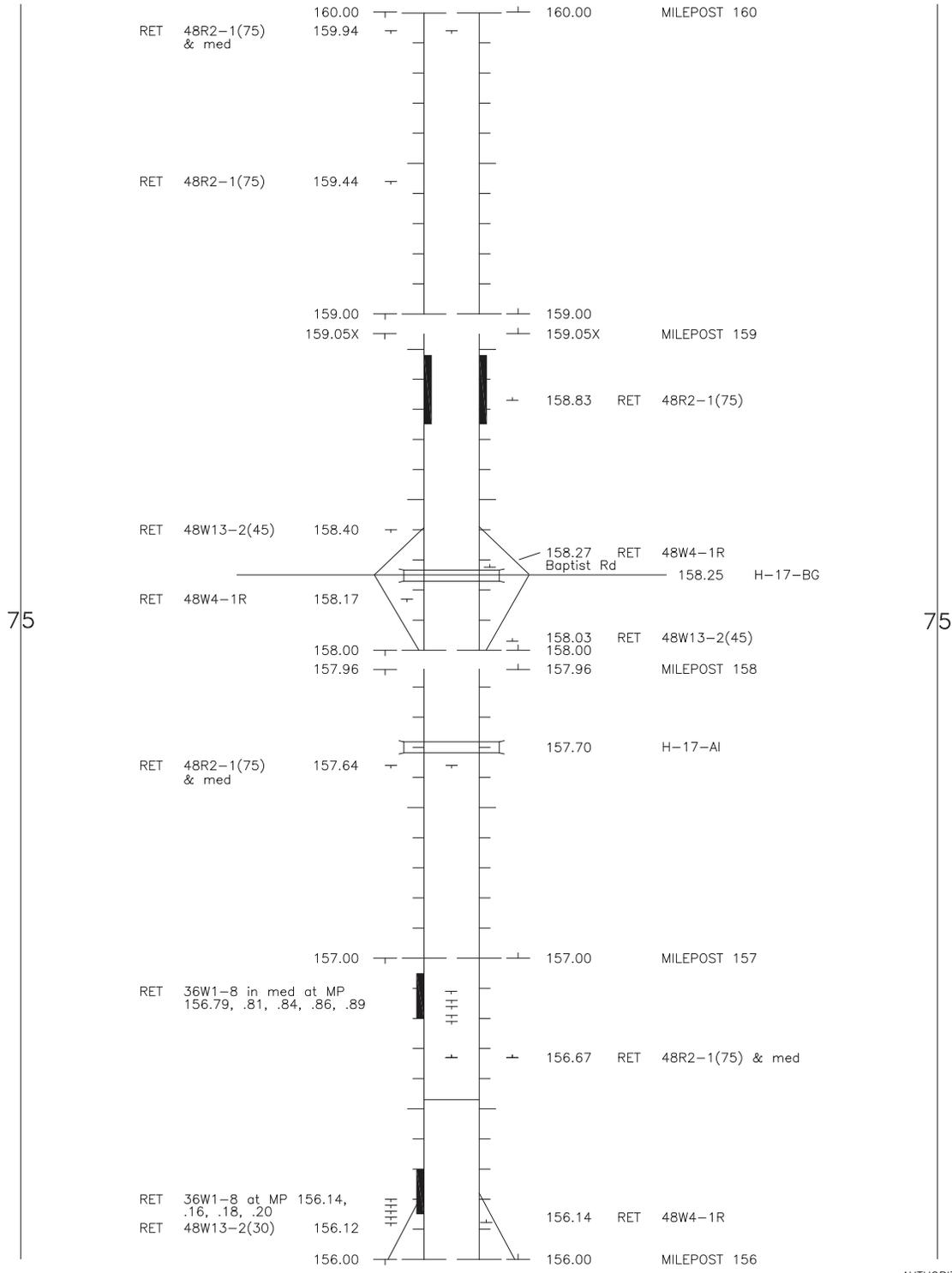
Vehicle Types	Vehicle 1	Vehicle 2	Vehicle 3
Passenger Car/Van:	38	11	1
Passenger Car/Van w/Trailer:	0	0	0
Pickup Truck/Utility Van:	3	12	0
Pickup Truck/Utility Van w/Trailer:	1	0	0
SUV:	18	12	3
SUV w/Trailer:	0	0	0
Truck 10k lbs or Less:	0	0	0
Trucks > 10k lbs/Busses > 15 People:	2	0	0
School Bus < 15 People:	0	0	0
Non School Bus < 15 People:	0	0	0
Motorhome:	0	0	0
Motorcycle:	3	0	0
Bicycle:	0	0	0
Motorized Bicycle:	0	0	0
Farm Equipment:	0	0	0
Hit and Run - Unknown:	4	0	0
Other:	1	0	0
Unknown:	0	0	0
Total:	70	35	4

ADT: 157,400 **Length: 0.51**

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



AUTHORIZED
SPEED
LIMITS



75

75

Traffic Engineer
Date: _____



AUTHORIZED
SPEED
LIMITS

SIGNLOG ON 10-13-02

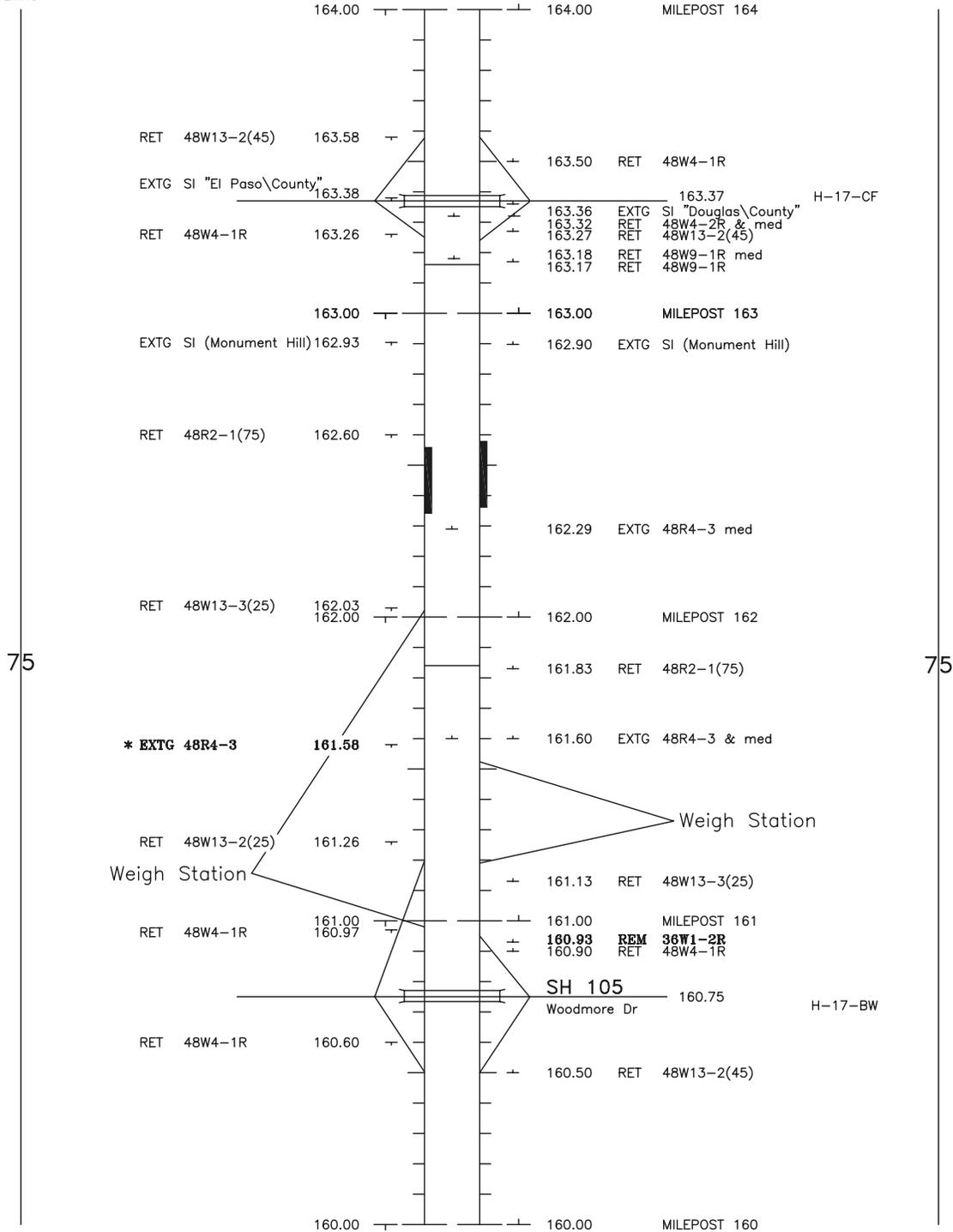
FILE	MEMO	SHEET	Implemented with changes as marked
880.025.03	6326	17	Date: _____ By: _____

SH 25 FROM SH 50 IN PUEBLO TO SH 85 IN CASTLE ROCK
SUPERSEDES SHEET 17, MEMO 6117, 8-08-00

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



AUTHORIZED
SPEED
LIMITS



AUTHORIZED
SPEED
LIMITS

Traffic Engineer

Date: _____



*MISSING ON 04-04-00
SIGNLOG ON 04-04-00

FILE	MEMO	SHEET	Implemented with changes as marked
880.025.03	6117	18	Date: _____ By: _____

SH 25 FROM SH 50 IN PUEBLO TO SH 85 N/O CASTLE ROCK

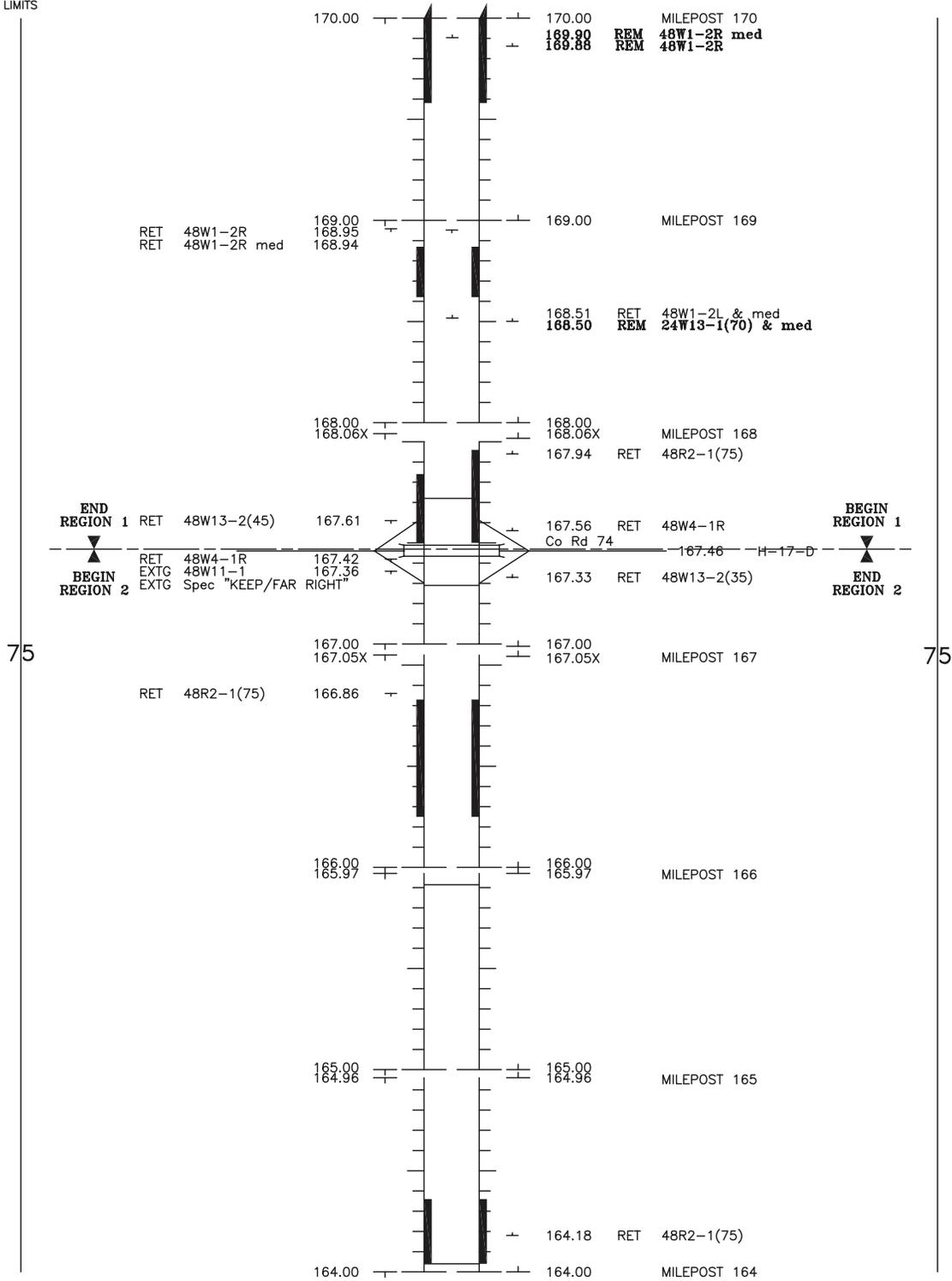
SUPERSEDES SHEET 18, MEMO 5932, 4-10-96

Version 96.11.1
Drawn By: CCK

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



AUTHORIZED
SPEED
LIMITS



75

75

Traffic Engineer _____

Date: _____



AUTHORIZED
SPEED
LIMITS

SIGNLOG ON 04-04-00

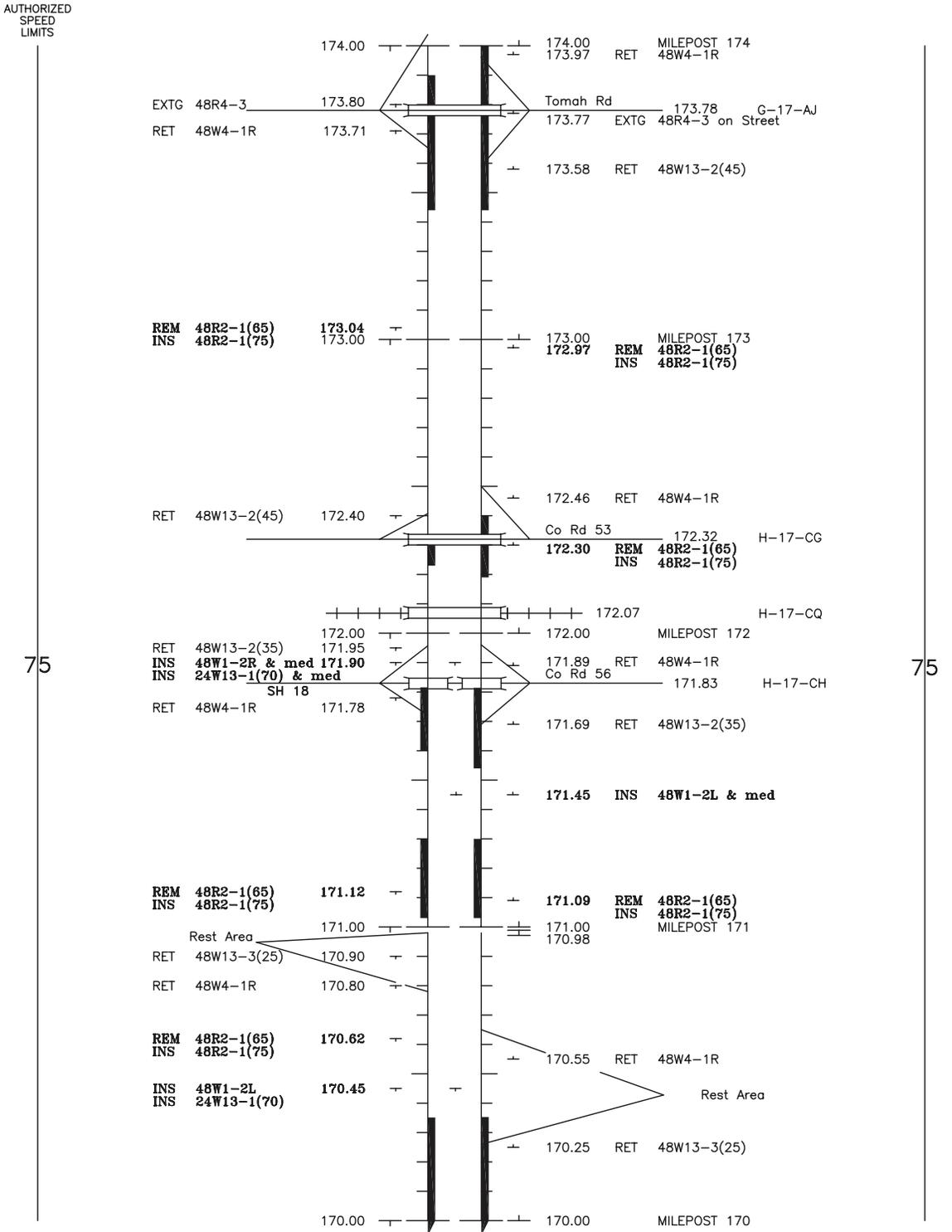
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880.025.03	6117	19	Date: _____ By: _____

SH 25 FROM SH 50 IN PUEBLO TO SH 85 N/O CASTLE ROCK

SUPERSEDES SHEET 19, MEMO 5932, 4-10-96

Version 96.11.1
Drawn By: CCK

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



Traffic Engineer
SIGNLOG ON N/A

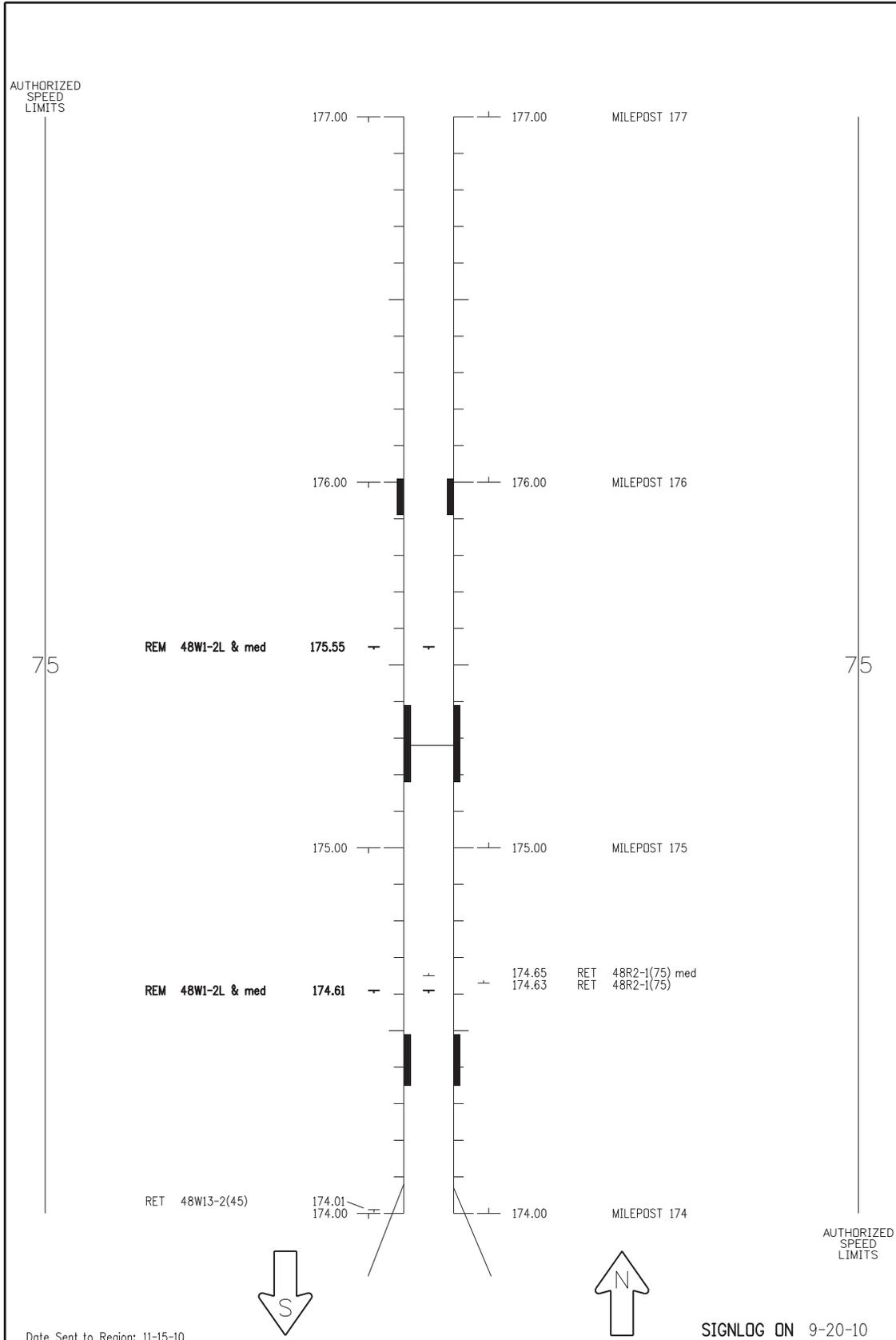
Date: _____
Version 96.11.1
Drawn By: CCK

FILE	MEMO	SHEET	Implemented with changes as marked
880.025.03	5932	20	Date: _____ By: _____

SH 25 FROM SH 50 IN PUEBLO TO SH 85 N/O CASTLE ROCK

SUPERSEDES SHEET 20, MEMO 5746, 2-22-94

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



Date Sent to Region: 11-15-10

SIGNLOG ON 9-20-10

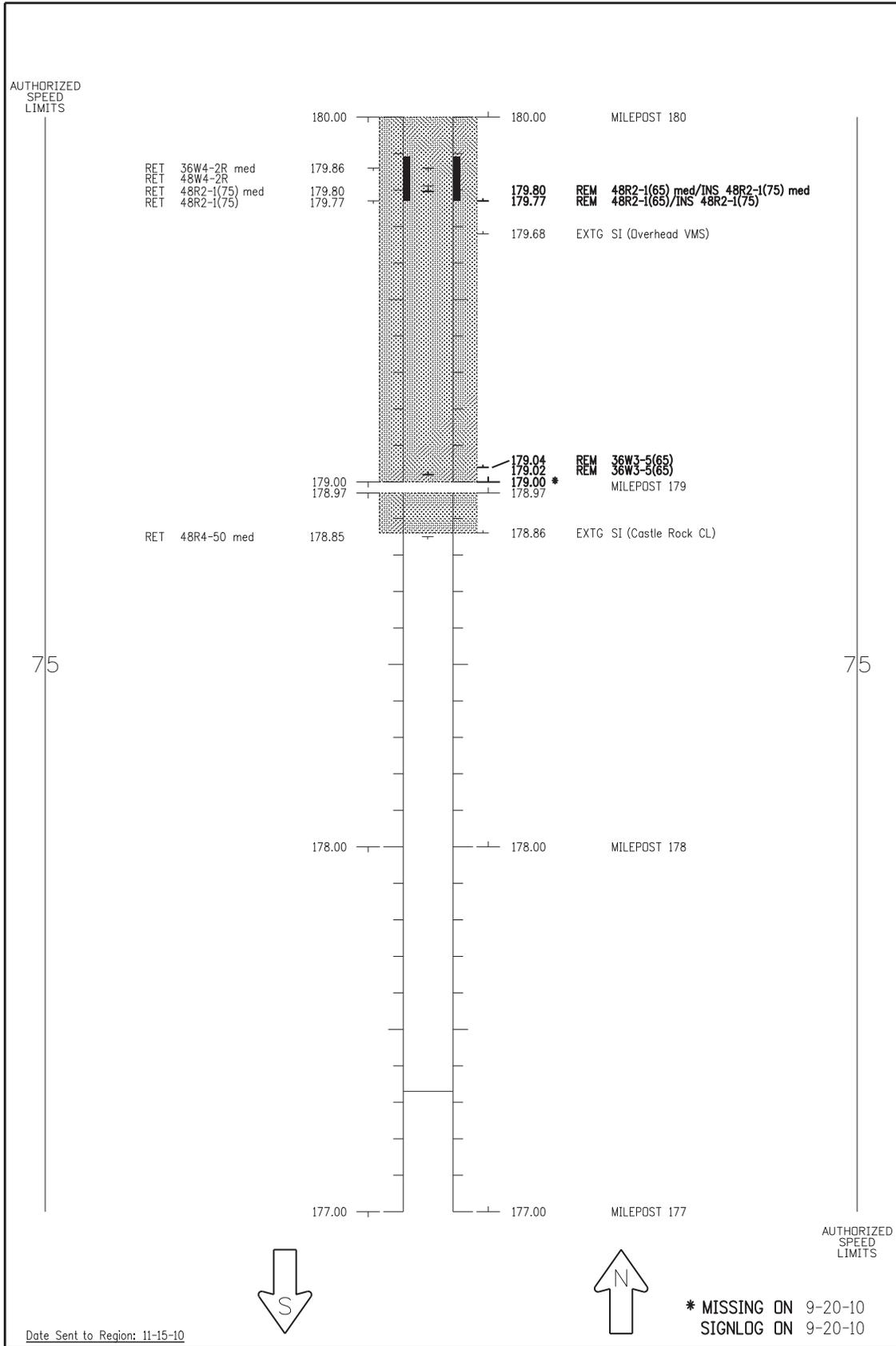
	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.03A	6661	21A	Date: _____ By: _____

SH 25A FROM SH 50A IN PUEBLO TO SH 85B IN CASTLE ROCK

SUPERSEDES PART OF SHEET 21, MEMO 6293, 9-11-02

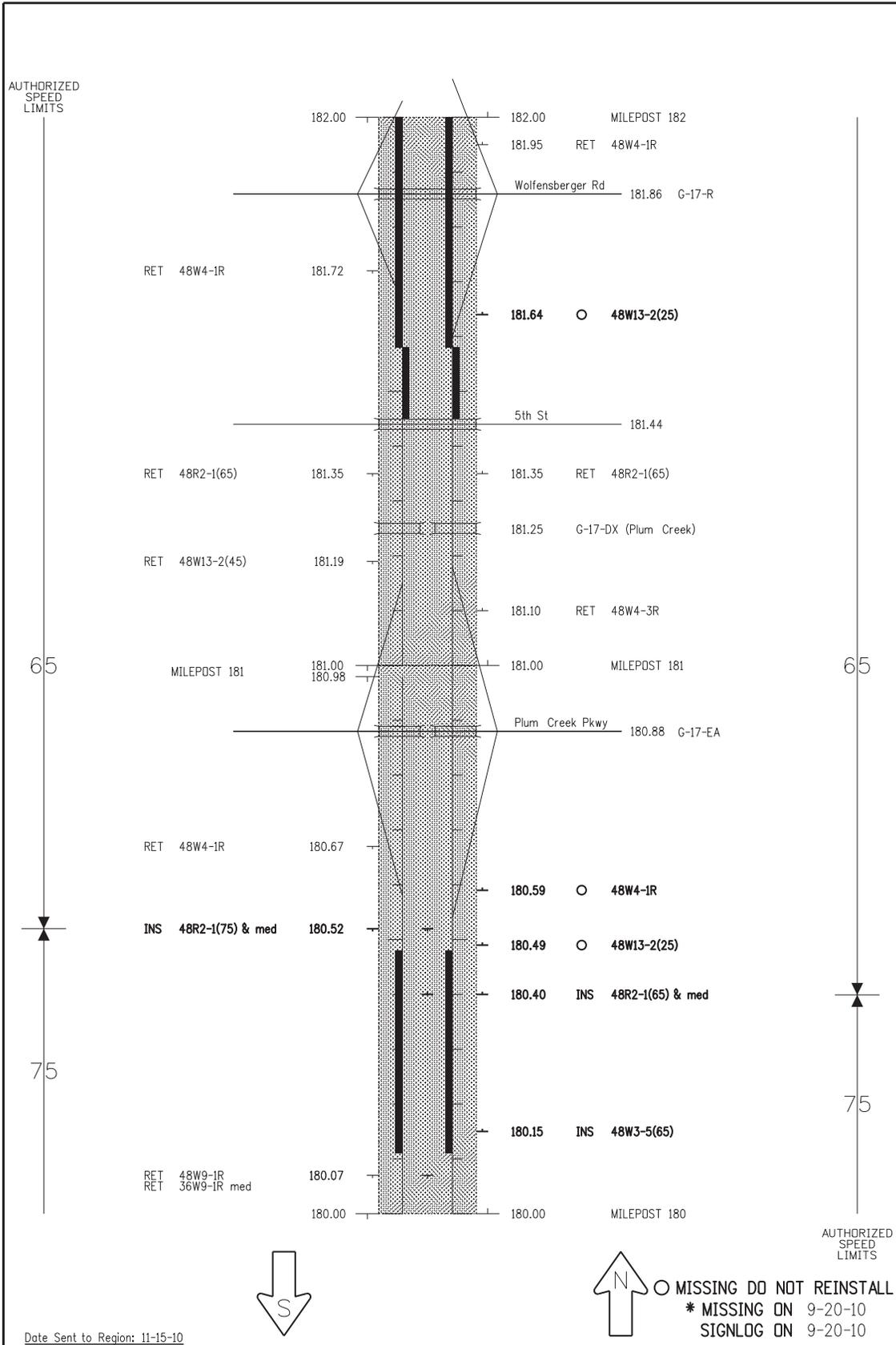
Version 2007.07.01
Drawn By: DML

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



Date Sent to Region: 11-15-10	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.03A	6661	21B	Date: _____ By: _____
SH 25A FROM SH 50A IN PUEBLO TO SH 85B IN CASTLE ROCK				
SUPERSEDES PART OF SHEET 21, MEMO 6293, 9-11-02				
				Version 2007.07.01 Drawn By: DML

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



Date Sent to Region: 11-15-10

	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.03A	6661	22A	Date: _____ By: _____

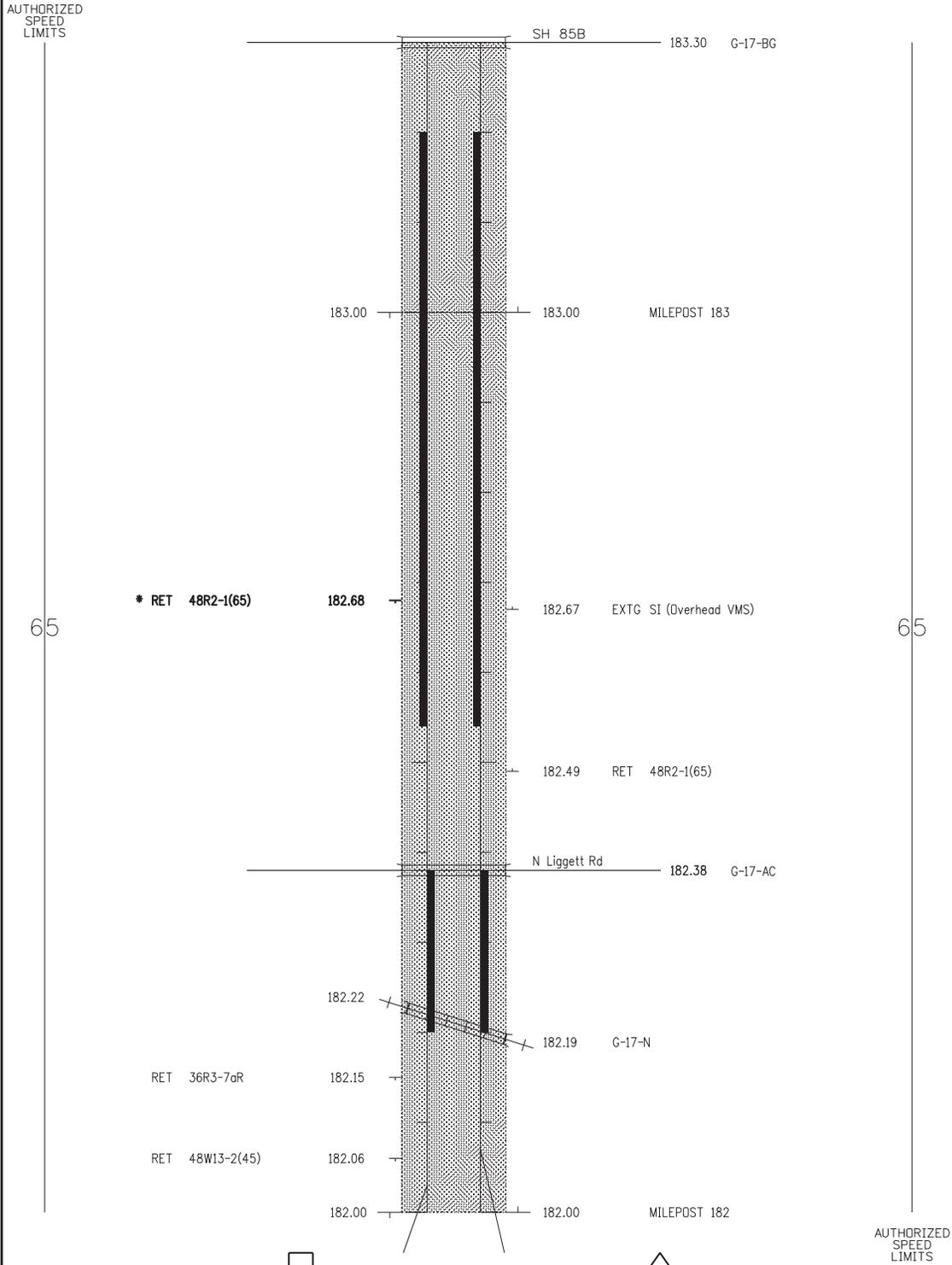
SH 25A FROM SH 50A IN PUEBLO TO SH 85B IN CASTLE ROCK

SUPERSEDES PART OF SHEET 22, MEMO 6293, 9-11-02

Version 2007.07.01
Drawn By: DML

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES

880.025.03A Ends at MP 183.30



Date Sent to Region: 11-15-10

* MISSING ON 9-20-10
SIGNLOG ON 9-20-10

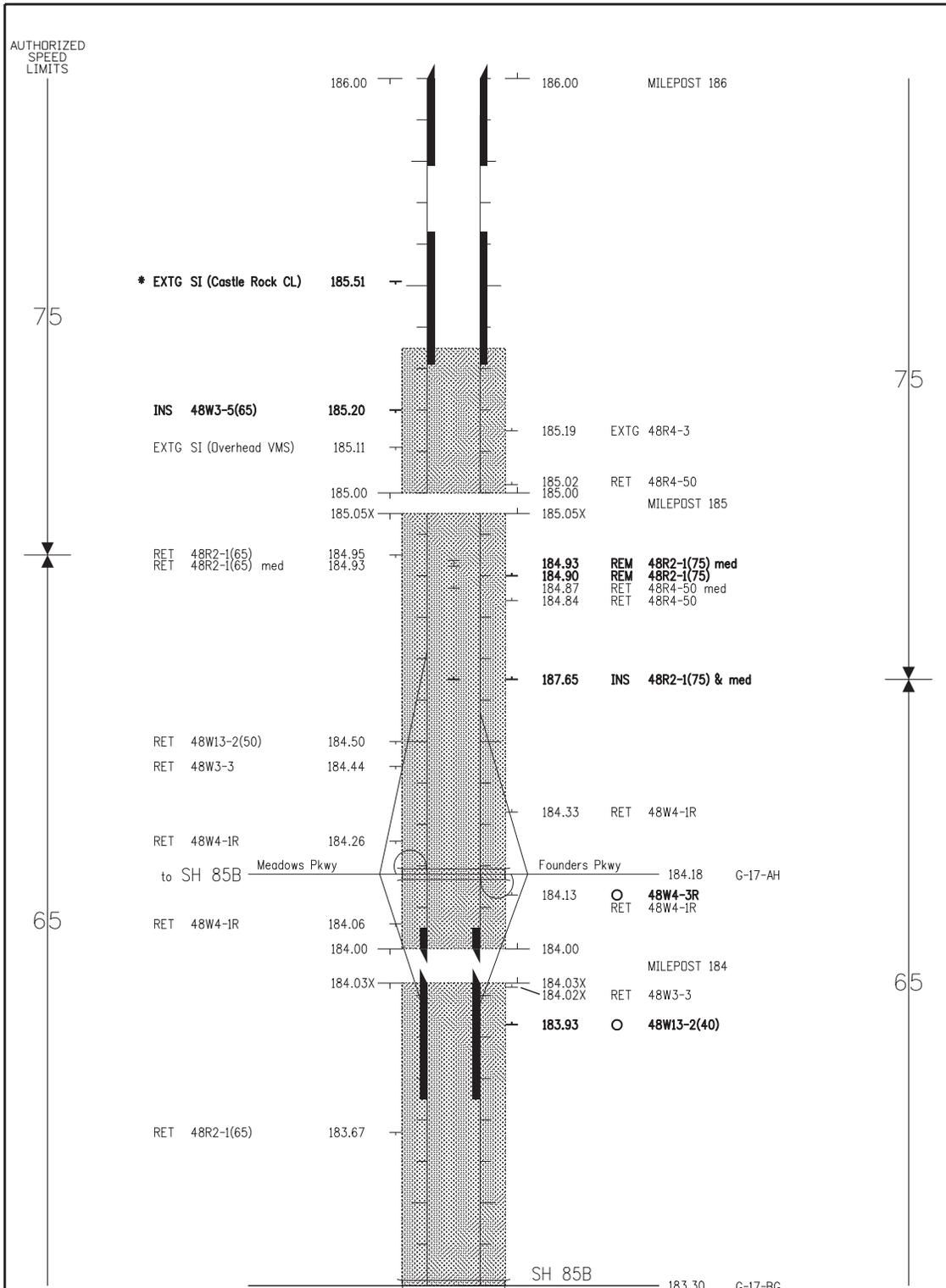
	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.03A	6661	22B	Date: _____ By: _____

SH 25A FROM SH 50A IN PUEBLO TO SH 85B IN CASTLE ROCK

SUPERSEDES PART OF SHEET 22, MEMO 6293, 9-11-02

Version 2007.07.01
Drawn By: DML

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



○ MISSING DO NOT REINSTALL
 * MISSING ON 9-20-10
 SIGNLOG ON 9-20-10

Date Sent to Region: 11-15-10

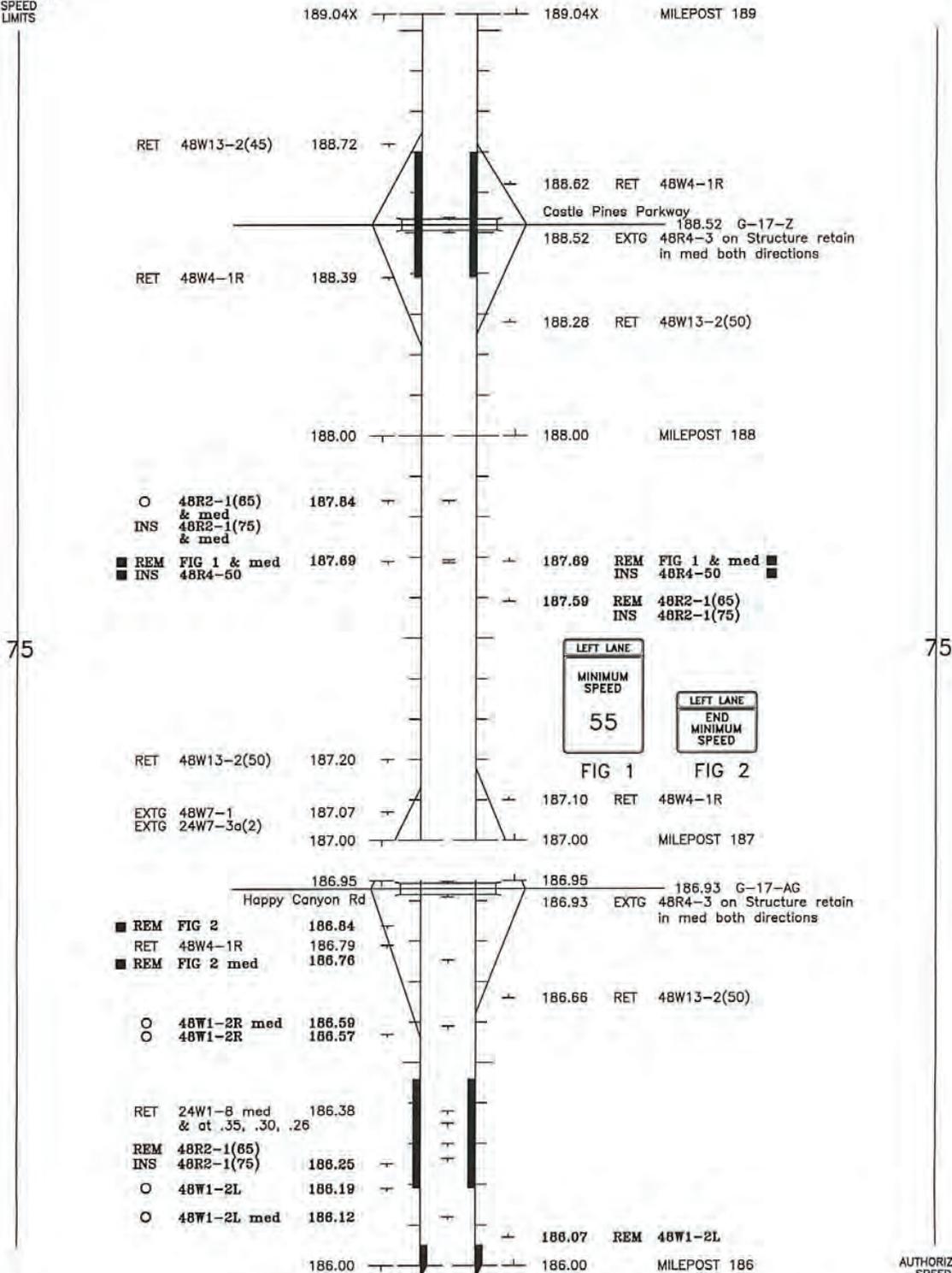
	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.04A	6661	1	Date: _____ By: _____

SH 25A FROM SH 50A IN PUEBLO TO SH 85B IN CASTLE ROCK
 SUPERSEDES SHEET 1, MEMO 6293R, 5-11-06

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



AUTHORIZED
SPEED
LIMITS



75

75



FIG 1 FIG 2

Traffic Engineer *V*
Date: *11 MAY 86*



■ PER REGION 3-27-06
○ MISSING DO NOT REINSTALL
*MISSING ON 4-29-02
SIGNLOG ON 4-29-02

FILE	MEMO	SHEET	Implemented with changes as marked
880.025.04	6293R	2	Date: _____ By: _____

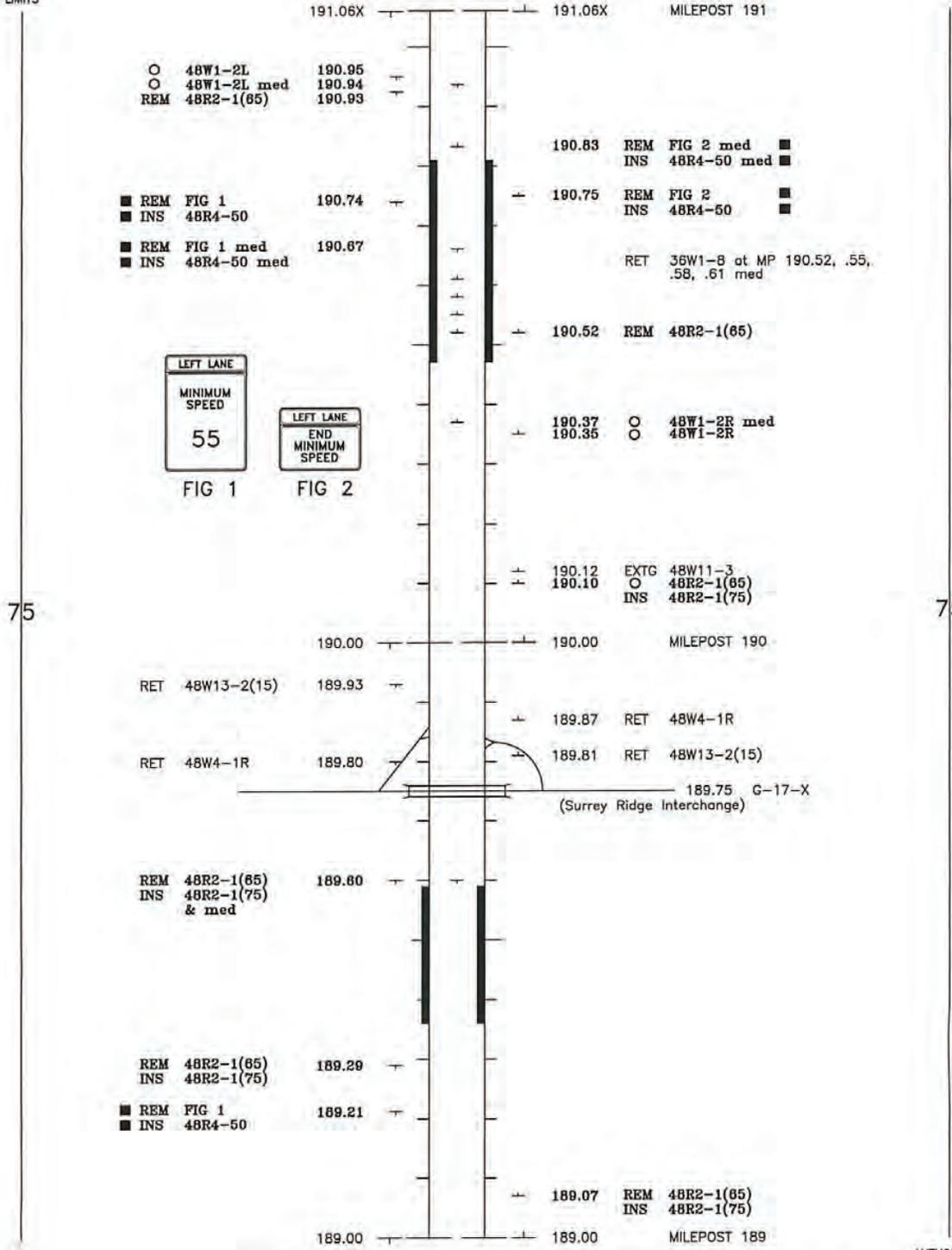
SH 25 FROM SH 85 N/O CASTLE ROCK TO DENVER

SUPERSEDES SHEET 2, MEMO 6000, 10-15-97

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



AUTHORIZED
SPEED
LIMITS



75

75

Traffic Engineer *V*
Date: *11 MAY 06*



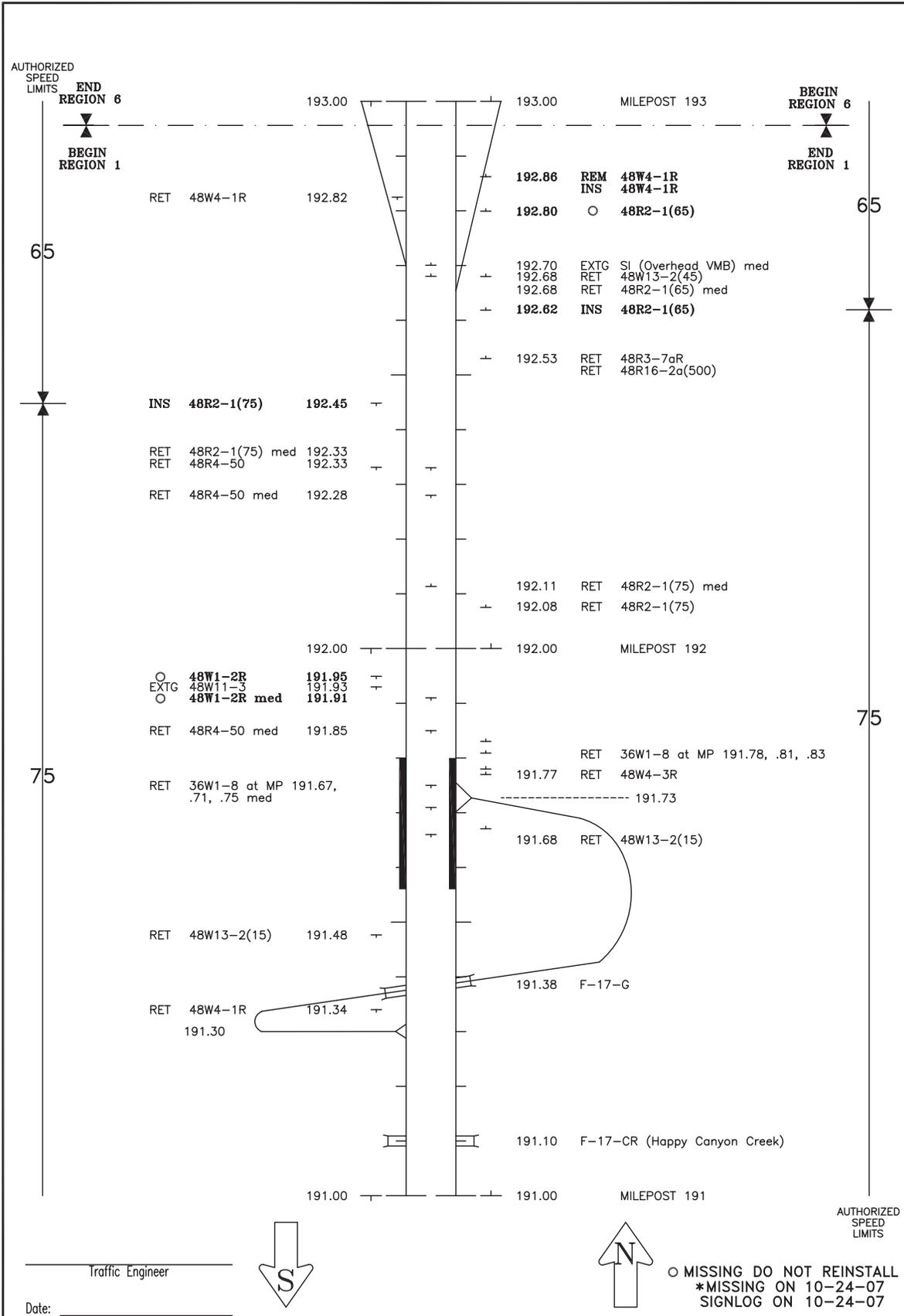
■ PER REGION 3-27-06
○ MISSING DO NOT REINSTALL
*MISSING ON 4-29-02
SIGNLOG ON 4-29-02

	FILE	MEMO	SHEET	Implemented with changes as marked
	880.025.04	6293R	3	Date: _____ By: _____

SH 25 FROM SH 85 N/O CASTLE ROCK TO DENVER N. CITY LIMIT

SUPERSEDES SHEET 3, MEMO 6000, 10-15-97

AUTHORIZED SPEED LIMITS AND RECOMMENDED TRAFFIC CONTROL DEVICES



Traffic Engineer _____
Date: _____



O MISSING DO NOT REINSTALL
*MISSING ON 10-24-07
SIGNLOG ON 10-24-07

FILE	MEMO	SHEET	Implemented with changes as marked
880.025.04	6502	4	Date: _____ By: _____

SH 25 FROM SH 85/SH 86 N/O CASTLE ROCK TO DENVER N CITY LIMIT

SUPERSEDES SHEET 4, MEMO 6293R, 05-11-06

Version 96.11.1
Drawn By: ATS

highway	milepoint	description	rcode	func_class	ptrucks	adt	adt_year
0025A	157.89	RAMPS OFF - (TO BAPTIST RD)	urban	Interstate	10.7	91,000	2014
0025A	158.00	MILEPOST 158	urban	Interstate	10.7	91,000	2014
0025A	158.02	RAMPS ON - (FROM BAPTIST RD)	urban	Interstate	10.7	91,000	2014
0025A	158.20	MAJOR STR (H-17-DC) -- RD E AND W (CO RD 32) (BAPTIST RD) -- UNDERPASS SEPARATION -- REPLACED HISTORICAL STRID (H-17-BG)	urban	Interstate	8.7	80,000	2014
0025A	158.29	RAMP ON - (FROM BAPTIST RD WB RAMP C) EXIT 158	urban	Interstate	8.7	80,000	2014
0025A	158.33	SB D RAMP OFF	urban	Interstate	8.7	80,000	2014
0025A	159.00	MILEPOST 159	urban	Interstate	8.7	80,000	2014
0025A	159.07	MINORSTR (025A159050BR) UNNAMED DRAINAGE	urban	Interstate	8.7	80,000	2014
0025A	159.34	MAJOR STR (H-17-BK) TEACHOUT CREEK	urban	Interstate	8.7	80,000	2014
0025A	159.82	SIGN BRIDGE STR (SIGN-H-17-CC) - I-25 NBND ML-VMS	urban	Interstate	8.7	80,000	2014
0025A	160.00	MILEPOST 160	urban	Interstate	8.7	80,000	2014
0025A	160.16	RAMP ON - (FROM SH 105A EB RAMP E) EXIT 5	urban	Interstate	8.7	80,000	2014
0025A	160.29	SIGN BRIDGE STR (SIGN-H-17-BE) - I-25 NBND ML-160.125	urban	Interstate	8.7	80,000	2014
0025A	160.40	RAMP OFF (TO 105A) NB	urban	Interstate	8.7	80,000	2014
0025A	160.50	RAMP OFF (TO 105A) SB	urban	Interstate	8.7	80,000	2014
0025A	160.58	MINORSTR (025A160600BR) UNNAMED DRAINAGE - NOW IS A MAJOR	urban	Interstate	8.7	80,000	2014
0025A	160.58	MAJOR STR (H-17-CA) ML AND (H-17-BB) E AND F RAMP STRUCTURES OVER DIRTY WOMAN CREEK TO THE WEST	urban	Interstate	8.7	80,000	2014
0025A	160.63	RAMP ON (FROM 105A) NB	urban	Interstate	8.7	80,000	2014
0025A	160.76	MONUMENT INTERCHANGE STR (H-17-CZ) -- JCT SH105A E AND W PALMER LAKE -- UNDERPASS SEPARATION -- REPLACED HISTORICAL STRID	urban	Interstate	6.6	63,000	2014
0025A	160.81	NB # LANES CHANGE TO 2	urban	Interstate	6.6	63,000	2015
0025A	161.00	MILEPOST 161	urban	Interstate	6.6	63,000	2014
0025A	161.23	RAMP OFF (WEIGH STATIONS)	urban	Interstate	6.6	63,000	2014
0025A	161.24	CHANGE PAVEMENT MATERIAL, AND SB # LANES CHANGE TO 3	urban	Interstate	6.6	63,000	2015
0025A	161.30	MINORSTR (025A161200BL) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	161.42	NB ON RAMP FROM WEIGH STATION, AND CHANGE NB # LANES TO 3	urban	Interstate	6.6	63,000	2015
0025A	161.44	RAMP OFF (TO MONUMENT WEIGH STATION) SB	urban	Interstate	6.6	63,000	2014
0025A	161.93	MONUMENT WEIGH STATION (SB RAMP OFF) PAVEMENT GORE POINT NOT PAINTED DIVIDE	urban	Interstate	6.6	63,000	2014
0025A	162.00	MILEPOST 162	urban	Interstate	6.6	63,000	2014
0025A	162.07	SIGN BRIDGE STR (SIGN-H-17-CX) - I-25 SBND ML + RAMP	urban	Interstate	6.6	63,000	2014
0025A	162.92	SIGN BRIDGE STR (SIGN-H-17-BF) - I-25 SBND TRUCK-CAMERA	urban	Interstate	6.6	63,000	2014
0025A	163.00	BLACK FOREST DIVIDE PASS SUMMIT - ELEVATION 7,343 FT - GRID CELL (H-17) -- MILEPOST 163	urban	Interstate	6.6	63,000	2014
0025A	163.17	RAMP ON - (FROM CO RD 404 RAMP E ENTER EL PASO COUNTY) EXIT 163	urban	Interstate	6.6	63,000	2014
0025A	163.24	RAMP OFF - (TO CO RD 404 RAMP B ENTER DOUGLAS COUNTY) EXIT 163	urban	Interstate	6.6	63,000	2014
0025A	163.32	PALMER DIVIDE INTERCHANGE STR (H-17-CF) - RD E AND W (PALMER DIVIDE AVE) (OLD COUNTY LINE RD) (CO RD 404) - AT MONUMENT HILL F	urban	Interstate	6.6	63,000	2014
0025A	163.48	RAMP ON - (FROM CO RD 404 RAMP C ENTER DOUGLAS COUNTY) EXIT 163	urban	Interstate	6.6	63,000	2014
0025A	163.49	RAMP OFF - (TO CO RD 404 RAMP D ENTER EL PASO COUNTY) EXIT 163	urban	Interstate	6.6	63,000	2014
0025A	164.00	MILEPOST 164	urban	Interstate	6.6	63,000	2014
0025A	164.08	MINORSTR (025A164080BL) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	164.77	MINORSTR (025A164760BL) DRAW DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	165.00	MILEPOST 165	urban	Interstate	6.6	63,000	2014
0025A	165.93	MINORSTR (025A165880BR) STOCK PASS	urban	Interstate	6.6	63,000	2014
0025A	166.00	MILEPOST 166	urban	Interstate	6.6	63,000	2014
0025A	167.00	MILEPOST 167	urban	Interstate	6.6	63,000	2014
0025A	167.30	MAJOR STR (H-17-CP) BR OF EAST PLUM CREEK -- OFFSET STRID (H-17-AJ) FRONTAGE RD	urban	Interstate	6.6	63,000	2014
0025A	167.41	RAMP ON AND OFF	urban	Interstate	6.6	63,000	2014
0025A	167.46	GREENLAND INTERCHANGE MINORSTR (025A167404BR) RD E AND W (GREENLAND RD) (CR 74) -- OVERPASS SEPARATION	urban	Interstate	6.6	63,000	2014
0025A	167.50	REGIONAL SECTION POLITICAL BOUNDARY OR DISTRICT CLASSIFICATION CHANGE	urban	Interstate	6.6	63,000	2014
0025A	167.59	RAMPS ON AND OFF	urban	Interstate	6.6	63,000	2014
0025A	167.68	SIGN BRIDGE STR (SIGN-H-17-D) - I-25 SBND ML - VMS	urban	Interstate	6.6	63,000	2014
0025A	167.69	MINORSTR (025A167680BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	168.00	MILEPOST 168	urban	Interstate	6.6	63,000	2014
0025A	168.34	MINORSTR (025A168330BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	168.51	MINORSTR (025A168510BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	168.57	MINORSTR (025A168860BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	169.00	MINORSTR (006F227320BL) DRAINAGE -- MILEPOST 169	urban	Interstate	6.6	63,000	2014

highway	milepoint	description	rucode	func_class	p trucks	adt	adt year
0025A	169.02	MINORSTR (025A169010BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	169.31	MINORSTR (025A169320BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	169.52	CHANGE MEDIAN WIDTH	urban	Interstate	6.6	63,000	2014
0025A	170.00	MILEPOST 170	urban	Interstate	6.6	63,000	2014
0025A	170.27	RD NE (TO REST AREA RT)	urban	Interstate	6.6	63,000	2014
0025A	170.61	RD SE (FROM REST AREA RT)	urban	Interstate	6.6	63,000	2014
0025A	170.71	RD NW(FROM REST AREA LT)	urban	Interstate	6.6	63,000	2014
0025A	170.89	RD SW (TO REST AREA LT)	urban	Interstate	6.6	63,000	2014
0025A	171.00	MILEPOST 171	urban	Interstate	6.6	63,000	2014
0025A	171.46	MINORSTR (025A171430BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	171.68	RAMP OFF	urban	Interstate	6.6	63,000	2014
0025A	171.71	RAMP ON	urban	Interstate	6.6	63,000	2014
0025A	171.78	MINORSTR (025A171820BR) UNNAMED DRAINAGE	urban	Interstate	6.6	63,000	2014
0025A	171.82	SOUTH LAKE GULCH ROAD INTERCHANGE STRS (H-17-CH) NB AND (H-17-CI) SB - JCT SH 18 WEST (LARKSPUR) - RD E (CO RD 56) OVERPASS S	urban	Interstate	6.6	63,000	2014
0025A	171.95	RAMP OFF (TO UPPER LAKE GULCH RD) SB	urban	Interstate	6.6	63,000	2014
0025A	171.97	RAMP ON (FROM UPPER LAKE GULCH RD) NB	urban	Interstate	6.6	63,000	2014
0025A	172.00	MILEPOST 172	urban	Interstate	6.6	63,000	2014
0025A	172.02	MAJOR STR (H-17-CQ)OVERPASS D AND RGW RR (253-069U)	urban	Interstate	6.6	63,000	2014
0025A	172.19	MAJOR STR (H-17-AH) EAST PLUM CREEK -- REPLACED HISTORICAL STRID (H-17-AQ)	urban	Interstate	6.6	63,000	2014
0025A	172.31	LARKSPUR INTERCHANGE STR (H-17-CG) RD NE (RAMP OFF) AND SW (CO 53) UNDERPASS SEPARATION	urban	Interstate	6.4	65,000	2014
0025A	172.38	RAMP OFF (TO SPRUCE MOUNTAIN RD) SB	urban	Interstate	6.4	65,000	2014
0025A	172.48	G RAMP ON (FROM SPRUCE MOUNTAIN RD) NB	urban	Interstate	6.4	65,000	2014
0025A	173.00	MILEPOST 173	urban	Interstate	6.4	65,000	2014
0025A	173.23	MINORSTR (025A173200BL) UNNAMED DRAINAGE	urban	Interstate	6.4	65,000	2014
0025A	173.60	RAMP OFF	urban	Interstate	6.4	65,000	2014
0025A	173.66	RAMP ON	urban	Interstate	6.4	65,000	2014
0025A	173.79	SKYVIEW/BEARDANCE INTERCHANGE STR (G-17-AJ) - RD (FRONTAGE - RD RT AND LT) - RD E AND W (SKYVIEW) (BEARDANCE) KOA (OLD TOMA	urban	Interstate	6	68,000	2014
0025A	173.85	MINORSTR (025A173860BL) DRAINAGE	urban	Interstate	6	68,000	2014
0025A	174.00	MILEPOST 174	urban	Interstate	6	68,000	2014
0025A	174.06	RAMPS ON AND OFF - (TO/FROM SKYVIEW/BEAR DANCE RD) EXIT 74	urban	Interstate	6	68,000	2014
0025A	174.62	MINORSTR (025A174620BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	174.85	MINORSTR (025A175250BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	175.00	MILEPOST 175	urban	Interstate	6	68,000	2014
0025A	175.86	RD W (TOMAH RD)	urban	Interstate	6	68,000	2014
0025A	175.89	MINORSTR (025A175890BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	176.00	MILEPOST 176	urban	Interstate	6	68,000	2014
0025A	176.36	MINORSTR (025A176470BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	176.49	MINORSTR (025A176700BL) STOCK PASS	urban	Interstate	6	68,000	2014
0025A	177.00	MINORSTR (006G259820BL) DRAINAGE -- MILEPOST 177	urban	Interstate	6	68,000	2014
0025A	177.37	MINORSTR (025A177400BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	177.87	MINORSTR (025A177870BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	178.00	MILEPOST 178	urban	Interstate	6	68,000	2014
0025A	178.10	MINORSTR (025A178120BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	178.38	MINORSTR (025A178380BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	178.93	RD (FRONTAGE RD) (CRYSTAL VALLEY RD) - ENTER SMALL URBAN AREA - (TERRITORIAL RD)	urban	Interstate	6	68,000	2014
0025A	178.94	MINORSTR (025A178940BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	179.00	MILEPOST 179	urban	Interstate	6	68,000	2014
0025A	179.12	CHANGE PAVEMENT MATERIAL	urban	Interstate	6	68,000	2015
0025A	179.17	ADD NB LANE TO 3	urban	Interstate	6	68,000	2015
0025A	179.27	MINORSTR (025A179260BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	179.52	SIGN BRIDGE STR (SIGN-G-17-AV) - I-25 NBND ML - VMS-179.525	urban	Interstate	6	68,000	2014
0025A	179.58	MINORSTR (025A179570BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	179.68	SIGN BRIDGE STR (SIGN-G-18-BQ) - I-25 NBND ML-VMS-179.680	urban	Interstate	6	68,000	2014
0025A	179.79	SIGN BRIDGE STR (SIGN-G-18-BP) - I-25 MEDIAN-VMS-179.793, REMOVE SB LANE	urban	Interstate	6	68,000	2014
0025A	180.00	MILEPOST 180	urban	Interstate	6	68,000	2014

highway	milepoint	description	rucode	func_class	ptrucks	adt	adt_year
0025A	180.04	SIGN BRIDGE STR (SIGN-G-18-BR) - I-25 NBND ML-VMS-180.045	urban	Interstate	6	68,000	2014
0025A	180.19	MINORSTR (025A180600BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	180.29	MINORSTR (025A180700BL) UNNAMED DRAINAGE	urban	Interstate	6	68,000	2014
0025A	180.33	RAMP OFF - (TO WILCOX ST RAMP B) EXIT 181	urban	Interstate	6	68,000	2014
0025A	180.40	RD (BROOKSIDE CIR)	urban	Interstate	6	68,000	2014
0025A	180.52	RAMP ON - (FROM PLUM CREEK PKWY RAMP E) EXIT 181	urban	Interstate	6	68,000	2014
0025A	180.54	SIGN BRIDGE STR (SIGN-G-18-BS) POSTED SIGNAGE	urban	Interstate	6	68,000	2014
0025A	180.55	RAMP TERMINI OF NB OFF AND NB ON	urban	Interstate	6	68,000	2014
0025A	180.62	RAMP ON (FROM WILCOX ST TO NB I 25)	urban	Interstate	6	68,000	2014
0025A	180.63	RAMP ON (FROM PLUM CREEK PKWY TO SB I-25)	urban	Interstate	6	68,000	2014
0025A	180.69	RAMP ON - (FROM WILCOX ST) EXIT 181	urban	Interstate	6	68,000	2014
0025A	180.81	LEGACY PATROL BOUNDRY - HISTORICAL LOCATION OF (G-17-Q) AND (G-17-P) REMOVED	urban	Interstate	6	68,000	2014
0025A	180.88	EXIT 181 / CASTLE ROCK INTERCHANGE STRS (G-17-DZ) NB AND (G-17-EA) SB - RD NE ANS SE (WILCOX ST) OVERPASS SEPARATIONS - PLUM C	urban	Interstate	5.8	79,000	2014
0025A	181.00	MILEPOST 181	urban	Interstate	5.8	79,000	2014
0025A	181.19	SIGN BRIDGE STR (SIGN-G-18-BT) - I-25 SBND ML-VMS-181.185	urban	Interstate	5.8	79,000	2014
0025A	181.19	SIGN BRIDGE STR (SIGN-G-17-DP) - I-25 NBND ML-181.632	urban	Interstate	5.8	79,000	2014
0025A	181.23	MAJOR STRS (G-17-DX) NB (G-17-DY) SB PLUM CREEK - RAMPS ON AND OFF -- HISTORICAL STRIDS (G-17-U) AND (G-17-V)	urban	Interstate	5.8	79,000	2014
0025A	181.47	SIGN BRIDGE STR (SIGN-G-18-BU) - I-25 MEDIAN-VMS-181.467	urban	Interstate	5.8	79,000	2014
0025A	181.52	MAJOR STR (G-17-DA) - RD E AND W (FIFTH ST)	urban	Interstate	5.8	79,000	2014
0025A	181.55	ENTER CASTLE ROCK CITY LIMITS	urban	Interstate	5.8	79,000	2014
0025A	181.63	SIGN BRIDGE STR (SIGN-G-17-DB) - I-25 NBND ML + RAMP	urban	Interstate	5.8	79,000	2014
0025A	181.67	RAMP OFF - (TO WOLFENSBERGER RD RAMP B) EXIT 182	urban	Interstate	5.8	79,000	2014
0025A	181.68	SIGN BRIDGE STR (SIGN-G-18-BV) - I-25 SBND ML-181.681	urban	Interstate	5.8	79,000	2014
0025A	181.72	RAMP ON - (FROM WOLFENSBERGER RD RAMP E) EXIT 182	urban	Interstate	5.8	79,000	2014
0025A	181.74	RAMP OFF	urban	Interstate	5.8	79,000	2014
0025A	181.81	MINORSTR (025A181750BR) UNNAMED DRAINAGE	urban	Interstate	5.8	79,000	2014
0025A	181.85	WOLFENSBERGER INTERCHANGE STRS (G-17-BH) AND (G-17-DF) - RD E AND W (WOLFENSBERGER RD) -- REPLACED HISTORICAL STRID (G-17	urban	Interstate	6	96,000	2014
0025A	181.98	MINORSTR (025A182000BR) UNNAMED DRAINAGE	urban	Interstate	6	96,000	2014
0025A	182.00	MINORSTR (006G264410BL) DRAINAGE -- MILEPOST 182	urban	Interstate	6	96,000	2014
0025A	182.02	RAMP OFF - (TO WOLFENSBERGER RD RAMP D) EXIT 182	urban	Interstate	6	96,000	2014
0025A	182.05	RAMP ON - (FROM WOLFENSBERGER RD RAMP C) EXIT 182	urban	Interstate	6	96,000	2014
0025A	182.08	SIGN BRIDGE STR (SIGN-G-17-DQ) - I-25 SBND OFF RAMP	urban	Interstate	6	96,000	2014
0025A	182.22	MAJOR STR (G-17-CS) D AND RGW RR (253-064K) UNDERPASS SEPARATION -- HISTIRICAL STRID (G-17-N)	urban	Interstate	6	96,000	2014
0025A	182.33	MINORSTR (025A182350BR) UNNAMED DRAINAGE	urban	Interstate	6	96,000	2014
0025A	182.37	MAJOR STR (G-17-AC) - RD E AND W (CR 107) (LIGGET RD) UNDERPASS SEPARATION	urban	Interstate	6	96,000	2014
0025A	182.52	SIGN BRIDGE STR (SIGN-G-17-EB) - I-25 SBND ML-182.520	urban	Interstate	6	96,000	2014
0025A	182.63	MINORSTR (025A182630BR) UNNAMED DRAINAGE	urban	Interstate	6	96,000	2014
0025A	182.67	SIGN BRIDGE STR (SIGN-G-17-BD) - I-25 NBND ML - VMS	urban	Interstate	6	96,000	2014
0025A	182.87	SIGN BRIDGE STR (SIGN-G-17-DR) - I-25 NBND ML	urban	Interstate	6	96,000	2014
0025A	183.00	MINORSTR (006G266200BR) DRAINAGE -- MILEPOST 183	urban	Interstate	6	96,000	2014
0025A	183.05	SIGN BRIDGE STR (SIGN-G-17-DS) - I- 25 MEDIAN-VMS-183.052	urban	Interstate	6	96,000	2014
0025A	183.10	SIGN BRIDGE STR (SIGN-G-17-DT) - I-25 SBND ML	urban	Interstate	6	96,000	2014
0025A	183.16	ENTER CASTLE ROCK CITY LIMITS	urban	Interstate	6	96,000	2014
0025A	183.30	MAJOR STR (G-17-BG) SEDALIA/LITTLETON - JCT S.H 85 NW (OLD UNDERPASS REMOVED) -- HISTORIC STRID (G-17-K)	urban	Interstate	6	96,000	2014
0025A	183.39	SIGN BRIDGE STR (SIGN-G-17-DO) - I-25 NBND ML-183.386	urban	Interstate	6	96,000	2014
0025A	183.56	MINORSTR (025A183550BR) UNNAMED DRAINAGE	urban	Interstate	6	96,000	2014
0025A	183.86	SIGN BRIDGE STR (SIGN-G-17-DU) POSTED SIGNAGE	urban	Interstate	6	96,000	2014
0025A	183.93	RAMP ON (EXIT 184- FROM US 086B EB RAMP E)	urban	Interstate	6	96,000	2014
0025A	183.97	RAMP OFF (EXIT 184 - O US 086B EB RAMP B)	urban	Interstate	6	96,000	2014
0025A	184.00	MILEPOST 184	urban	Interstate	6	96,000	2014
0025A	184.18	LEAVE CASTLE ROCK CITY LIMITS	urban	Interstate	6	96,000	2014
0025A	184.19	LOOP RAMP ON - (FROM US 086B EB RAMP G) EXIT 184	urban	Interstate	6	96,000	2014
0025A	184.21	FOUNDERS PARKWAY INTERCHANGE STR (G-17-AM) - JCT SH 085 EAST - UNDERPASS SEPARATION	urban	Interstate	5.6	110,000	2014
0025A	184.23	SIGN BRIDGE STR (SIGN-G-17-AS) -- LOOP RAMP ON - (FROM US 086B WB RAMP I) EXIT 184	urban	Interstate	5.6	110,000	2014
0025A	184.59	RAMP ON - (FROM US 086B WB RAMP C) EXIT 184, AND ADD NB LANE	urban	Interstate	5.6	110,000	2014

highway	milepoint	description	rucode	func_class	ptrucks	adt	adt year
0025A	184.61	MINORSTR (025A184640BR) UNNAMED DRAINAGE	urban	Interstate	5.6	110,000	2014
0025A	184.65	MINORSTR (025A184860BR) UNNAMED DRAINAGE	urban	Interstate	5.6	110,000	2014
0025A	184.70	RAMP OFF - (TO US 086B WB RAMP D) EXIT 184, AND DROP SB LANE	urban	Interstate	5.6	110,000	2014
0025A	184.80	SIGN BRIDGE STR (SIGN-G-17-DG) - I-25 SBND ML. DROP SB LANE	urban	Interstate	5.6	110,000	2014
0025A	185.00	MILEPOST 185	urban	Interstate	5.6	110,000	2014
0025A	185.12	SIGN BRIDGE STR (SIGN-G-17-BU) - I- 25 SBND ML - VMS	urban	Interstate	5.6	110,000	2014
0025A	185.71	SIGN BRIDGE STR (SIGN-G-17-DI) - I-25 NBND ML	urban	Interstate	5.6	110,000	2014
0025A	185.73	MINORSTR (025A185700BR) UNNAMED DRAINAGE	urban	Interstate	5.6	110,000	2014
0025A	185.78	SIGN BRIDGE STR (SIGN-G-17-DH) - I-25 SBND ML	urban	Interstate	5.6	110,000	2014
0025A	186.00	MILEPOST 186	urban	Interstate	5.6	110,000	2014
0025A	186.61	MINORSTR (025A186600BR) UNNAMED DRAINAGE	urban	Interstate	5.6	110,000	2014
0025A	186.68	SIGN BRIDGE STR (G-17-DJ) -- RAMP OFF - (TO HAPPY CANYON RD WB RAMP B) EXIT 187	urban	Interstate	5.6	110,000	2014
0025A	186.73	RAMP ON - (FROM HAPPY CANYON RD EB RAMP E) EXIT 187	urban	Interstate	5.6	110,000	2014
0025A	186.94	HAPPY CANYON INTERCHANGE STR (G-17-AG G-17-CK) - RD W (CO RD 33) (HAPPY CANYON RD) -- RD E (LARIAT DR) -- UNDERPASS SEPARATI	urban	Interstate	5.4	114,000	2014
0025A	187.00	MILEPOST 187	urban	Interstate	5.4	114,000	2014
0025A	187.17	MINORSTR (025A187170BL) NEWLIN GULCH	urban	Interstate	5.4	114,000	2014
0025A	187.20	SIGN BRIDGE STR (E-17-ZR)	urban	Interstate	5.4	114,000	2014
0025A	187.38	SIGN BRIDGE STR (SIGN-G-17-DK) -- RAMP ON - (FROM HAPPY CANYON RD WB RAMP C) EXIT 187	urban	Interstate	5.4	114,000	2014
0025A	187.50	SIGN BRIDGE STR (SIGN-G-17-DL) - I-25 NBND ML	urban	Interstate	5.4	114,000	2014
0025A	187.63	(GIS) CONDITION LOCATION MARKER	urban	Interstate	5.4	114,000	2014
0025A	187.77	MINORSTR (025A187750BL) UNNAMED DRAINAGE	urban	Interstate	5.4	114,000	2014
0025A	187.95	SIGN BRIDGE STR (SIGN-G-17-DM) - I-25 SBND ML	urban	Interstate	5.4	114,000	2014
0025A	188.00	MINORSTR (025A187970BL) MILEPOST 188	urban	Interstate	5.4	114,000	2014
0025A	188.15	SIGN BRIDGE STR (G-17-DN) -- RAMP OFF - (TO CASTLE PINES PKWY/S HAVANA ST RAMP B) EXIT 188	urban	Interstate	5.4	114,000	2014
0025A	188.26	RAMP ON - (FROM CASTLE PINES PKWY EB RAMP E) EXIT 188	urban	Interstate	5.4	114,000	2014
0025A	188.46	LOOP RAMP ON - (FROM CASTLE PINES PKWY EB RAMP G) EXIT 188	urban	Interstate	5.4	114,000	2014
0025A	188.49	CASTLE PINES PARKWAY INTERCHANGE STR (G-17-CT) - RD E AND W (CO RD BH1) (CASTLE PINES PKWY) UNDERPASS SEPARATION -- HISTOR	urban	Interstate	5.4	122,000	2014
0025A	188.50	SIGN BRIDGE STR (F-16-FC) SB AND RAMP ACCESS	urban	Interstate	5.4	122,000	2014
0025A	188.69	SIGN BRIDGE STR (G-17-AY) -- RAMP OFF - (TO CASTLE PINES PKWY WB RAMP D) EXIT 188	urban	Interstate	5.4	122,000	2014
0025A	188.82	RAMP ON - (FROM CASTLE PINES PKWY WB/S HAVANA ST RAMP C) EXIT 188	urban	Interstate	5.4	122,000	2014
0025A	189.00	MILEPOST 189	urban	Interstate	5.4	122,000	2014
0025A	189.50	SIGN BRIDGE STR (F-16-FD) NB AND RAMP ACCESS	urban	Interstate	5.4	122,000	2014
0025A	189.61	SIGN BRIDGE STR (SIGN-G-17-AZ) - I-25 SBND ML	urban	Interstate	5.4	122,000	2014
0025A	189.73	MAJOR INTERCHANGE STR (G-17-T) SURREY RIDGE ROAD OVERPASS SEPARTATION - RD W (OAK LN) - RD E (CLYDESDALE RD)	urban	Interstate	5.4	122,000	2014
0025A	189.85	RAMPS ON AND OFF	urban	Interstate	5.4	122,000	2014
0025A	190.00	MILEPOST 190	urban	Interstate	5.4	122,000	2014
0025A	190.22	SIGN BRIDGE STR (SIGN-F-17-MO) - I-25 NBND ML--190.732	urban	Interstate	5.4	122,000	2014
0025A	190.57	MINORSTR (025A190580BR) UNNAMED DRAINAGE	urban	Interstate	5.4	122,000	2014
0025A	190.73	SIGN BRIDGE STR (SIGN-F-17-QN) - I-25 NBND ML-VMS-191.378	urban	Interstate	5.4	122,000	2014
0025A	190.93	MINORSTR (025A190940BR) -- ENTER LONE TREE CITY LIMITS - IN DOUGLAS COUNTY LIMITS	urban	Interstate	5.4	122,000	2014
0025A	191.00	MILEPOST 191	urban	Interstate	5.4	122,000	2014
0025A	191.13	MAJOR STRS (F-17-CR) ML -- HAPPY CANYON CREEK	urban	Interstate	5.4	122,000	2014
0025A	191.33	MINORSTR (025A191330BL) DRAINAGE	urban	Interstate	5.4	122,000	2014
0025A	191.39	SIGN BRIDGE STR (SIGN-F-17-FM) - I-25 NBND ML-191.434	urban	Interstate	5.4	122,000	2014
0025A	191.40	MINORSTR (025A191389BL) COUNTY ROAD	urban	Interstate	5.4	122,000	2014
0025A	191.41	MINORSTR (025A191389BL) COUNTY ROAD	urban	Interstate	5.4	122,000	2014
0025A	191.41	RD SW (SURREY DR) - RD NE (RAMP AND FRONTAGE) -- OVERPASS SEPARATION	urban	Interstate	5.4	122,000	2014
0025A	191.43	SIGN BRIDGE STR (SIGN-F-17-WE) - I-25 NBND ML-191.687, AND DROP NB LANE	urban	Interstate	5.4	122,000	2014
0025A	191.48	MINORSTR (025A191500BL) DRAINAGE	urban	Interstate	5.4	122,000	2014
0025A	191.69	SIGN BRIDGE STR (SIGN-F-17-PX) - I-25 NBND ML	urban	Interstate	5.4	122,000	2014
0025A	191.76	RAMP OFF PAINTED GORE (EXIT-192 TO RIDGEGATE PKWY) NB	urban	Interstate	5.4	122,000	2014
0025A	191.89	RAMP ON DIVE TAPER - PAINTED GORE (FROM RIDGEGATE PKWY) SB	urban	Interstate	5.4	122,000	2014
0025A	191.96	SIGN BRIDGE STR (SIGN-F-17-AD) - I-25 NBND ML	urban	Interstate	5.4	122,000	2014
0025A	192.00	MILEPOST 192	urban	Interstate	5.4	122,000	2014
0025A	192.05	LOOP RAMP ON (FROM RIDGEGATE PKWY) NB	urban	Interstate	5.4	122,000	2014

highway	milepoint	description	rucode	func_class	ptrucks	adt	adt_year
0025A	192.08	RIDGEGATE PKWY INTERCHANGE STR (F-17-PT) RD E AND W (RIDGEGATE PKWY) - OVERPASS SEPARATION	urban	Interstate	5.2	135,000	2014
0025A	192.12	ADD NB LANE	urban	Interstate	5.2	135,000	2015
0025A	192.15	LOOP RAMP ON (FROM RIDGEGATE PKWY) SB	urban	?	5.2	135,000	2014
0025A	192.19	MINORSTR (025A192540BL) COTTONWOOD CREEK	urban	Interstate	5.2	135,000	2014
0025A	192.32	SIGN BRIDGE STR (SIGN-F-17-JT) POSTED SIGNAGE	urban	Interstate	5.2	135,000	2014
0025A	192.35	RAMP ON (NB) PAINTED GORE POINT - RAMP OFF (SB) PAVEMENT GORE (TO/FROM RIDGEGATE PKWY)	urban	Interstate	5.2	135,000	2014
0025A	192.38	RAMP OFF - (TO RIDGEGATE PKWY EB RAMP D) EXIT 192	urban	Interstate	5.2	135,000	2014
0025A	192.43	SIGN BRIDGE STR (SIGN-F-17-QM) - I-25 SBND ML-192.431	urban	Interstate	5.2	135,000	2014
0025A	192.50	LEGACY LOCATION MARKER. ADD SB LANE	urban	Interstate	5.2	135,000	2014
0025A	192.62	(GIS) CONDITION LOCATION MARKER	urban	Interstate	5.2	135,000	2014
0025A	192.64	SIGN BRIDGE STR (F-17-MM) -- RAMP OFF - (TO LINCOLN AVE EB RAMP B) EXIT 193. ADD SB LANE.	urban	Interstate	5.2	135,000	2014
0025A	192.67	RAMP ON - (FROM LINCOLN AVE EB RAMP E) EXIT 193	urban	Interstate	5.2	135,000	2014
0025A	192.71	CHANGE PAVEMENT TO ASPHALT	urban	Interstate	5.2	135,000	2015
0025A	192.85	SIGN BRIDGE STR (SIGN-F-17-QL) - I-25 SBND ML-192.852	urban	Interstate	5.2	135,000	2014
0025A	192.99	LINCOLN AVE INTERCHANGE STR (F-17-JM) - RD E AND W (CO RD 8) (LINCOLN AVE) -- UNDERPASS SEPARATION	urban	Interstate	6.2	162,000	2014
0025A	193.00	MILEPOST 193	urban	Interstate	6.2	162,000	2014
0025A	193.05	LOOP RAMPS ON AND OFF - (TO/FROM LINCOLN AVE) EXIT 193	urban	Interstate	6.2	162,000	2014
0025A	193.33	RAMP OFF - (TO LINCOLN AVE WB RAMP D) EXIT 193	urban	Interstate	6.2	162,000	2014
0025A	193.37	SIGN BRIDGE STR (SIGN-F-17-JR) - I-25 SBND ML + RAMP	urban	Interstate	6.2	162,000	2014
0025A	193.48	RAMP ON - (FROM LINCOLN AVE WB RAMP C) EXIT 193	urban	Interstate	6.2	162,000	2014
0025A	193.56	SIGN BRIDGE STR (SIGN-F-17-SU) POSTED SIGNAGE	urban	Interstate	6.2	162,000	2014
0025A	193.58	SIGN BRIDGE STR (SIGN-F-17-II) MEDIAN	urban	Interstate	6.2	162,000	2014
0025A	193.69	MAJOR STR (F-17-WJ) LIGHTRAIL PEDESTRIAN WALKWAY (LINCOLN STATION)	urban	Interstate	6.2	162,000	2014
0025A	193.82	RAMP ON - (FROM E-470 EB AND WB RAMP E) EXIT 26	urban	Interstate	6.2	162,000	2014
0025A	193.88	SIGN BRIDGE STR (SIGN-F-17-IH) RAMP OFF (TO WB SH 470) NB	urban	Interstate	6.2	162,000	2014
0025A	193.92	RAMP OFF (TO 470 EB AND WB) NB	urban	Interstate	6.2	162,000	2014
0025A	193.97	RAMP ON (FROM EB SH 470) SB	urban	Interstate	6.2	162,000	2014
0025A	194.00	MILEPOST 194	urban	Interstate	6.2	162,000	2014
0025A	194.04	SIGN BRIDGE STR (F-17-KB) -- RAMP GORE SPLIT (EB RAMP B AND WB FLYOVER RAMP)	urban	Interstate	6.2	162,000	2014
0025A	194.05	PAVEMENT GORE -- RAMP B CONNECTION SPLIT (TO 470 EB AND WB)	urban	Interstate	6.2	162,000	2014
0025A	194.06	SIGN BRIDGE STR (SIGN-F-17-JQ) POSTED SIGNAGE	urban	Interstate	6.2	162,000	2014
0025A	194.06	SIGN BRIDGE STR (SIGN-F-17-JP) POSTED SIGNAGE	urban	Interstate	6.2	162,000	2014
0025A	194.09	RAMP GORE SPLIT (SB RAMP E AND NB FLYOVER RAMP)	urban	Interstate	6.2	162,000	2014
0025A	194.11	SIGN BRIDGE STR (SIGN-F-17-IG) -- RAMP GORE -- RAMP E CONNECTION SPLIT(FROM 470 EB AND WB)	urban	Interstate	6.2	162,000	2014
0025A	194.23	MAJOR STR (F-17-IQ) E-470 RAMPS - OFFSET REFERENCE	urban	Interstate	6.2	162,000	2014
0025A	194.27	RAMP OFF (EXIT 195 - TO COUNTY LINE ROAD) NB - RAMP ON (FROM PARK MEADOWS MALL VIA COUNTY LINE RD) SB	urban	Interstate	6.2	162,000	2014
0025A	194.28	SIGN BRIDGE STR (F-17-IF) -- RAMP ON - (FROM CENTENNIAL/DOUGLAS CTY LINE RD) EXIT 195	urban	Interstate	6.2	162,000	2014
0025A	194.31	SH-470A/SH-470B MAJOR INTERCHANGE STRS (F-17-JV) NB AND (F-17-JX) RAMP AND (F-17-JU) SB - JCT E-470A EAST - JCT C-470 WEST - RAMP	urban	Interstate	7.2	173,000	2014
0025A	194.32	RAMP K CONNECTION SPLIT (FROM 470B EB)	urban	Interstate	7.2	173,000	2014
0025A	194.38	MAJOR STR (F-17-OY) LIGHT RAIL - OFFSET REFERENCE	urban	Interstate	7.2	173,000	2014
0025A	194.38	MAJOR STR (F-17-IT) SH 470 ML	urban	Interstate	7.2	173,000	2014
0025A	194.49	MAJOR STR (F-17-JW) SB - RAMP Y CONNECTION SPLIT (TO SH-470A WEST) AND (TO SH-470B EAST) SB - OFFSET ON WEST FRONTAGE	urban	Interstate	7.2	173,000	2014
0025A	194.50	MAJOR STR (F-17-ID) NB - 3-LANE RAMP Y CONNECTION (FROM SH-470A EAST) AND (FROM SH-470B WEST) - RD N AND S (FRONTAGE RD) OVER	urban	Interstate	7.2	173,000	2014
0025A	194.51	MAJOR STR (F-17-ID) -- EAST SIDE RAMP SPLIT (RAMP C AND FLYOVER RAMP)	urban	Interstate	7.2	173,000	2014
0025A	194.54	MAJOR STR (F-17-IE) LIGHT RAIL CROSSING - SB RAMP OVERPASS SEPARATION	urban	Interstate	7.2	173,000	2014
0025A	194.57	SIGN BRIDGE STR (F-17-SO) -- RAMP GORE SPLIT (WB RAMP D AND EB FLYOVER RAMP)	urban	Interstate	7.2	173,000	2014
0025A	194.74	RAMP OFF (EXIT 26 - TO 470 EB AND WB) SB	urban	Interstate	7.2	173,000	2014
0025A	194.77	MISCSTR (F-17-PD) LIGHTRAIL PEDESTRIAN WALKWAY (PARK MEADOWS MALL) - RAMP ON (3-LANES FROM SH 470A EB AND SH 470B WB) NB	urban	Interstate	7.2	173,000	2014

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1	025A	157.70	07/29/11	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2	025A	157.70	09/29/12	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3	025A	157.70	05/09/13	1203	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4	025A	157.70	08/29/13	1705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5	025A	157.70	08/28/14	2126	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
6	025A	157.70	09/23/14	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
7	025A	157.70	06/08/12	2138	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
8	025A	157.70	11/17/14	1740	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
9	025A	157.70	10/19/15	2130	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
10	025A	157.70	07/12/13	1700	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
11	025A	157.70	06/27/14	1110	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
12	025A	157.75	06/27/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
13	025A	157.79	11/07/14	0330	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
14	025A	157.80	04/12/13	1714	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
15	025A	157.80	08/12/13	0656	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
16	025A	157.80	08/06/11	0954	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
17	025A	157.80	11/28/12	1625	INJ	ON	NON-INTERSECTION	6	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
18	025A	157.80	01/07/14	0744	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
19	025A	157.80	11/30/12	1120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
20	025A	157.80	06/02/11	0945	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
21	025A	157.80	05/17/12	0450	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
22	025A	157.80	06/25/12	0045	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
23	025A	157.80	05/25/14	1802	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
24	025A	157.80	10/06/13	0350	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
25	025A	157.80	06/19/11	1850	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
26	025A	157.88	10/03/11	1409	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
27	025A	157.89	12/26/14	1020	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
28	025A	157.90	06/25/11	0810	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
29	025A	157.90	04/18/13	0130	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
30	025A	157.90	12/01/13	0314	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
31	025A	157.90	04/20/12	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
32	025A	157.90	11/28/12	1557	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
33	025A	157.90	09/21/13	1325	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
34	025A	157.90	10/24/12	0720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
35	025A	157.90	11/14/11	0630	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
36	025A	157.90	04/14/12	1435	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
37	025A	157.90	03/01/12	2220	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
38	025A	157.91	03/30/13	1805	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
39	025A	157.95	11/15/12	1710	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
40	025A	157.96	12/12/14	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
41	025A	157.96	04/03/12	0700	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
42	025A	157.98	06/18/14	1520	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
43	025A	157.99	10/20/11	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
44	025A	157.99	02/04/11	1710	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
45	025A	157.99	02/07/13	1754	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
46	025A	158.00	03/23/13	1340	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
47	025A	158.00	10/09/14	1545	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
48	025A	158.00	05/09/15	1438	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
49	025A	158.00	06/24/13	2125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
50	025A	158.00	02/02/15	2210	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
51	025A	158.00	06/18/11	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
52	025A	158.00	09/17/11	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
53	025A	158.00	12/27/11	1721	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
54	025A	158.00	01/19/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
55	025A	158.00	05/28/12	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
56	025A	158.00	10/27/12	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
57	025A	158.00	12/08/12	1838	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	WIND
58	025A	158.00	03/01/13	1555	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
59	025A	158.00	04/11/13	0740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
60	025A	158.00	04/12/13	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1	025A	157.70	07/29/11	1500	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2	025A	157.70	09/29/12	1130	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
3	025A	157.70	05/09/13	1203	N	REAR-END		N SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4	025A	157.70	08/29/13	1705	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5	025A	157.70	08/28/14	2126	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
6	025A	157.70	09/23/14	0900	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	015	CHANGING LANES
7	025A	157.70	06/08/12	2138	N	WILD ANIMAL		S TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
8	025A	157.70	11/17/14	1740	N	WILD ANIMAL		S SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
9	025A	157.70	10/19/15	2130	N	GUARD RAIL		N PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
10	025A	157.70	07/12/13	1700	N	REAR-END		S SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
11	025A	157.70	06/27/14	1110	N	REAR-END		S PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
12	025A	157.75	06/27/14	1200	N	REAR-END		S SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
13	025A	157.79	11/07/14	0330	N	GUARD RAIL		N PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
14	025A	157.80	04/12/13	1714	N	OVERTURNING		S MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	060	SLOWING
15	025A	157.80	08/12/13	0656	N	SIDESWIPE SAME DIRECTION		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
16	025A	157.80	08/06/11	0954	N	REAR-END		N PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	020	GOING STRAIGHT
17	025A	157.80	11/28/12	1625	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
18	025A	157.80	01/07/14	0744	N	REAR-END		S PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
19	025A	157.80	11/30/12	1120	N	SIDESWIPE SAME DIRECTION		N SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
20	025A	157.80	06/02/11	0945	N	WILD ANIMAL		N SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
21	025A	157.80	05/17/12	0450	N	GUARD RAIL		N PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
22	025A	157.80	06/25/12	0045	N	GUARD RAIL		N PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
23	025A	157.80	05/25/14	1802	N	GUARD RAIL		N PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
24	025A	157.80	10/06/13	0350	N	CRASH CUSHION		S PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
25	025A	157.80	06/19/11	1850	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
26	025A	157.88	10/03/11	1409	N	SIDESWIPE SAME DIRECTION		S PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
27	025A	157.89	12/26/14	1020	N	GUARD RAIL		S SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	MAKING RIGHT TURN
28	025A	157.90	06/25/11	0810	N	OVERTURNING		S PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
29	025A	157.90	04/18/13	0130	N	OVERTURNING		N PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	055	OTHER
30	025A	157.90	12/01/13	0314	N	OVERTURNING		S PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	050	OTHER
31	025A	157.90	04/20/12	1815	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
32	025A	157.90	11/28/12	1557	N	REAR-END		S SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	050	GOING STRAIGHT
33	025A	157.90	09/21/13	1325	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
34	025A	157.90	10/24/12	0720	N	SIDESWIPE SAME DIRECTION		S SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
35	025A	157.90	11/14/11	0630	N	WILD ANIMAL		S PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
36	025A	157.90	04/14/12	1435	N	GUARD RAIL		S SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
37	025A	157.90	03/01/12	2220	Y (B)	FENCE		N PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	OTHER
38	025A	157.91	03/30/13	1805	N	REAR-END		S MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT
39	025A	157.95	11/15/12	1710	Y (B)	LIGHT/UTILITY POLE		N PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
40	025A	157.96	12/12/14	1115	N	SIDESWIPE SAME DIRECTION		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
41	025A	157.96	04/03/12	0700	Y (E)	LARGE BOULDERS OR ROCKS		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
42	025A	157.98	06/18/14	1520	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
43	025A	157.99	10/20/11	0945	N	SIDESWIPE SAME DIRECTION		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
44	025A	157.99	02/04/11	1710	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
45	025A	157.99	02/07/13	1754	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
46	025A	158.00	03/23/13	1340	N	OVERTURNING		N PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
47	025A	158.00	10/09/14	1545	N	OVERTURNING		S PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	OTHER
48	025A	158.00	05/09/15	1438	N	OVERTURNING		S SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
49	025A	158.00	06/24/13	2125	N	VEHICLE CARGO/DEBRIS		S PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
50	025A	158.00	02/02/15	2210	N	PEDESTRIAN		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	BACKING
51	025A	158.00	06/18/11	1100	N	REAR-END		N PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	SLOWING
52	025A	158.00	09/17/11	1715	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
53	025A	158.00	12/27/11	1721	N	REAR-END		S PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
54	025A	158.00	01/19/12	0730	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
55	025A	158.00	05/28/12	1315	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
56	025A	158.00	10/27/12	1145	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
57	025A	158.00	12/08/12	1838	N	REAR-END		S PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
58	025A	158.00	03/01/13	1555	N	REAR-END		N SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
59	025A	158.00	04/11/13	0740	N	REAR-END		S TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
60	025A	158.00	04/12/13	1730	N	REAR-END		S SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
61	025A	158.00	05/09/13	1020	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
62	025A	158.00	05/10/13	1459	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
63	025A	158.00	08/30/13	1618	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
64	025A	158.00	09/26/13	0719	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
65	025A	158.00	10/16/13	0751	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
66	025A	158.00	03/29/14	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
67	025A	158.00	05/21/14	1540	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
68	025A	158.00	05/22/14	2135	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
69	025A	158.00	05/23/14	1000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
70	025A	158.00	05/16/15	2045	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
71	025A	158.00	08/27/15	1713	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
72	025A	158.00	08/27/15	1713	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
73	025A	158.00	05/27/14	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
74	025A	158.00	05/31/14	2040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
75	025A	158.00	06/24/14	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
76	025A	158.00	02/02/15	2130	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
77	025A	158.00	07/04/13	1245	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
78	025A	158.00	09/18/12	0100	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
79	025A	158.00	11/21/13	0525	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
80	025A	158.00	12/28/13	2100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
81	025A	158.00	04/30/13	1915	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
82	025A	158.00	01/30/11	1800	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
83	025A	158.00	06/01/14	1435	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
84	025A	158.02	01/07/14	1153	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
85	025A	158.02	09/29/14	1400	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
86	025A	158.04	10/19/13	1710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
87	025A	158.04	06/06/13	1115	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
88	025A	158.05	07/28/15	1159	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
89	025A	158.07	06/13/11	0935	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
90	025A	158.07	09/11/13	0840	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
91	025A	158.09	10/01/11	1400	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
92	025A	158.10	09/11/14	1531	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
93	025A	158.10	03/05/11	0230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	FOG
94	025A	158.10	06/17/11	1120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
95	025A	158.10	06/17/11	1125	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
96	025A	158.10	05/10/12	2230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
97	025A	158.10	05/28/12	1315	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
98	025A	158.10	03/30/13	1154	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
99	025A	158.10	07/21/13	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
100	025A	158.10	09/16/14	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
101	025A	158.10	03/08/13	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
102	025A	158.10	12/20/11	1030	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
103	025A	158.10	01/13/13	0126	FAT	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
104	025A	158.10	01/31/13	0825	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
105	025A	158.10	05/01/13	1635	INJ	OFF LEFT	NON-INTERSECTION	8	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
106	025A	158.10	06/21/15	0750	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
107	025A	158.10	06/10/13	0331	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
108	025A	158.10	05/27/11	1510	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
109	025A	158.10	06/19/11	1915	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
110	025A	158.10	05/28/12	1448	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
111	025A	158.10	07/11/12	1745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
112	025A	158.10	05/10/13	1855	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
113	025A	158.10	05/23/14	1015	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
114	025A	158.10	06/30/14	0740	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
115	025A	158.10	07/13/12	0100	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
116	025A	158.10	06/15/12	1530	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
117	025A	158.14	04/16/15	1600	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
118	025A	158.15	05/29/15	1955	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
119	025A	158.15	10/08/12	0902	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
120	025A	158.18	07/15/14	1836	INJ	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
61	025A	158.00	05/09/13	1020	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
62	025A	158.00	05/10/13	1459	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
63	025A	158.00	08/30/13	1618	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
64	025A	158.00	09/26/13	0719	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
65	025A	158.00	10/16/13	0751	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
66	025A	158.00	03/29/14	1520	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
67	025A	158.00	05/21/14	1540	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	SLOWING
68	025A	158.00	05/22/14	2135	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
69	025A	158.00	05/23/14	1000	N	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
70	025A	158.00	05/16/15	2045	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
71	025A	158.00	08/27/15	1713	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
72	025A	158.00	08/27/15	1713	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
73	025A	158.00	05/27/14	1530	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
74	025A	158.00	05/31/14	2040	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
75	025A	158.00	06/24/14	1400	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
76	025A	158.00	02/02/15	2130	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	CHANGING LANES
77	025A	158.00	07/04/13	1245	N	SIGN	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	055	AVOIDING OBJECT/VEHICLE IN ROAD
78	025A	158.00	09/18/12	0100	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
79	025A	158.00	11/21/13	0525	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
80	025A	158.00	12/28/13	2100	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
81	025A	158.00	04/30/13	1915	N	LARGE BOULDERS OR ROCKS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	076	AVOIDING OBJECT/VEHICLE IN ROAD
82	025A	158.00	01/30/11	1800	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
83	025A	158.00	06/01/14	1435	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
84	025A	158.02	01/07/14	1153	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
85	025A	158.02	09/29/14	1400	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
86	025A	158.04	10/19/13	1710	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
87	025A	158.04	06/06/13	1115	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
88	025A	158.05	07/28/15	1159	Y (E)	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	CHANGING LANES
89	025A	158.07	06/13/11	0935	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
90	025A	158.07	09/11/13	0840	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	GOING STRAIGHT
91	025A	158.09	10/01/11	1400	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
92	025A	158.10	09/11/14	1531	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
93	025A	158.10	03/05/11	0230	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
94	025A	158.10	06/17/11	1120	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	SLOWING
95	025A	158.10	06/17/11	1125	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
96	025A	158.10	05/10/12	2230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
97	025A	158.10	05/28/12	1315	N	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
98	025A	158.10	03/30/13	1154	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
99	025A	158.10	07/21/13	2000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
100	025A	158.10	09/16/14	1645	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
101	025A	158.10	03/08/13	1640	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	CHANGING LANES
102	025A	158.10	12/20/11	1030	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
103	025A	158.10	01/13/13	0126	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	OTHER
104	025A	158.10	01/31/13	0825	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	AVOIDING OBJECT/VEHICLE IN ROAD
105	025A	158.10	05/01/13	1635	N	GUARD RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	AVOIDING OBJECT/VEHICLE IN ROAD
106	025A	158.10	06/21/15	0750	N	CULVERT/HEADWALL	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
107	025A	158.10	06/10/13	0331	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
108	025A	158.10	05/27/11	1510	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
109	025A	158.10	06/19/11	1915	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
110	025A	158.10	05/28/12	1448	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
111	025A	158.10	07/11/12	1745	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
112	025A	158.10	05/10/13	1855	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
113	025A	158.10	05/23/14	1015	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
114	025A	158.10	06/30/14	0740	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
115	025A	158.10	07/13/12	0100	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
116	025A	158.10	06/15/12	1530	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	045	GOING STRAIGHT
117	025A	158.14	04/16/15	1600	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
118	025A	158.15	05/29/15	1955	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
119	025A	158.15	10/08/12	0902	Y (E)	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
120	025A	158.18	07/15/14	1836	Y (E)	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
121	025A	158.18	07/09/14	1545	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
122	025A	158.18	08/27/14	0740	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
123	025A	158.19	10/30/15	1214	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
124	025A	158.20	04/09/13	1010	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
125	025A	158.20	05/15/13	1555	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
126	025A	158.20	06/19/11	1930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
127	025A	158.20	09/03/11	1644	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
128	025A	158.20	08/03/12	1927	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
129	025A	158.20	11/03/12	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
130	025A	158.20	11/30/12	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
131	025A	158.20	07/20/13	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
132	025A	158.20	02/12/14	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
133	025A	158.20	06/19/11	1820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
134	025A	158.20	11/20/12	2115	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
135	025A	158.20	02/06/11	1610	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
136	025A	158.20	04/09/13	0745	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
137	025A	158.20	08/28/12	0755	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
138	025A	158.20	09/02/12	1215	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
139	025A	158.20	06/01/14	2055	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
140	025A	158.20	03/12/15	1949	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
141	025A	158.20	12/28/12	1117	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
142	025A	158.20	10/06/12	0630	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
143	025A	158.20	10/25/12	1950	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
144	025A	158.20	09/15/14	1639	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
145	025A	158.20	12/04/14	1526	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
146	025A	158.20	02/11/15	1840	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
147	025A	158.20	08/05/15	1720	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
148	025A	158.20	08/05/15	1720	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
149	025A	158.20	12/31/12	1820	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
150	025A	158.20	04/17/13	0710	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
151	025A	158.20	11/06/15	1724	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
152	025A	158.20	09/16/13	2013	PDO	OFF LEFT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
153	025A	158.20	12/09/12	1045	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
154	025A	158.20	09/05/11	2045	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
155	025A	158.20	09/30/11	1315	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
156	025A	158.20	05/21/14	1210	INJ	ON	AT INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
157	025A	158.20	10/20/14	0945	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
158	025A	158.20	11/20/11	2005	PDO	ON	INTERSECTION RELATED	2	HILLCREST	DRY	DARK-UNLIGHTED	NONE
159	025A	158.20	11/26/12	1341	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
160	025A	158.20	09/13/13	1900	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
161	025A	158.20	10/11/12	1930	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
162	025A	158.21	10/08/15	1850	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
163	025A	158.22	09/05/14	1705	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
164	025A	158.22	01/02/14	1255	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
165	025A	158.24	05/10/13	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
166	025A	158.24	08/30/13	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
167	025A	158.24	05/10/13	1510	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
168	025A	158.25	04/20/13	1200	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
169	025A	158.25	06/13/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
170	025A	158.25	04/20/13	1200	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
171	025A	158.25	07/07/13	1630	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
172	025A	158.25	01/15/13	0920	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
173	025A	158.26	07/21/12	1745	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
174	025A	158.26	01/24/15	1540	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
175	025A	158.27	12/01/11	1215	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
176	025A	158.28	09/21/13	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
177	025A	158.28	02/03/12	1400	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
178	025A	158.30	04/17/13	1735	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
179	025A	158.30	09/15/12	1213	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
180	025A	158.30	08/30/13	1428	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
121	025A	158.18	07/09/14	1545	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	SLOWING
122	025A	158.18	08/27/14	0740	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	005	GOING STRAIGHT
123	025A	158.19	10/30/15	1214	Y (B)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	MAKING RIGHT TURN
124	025A	158.20	04/09/13	1010	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
125	025A	158.20	05/15/13	1555	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
126	025A	158.20	06/19/11	1930	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
127	025A	158.20	09/03/11	1644	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
128	025A	158.20	08/03/12	1927	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
129	025A	158.20	11/03/12	1750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
130	025A	158.20	11/30/12	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
131	025A	158.20	07/20/13	1350	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
132	025A	158.20	02/12/14	1245	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
133	025A	158.20	06/19/11	1820	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
134	025A	158.20	11/20/12	2115	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
135	025A	158.20	02/06/11	1610	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
136	025A	158.20	04/09/13	0745	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
137	025A	158.20	08/28/12	0755	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
138	025A	158.20	09/02/12	1215	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
139	025A	158.20	06/01/14	2055	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
140	025A	158.20	03/12/15	1949	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
141	025A	158.20	12/28/12	1117	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
142	025A	158.20	10/06/12	0630	Y (A)	BRIDGE RAIL	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
143	025A	158.20	10/25/12	1950	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
144	025A	158.20	09/15/14	1639	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
145	025A	158.20	12/04/14	1526	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
146	025A	158.20	02/11/15	1840	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
147	025A	158.20	08/05/15	1720	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
148	025A	158.20	08/05/15	1720	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
149	025A	158.20	12/31/12	1820	Y (N)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
150	025A	158.20	04/17/13	0710	Y (N)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	UNKNOWN	020	OTHER
151	025A	158.20	11/06/15	1724	Y (N)	APPROACH TURN	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
152	025A	158.20	09/16/13	2013	Y (N)	LIGHT/UTILITY POLE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	MAKING LEFT TURN
153	025A	158.20	12/09/12	1045	Y (N)	LIGHT/UTILITY POLE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
154	025A	158.20	09/05/11	2045	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
155	025A	158.20	09/30/11	1315	Y (N)	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
156	025A	158.20	05/21/14	1210	Y (O)	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
157	025A	158.20	10/20/14	0945	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
158	025A	158.20	11/20/11	2005	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
159	025A	158.20	11/26/12	1341	Y (O)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	MAKING RIGHT TURN
160	025A	158.20	09/13/13	1900	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	GOING STRAIGHT
161	025A	158.20	10/11/12	1930	Y (O)	OVERTAKING TURN	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	MAKING LEFT TURN
162	025A	158.21	10/08/15	1850	Y (D)	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	GOING STRAIGHT
163	025A	158.22	09/05/14	1705	Y (D)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
164	025A	158.22	01/02/14	1255	Y (D)	FENCE	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	060	OTHER
165	025A	158.24	05/10/13	1450	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
166	025A	158.24	08/30/13	1610	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	SLOWING
167	025A	158.24	05/10/13	1510	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
168	025A	158.25	04/20/13	1200	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
169	025A	158.25	06/13/14	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	SLOWING
170	025A	158.25	04/20/13	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
171	025A	158.25	07/07/13	1630	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
172	025A	158.25	01/15/13	0920	Y (C)	CULVERT/HEADWALL	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	OTHER
173	025A	158.26	07/21/12	1745	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
174	025A	158.26	01/24/15	1540	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
175	025A	158.27	12/01/11	1215	N	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
176	025A	158.28	09/21/13	1430	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
177	025A	158.28	02/03/12	1400	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	OTHER
178	025A	158.30	04/17/13	1735	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
179	025A	158.30	09/15/12	1213	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
180	025A	158.30	08/30/13	1428	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
181	025A	158.30	06/29/15	1125	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
182	025A	158.30	11/04/15	2010	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
183	025A	158.30	03/31/11	1140	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
184	025A	158.30	02/17/15	1600	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
185	025A	158.33	12/27/11	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
186	025A	158.40	04/18/13	0650	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
187	025A	158.40	07/23/12	0720	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
188	025A	158.40	05/21/11	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
189	025A	158.40	05/30/13	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
190	025A	158.40	06/22/14	1222	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
191	025A	158.40	12/01/11	1045	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
192	025A	158.40	03/24/12	1155	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
193	025A	158.40	11/22/12	1118	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
194	025A	158.45	03/17/11	0740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
195	025A	158.45	08/06/11	0945	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
196	025A	158.47	05/14/11	1045	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
197	025A	158.48	03/05/13	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
198	025A	158.50	02/23/12	0440	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
199	025A	158.50	12/21/12	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
200	025A	158.50	05/11/14	2128	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
201	025A	158.50	07/11/13	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
202	025A	158.50	06/17/12	0914	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
203	025A	158.50	12/28/12	1145	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
204	025A	158.50	10/02/13	0750	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
205	025A	158.50	01/05/11	2010	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
206	025A	158.50	06/17/11	1648	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
207	025A	158.50	03/09/12	1415	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
208	025A	158.50	02/20/11	0200	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
209	025A	158.50	04/26/15	0638	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
210	025A	158.50	01/28/11	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
211	025A	158.50	02/02/12	2040	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
212	025A	158.50	11/21/12	1900	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
213	025A	158.51	08/17/12	1810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
214	025A	158.51	05/24/11	0845	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
215	025A	158.57	05/25/15	2300	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
216	025A	158.57	09/26/12	0750	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
217	025A	158.60	11/01/11	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
218	025A	158.60	07/08/12	1310	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
219	025A	158.60	06/25/11	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
220	025A	158.60	12/20/14	1820	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
221	025A	158.60	06/28/11	2116	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
222	025A	158.60	09/29/14	1420	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
223	025A	158.60	07/24/12	1845	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
224	025A	158.60	04/14/12	1700	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
225	025A	158.60	05/20/15	1240	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
226	025A	158.70	12/01/11	0450	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	NONE
227	025A	158.70	11/01/11	0740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
228	025A	158.70	01/13/13	2115	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
229	025A	158.75	08/01/11	1410	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
230	025A	158.75	04/18/13	0700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
231	025A	158.75	05/08/15	0655	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
232	025A	158.80	02/19/14	2215	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
233	025A	158.80	06/09/15	1245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
234	025A	158.80	04/27/12	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
235	025A	158.80	05/11/12	1845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
236	025A	158.80	08/11/12	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
237	025A	158.80	01/28/11	1850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
238	025A	158.80	10/21/12	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
239	025A	158.80	10/22/15	1015	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
240	025A	158.80	06/03/11	0945	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
181	025A	158.30	06/29/15	1125	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
182	025A	158.30	11/04/15	2010	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
183	025A	158.30	03/31/11	1140	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	AVOIDING OBJECT/VEHICLE IN ROAD
184	025A	158.30	02/17/15	1600	Y (C)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	CHANGING LANES
185	025A	158.33	12/27/11	1730	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
186	025A	158.40	04/18/13	0650	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
187	025A	158.40	07/23/12	0720	N	OTHER NON-COLLISION	S	SUV	NO IMPAIRMENT	NONE APPARENT	064	OTHER
188	025A	158.40	05/21/11	1300	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	015	GOING STRAIGHT
189	025A	158.40	05/30/13	1815	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
190	025A	158.40	06/22/14	1222	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
191	025A	158.40	12/01/11	1045	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
192	025A	158.40	03/24/12	1155	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
193	025A	158.40	11/22/12	1118	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
194	025A	158.45	03/17/11	0740	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
195	025A	158.45	08/06/11	0945	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
196	025A	158.47	05/14/11	1045	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
197	025A	158.48	03/05/13	1600	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
198	025A	158.50	02/23/12	0440	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
199	025A	158.50	12/21/12	1425	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
200	025A	158.50	05/11/14	2128	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
201	025A	158.50	07/11/13	0715	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
202	025A	158.50	06/17/12	0914	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
203	025A	158.50	12/28/12	1145	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
204	025A	158.50	10/02/13	0750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	SLOWING
205	025A	158.50	01/05/11	2010	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
206	025A	158.50	06/17/11	1648	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
207	025A	158.50	03/09/12	1415	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
208	025A	158.50	02/20/11	0200	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	085	GOING STRAIGHT
209	025A	158.50	04/26/15	0638	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	OTHER
210	025A	158.50	01/28/11	1650	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
211	025A	158.50	02/02/12	2040	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
212	025A	158.50	11/21/12	1900	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	CHANGING LANES
213	025A	158.51	08/17/12	1810	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
214	025A	158.51	05/24/11	0845	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
215	025A	158.57	05/25/15	2300	N	CULVERT/HEADWALL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
216	025A	158.57	09/26/12	0750	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	070	GOING STRAIGHT
217	025A	158.60	11/01/11	0815	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
218	025A	158.60	07/08/12	1310	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
219	025A	158.60	06/25/11	1350	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	SLOWING
220	025A	158.60	12/20/14	1820	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
221	025A	158.60	06/28/11	2116	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	WEAVING
222	025A	158.60	09/29/14	1420	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
223	025A	158.60	07/24/12	1845	N	LARGE BOULDERS OR ROCKS	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	OTHER
224	025A	158.60	04/14/12	1700	N	REAR-END	N	MOTOR HOME	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
225	025A	158.60	05/20/15	1240	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
226	025A	158.70	12/01/11	0450	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
227	025A	158.70	11/01/11	0740	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	GOING STRAIGHT
228	025A	158.70	01/13/13	2115	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
229	025A	158.75	08/01/11	1410	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
230	025A	158.75	04/18/13	0700	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
231	025A	158.75	05/08/15	0655	N	CULVERT/HEADWALL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
232	025A	158.80	02/19/14	2215	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	OTHER
233	025A	158.80	06/09/15	1245	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
234	025A	158.80	04/27/12	1730	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
235	025A	158.80	05/11/12	1845	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
236	025A	158.80	08/11/12	0945	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
237	025A	158.80	01/28/11	1850	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
238	025A	158.80	10/21/12	1600	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
239	025A	158.80	10/22/15	1015	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
240	025A	158.80	06/03/11	0945	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
241	025A	158.80	10/09/14	1404	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
242	025A	158.80	02/13/12	1335	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
243	025A	158.80	10/12/14	1541	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
244	025A	158.80	07/09/15	1815	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
245	025A	158.80	07/30/14	1820	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
246	025A	158.80	11/27/13	0855	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
247	025A	158.88	04/18/15	2026	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
248	025A	158.90	09/05/14	0700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
249	025A	158.90	05/19/15	0625	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
250	025A	158.90	08/28/11	1735	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
251	025A	158.90	09/04/13	0820	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
252	025A	158.90	11/17/14	0435	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
253	025A	158.90	03/25/15	1255	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
254	025A	158.90	05/05/15	1310	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
255	025A	158.90	12/11/12	1650	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
256	025A	158.90	05/08/15	1946	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
257	025A	158.90	09/06/13	2038	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
258	025A	158.92	10/22/15	0715	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
259	025A	158.94	03/20/11	1415	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
260	025A	158.98	11/29/11	0130	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
261	025A	159.00	08/16/13	0950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
262	025A	159.00	07/16/11	1420	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
263	025A	159.00	08/05/11	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
264	025A	159.00	12/30/11	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
265	025A	159.00	08/18/12	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
266	025A	159.00	08/25/12	1140	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
267	025A	159.00	10/05/12	1745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
268	025A	159.00	08/02/13	1200	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
269	025A	159.00	04/01/14	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
270	025A	159.00	07/24/14	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
271	025A	159.00	06/28/15	1455	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
272	025A	159.00	06/15/14	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
273	025A	159.00	03/06/15	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
274	025A	159.00	05/11/11	1925	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	WIND
275	025A	159.00	07/08/15	1942	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
276	025A	159.00	05/15/13	1355	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
277	025A	159.05	04/03/12	0801	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
278	025A	159.10	11/22/15	1255	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
279	025A	159.10	11/18/12	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
280	025A	159.10	02/07/13	0820	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
281	025A	159.10	07/26/13	1905	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
282	025A	159.10	07/07/15	1655	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
283	025A	159.10	07/14/11	0135	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
284	025A	159.10	04/17/15	0515	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
285	025A	159.10	05/04/14	2005	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
286	025A	159.15	12/13/11	1336	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
287	025A	159.20	04/20/12	1845	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
288	025A	159.20	06/18/13	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
289	025A	159.20	07/29/11	1450	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
290	025A	159.25	02/09/13	2211	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
291	025A	159.25	07/16/12	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
292	025A	159.30	05/18/12	1850	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
293	025A	159.30	01/12/13	1103	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
294	025A	159.30	05/11/13	1325	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
295	025A	159.30	02/08/11	0513	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
296	025A	159.30	05/04/13	1110	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
297	025A	159.40	04/10/12	0515	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	FOG
298	025A	159.40	05/26/12	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
299	025A	159.40	09/30/12	1108	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
300	025A	159.40	02/03/12	1615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
241	025A	158.80	10/09/14	1404	N	SIGN	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
242	025A	158.80	02/13/12	1335	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
243	025A	158.80	10/12/14	1541	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	068	OTHER
244	025A	158.80	07/09/15	1815	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	075	OTHER
245	025A	158.80	07/30/14	1820	N	CULVERT/HEADWALL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
246	025A	158.80	11/27/13	0855	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	SLOWING
247	025A	158.88	04/18/15	2026	N	OTHER FIXED OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
248	025A	158.90	09/05/14	0700	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
249	025A	158.90	05/19/15	0625	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
250	025A	158.90	08/28/11	1735	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
251	025A	158.90	09/04/13	0820	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
252	025A	158.90	11/17/14	0435	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
253	025A	158.90	03/25/15	1255	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
254	025A	158.90	05/05/15	1310	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
255	025A	158.90	12/11/12	1650	N	CULVERT/HEADWALL	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT
256	025A	158.90	05/08/15	1946	N	FENCE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
257	025A	158.90	09/06/13	2038	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
258	025A	158.92	10/22/15	0715	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
259	025A	158.94	03/20/11	1415	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
260	025A	158.98	11/29/11	0130	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	AVOIDING OBJECT/VEHICLE IN ROAD
261	025A	159.00	08/16/13	0950	N	VEHICLE CARGO/DEBRIS	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
262	025A	159.00	07/16/11	1420	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	GOING STRAIGHT
263	025A	159.00	08/05/11	1220	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
264	025A	159.00	12/30/11	1800	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	030	GOING STRAIGHT
265	025A	159.00	08/18/12	1315	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
266	025A	159.00	08/25/12	1140	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
267	025A	159.00	10/05/12	1745	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
268	025A	159.00	08/02/13	1200	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	SLOWING
269	025A	159.00	04/01/14	1320	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
270	025A	159.00	07/24/14	1425	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
271	025A	159.00	06/28/15	1455	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
272	025A	159.00	06/15/14	1245	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
273	025A	159.00	03/06/15	0910	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	WEAVING
274	025A	159.00	05/11/11	1925	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	GOING STRAIGHT
275	025A	159.00	07/08/15	1942	N	OTHER FIXED OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
276	025A	159.00	05/15/13	1355	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
277	025A	159.05	04/03/12	0801	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	OTHER
278	025A	159.10	11/22/15	1255	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
279	025A	159.10	11/18/12	1150	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
280	025A	159.10	02/07/13	0820	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
281	025A	159.10	07/26/13	1905	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	GOING STRAIGHT
282	025A	159.10	07/07/15	1655	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
283	025A	159.10	07/14/11	0135	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
284	025A	159.10	04/17/15	0515	N	GUARD RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	OTHER
285	025A	159.10	05/04/14	2005	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
286	025A	159.15	12/13/11	1336	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
287	025A	159.20	04/20/12	1845	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	OTHER
288	025A	159.20	06/18/13	1515	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
289	025A	159.20	07/29/11	1450	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
290	025A	159.25	02/09/13	2211	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
291	025A	159.25	07/16/12	1245	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
292	025A	159.30	05/18/12	1850	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
293	025A	159.30	01/12/13	1103	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
294	025A	159.30	05/11/13	1325	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
295	025A	159.30	02/08/11	0513	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
296	025A	159.30	05/04/13	1110	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	015	SLOWING
297	025A	159.40	04/10/12	0515	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
298	025A	159.40	05/26/12	1240	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
299	025A	159.40	09/30/12	1108	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
300	025A	159.40	02/03/12	1615	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
301	025A	159.46	10/22/12	1240	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
302	025A	159.49	03/04/13	1210	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
303	025A	159.50	07/30/15	1305	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
304	025A	159.50	11/13/12	1745	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
305	025A	159.50	05/25/12	1725	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
306	025A	159.50	09/22/12	1235	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
307	025A	159.50	08/04/13	1540	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
308	025A	159.50	07/24/14	1420	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
309	025A	159.50	09/05/14	2240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
310	025A	159.50	11/03/14	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
311	025A	159.50	02/24/12	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
312	025A	159.50	09/05/12	2344	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
313	025A	159.51	12/01/12	2310	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
314	025A	159.52	08/30/13	1435	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
315	025A	159.56	12/20/14	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
316	025A	159.60	08/22/11	1035	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
317	025A	159.60	07/14/12	0955	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
318	025A	159.60	05/26/12	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
319	025A	159.60	08/15/12	2240	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
320	025A	159.60	08/19/12	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
321	025A	159.60	06/26/14	2335	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
322	025A	159.60	04/17/15	0330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
323	025A	159.62	09/05/12	2230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
324	025A	159.68	09/06/11	1120	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
325	025A	159.70	07/23/11	1905	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
326	025A	159.70	06/17/13	0240	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
327	025A	159.70	07/01/14	2234	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
328	025A	159.70	09/29/12	1957	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
329	025A	159.75	12/30/12	1120	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
330	025A	159.80	09/18/15	0430	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	UNKNOWN
331	025A	159.80	10/19/13	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
332	025A	159.80	07/01/14	2105	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
333	025A	159.80	06/06/14	0033	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
334	025A	159.80	10/15/14	2256	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
335	025A	159.80	12/16/15	0500	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	NONE
336	025A	159.90	01/21/12	1955	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	WIND
337	025A	159.90	06/07/12	0940	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
338	025A	159.90	06/17/13	0310	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
339	025A	159.91	07/19/12	1538	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
340	025A	159.92	01/29/12	1709	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
341	025A	159.92	04/03/12	1200	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
342	025A	159.96	02/06/12	0940	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
343	025A	160.00	05/09/15	0450	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
344	025A	160.00	02/01/11	1820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
345	025A	160.00	08/02/13	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
346	025A	160.00	12/29/13	1215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
347	025A	160.00	11/16/11	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
348	025A	160.00	05/05/12	2030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
349	025A	160.00	07/21/12	2330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
350	025A	160.00	05/07/15	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
351	025A	160.00	06/14/15	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
352	025A	160.00	07/10/15	0315	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
353	025A	160.00	01/06/13	2140	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
354	025A	160.00	04/13/14	1805	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
355	025A	160.00	04/27/13	1938	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
356	025A	160.05	07/31/11	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
357	025A	160.10	12/29/13	1220	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
358	025A	160.10	06/29/14	2358	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
359	025A	160.10	07/25/14	1345	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
360	025A	160.10	07/25/14	1415	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
301	025A	159.46	10/22/12	1240	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
302	025A	159.49	03/04/13	1210	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
303	025A	159.50	07/30/15	1305	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
304	025A	159.50	11/13/12	1745	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
305	025A	159.50	05/25/12	1725	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
306	025A	159.50	09/22/12	1235	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
307	025A	159.50	08/04/13	1540	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
308	025A	159.50	07/24/14	1420	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
309	025A	159.50	09/05/14	2240	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
310	025A	159.50	11/03/14	1530	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
311	025A	159.50	02/24/12	1700	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
312	025A	159.50	09/05/12	2344	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
313	025A	159.51	12/01/12	2310	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
314	025A	159.52	08/30/13	1435	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	GOING STRAIGHT
315	025A	159.56	12/20/14	1115	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
316	025A	159.60	08/22/11	1035	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
317	025A	159.60	07/14/12	0955	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	040	AVOIDING OBJECT/VEHICLE IN ROAD
318	025A	159.60	05/26/12	1240	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
319	025A	159.60	08/15/12	2240	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	090	OTHER
320	025A	159.60	08/19/12	1020	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
321	025A	159.60	06/26/14	2335	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
322	025A	159.60	04/17/15	0330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
323	025A	159.62	09/05/12	2230	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
324	025A	159.68	09/06/11	1120	N	SIGN	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
325	025A	159.70	07/23/11	1905	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
326	025A	159.70	06/17/13	0240	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
327	025A	159.70	07/01/14	2234	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
328	025A	159.70	09/29/12	1957	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	065	GOING STRAIGHT
329	025A	159.75	12/30/12	1120	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
330	025A	159.80	09/18/15	0430	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
331	025A	159.80	10/19/13	1110	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
332	025A	159.80	07/01/14	2105	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	030	GOING STRAIGHT
333	025A	159.80	06/06/14	0033	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
334	025A	159.80	10/15/14	2256	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	OTHER
335	025A	159.80	12/16/15	0500	N	GUARD RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
336	025A	159.90	01/21/12	1955	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
337	025A	159.90	06/07/12	0940	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
338	025A	159.90	06/17/13	0310	N	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
339	025A	159.91	07/19/12	1538	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
340	025A	159.92	01/29/12	1709	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
341	025A	159.92	04/03/12	1200	N	CULVERT/HEADWALL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
342	025A	159.96	02/06/12	0940	N	GUARD RAIL	N	SUV W/TRAILER	NO IMPAIRMENT	UNKNOWN	060	OTHER
343	025A	160.00	05/09/15	0450	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
344	025A	160.00	02/01/11	1820	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
345	025A	160.00	08/02/13	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	003	SLOWING
346	025A	160.00	12/29/13	1215	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
347	025A	160.00	11/16/11	1110	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	CHANGING LANES
348	025A	160.00	05/05/12	2030	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	WEAVING
349	025A	160.00	07/21/12	2330	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
350	025A	160.00	05/07/15	1300	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
351	025A	160.00	06/14/15	1700	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
352	025A	160.00	07/10/15	0315	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
353	025A	160.00	01/06/13	2140	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
354	025A	160.00	04/13/14	1805	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
355	025A	160.00	04/27/13	1938	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
356	025A	160.05	07/31/11	1400	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	GOING STRAIGHT
357	025A	160.10	12/29/13	1220	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
358	025A	160.10	06/29/14	2358	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
359	025A	160.10	07/25/14	1345	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	020	GOING STRAIGHT
360	025A	160.10	07/25/14	1415	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	045	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
361	025A	160.10	11/09/15	1920	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
362	025A	160.10	01/29/13	2230	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
363	025A	160.10	06/29/14	2215	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
364	025A	160.15	12/19/12	0718	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
365	025A	160.16	11/17/15	1336	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	WIND
366	025A	160.18	02/18/14	1025	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
367	025A	160.20	10/26/11	1435	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
368	025A	160.20	09/03/11	1044	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
369	025A	160.20	08/06/15	0650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
370	025A	160.20	08/06/15	0650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
371	025A	160.20	02/20/14	2015	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	WIND
372	025A	160.20	06/23/14	2130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
373	025A	160.20	06/24/14	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
374	025A	160.20	09/23/13	0510	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
375	025A	160.20	04/13/14	0900	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
376	025A	160.20	11/06/13	0315	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
377	025A	160.20	04/23/11	1728	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
378	025A	160.20	08/03/14	1000	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
379	025A	160.20	10/08/11	0820	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	SLUSHY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
380	025A	160.20	05/16/13	0745	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
381	025A	160.20	04/05/14	1555	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
382	025A	160.20	04/12/14	1059	INJ	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
383	025A	160.23	04/03/14	2245	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
384	025A	160.25	07/17/15	1239	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
385	025A	160.25	08/14/11	1540	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
386	025A	160.28	03/01/14	1115	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
387	025A	160.30	04/23/13	0615	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
388	025A	160.30	06/10/12	1600	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
389	025A	160.30	02/24/13	1121	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
390	025A	160.30	11/02/11	1220	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
391	025A	160.38	12/18/14	0800	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	FOG
392	025A	160.38	03/08/14	1532	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
393	025A	160.38	09/02/14	0935	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
394	025A	160.39	07/12/15	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
395	025A	160.39	10/29/12	1750	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
396	025A	160.40	07/12/15	1440	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
397	025A	160.40	06/13/13	2350	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
398	025A	160.40	07/28/14	1525	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
399	025A	160.40	06/16/15	1835	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
400	025A	160.40	01/05/12	1835	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
401	025A	160.40	05/04/14	0117	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
402	025A	160.40	07/06/11	1320	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
403	025A	160.40	10/26/11	1420	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
404	025A	160.40	04/26/14	0318	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
405	025A	160.40	06/21/14	1355	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
406	025A	160.40	04/25/15	2107	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
407	025A	160.43	04/14/12	0045	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
408	025A	160.44	07/28/13	0000	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
409	025A	160.45	06/01/11	1640	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
410	025A	160.45	01/11/12	0830	PDO	ON	RAMP	2	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
411	025A	160.45	03/25/11	1533	INJ	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
412	025A	160.45	06/16/12	1800	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
413	025A	160.46	09/26/13	1545	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
414	025A	160.46	12/05/11	2245	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DARK-LIGHTED	NONE
415	025A	160.46	01/12/12	1645	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
416	025A	160.46	12/08/12	1745	INJ	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
417	025A	160.46	12/30/14	1645	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
418	025A	160.46	12/30/14	1645	INJ	OFF LEFT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
419	025A	160.46	12/30/14	1645	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
420	025A	160.46	09/10/11	1240	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
361	025A	160.10	11/09/15	1920	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
362	025A	160.10	01/29/13	2230	N	LIGHT/UTILITY POLE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
363	025A	160.10	06/29/14	2215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
364	025A	160.15	12/19/12	0718	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
365	025A	160.16	11/17/15	1336	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
366	025A	160.18	02/18/14	1025	Y (E)	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	OTHER
367	025A	160.20	10/26/11	1435	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
368	025A	160.20	09/03/11	1044	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	070	GOING STRAIGHT
369	025A	160.20	08/06/15	0650	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
370	025A	160.20	08/06/15	0650	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
371	025A	160.20	02/20/14	2015	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
372	025A	160.20	06/23/14	2130	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
373	025A	160.20	06/24/14	1730	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
374	025A	160.20	09/23/13	0510	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
375	025A	160.20	04/13/14	0900	N	CONCRETE BARRIER	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	GOING STRAIGHT
376	025A	160.20	11/06/13	0315	N	FENCE	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
377	025A	160.20	04/23/11	1728	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
378	025A	160.20	08/03/14	1000	Y (E)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
379	025A	160.20	10/08/11	0820	Y (E)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
380	025A	160.20	05/16/13	0745	Y (E)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
381	025A	160.20	04/05/14	1555	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
382	025A	160.20	04/12/14	1059	Y (E)	SIDESWIPE SAME DIRECTION	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	055	PASSING
383	025A	160.23	04/03/14	2245	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	032	OTHER
384	025A	160.25	07/17/15	1239	Y (E)	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
385	025A	160.25	08/14/11	1540	Y (E)	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
386	025A	160.28	03/01/14	1115	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
387	025A	160.30	04/23/13	0615	N	REAR-END	S	SUV	ALCOHOL	UNKNOWN	065	OTHER
388	025A	160.30	06/10/12	1600	N	LIGHT/UTILITY POLE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
389	025A	160.30	02/24/13	1121	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
390	025A	160.30	11/02/11	1220	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
391	025A	160.38	12/18/14	0800	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
392	025A	160.38	03/08/14	1532	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	CHANGING LANES
393	025A	160.38	09/02/14	0935	Y (E)	FENCE	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
394	025A	160.39	07/12/15	1400	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
395	025A	160.39	10/29/12	1750	Y (E)	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
396	025A	160.40	07/12/15	1440	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
397	025A	160.40	06/13/13	2350	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
398	025A	160.40	07/28/14	1525	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	UK	GOING STRAIGHT
399	025A	160.40	06/16/15	1835	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
400	025A	160.40	01/05/12	1835	N	LIGHT/UTILITY POLE	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
401	025A	160.40	05/04/14	0117	N	LIGHT/UTILITY POLE	S	SUV	ALCOHOL	ASLEEP AT WHEEL	065	OTHER
402	025A	160.40	07/06/11	1320	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
403	025A	160.40	10/26/11	1420	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	075	SLOWING
404	025A	160.40	04/26/14	0318	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
405	025A	160.40	06/21/14	1355	N	CULVERT/HEADWALL	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
406	025A	160.40	04/25/15	2107	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
407	025A	160.43	04/14/12	0045	Y (B)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	MAKING LEFT TURN
408	025A	160.44	07/28/13	0000	Y (F)	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	040	GOING STRAIGHT
409	025A	160.45	06/01/11	1640	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
410	025A	160.45	01/11/12	0830	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
411	025A	160.45	03/25/11	1533	Y (F)	OVERTURNING	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	MAKING RIGHT TURN
412	025A	160.45	06/16/12	1800	Y (F)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	040	OTHER
413	025A	160.46	09/26/13	1545	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	080	GOING STRAIGHT
414	025A	160.46	12/05/11	2245	Y (E)	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	OTHER
415	025A	160.46	01/12/12	1645	Y (E)	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
416	025A	160.46	12/08/12	1745	Y (E)	GUARD RAIL	S	MOTORCYCLE	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
417	025A	160.46	12/30/14	1645	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
418	025A	160.46	12/30/14	1645	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
419	025A	160.46	12/30/14	1645	Y (E)	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	OTHER
420	025A	160.46	09/10/11	1240	Y (F)	SIGN	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
421	025A	160.46	06/27/11	1017	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
422	025A	160.47	10/25/11	1300	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
423	025A	160.47	09/17/14	1215	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
424	025A	160.48	07/22/13	1654	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
425	025A	160.48	01/26/14	2350	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	SNOW/SLEET/HAIL
426	025A	160.48	04/11/14	1950	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
427	025A	160.49	10/27/15	0358	FAT	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
428	025A	160.49	06/26/11	2230	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
429	025A	160.50	01/01/14	1802	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
430	025A	160.50	02/21/14	0947	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
431	025A	160.50	08/01/15	1150	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
432	025A	160.50	07/27/12	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
433	025A	160.50	01/09/14	1920	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
434	025A	160.50	04/17/15	0958	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
435	025A	160.50	05/23/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
436	025A	160.50	06/07/15	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
437	025A	160.50	02/03/12	1120	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
438	025A	160.50	10/13/13	2030	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
439	025A	160.50	07/26/14	1045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
440	025A	160.50	05/03/15	1810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
441	025A	160.50	01/07/13	1135	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
442	025A	160.50	06/17/13	2345	PDO	ON	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
443	025A	160.50	10/31/15	1355	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
444	025A	160.50	11/26/15	1522	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
445	025A	160.50	03/05/14	0240	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-LIGHTED	NONE
446	025A	160.50	02/20/14	1900	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
447	025A	160.50	09/05/15	1155	PDO	OFF LEFT	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	UNKNOWN
448	025A	160.50	12/17/12	1330	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
449	025A	160.50	09/03/11	1055	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
450	025A	160.50	10/29/11	1449	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
451	025A	160.50	04/17/13	0753	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
452	025A	160.50	04/18/13	0115	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
453	025A	160.50	08/30/13	1840	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
454	025A	160.50	08/07/14	1105	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
455	025A	160.50	05/03/15	1740	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
456	025A	160.50	07/11/15	1210	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
457	025A	160.50	06/11/15	0520	INJ	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
458	025A	160.50	02/01/11	1450	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
459	025A	160.50	01/12/13	2010	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
460	025A	160.50	02/14/15	1715	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
461	025A	160.50	01/19/15	1005	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
462	025A	160.50	03/01/15	2055	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DARK-LIGHTED	SNOW/SLEET/HAIL
463	025A	160.50	02/16/13	1545	PDO	ON	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
464	025A	160.50	04/11/13	0930	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
465	025A	160.50	09/14/14	1709	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
466	025A	160.50	10/05/12	0635	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SLUSHY	DAWN OR DUSK	SNOW/SLEET/HAIL
467	025A	160.52	10/11/12	1814	FAT	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
468	025A	160.58	04/03/14	0720	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
469	025A	160.60	07/02/11	1015	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
470	025A	160.60	10/26/11	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
471	025A	160.60	09/01/12	1320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
472	025A	160.60	07/29/13	0639	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
473	025A	160.60	06/20/15	1255	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
474	025A	160.60	04/23/12	0523	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
475	025A	160.60	04/03/14	1035	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
476	025A	160.60	07/06/14	1300	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
477	025A	160.60	08/02/14	1335	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
478	025A	160.60	10/27/15	2130	PDO	ON	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
479	025A	160.60	10/29/11	1732	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
480	025A	160.60	02/03/13	1145	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
421	025A	160.46	06/27/11	1017	Y (F)	CULVERT/HEADWALL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	GOING STRAIGHT
422	025A	160.47	10/25/11	1300	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
423	025A	160.47	09/17/14	1215	Y (F)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	060	OTHER
424	025A	160.48	07/22/13	1654	Y (F)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
425	025A	160.48	01/26/14	2350	Y (F)	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
426	025A	160.48	04/11/14	1950	Y (F)	FENCE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
427	025A	160.49	10/27/15	0358	Y (E)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
428	025A	160.49	06/26/11	2230	Y (F)	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	SLOWING
429	025A	160.50	01/01/14	1802	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	OTHER
430	025A	160.50	02/21/14	0947	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	020	OTHER
431	025A	160.50	08/01/15	1150	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
432	025A	160.50	07/27/12	1330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
433	025A	160.50	01/09/14	1920	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
434	025A	160.50	04/17/15	0958	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
435	025A	160.50	05/23/15	1220	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
436	025A	160.50	06/07/15	1445	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
437	025A	160.50	02/03/12	1120	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	OTHER
438	025A	160.50	10/13/13	2030	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	CHANGING LANES
439	025A	160.50	07/26/14	1045	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
440	025A	160.50	05/03/15	1810	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	CHANGING LANES
441	025A	160.50	01/07/13	1135	N	DOMESTIC ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
442	025A	160.50	06/17/13	2345	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
443	025A	160.50	10/31/15	1355	N	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
444	025A	160.50	11/26/15	1522	N	LIGHT/UTILITY POLE	S	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
445	025A	160.50	03/05/14	0240	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
446	025A	160.50	02/20/14	1900	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
447	025A	160.50	09/05/15	1155	N	CONCRETE BARRIER	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
448	025A	160.50	12/17/12	1330	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
449	025A	160.50	09/03/11	1055	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	SLOWING
450	025A	160.50	10/29/11	1449	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	075	GOING STRAIGHT
451	025A	160.50	04/17/13	0753	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
452	025A	160.50	04/18/13	0115	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	UK	OTHER
453	025A	160.50	08/30/13	1840	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
454	025A	160.50	08/07/14	1105	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
455	025A	160.50	05/03/15	1740	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
456	025A	160.50	07/11/15	1210	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
457	025A	160.50	06/11/15	0520	Y (B)	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
458	025A	160.50	02/01/11	1450	Y (E)	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
459	025A	160.50	01/12/13	2010	Y (E)	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	OTHER
460	025A	160.50	02/14/15	1715	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
461	025A	160.50	01/19/15	1005	Y (E)	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
462	025A	160.50	03/01/15	2055	Y (E)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
463	025A	160.50	02/16/13	1545	Y (F)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	OTHER
464	025A	160.50	04/11/13	0930	Y (F)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
465	025A	160.50	09/14/14	1709	Y (F)	OVERTURNING	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	OTHER
466	025A	160.50	10/05/12	0635	Y (F)	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
467	025A	160.52	10/11/12	1814	Y (E)	OVERTURNING	S	MOTORCYCLE	ALCOHOL	NONE APPARENT	055	PASSING
468	025A	160.58	04/03/14	0720	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
469	025A	160.60	07/02/11	1015	N	OTHER NON-COLLISION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
470	025A	160.60	10/26/11	1400	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
471	025A	160.60	09/01/12	1320	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	GOING STRAIGHT
472	025A	160.60	07/29/13	0639	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
473	025A	160.60	06/20/15	1255	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
474	025A	160.60	04/23/12	0523	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
475	025A	160.60	04/03/14	1035	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
476	025A	160.60	07/06/14	1300	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
477	025A	160.60	08/02/14	1335	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
478	025A	160.60	10/27/15	2130	Y (E)	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
479	025A	160.60	10/29/11	1732	Y (E)	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
480	025A	160.60	02/03/13	1145	Y (G)	LIGHT/UTILITY POLE	N	SUV	ALCOHOL	UNKNOWN	050	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
481	025A	160.61	10/06/12	0735	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	WET W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
482	025A	160.63	04/14/13	1210	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
483	025A	160.63	11/12/14	1200	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
484	025A	160.65	02/07/12	0110	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
485	025A	160.65	11/12/11	2200	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
486	025A	160.66	11/21/15	1112	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
487	025A	160.66	11/08/12	1740	INJ	ON	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
488	025A	160.66	10/24/15	0745	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
489	025A	160.66	09/30/13	1845	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
490	025A	160.66	05/17/11	0125	INJ	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
491	025A	160.66	01/20/11	1455	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
492	025A	160.68	10/05/12	0820	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
493	025A	160.69	03/22/15	2101	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
494	025A	160.70	07/21/13	2335	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
495	025A	160.70	05/21/13	0950	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
496	025A	160.70	09/25/12	1340	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
497	025A	160.70	10/26/11	1400	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
498	025A	160.70	09/13/13	2345	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
499	025A	160.70	05/11/14	1700	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
500	025A	160.70	01/21/15	2105	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
501	025A	160.70	03/15/14	0254	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
502	025A	160.70	10/05/12	0821	PDO	ON	RAMP	2	CURVE ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
503	025A	160.70	12/03/11	2000	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
504	025A	160.70	10/05/12	0810	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
505	025A	160.70	01/22/12	0048	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	WIND
506	025A	160.70	10/21/15	1055	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
507	025A	160.73	01/19/11	1115	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
508	025A	160.74	10/26/13	1115	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
509	025A	160.74	08/08/12	1730	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
510	025A	160.74	12/08/13	1815	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
511	025A	160.75	08/25/13	2100	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
512	025A	160.75	10/21/12	0730	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
513	025A	160.75	08/04/13	1540	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
514	025A	160.75	08/16/13	1855	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
515	025A	160.75	10/03/13	1320	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
516	025A	160.75	11/02/15	1600	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
517	025A	160.76	10/18/12	2025	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
518	025A	160.76	05/08/11	1230	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
519	025A	160.76	11/03/12	1520	INJ	ON	AT INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
520	025A	160.76	01/11/12	0915	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
521	025A	160.76	02/10/12	1230	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
522	025A	160.80	12/21/15	1040	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
523	025A	160.80	06/29/14	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
524	025A	160.80	08/03/13	2000	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
525	025A	160.80	07/04/15	0320	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
526	025A	160.80	08/22/11	1305	INJ	ON	NON-INTERSECTION	6	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
527	025A	160.80	06/14/14	1530	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
528	025A	160.89	02/03/12	1535	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
529	025A	160.90	01/28/11	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
530	025A	160.90	08/02/14	2335	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
531	025A	160.90	08/03/14	1530	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
532	025A	160.90	10/10/15	0010	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
533	025A	160.90	04/27/11	0001	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
534	025A	160.90	12/01/11	0555	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
535	025A	160.90	12/25/11	1140	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
536	025A	160.90	10/07/14	1315	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
537	025A	160.96	06/24/11	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
538	025A	160.96	04/03/11	2220	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
539	025A	160.98	05/09/13	1114	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
540	025A	160.98	06/15/13	2229	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
481	025A	160.61	10/06/12	0735	Y (G)	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
482	025A	160.63	04/14/13	1210	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	085	OTHER
483	025A	160.63	11/12/14	1200	Y (G)	DELINEATOR POST	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	OTHER
484	025A	160.65	02/07/12	0110	Y (B)	DELINEATOR POST	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
485	025A	160.65	11/12/11	2200	Y (E)	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
486	025A	160.66	11/21/15	1112	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
487	025A	160.66	11/08/12	1740	Y (E)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
488	025A	160.66	10/24/15	0745	Y (E)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	045	OTHER
489	025A	160.66	09/30/13	1845	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	CHANGING LANES
490	025A	160.66	05/17/11	0125	Y (E)	GUARD RAIL	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
491	025A	160.66	01/20/11	1455	Y (F)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
492	025A	160.68	10/05/12	0820	Y (E)	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
493	025A	160.69	03/22/15	2101	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	035	CHANGING LANES
494	025A	160.70	07/21/13	2335	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
495	025A	160.70	05/21/13	0950	N	SIGN	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
496	025A	160.70	09/25/12	1340	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
497	025A	160.70	10/26/11	1400	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
498	025A	160.70	09/13/13	2345	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
499	025A	160.70	05/11/14	1700	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
500	025A	160.70	01/21/15	2105	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
501	025A	160.70	03/15/14	0254	N	LARGE BOULDERS OR ROCKS	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	WEAVING
502	025A	160.70	10/05/12	0821	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	OTHER
503	025A	160.70	12/03/11	2000	Y (E)	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
504	025A	160.70	10/05/12	0810	Y (E)	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
505	025A	160.70	01/22/12	0048	Y (E)	DELINEATOR POST	S	SUV	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
506	025A	160.70	10/21/15	1055	Y (G)	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
507	025A	160.73	01/19/11	1115	Y (B)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
508	025A	160.74	10/26/13	1115	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
509	025A	160.74	08/08/12	1730	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
510	025A	160.74	12/08/13	1815	Y (E)	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	OTHER
511	025A	160.75	08/25/13	2100	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
512	025A	160.75	10/21/12	0730	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	078	GOING STRAIGHT
513	025A	160.75	08/04/13	1540	Y (B)	REAR-END	NW	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	MAKING LEFT TURN
514	025A	160.75	08/16/13	1855	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
515	025A	160.75	10/03/13	1320	Y (B)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
516	025A	160.75	11/02/15	1600	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	002	GOING STRAIGHT
517	025A	160.76	10/18/12	2025	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	075	OTHER
518	025A	160.76	05/08/11	1230	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	050	SLOWING
519	025A	160.76	11/03/12	1520	Y (N)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	OTHER
520	025A	160.76	01/11/12	0915	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
521	025A	160.76	02/10/12	1230	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING RIGHT TURN
522	025A	160.80	12/21/15	1040	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
523	025A	160.80	06/29/14	1145	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
524	025A	160.80	08/03/13	2000	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	070	OTHER
525	025A	160.80	07/04/15	0320	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
526	025A	160.80	08/22/11	1305	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
527	025A	160.80	06/14/14	1530	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	GOING STRAIGHT
528	025A	160.89	02/03/12	1535	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
529	025A	160.90	01/28/11	2000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
530	025A	160.90	08/02/14	2335	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	GOING STRAIGHT
531	025A	160.90	08/03/14	1530	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
532	025A	160.90	10/10/15	0010	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
533	025A	160.90	04/27/11	0001	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
534	025A	160.90	12/01/11	0555	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
535	025A	160.90	12/25/11	1140	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
536	025A	160.90	10/07/14	1315	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
537	025A	160.96	06/24/11	1430	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
538	025A	160.96	04/03/11	2220	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
539	025A	160.98	05/09/13	1114	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
540	025A	160.98	06/15/13	2229	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
541	025A	160.99	08/12/11	1855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
542	025A	160.99	07/17/12	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
543	025A	161.00	02/01/15	0205	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
544	025A	161.00	07/21/11	1720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
545	025A	161.00	09/26/12	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
546	025A	161.00	08/19/13	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
547	025A	161.00	09/20/13	2120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
548	025A	161.00	10/13/13	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
549	025A	161.00	07/18/14	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
550	025A	161.00	10/25/14	1440	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
551	025A	161.00	01/02/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
552	025A	161.00	07/11/15	1641	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
553	025A	161.00	08/05/15	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
554	025A	161.00	08/05/15	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
555	025A	161.00	09/13/15	1154	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
556	025A	161.00	11/05/15	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
557	025A	161.00	11/21/15	1345	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
558	025A	161.00	12/21/15	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
559	025A	161.00	03/18/11	1615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
560	025A	161.00	06/15/14	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
561	025A	161.00	03/03/15	1840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
562	025A	161.00	11/05/15	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
563	025A	161.00	06/23/15	2245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
564	025A	161.00	12/27/14	1340	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
565	025A	161.00	09/29/13	0130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
566	025A	161.00	04/04/11	0230	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	NONE
567	025A	161.00	05/09/13	1100	INJ	OFF LEFT	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
568	025A	161.00	02/02/12	2115	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
569	025A	161.00	11/30/14	1726	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	FOG
570	025A	161.00	01/02/15	1205	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
571	025A	161.00	10/10/15	0010	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
572	025A	161.03	04/27/14	0635	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
573	025A	161.05	04/27/12	0110	PDO	OFF RIGHT	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
574	025A	161.06	10/24/14	1530	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
575	025A	161.08	07/30/13	0025	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
576	025A	161.10	08/12/14	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
577	025A	161.10	10/16/11	1531	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
578	025A	161.10	12/13/14	0110	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DARK-LIGHTED	NONE
579	025A	161.10	02/07/15	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
580	025A	161.10	09/19/15	1135	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
581	025A	161.10	10/27/13	2010	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
582	025A	161.10	10/20/14	0358	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
583	025A	161.10	05/19/11	0545	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
584	025A	161.10	08/10/11	1517	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
585	025A	161.10	09/13/11	1920	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
586	025A	161.10	09/14/11	1514	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
587	025A	161.10	10/17/11	1252	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
588	025A	161.10	09/25/12	1514	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
589	025A	161.10	09/25/12	1607	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
590	025A	161.10	09/25/12	1654	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
591	025A	161.10	09/25/12	1710	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
592	025A	161.10	05/08/13	2300	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
593	025A	161.10	08/17/15	1620	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
594	025A	161.10	06/21/14	0032	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
595	025A	161.10	06/14/13	1533	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
596	025A	161.10	08/10/14	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
597	025A	161.20	04/27/11	0930	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
598	025A	161.20	05/23/14	2240	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
599	025A	161.20	12/27/14	1228	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
600	025A	161.20	05/31/15	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
541	025A	160.99	08/12/11	1855	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
542	025A	160.99	07/17/12	0945	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
543	025A	161.00	02/01/15	0205	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	070	OTHER
544	025A	161.00	07/21/11	1720	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	UK	GOING STRAIGHT
545	025A	161.00	09/26/12	0815	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	SLOWING
546	025A	161.00	08/19/13	1610	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
547	025A	161.00	09/20/13	2120	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
548	025A	161.00	10/13/13	1600	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	AVOIDING OBJECT/VEHICLE IN ROAD
549	025A	161.00	07/18/14	1115	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	010	CHANGING LANES
550	025A	161.00	10/25/14	1440	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	CHANGING LANES
551	025A	161.00	01/02/15	1220	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
552	025A	161.00	07/11/15	1641	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
553	025A	161.00	08/05/15	0645	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	045	SLOWING
554	025A	161.00	08/05/15	0645	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	045	SLOWING
555	025A	161.00	09/13/15	1154	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
556	025A	161.00	11/05/15	1815	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
557	025A	161.00	11/21/15	1345	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
558	025A	161.00	12/21/15	1130	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
559	025A	161.00	03/18/11	1615	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
560	025A	161.00	06/15/14	1520	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
561	025A	161.00	03/03/15	1840	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
562	025A	161.00	11/05/15	1815	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
563	025A	161.00	06/23/15	2245	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
564	025A	161.00	12/27/14	1340	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
565	025A	161.00	09/29/13	0130	N	BRIDGE RAIL	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	075	GOING STRAIGHT
566	025A	161.00	04/04/11	0230	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
567	025A	161.00	05/09/13	1100	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	CHANGING LANES
568	025A	161.00	02/02/12	2115	N	FENCE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
569	025A	161.00	11/30/14	1726	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
570	025A	161.00	01/02/15	1205	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	GOING STRAIGHT
571	025A	161.00	10/10/15	0010	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	073	GOING STRAIGHT
572	025A	161.03	04/27/14	0635	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
573	025A	161.05	04/27/12	0110	N	LIGHT/UTILITY POLE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
574	025A	161.06	10/24/14	1530	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
575	025A	161.08	07/30/13	0025	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
576	025A	161.10	08/12/14	1320	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
577	025A	161.10	10/16/11	1531	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
578	025A	161.10	12/13/14	0110	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	085	GOING STRAIGHT
579	025A	161.10	02/07/15	1145	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
580	025A	161.10	09/19/15	1135	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
581	025A	161.10	10/27/13	2010	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
582	025A	161.10	10/20/14	0358	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
583	025A	161.10	05/19/11	0545	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
584	025A	161.10	08/10/11	1517	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
585	025A	161.10	09/13/11	1920	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
586	025A	161.10	09/14/11	1514	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
587	025A	161.10	10/17/11	1252	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
588	025A	161.10	09/25/12	1514	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
589	025A	161.10	09/25/12	1607	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
590	025A	161.10	09/25/12	1654	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
591	025A	161.10	09/25/12	1710	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
592	025A	161.10	05/08/13	2300	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
593	025A	161.10	08/17/15	1620	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
594	025A	161.10	06/21/14	0032	N	CULVERT/HEADWALL	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	080	OTHER
595	025A	161.10	06/14/13	1533	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	SLOWING
596	025A	161.10	08/10/14	1530	N	REAR-END	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	045	GOING STRAIGHT
597	025A	161.20	04/27/11	0930	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
598	025A	161.20	05/23/14	2240	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	GOING STRAIGHT
599	025A	161.20	12/27/14	1228	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
600	025A	161.20	05/31/15	1640	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
601	025A	161.20	04/27/11	0500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
602	025A	161.20	08/18/14	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
603	025A	161.20	08/15/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
604	025A	161.20	11/21/15	1105	PDO	ON	NON-INTERSECTION	6	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
605	025A	161.20	07/01/12	2245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
606	025A	161.20	06/23/14	2010	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
607	025A	161.20	06/27/14	2041	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
608	025A	161.20	09/14/11	1711	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
609	025A	161.20	04/17/13	1350	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
610	025A	161.20	06/21/14	1535	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
611	025A	161.20	04/18/12	2215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
612	025A	161.23	07/07/14	1427	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
613	025A	161.25	12/12/12	1652	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
614	025A	161.25	05/07/12	1125	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
615	025A	161.25	05/07/12	1130	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
616	025A	161.25	05/22/15	0600	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
617	025A	161.26	10/26/11	0645	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
618	025A	161.30	11/30/12	1836	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
619	025A	161.30	04/07/13	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
620	025A	161.30	04/27/11	0445	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
621	025A	161.30	03/18/11	0505	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
622	025A	161.30	09/25/12	1430	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
623	025A	161.30	09/25/12	1430	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
624	025A	161.30	01/31/11	1950	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
625	025A	161.30	02/20/14	0930	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
626	025A	161.33	11/12/13	1120	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
627	025A	161.35	02/22/12	0340	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
628	025A	161.40	04/16/12	2020	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
629	025A	161.40	06/16/15	2015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
630	025A	161.40	04/03/11	2135	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
631	025A	161.40	07/06/15	2307	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
632	025A	161.40	10/07/14	1105	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
633	025A	161.40	11/29/15	1240	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
634	025A	161.45	07/23/11	1045	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
635	025A	161.50	07/15/11	1455	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
636	025A	161.50	07/28/11	1530	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
637	025A	161.50	08/11/13	1256	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
638	025A	161.50	08/30/13	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
639	025A	161.50	10/24/15	1615	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
640	025A	161.50	07/16/13	1357	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
641	025A	161.50	09/02/14	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
642	025A	161.50	12/14/15	2030	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
643	025A	161.50	08/06/11	2200	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
644	025A	161.50	11/20/11	2055	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
645	025A	161.50	08/25/13	0345	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
646	025A	161.50	11/09/13	2300	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
647	025A	161.50	06/06/15	0705	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
648	025A	161.50	03/23/13	0515	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
649	025A	161.50	04/03/15	0421	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
650	025A	161.50	04/13/11	2030	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
651	025A	161.50	05/19/15	0308	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
652	025A	161.54	02/20/14	0945	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
653	025A	161.58	10/26/13	1120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
654	025A	161.60	08/03/11	2008	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
655	025A	161.60	05/07/12	1145	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
656	025A	161.60	07/12/14	1255	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
657	025A	161.60	11/29/15	1645	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
658	025A	161.65	08/26/14	1115	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
659	025A	161.70	01/21/11	0600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
660	025A	161.70	06/12/15	0835	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
601	025A	161.20	04/27/11	0500	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
602	025A	161.20	08/18/14	1300	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	GOING STRAIGHT
603	025A	161.20	08/15/15	1330	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
604	025A	161.20	11/21/15	1105	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
605	025A	161.20	07/01/12	2245	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
606	025A	161.20	06/23/14	2010	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
607	025A	161.20	06/27/14	2041	N	GUARD RAIL	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	WEAVING
608	025A	161.20	09/14/11	1711	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
609	025A	161.20	04/17/13	1350	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
610	025A	161.20	06/21/14	1535	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	060	OTHER
611	025A	161.20	04/18/12	2215	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
612	025A	161.23	07/07/14	1427	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
613	025A	161.25	12/12/12	1652	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
614	025A	161.25	05/07/12	1125	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
615	025A	161.25	05/07/12	1130	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
616	025A	161.25	05/22/15	0600	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
617	025A	161.26	10/26/11	0645	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
618	025A	161.30	11/30/12	1836	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
619	025A	161.30	04/07/13	1145	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
620	025A	161.30	04/27/11	0445	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
621	025A	161.30	03/18/11	0505	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	PASSING
622	025A	161.30	09/25/12	1430	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
623	025A	161.30	09/25/12	1430	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
624	025A	161.30	01/31/11	1950	N	FENCE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
625	025A	161.30	02/20/14	0930	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
626	025A	161.33	11/12/13	1120	Y (T)	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	BACKING
627	025A	161.35	02/22/12	0340	Y (T)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL/DRUGS	UNKNOWN	070	GOING STRAIGHT
628	025A	161.40	04/16/12	2020	N	OTHER NON-COLLISION	NE	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
629	025A	161.40	06/16/15	2015	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
630	025A	161.40	04/03/11	2135	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
631	025A	161.40	07/06/15	2307	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
632	025A	161.40	10/07/14	1105	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	SLOWING
633	025A	161.40	11/29/15	1240	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
634	025A	161.45	07/23/11	1045	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
635	025A	161.50	07/15/11	1455	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
636	025A	161.50	07/28/11	1530	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
637	025A	161.50	08/11/13	1256	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
638	025A	161.50	08/30/13	2000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
639	025A	161.50	10/24/15	1615	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
640	025A	161.50	07/16/13	1357	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
641	025A	161.50	09/02/14	1415	N	SIDESWIPE SAME DIRECTION	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
642	025A	161.50	12/14/15	2030	N	PARKED MOTOR VEHICLE	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	BACKING
643	025A	161.50	08/06/11	2200	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
644	025A	161.50	11/20/11	2055	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
645	025A	161.50	08/25/13	0345	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
646	025A	161.50	11/09/13	2300	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
647	025A	161.50	06/06/15	0705	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
648	025A	161.50	03/23/13	0515	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
649	025A	161.50	04/03/15	0421	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	055	OTHER
650	025A	161.50	04/13/11	2030	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
651	025A	161.50	05/19/15	0308	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
652	025A	161.54	02/20/14	0945	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
653	025A	161.58	10/26/13	1120	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
654	025A	161.60	08/03/11	2008	N	OVERTURNING	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	CHANGING LANES
655	025A	161.60	05/07/12	1145	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
656	025A	161.60	07/12/14	1255	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	SLOWING
657	025A	161.60	11/29/15	1645	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	SLOWING
658	025A	161.65	08/26/14	1115	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	068	CHANGING LANES
659	025A	161.70	01/21/11	0600	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
660	025A	161.70	06/12/15	0835	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	025	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
661	025A	161.70	01/19/15	1655	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
662	025A	161.70	01/12/13	1835	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
663	025A	161.70	02/20/13	1548	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
664	025A	161.75	05/04/13	0650	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
665	025A	161.80	08/26/13	1456	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
666	025A	161.80	07/23/15	0905	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
667	025A	161.80	04/28/11	1550	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
668	025A	161.80	03/07/13	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
669	025A	161.80	08/23/12	0144	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
670	025A	161.80	12/14/12	2338	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
671	025A	161.80	06/19/12	0856	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
672	025A	161.80	07/02/15	1200	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
673	025A	161.81	01/18/15	1410	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
674	025A	161.83	03/09/11	1231	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
675	025A	161.88	01/14/14	0352	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	WIND
676	025A	161.90	07/14/13	1215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
677	025A	161.90	05/15/14	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
678	025A	161.90	12/21/12	0155	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
679	025A	161.90	04/27/12	0100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
680	025A	161.99	09/26/14	1509	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
681	025A	161.99	10/22/12	0540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
682	025A	161.99	06/20/14	2028	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
683	025A	162.00	02/26/13	1520	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
684	025A	162.00	04/23/11	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
685	025A	162.00	05/07/12	1210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
686	025A	162.00	02/26/13	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
687	025A	162.00	06/16/13	1800	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
688	025A	162.00	10/18/13	0849	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
689	025A	162.00	06/21/14	1220	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
690	025A	162.00	06/21/14	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
691	025A	162.00	09/26/14	1358	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
692	025A	162.00	11/27/14	1040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
693	025A	162.00	08/07/15	1552	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
694	025A	162.00	11/01/15	2320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
695	025A	162.00	11/29/15	0330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
696	025A	162.00	10/10/13	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
697	025A	162.00	09/19/14	1800	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
698	025A	162.00	02/16/15	1202	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
699	025A	162.00	07/07/14	0345	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
700	025A	162.00	05/10/13	1705	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
701	025A	162.00	02/08/11	0750	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
702	025A	162.00	04/27/11	0615	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
703	025A	162.00	09/29/12	0818	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
704	025A	162.00	04/17/13	0620	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
705	025A	162.00	01/20/15	2120	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
706	025A	162.00	05/11/12	2155	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	NONE
707	025A	162.00	05/03/11	1350	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
708	025A	162.06	08/02/12	1130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
709	025A	162.08	08/23/13	1835	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
710	025A	162.10	06/21/14	1238	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
711	025A	162.10	05/06/15	1506	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
712	025A	162.10	11/28/14	1950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
713	025A	162.10	03/01/14	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
714	025A	162.10	08/13/12	0115	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
715	025A	162.10	02/16/11	1725	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
716	025A	162.10	04/19/15	0830	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
717	025A	162.10	08/13/12	0130	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
718	025A	162.10	05/24/11	0855	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
719	025A	162.13	06/20/15	2251	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
720	025A	162.15	04/03/12	0725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
661	025A	161.70	01/19/15	1655	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	070	GOING STRAIGHT
662	025A	161.70	01/12/13	1835	N	CULVERT/HEADWALL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	050	OTHER
663	025A	161.70	02/20/13	1548	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	OTHER
664	025A	161.75	05/04/13	0650	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
665	025A	161.80	08/26/13	1456	N	OVERTURNING	S	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
666	025A	161.80	07/23/15	0905	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
667	025A	161.80	04/28/11	1550	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
668	025A	161.80	03/07/13	0745	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
669	025A	161.80	08/23/12	0144	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
670	025A	161.80	12/14/12	2338	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	075	GOING STRAIGHT
671	025A	161.80	06/19/12	0856	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
672	025A	161.80	07/02/15	1200	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	AVOIDING OBJECT/VEHICLE IN ROAD
673	025A	161.81	01/18/15	1410	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
674	025A	161.83	03/09/11	1231	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
675	025A	161.88	01/14/14	0352	N	OVERTURNING	S	SUV	ALCOHOL	UNKNOWN	085	OTHER
676	025A	161.90	07/14/13	1215	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
677	025A	161.90	05/15/14	1115	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
678	025A	161.90	12/21/12	0155	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
679	025A	161.90	04/27/12	0100	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
680	025A	161.99	09/26/14	1509	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	060	GOING STRAIGHT
681	025A	161.99	10/22/12	0540	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
682	025A	161.99	06/20/14	2028	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
683	025A	162.00	02/26/13	1520	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	UK	AVOIDING OBJECT/VEHICLE IN ROAD
684	025A	162.00	04/23/11	1750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
685	025A	162.00	05/07/12	1210	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
686	025A	162.00	02/26/13	1520	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
687	025A	162.00	06/16/13	1800	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
688	025A	162.00	10/18/13	0849	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	SLOWING
689	025A	162.00	06/21/14	1220	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	GOING STRAIGHT
690	025A	162.00	06/21/14	1245	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	025	GOING STRAIGHT
691	025A	162.00	09/26/14	1358	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
692	025A	162.00	11/27/14	1040	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
693	025A	162.00	08/07/15	1552	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
694	025A	162.00	11/01/15	2320	N	REAR-END	S	SUV	ALCOHOL	UNKNOWN	085	CHANGING LANES
695	025A	162.00	11/29/15	0330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	085	GOING STRAIGHT
696	025A	162.00	10/10/13	0850	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	070	CHANGING LANES
697	025A	162.00	09/19/14	1800	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
698	025A	162.00	02/16/15	1202	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	OTHER
699	025A	162.00	07/07/14	0345	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
700	025A	162.00	05/10/13	1705	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
701	025A	162.00	02/08/11	0750	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
702	025A	162.00	04/27/11	0615	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
703	025A	162.00	09/29/12	0818	N	GUARD RAIL	S	SUV W/TRAILER	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	OTHER
704	025A	162.00	04/17/13	0620	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	AVOIDING OBJECT/VEHICLE IN ROAD
705	025A	162.00	01/20/15	2120	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	080	WEAVING
706	025A	162.00	05/11/12	2155	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
707	025A	162.00	05/03/11	1350	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	WEAVING
708	025A	162.06	08/02/12	1130	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
709	025A	162.08	08/23/13	1835	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	085	GOING STRAIGHT
710	025A	162.10	06/21/14	1238	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	060	SLOWING
711	025A	162.10	05/06/15	1506	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
712	025A	162.10	11/28/14	1950	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
713	025A	162.10	03/01/14	1145	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	075	PASSING
714	025A	162.10	08/13/12	0115	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
715	025A	162.10	02/16/11	1725	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
716	025A	162.10	04/19/15	0830	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
717	025A	162.10	08/13/12	0130	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
718	025A	162.10	05/24/11	0855	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
719	025A	162.13	06/20/15	2251	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
720	025A	162.15	04/03/12	0725	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
721	025A	162.20	05/14/15	1534	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
722	025A	162.20	01/31/13	2037	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
723	025A	162.20	01/31/13	2037	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
724	025A	162.20	10/01/14	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
725	025A	162.20	02/09/14	0755	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
726	025A	162.20	08/26/14	0050	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
727	025A	162.20	11/23/14	0630	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
728	025A	162.20	07/23/11	1035	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
729	025A	162.20	12/21/11	1245	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
730	025A	162.20	05/02/12	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
731	025A	162.20	07/02/15	1130	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
732	025A	162.23	03/07/11	0600	INJ	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	FOG
733	025A	162.25	11/28/14	1740	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
734	025A	162.30	07/06/14	1841	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
735	025A	162.30	11/01/12	1845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
736	025A	162.30	03/01/12	1910	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
737	025A	162.35	10/14/12	0410	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
738	025A	162.40	11/21/13	0310	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
739	025A	162.40	12/28/13	2130	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAIL
740	025A	162.40	02/04/14	2330	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	NONE
741	025A	162.40	02/10/11	0015	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
742	025A	162.43	09/22/12	1125	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
743	025A	162.44	03/29/13	2230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
744	025A	162.50	05/07/15	0625	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
745	025A	162.50	04/18/14	1915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
746	025A	162.50	07/26/14	1536	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
747	025A	162.50	07/10/15	0835	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
748	025A	162.50	04/17/13	0615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	RAIN
749	025A	162.50	06/03/13	1021	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
750	025A	162.50	04/16/15	0625	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
751	025A	162.50	05/25/11	2010	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
752	025A	162.50	07/13/11	1323	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
753	025A	162.50	06/22/15	0550	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
754	025A	162.50	01/04/14	0930	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
755	025A	162.50	01/19/11	1608	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
756	025A	162.50	04/03/14	0920	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
757	025A	162.50	04/17/15	0953	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
758	025A	162.50	11/09/13	0556	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
759	025A	162.50	05/28/13	2005	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
760	025A	162.50	05/11/14	2105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
761	025A	162.60	09/11/12	1150	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
762	025A	162.60	01/08/13	1215	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
763	025A	162.60	01/31/13	2038	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
764	025A	162.60	10/30/11	0420	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
765	025A	162.60	08/09/15	1530	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
766	025A	162.60	03/09/13	1030	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
767	025A	162.60	04/28/14	0840	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
768	025A	162.70	03/29/11	0530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
769	025A	162.70	11/05/14	1520	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
770	025A	162.70	06/06/13	1929	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
771	025A	162.70	11/15/14	1438	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
772	025A	162.70	04/05/15	1638	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
773	025A	162.70	12/16/14	1035	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
774	025A	162.70	08/20/13	1815	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
775	025A	162.75	03/18/15	0645	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
776	025A	162.76	04/11/11	0530	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
777	025A	162.77	03/29/11	0510	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
778	025A	162.80	07/15/11	1835	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
779	025A	162.80	05/20/11	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
780	025A	162.80	07/01/15	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
721	025A	162.20	05/14/15	1534	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
722	025A	162.20	01/31/13	2037	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
723	025A	162.20	01/31/13	2037	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	SLOWING
724	025A	162.20	10/01/14	1600	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
725	025A	162.20	02/09/14	0755	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
726	025A	162.20	08/26/14	0050	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	080	GOING STRAIGHT
727	025A	162.20	11/23/14	0630	N	GUARD RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	OTHER
728	025A	162.20	07/23/11	1035	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	SLOWING
729	025A	162.20	12/21/11	1245	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
730	025A	162.20	05/02/12	1020	N	REAR-END	N	SUV	RX/MEDICATION/DR	UNKNOWN	075	GOING STRAIGHT
731	025A	162.20	07/02/15	1130	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	SLOWING
732	025A	162.23	03/07/11	0600	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
733	025A	162.25	11/28/14	1740	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
734	025A	162.30	07/06/14	1841	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
735	025A	162.30	11/01/12	1845	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
736	025A	162.30	03/01/12	1910	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	050	OTHER
737	025A	162.35	10/14/12	0410	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	ALCOHOL/DRUGS	ASLEEP AT WHEEL	079	GOING STRAIGHT
738	025A	162.40	11/21/13	0310	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
739	025A	162.40	12/28/13	2130	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
740	025A	162.40	02/04/14	2330	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	CHANGING LANES
741	025A	162.40	02/10/11	0015	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
742	025A	162.43	09/22/12	1125	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
743	025A	162.44	03/29/13	2230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
744	025A	162.50	05/07/15	0625	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	035	OTHER
745	025A	162.50	04/18/14	1915	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
746	025A	162.50	07/26/14	1536	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
747	025A	162.50	07/10/15	0835	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
748	025A	162.50	04/17/13	0615	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
749	025A	162.50	06/03/13	1021	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
750	025A	162.50	04/16/15	0625	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
751	025A	162.50	05/25/11	2010	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
752	025A	162.50	07/13/11	1323	N	WILD ANIMAL	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
753	025A	162.50	06/22/15	0550	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
754	025A	162.50	01/04/14	0930	N	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	UNKNOWN	025	CHANGING LANES
755	025A	162.50	01/19/11	1608	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
756	025A	162.50	04/03/14	0920	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
757	025A	162.50	04/17/15	0953	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
758	025A	162.50	11/09/13	0556	N	LARGE BOULDERS OR ROCKS	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
759	025A	162.50	05/28/13	2005	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
760	025A	162.50	05/11/14	2105	N	ROAD MAINTENANCE EQUIPMENT	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	PASSING
761	025A	162.60	09/11/12	1150	N	REAR-END	S	SUV	RX/MEDICATION/DR	DRIVER PREOCCUPIED	090	GOING STRAIGHT
762	025A	162.60	01/08/13	1215	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
763	025A	162.60	01/31/13	2038	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
764	025A	162.60	10/30/11	0420	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
765	025A	162.60	08/09/15	1530	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
766	025A	162.60	03/09/13	1030	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
767	025A	162.60	04/28/14	0840	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
768	025A	162.70	03/29/11	0530	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
769	025A	162.70	11/05/14	1520	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
770	025A	162.70	06/06/13	1929	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
771	025A	162.70	11/15/14	1438	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
772	025A	162.70	04/05/15	1638	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
773	025A	162.70	12/16/14	1035	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
774	025A	162.70	08/20/13	1815	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
775	025A	162.75	03/18/15	0645	N	OTHER FIXED OBJECT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ILLNESS	050	OTHER
776	025A	162.76	04/11/11	0530	N	GUARD RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
777	025A	162.77	03/29/11	0510	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
778	025A	162.80	07/15/11	1835	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
779	025A	162.80	05/20/11	1645	N	SIDESWIPE SAME DIRECTION	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
780	025A	162.80	07/01/15	0945	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
781	025A	162.80	02/04/12	0710	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
782	025A	162.80	10/24/12	2250	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
783	025A	162.80	12/09/12	0951	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
784	025A	162.80	04/13/14	1545	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
785	025A	162.80	04/13/14	1610	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
786	025A	162.80	10/04/14	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
787	025A	162.80	06/06/15	1448	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
788	025A	162.81	09/01/15	0540	FAT	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
789	025A	162.82	06/20/15	1518	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
790	025A	162.86	01/28/13	0645	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
791	025A	162.90	08/06/12	1235	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
792	025A	162.90	11/09/14	0425	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DARK-LIGHTED	NONE
793	025A	162.90	07/03/15	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
794	025A	162.90	11/25/11	2040	PDO	ON	NON-INTERSECTION	1	HILLCREST	DRY	DARK-LIGHTED	NONE
795	025A	162.90	04/15/12	0221	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
796	025A	162.90	03/10/13	2345	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
797	025A	162.90	03/04/15	0825	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
798	025A	162.90	10/24/12	2240	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
799	025A	162.90	01/04/14	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
800	025A	162.93	03/07/11	0520	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
801	025A	162.95	10/22/13	1440	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
802	025A	162.96	09/30/15	2115	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DARK-UNLIGHTED	NONE
803	025A	162.96	11/18/13	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
804	025A	162.97	01/06/14	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
805	025A	162.98	09/22/12	0505	PDO	ON	NON-INTERSECTION	1	HILLCREST	DRY	DARK-LIGHTED	NONE
806	025A	163.00	05/12/13	1045	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
807	025A	163.00	03/07/11	0615	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	FOG
808	025A	163.00	03/24/12	1035	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
809	025A	163.00	06/16/13	1705	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
810	025A	163.00	09/27/13	0140	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
811	025A	163.00	04/07/14	0720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
812	025A	163.00	03/16/11	0808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
813	025A	163.00	10/31/14	2020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	WIND
814	025A	163.00	11/04/14	1740	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DARK-LIGHTED	NONE
815	025A	163.00	02/11/15	2035	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
816	025A	163.00	04/02/15	2310	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	WIND
817	025A	163.00	04/17/15	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
818	025A	163.00	07/05/14	0905	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
819	025A	163.00	04/03/14	0815	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
820	025A	163.00	04/13/14	1603	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
821	025A	163.00	04/17/13	0550	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	RAIN
822	025A	163.00	01/09/11	1235	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
823	025A	163.00	03/07/11	0530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
824	025A	163.00	10/12/12	0420	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
825	025A	163.00	06/04/11	1250	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
826	025A	163.02	04/17/13	1646	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
827	025A	163.03	04/30/13	0605	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
828	025A	163.03	01/06/13	1830	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
829	025A	163.04	03/07/11	0520	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
830	025A	163.04	10/09/11	1230	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
831	025A	163.06	04/17/13	0520	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	RAIN
832	025A	163.10	02/05/14	0907	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
833	025A	163.10	07/17/11	2335	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
834	025A	163.10	03/23/13	1155	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
835	025A	163.10	04/17/13	0630	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	RAIN
836	025A	163.10	08/24/13	1045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
837	025A	163.10	04/03/14	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
838	025A	163.10	06/14/14	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
839	025A	163.10	06/14/14	1150	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
840	025A	163.10	06/20/14	1545	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
781	025A	162.80	02/04/12	0710	N	LIGHT/UTILITY POLE	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
782	025A	162.80	10/24/12	2250	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	OTHER
783	025A	162.80	12/09/12	0951	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
784	025A	162.80	04/13/14	1545	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
785	025A	162.80	04/13/14	1610	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
786	025A	162.80	10/04/14	1150	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
787	025A	162.80	06/06/15	1448	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
788	025A	162.81	09/01/15	0540	N	SIDESWIPE SAME DIRECTION	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	075	PASSING
789	025A	162.82	06/20/15	1518	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
790	025A	162.86	01/28/13	0645	N	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	085	GOING STRAIGHT
791	025A	162.90	08/06/12	1235	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
792	025A	162.90	11/09/14	0425	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
793	025A	162.90	07/03/15	1125	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	040	CHANGING LANES
794	025A	162.90	11/25/11	2040	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
795	025A	162.90	04/15/12	0221	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	OTHER
796	025A	162.90	03/10/13	2345	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
797	025A	162.90	03/04/15	0825	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	065	OTHER
798	025A	162.90	10/24/12	2240	N	TREE/SHRUBBERY	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
799	025A	162.90	01/04/14	0750	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
800	025A	162.93	03/07/11	0520	N	TREE/SHRUBBERY	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
801	025A	162.95	10/22/13	1440	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
802	025A	162.96	09/30/15	2115	N	REAR-END	N	NON-SCHOOL BUS < 15 PEOPLE	NO IMPAIRMENT	DISTRACTED BY PASSENGER	075	GOING STRAIGHT
803	025A	162.96	11/18/13	1245	N	PARKED MOTOR VEHICLE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	OTHER
804	025A	162.97	01/06/14	1540	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	070	CHANGING LANES
805	025A	162.98	09/22/12	0505	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
806	025A	163.00	05/12/13	1045	N	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	075	SLOWING
807	025A	163.00	03/07/11	0615	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
808	025A	163.00	03/24/12	1035	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
809	025A	163.00	06/16/13	1705	N	REAR-END	S	SUV W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	SLOWING
810	025A	163.00	09/27/13	0140	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
811	025A	163.00	04/07/14	0720	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
812	025A	163.00	03/16/11	0808	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
813	025A	163.00	10/31/14	2020	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
814	025A	163.00	11/04/14	1740	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	WEAVING
815	025A	163.00	02/11/15	2035	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
816	025A	163.00	04/02/15	2310	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
817	025A	163.00	04/17/15	1430	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
818	025A	163.00	07/05/14	0905	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
819	025A	163.00	04/03/14	0815	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	OTHER
820	025A	163.00	04/13/14	1603	N	LIGHT/UTILITY POLE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
821	025A	163.00	04/17/13	0550	N	SIGN	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
822	025A	163.00	01/09/11	1235	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
823	025A	163.00	03/07/11	0530	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
824	025A	163.00	10/12/12	0420	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
825	025A	163.00	06/04/11	1250	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
826	025A	163.02	04/17/13	1646	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
827	025A	163.03	04/30/13	0605	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
828	025A	163.03	01/06/13	1830	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
829	025A	163.04	03/07/11	0520	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
830	025A	163.04	10/09/11	1230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
831	025A	163.06	04/17/13	0520	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
832	025A	163.10	02/05/14	0907	N	OTHER NON-COLLISION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
833	025A	163.10	07/17/11	2335	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	NONE APPARENT	090	WEAVING
834	025A	163.10	03/23/13	1155	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	SLOWING
835	025A	163.10	04/17/13	0630	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
836	025A	163.10	08/24/13	1045	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
837	025A	163.10	04/03/14	0840	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
838	025A	163.10	06/14/14	1145	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
839	025A	163.10	06/14/14	1150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
840	025A	163.10	06/20/14	1545	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
841	025A	163.10	06/21/14	1150	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
842	025A	163.10	12/28/14	1320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
843	025A	163.10	07/26/14	1300	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
844	025A	163.10	04/07/14	0826	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
845	025A	163.10	10/26/11	1510	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
846	025A	163.10	03/29/12	1550	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
847	025A	163.10	05/06/15	0640	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
848	025A	163.10	06/14/14	1015	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
849	025A	163.20	02/03/12	1330	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
850	025A	163.20	02/05/14	0840	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
851	025A	163.20	09/25/15	0750	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
852	025A	163.20	03/07/11	0715	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	FOG
853	025A	163.20	07/09/11	1722	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
854	025A	163.20	09/29/12	1054	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
855	025A	163.20	09/02/13	1625	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
856	025A	163.20	05/25/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
857	025A	163.20	04/16/11	0900	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
858	025A	163.20	10/21/12	1253	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
859	025A	163.20	04/17/13	0545	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	RAIN
860	025A	163.20	05/24/14	1405	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
861	025A	163.20	05/05/15	1435	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
862	025A	163.20	05/20/15	1600	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
863	025A	163.22	03/07/11	0701	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
864	025A	163.22	12/21/12	1536	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
865	025A	163.23	06/05/15	1410	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
866	025A	163.25	12/27/14	1438	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
867	025A	163.25	06/03/15	1430	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
868	025A	163.28	10/18/13	0820	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
869	025A	163.30	12/20/11	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
870	025A	163.30	07/11/12	1847	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
871	025A	163.30	03/28/13	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
872	025A	163.30	05/10/13	1805	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
873	025A	163.30	04/07/14	0705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
874	025A	163.30	07/29/14	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
875	025A	163.30	04/07/14	0530	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
876	025A	163.30	03/22/13	0140	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
877	025A	163.30	06/03/13	1000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
878	025A	163.30	02/13/12	2230	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	NONE
879	025A	163.31	12/24/15	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
880	025A	163.31	03/09/13	0925	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
881	025A	163.32	10/21/14	0725	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
882	025A	163.32	11/07/15	1623	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
883	025A	163.32	01/02/15	0930	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
884	025A	163.32	01/12/12	1300	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
885	025A	163.32	10/11/14	1300	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
886	025A	163.32	04/05/15	1215	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
887	025A	163.32	06/08/13	1549	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
888	025A	163.35	08/17/13	1335	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
889	025A	163.35	01/16/11	1500	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
890	025A	163.36	08/01/12	1540	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
891	025A	163.37	06/16/14	1545	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
892	025A	163.39	08/30/13	1515	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
893	025A	163.40	06/13/12	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
894	025A	163.40	11/19/11	2130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
895	025A	163.41	05/13/11	1433	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
896	025A	163.46	12/16/12	0930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
897	025A	163.47	02/19/12	1305	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
898	025A	163.48	01/10/13	1500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
899	025A	163.49	10/22/13	1320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
900	025A	163.50	04/17/13	0644	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
841	025A	163.10	06/21/14	1150	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
842	025A	163.10	12/28/14	1320	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
843	025A	163.10	07/26/14	1300	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
844	025A	163.10	04/07/14	0826	N	LIGHT/UTILITY POLE	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
845	025A	163.10	10/26/11	1510	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
846	025A	163.10	03/29/12	1550	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
847	025A	163.10	05/06/15	0640	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
848	025A	163.10	06/14/14	1015	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
849	025A	163.20	02/03/12	1330	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
850	025A	163.20	02/05/14	0840	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
851	025A	163.20	09/25/15	0750	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
852	025A	163.20	03/07/11	0715	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
853	025A	163.20	07/09/11	1722	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
854	025A	163.20	09/29/12	1054	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
855	025A	163.20	09/02/13	1625	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
856	025A	163.20	05/25/14	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
857	025A	163.20	04/16/11	0900	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
858	025A	163.20	10/21/12	1253	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
859	025A	163.20	04/17/13	0545	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
860	025A	163.20	05/24/14	1405	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	GOING STRAIGHT
861	025A	163.20	05/05/15	1435	N	GUARD RAIL	S	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	075	OTHER
862	025A	163.20	05/20/15	1600	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
863	025A	163.22	03/07/11	0701	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
864	025A	163.22	12/21/12	1536	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
865	025A	163.23	06/05/15	1410	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
866	025A	163.25	12/27/14	1438	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
867	025A	163.25	06/03/15	1430	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
868	025A	163.28	10/18/13	0820	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
869	025A	163.30	12/20/11	1245	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
870	025A	163.30	07/11/12	1847	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
871	025A	163.30	03/28/13	1620	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	SLOWING
872	025A	163.30	05/10/13	1805	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
873	025A	163.30	04/07/14	0705	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	AVOIDING OBJECT/VEHICLE IN ROAD
874	025A	163.30	07/29/14	0645	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
875	025A	163.30	04/07/14	0530	N	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
876	025A	163.30	03/22/13	0140	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
877	025A	163.30	06/03/13	1000	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	OTHER
878	025A	163.30	02/13/12	2230	Y (B)	DELINEATOR POST	N	PASS CAR/VAN	ALCOHOL	DRIVER PREOCCUPIED	060	GOING STRAIGHT
879	025A	163.31	12/24/15	1450	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	GOING STRAIGHT
880	025A	163.31	03/09/13	0925	Y (B)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
881	025A	163.32	10/21/14	0725	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
882	025A	163.32	11/07/15	1623	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	MAKING RIGHT TURN
883	025A	163.32	01/02/15	0930	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
884	025A	163.32	01/12/12	1300	Y (O)	BROADSIDE	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
885	025A	163.32	10/11/14	1300	Y (O)	BROADSIDE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
886	025A	163.32	04/05/15	1215	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	010	MAKING LEFT TURN
887	025A	163.32	06/08/13	1549	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
888	025A	163.35	08/17/13	1335	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
889	025A	163.35	01/16/11	1500	Y (R)	FENCE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING RIGHT TURN
890	025A	163.36	08/01/12	1540	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
891	025A	163.37	06/16/14	1545	N	PARKED MOTOR VEHICLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
892	025A	163.39	08/30/13	1515	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
893	025A	163.40	06/13/12	0840	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
894	025A	163.40	11/19/11	2130	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
895	025A	163.41	05/13/11	1433	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	SLOWING
896	025A	163.46	12/16/12	0930	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
897	025A	163.47	02/19/12	1305	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
898	025A	163.48	01/10/13	1500	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
899	025A	163.49	10/22/13	1320	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
900	025A	163.50	04/17/13	0644	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
901	025A	163.50	02/16/15	1210	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
902	025A	163.50	10/09/11	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
903	025A	163.50	10/09/11	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
904	025A	163.50	03/30/12	1620	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
905	025A	163.50	07/20/12	2140	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
906	025A	163.50	02/28/14	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
907	025A	163.50	03/01/14	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
908	025A	163.50	06/10/14	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
909	025A	163.50	05/17/15	1507	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
910	025A	163.50	11/14/15	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
911	025A	163.50	03/01/12	1755	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
912	025A	163.50	06/30/11	0230	PDO	ON	NON-INTERSECTION	1	HILLCREST	DRY	DARK-LIGHTED	NONE
913	025A	163.50	06/25/11	2124	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
914	025A	163.50	05/12/11	0440	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
915	025A	163.50	05/14/12	1540	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
916	025A	163.50	08/30/15	1400	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
917	025A	163.50	08/01/12	1430	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
918	025A	163.50	06/10/14	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
919	025A	163.50	11/14/15	1200	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
920	025A	163.54	11/04/11	1820	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
921	025A	163.57	06/01/12	1525	PDO	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
922	025A	163.60	11/04/11	1845	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
923	025A	163.60	10/04/14	1155	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
924	025A	163.60	04/17/15	1535	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
925	025A	163.60	06/24/15	1158	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
926	025A	163.60	05/05/13	0239	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
927	025A	163.60	12/07/14	1755	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
928	025A	163.65	07/03/15	1419	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
929	025A	163.70	07/23/13	1405	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
930	025A	163.70	08/02/14	0955	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
931	025A	163.77	12/25/11	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
932	025A	163.80	07/09/14	0920	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
933	025A	163.80	06/21/15	1310	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
934	025A	163.80	08/14/15	1310	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
935	025A	163.80	04/03/15	1215	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
936	025A	163.80	06/26/14	1104	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
937	025A	163.80	07/16/14	1113	INJ	ON	NON-INTERSECTION	6	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
938	025A	163.80	07/26/14	1300	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
939	025A	163.90	01/09/12	0550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
940	025A	163.90	06/20/12	1035	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
941	025A	163.90	10/22/13	1414	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
942	025A	163.90	07/03/15	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
943	025A	163.90	07/03/15	1236	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
944	025A	163.90	07/17/15	1420	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
945	025A	163.90	10/09/11	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
946	025A	163.90	05/23/15	2230	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
947	025A	163.90	03/24/12	1015	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
948	025A	163.90	09/08/14	1645	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
949	025A	163.90	09/22/12	0945	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
950	025A	163.90	03/11/14	1340	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
951	025A	163.91	10/05/14	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
952	025A	163.92	08/02/14	1751	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
953	025A	163.95	08/02/11	1050	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
954	025A	163.98	05/16/14	1200	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
955	025A	163.98	09/01/14	1600	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
956	025A	163.99	03/21/12	2300	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
957	025A	164.00	03/21/12	2255	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
958	025A	164.00	02/14/14	0702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
959	025A	164.00	09/10/15	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
960	025A	164.00	01/09/12	0550	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
901	025A	163.50	02/16/15	1210	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	AVOIDING OBJECT/VEHICLE IN ROAD
902	025A	163.50	10/09/11	1730	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
903	025A	163.50	10/09/11	1730	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	CHANGING LANES
904	025A	163.50	03/30/12	1620	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
905	025A	163.50	07/20/12	2140	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	BACKING
906	025A	163.50	02/28/14	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	077	GOING STRAIGHT
907	025A	163.50	03/01/14	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	073	GOING STRAIGHT
908	025A	163.50	06/10/14	0710	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	100	GOING STRAIGHT
909	025A	163.50	05/17/15	1507	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
910	025A	163.50	11/14/15	1200	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
911	025A	163.50	03/01/12	1755	N	DOMESTIC ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
912	025A	163.50	06/30/11	0230	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
913	025A	163.50	06/25/11	2124	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	GOING STRAIGHT
914	025A	163.50	05/12/11	0440	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
915	025A	163.50	05/14/12	1540	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	GOING STRAIGHT
916	025A	163.50	08/30/15	1400	N	SIGN	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
917	025A	163.50	08/01/12	1430	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
918	025A	163.50	06/10/14	0910	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
919	025A	163.50	11/14/15	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
920	025A	163.54	11/04/11	1820	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
921	025A	163.57	06/01/12	1525	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
922	025A	163.60	11/04/11	1845	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
923	025A	163.60	10/04/14	1155	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
924	025A	163.60	04/17/15	1535	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
925	025A	163.60	06/24/15	1158	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
926	025A	163.60	05/05/13	0239	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
927	025A	163.60	12/07/14	1755	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
928	025A	163.65	07/03/15	1419	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
929	025A	163.70	07/23/13	1405	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
930	025A	163.70	08/02/14	0955	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
931	025A	163.77	12/25/11	1245	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
932	025A	163.80	07/09/14	0920	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
933	025A	163.80	06/21/15	1310	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
934	025A	163.80	08/14/15	1310	N	OTHER NON-COLLISION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	053	OTHER
935	025A	163.80	04/03/15	1215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
936	025A	163.80	06/26/14	1104	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
937	025A	163.80	07/16/14	1113	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
938	025A	163.80	07/26/14	1300	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
939	025A	163.90	01/09/12	0550	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
940	025A	163.90	06/20/12	1035	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
941	025A	163.90	10/22/13	1414	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
942	025A	163.90	07/03/15	1020	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
943	025A	163.90	07/03/15	1236	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
944	025A	163.90	07/17/15	1420	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
945	025A	163.90	10/09/11	1900	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
946	025A	163.90	05/23/15	2230	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
947	025A	163.90	03/24/12	1015	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
948	025A	163.90	09/08/14	1645	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
949	025A	163.90	09/22/12	0945	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	070	SLOWING
950	025A	163.90	03/11/14	1340	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
951	025A	163.91	10/05/14	1400	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
952	025A	163.92	08/02/14	1751	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	070	OTHER
953	025A	163.95	08/02/11	1050	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
954	025A	163.98	05/16/14	1200	N	EMBANKMENT CUT/FILL SLOPE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
955	025A	163.98	09/01/14	1600	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
956	025A	163.99	03/21/12	2300	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
957	025A	164.00	03/21/12	2255	N	OVERTURNING	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	095	OTHER
958	025A	164.00	02/14/14	0702	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
959	025A	164.00	09/10/15	1630	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
960	025A	164.00	01/09/12	0550	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
961	025A	164.00	09/08/12	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
962	025A	164.00	03/27/15	1345	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
963	025A	164.00	05/22/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
964	025A	164.00	06/27/15	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
965	025A	164.00	10/07/11	2115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
966	025A	164.00	04/27/13	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
967	025A	164.00	10/15/13	1935	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
968	025A	164.00	02/21/14	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
969	025A	164.00	06/05/13	0430	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
970	025A	164.04	06/24/15	1535	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
971	025A	164.08	11/05/11	1515	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
972	025A	164.10	03/16/14	1330	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
973	025A	164.10	08/23/14	1615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
974	025A	164.10	11/03/15	1910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
975	025A	164.10	03/21/12	2245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
976	025A	164.10	09/14/11	0540	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
977	025A	164.10	10/23/14	0017	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
978	025A	164.10	06/27/15	1455	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
979	025A	164.10	01/29/12	0321	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
980	025A	164.10	11/07/15	1115	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
981	025A	164.20	03/22/12	0120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
982	025A	164.20	04/19/13	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
983	025A	164.20	12/27/14	1539	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
984	025A	164.20	12/27/14	1540	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
985	025A	164.20	10/31/13	1710	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
986	025A	164.20	04/01/14	1735	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
987	025A	164.25	05/10/14	1755	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
988	025A	164.30	06/23/15	2325	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
989	025A	164.30	08/08/13	1453	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
990	025A	164.34	01/29/12	1440	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
991	025A	164.40	03/15/13	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
992	025A	164.40	06/21/15	1725	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
993	025A	164.43	01/27/15	2340	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
994	025A	164.49	03/08/13	2235	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
995	025A	164.50	10/17/15	2228	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
996	025A	164.50	10/29/15	1021	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
997	025A	164.50	09/16/11	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
998	025A	164.50	05/15/12	1335	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
999	025A	164.50	03/15/13	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1000	025A	164.50	11/07/13	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1001	025A	164.50	10/15/15	0435	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1002	025A	164.50	11/01/13	0645	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1003	025A	164.50	01/30/15	1750	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1004	025A	164.56	06/29/12	0310	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1005	025A	164.60	06/02/13	1310	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
1006	025A	164.60	08/01/15	1226	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1007	025A	164.60	08/21/15	0137	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1008	025A	164.60	01/30/15	1750	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1009	025A	164.60	11/08/14	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1010	025A	164.60	03/14/14	1310	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1011	025A	164.70	04/23/13	0945	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
1012	025A	164.70	09/27/14	1323	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1013	025A	164.70	11/23/15	1630	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1014	025A	164.75	06/15/13	2315	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1015	025A	164.80	12/22/12	1730	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1016	025A	164.80	07/12/15	0455	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1017	025A	164.80	07/27/12	0200	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1018	025A	164.90	04/21/12	0515	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1019	025A	164.90	10/18/11	0405	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1020	025A	164.92	10/19/14	0015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
961	025A	164.00	09/08/12	1125	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
962	025A	164.00	03/27/15	1345	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
963	025A	164.00	05/22/15	1315	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
964	025A	164.00	06/27/15	1110	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
965	025A	164.00	10/07/11	2115	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
966	025A	164.00	04/27/13	0815	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	PASSING
967	025A	164.00	10/15/13	1935	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	070	PASSING
968	025A	164.00	02/21/14	1815	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
969	025A	164.00	06/05/13	0430	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
970	025A	164.04	06/24/15	1535	N	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
971	025A	164.08	11/05/11	1515	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	083	GOING STRAIGHT
972	025A	164.10	03/16/14	1330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
973	025A	164.10	08/23/14	1615	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
974	025A	164.10	11/03/15	1910	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
975	025A	164.10	03/21/12	2245	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
976	025A	164.10	09/14/11	0540	N	WILD ANIMAL	S	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
977	025A	164.10	10/23/14	0017	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
978	025A	164.10	06/27/15	1455	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	MAKING U-TURN
979	025A	164.10	01/29/12	0321	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	079	GOING STRAIGHT
980	025A	164.10	11/07/15	1115	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
981	025A	164.20	03/22/12	0120	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	025	GOING STRAIGHT
982	025A	164.20	04/19/13	1630	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
983	025A	164.20	12/27/14	1539	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
984	025A	164.20	12/27/14	1540	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
985	025A	164.20	10/31/13	1710	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
986	025A	164.20	04/01/14	1735	N	GUARD RAIL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
987	025A	164.25	05/10/14	1755	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
988	025A	164.30	06/23/15	2325	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	AVOIDING OBJECT/VEHICLE IN ROAD
989	025A	164.30	08/08/13	1453	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	OTHER
990	025A	164.34	01/29/12	1440	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
991	025A	164.40	03/15/13	2000	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
992	025A	164.40	06/21/15	1725	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
993	025A	164.43	01/27/15	2340	N	FENCE	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	080	BACKING
994	025A	164.49	03/08/13	2235	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	080	CHANGING LANES
995	025A	164.50	10/17/15	2228	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
996	025A	164.50	10/29/15	1021	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
997	025A	164.50	09/16/11	1300	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
998	025A	164.50	05/15/12	1335	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
999	025A	164.50	03/15/13	2000	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
1000	025A	164.50	11/07/13	1320	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	ENTERING/LEAVING PARKED POSITION
1001	025A	164.50	10/15/15	0435	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1002	025A	164.50	11/01/13	0645	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	SLOWING
1003	025A	164.50	01/30/15	1750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1004	025A	164.56	06/29/12	0310	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1005	025A	164.60	06/02/13	1310	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1006	025A	164.60	08/01/15	1226	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1007	025A	164.60	08/21/15	0137	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1008	025A	164.60	01/30/15	1750	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
1009	025A	164.60	11/08/14	1600	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
1010	025A	164.60	03/14/14	1310	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1011	025A	164.70	04/23/13	0945	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
1012	025A	164.70	09/27/14	1323	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1013	025A	164.70	11/23/15	1630	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1014	025A	164.75	06/15/13	2315	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1015	025A	164.80	12/22/12	1730	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1016	025A	164.80	07/12/15	0455	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1017	025A	164.80	07/27/12	0200	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1018	025A	164.90	04/21/12	0515	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
1019	025A	164.90	10/18/11	0405	N	WILD ANIMAL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1020	025A	164.92	10/19/14	0015	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1021	025A	164.98	08/16/15	1452	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1022	025A	164.98	09/26/13	0900	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1023	025A	165.00	12/27/11	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1024	025A	165.00	07/28/12	1457	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1025	025A	165.00	12/08/12	1649	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1026	025A	165.00	12/06/14	1704	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1027	025A	165.00	07/31/15	1403	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1028	025A	165.00	08/09/15	1210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1029	025A	165.00	07/20/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1030	025A	165.00	01/19/15	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1031	025A	165.00	03/18/15	1626	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1032	025A	165.00	08/23/15	1440	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1033	025A	165.00	09/03/15	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1034	025A	165.00	11/20/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1035	025A	165.00	08/13/14	0428	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1036	025A	165.00	08/13/14	0430	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1037	025A	165.00	09/18/15	1450	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1038	025A	165.02	01/25/13	1441	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1039	025A	165.04	09/11/11	1208	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1040	025A	165.10	10/09/11	1545	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1041	025A	165.10	02/18/12	1015	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1042	025A	165.10	06/14/14	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1043	025A	165.10	05/21/11	1330	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1044	025A	165.10	11/29/15	1250	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1045	025A	165.20	11/04/15	2342	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1046	025A	165.25	11/23/13	1715	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1047	025A	165.30	06/04/13	0250	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1048	025A	165.40	06/16/12	1050	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1049	025A	165.40	11/18/11	0310	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1050	025A	165.41	07/18/15	1553	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1051	025A	165.44	05/12/13	1115	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1052	025A	165.45	10/30/15	2011	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1053	025A	165.47	06/21/14	0300	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1054	025A	165.48	01/13/13	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1055	025A	165.50	07/11/12	1800	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1056	025A	165.50	12/28/13	2010	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1057	025A	165.50	08/26/15	1225	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1058	025A	165.50	06/01/12	1912	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1059	025A	165.50	09/17/14	1335	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1060	025A	165.50	04/23/11	1535	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1061	025A	165.50	09/21/14	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1062	025A	165.50	12/12/14	1705	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1063	025A	165.50	07/19/15	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1064	025A	165.50	09/30/15	1018	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1065	025A	165.50	10/01/11	0650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1066	025A	165.50	04/26/15	1512	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1067	025A	165.50	08/07/15	0930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1068	025A	165.50	09/17/14	2355	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1069	025A	165.51	11/30/15	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1070	025A	165.63	07/01/12	2010	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1071	025A	165.70	05/04/15	2340	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1072	025A	165.70	10/25/14	1132	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1073	025A	165.70	05/16/15	1455	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1074	025A	165.70	07/06/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1075	025A	165.70	11/10/15	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1076	025A	165.70	06/30/11	1155	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1077	025A	165.70	06/30/11	1200	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1078	025A	165.75	04/03/14	0720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1079	025A	165.75	11/30/14	1215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1080	025A	165.80	09/06/15	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1021	025A	164.98	08/16/15	1452	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
1022	025A	164.98	09/26/13	0900	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1023	025A	165.00	12/27/11	1600	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	064	GOING STRAIGHT
1024	025A	165.00	07/28/12	1457	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1025	025A	165.00	12/08/12	1649	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
1026	025A	165.00	12/06/14	1704	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1027	025A	165.00	07/31/15	1403	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1028	025A	165.00	08/09/15	1210	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1029	025A	165.00	07/20/14	1515	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1030	025A	165.00	01/19/15	1445	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1031	025A	165.00	03/18/15	1626	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1032	025A	165.00	08/23/15	1440	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	085	CHANGING LANES
1033	025A	165.00	09/03/15	1900	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1034	025A	165.00	11/20/15	1330	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
1035	025A	165.00	08/13/14	0428	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1036	025A	165.00	08/13/14	0430	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	074	GOING STRAIGHT
1037	025A	165.00	09/18/15	1450	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	090	GOING STRAIGHT
1038	025A	165.02	01/25/13	1441	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
1039	025A	165.04	09/11/11	1208	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	SLOWING
1040	025A	165.10	10/09/11	1545	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
1041	025A	165.10	02/18/12	1015	N	OTHER NON-COLLISION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1042	025A	165.10	06/14/14	1415	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	073	GOING STRAIGHT
1043	025A	165.10	05/21/11	1330	N	SIGN	N	SUV	RX/MEDICATION/DR	UNKNOWN	060	WEAVING
1044	025A	165.10	11/29/15	1250	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1045	025A	165.20	11/04/15	2342	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1046	025A	165.25	11/23/13	1715	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1047	025A	165.30	06/04/13	0250	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1048	025A	165.40	06/16/12	1050	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
1049	025A	165.40	11/18/11	0310	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1050	025A	165.41	07/18/15	1553	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1051	025A	165.44	05/12/13	1115	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1052	025A	165.45	10/30/15	2011	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1053	025A	165.47	06/21/14	0300	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1054	025A	165.48	01/13/13	1730	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1055	025A	165.50	07/11/12	1800	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1056	025A	165.50	12/28/13	2010	N	OVERTURNING	S	SUV	RX/MEDICATION/DR	UNKNOWN	065	OTHER
1057	025A	165.50	08/26/15	1225	N	OTHER NON-COLLISION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1058	025A	165.50	06/01/12	1912	N	VEHICLE CARGO/DEBRIS	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1059	025A	165.50	09/17/14	1335	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1060	025A	165.50	04/23/11	1535	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1061	025A	165.50	09/21/14	1330	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1062	025A	165.50	12/12/14	1705	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1063	025A	165.50	07/19/15	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1064	025A	165.50	09/30/15	1018	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1065	025A	165.50	10/01/11	0650	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1066	025A	165.50	04/26/15	1512	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	WEAVING
1067	025A	165.50	08/07/15	0930	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
1068	025A	165.50	09/17/14	2355	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1069	025A	165.51	11/30/15	1540	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1070	025A	165.63	07/01/12	2010	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL/DRUGS	DISTRACTED BY PASSENGER	085	CHANGING LANES
1071	025A	165.70	05/04/15	2340	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
1072	025A	165.70	10/25/14	1132	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1073	025A	165.70	05/16/15	1455	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1074	025A	165.70	07/06/15	1130	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1075	025A	165.70	11/10/15	1550	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
1076	025A	165.70	06/30/11	1155	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
1077	025A	165.70	06/30/11	1200	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1078	025A	165.75	04/03/14	0720	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1079	025A	165.75	11/30/14	1215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1080	025A	165.80	09/06/15	1030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1081	025A	165.80	10/03/15	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1082	025A	165.80	04/17/13	0700	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1083	025A	165.80	08/07/11	1030	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1084	025A	165.90	04/17/13	1725	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1085	025A	165.90	01/19/13	1310	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1086	025A	165.90	05/19/13	1205	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1087	025A	165.90	01/17/14	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1088	025A	165.90	08/02/14	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1089	025A	165.90	06/01/14	1050	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1090	025A	165.90	05/19/13	1205	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1091	025A	165.90	06/21/14	1148	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1092	025A	165.94	04/15/15	1515	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
1093	025A	165.95	03/03/13	1400	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1094	025A	165.95	06/03/11	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1095	025A	165.96	08/24/13	0910	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1096	025A	166.00	08/31/12	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1097	025A	166.00	08/31/12	1208	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1098	025A	166.00	01/21/13	1830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1099	025A	166.00	09/21/13	0915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1100	025A	166.00	10/04/13	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1101	025A	166.00	05/09/15	1750	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1102	025A	166.00	07/16/15	1423	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1103	025A	166.00	01/30/13	0520	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
1104	025A	166.00	06/23/14	0428	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1105	025A	166.00	07/15/14	0305	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1106	025A	166.00	07/01/15	0645	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1107	025A	166.00	04/17/15	1610	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1108	025A	166.00	07/12/12	1523	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1109	025A	166.00	12/29/12	1215	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1110	025A	166.01	07/22/11	1615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1111	025A	166.01	11/30/14	1320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1112	025A	166.02	06/14/14	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1113	025A	166.10	08/09/15	1449	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1114	025A	166.10	01/17/12	2055	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1115	025A	166.12	05/28/15	1844	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1116	025A	166.15	08/17/13	1415	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1117	025A	166.20	05/18/12	1415	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1118	025A	166.20	12/22/14	1510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
1119	025A	166.20	06/01/15	1815	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1120	025A	166.20	12/19/12	1910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1121	025A	166.20	02/25/14	1645	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1122	025A	166.25	06/03/11	1635	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1123	025A	166.30	03/03/13	1337	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1124	025A	166.30	08/16/14	0835	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1125	025A	166.35	08/02/11	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1126	025A	166.40	10/08/11	0405	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1127	025A	166.40	05/14/11	1129	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1128	025A	166.40	11/16/13	1722	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1129	025A	166.40	09/07/14	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1130	025A	166.40	07/06/14	0615	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1131	025A	166.40	12/20/14	1844	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1132	025A	166.43	09/20/13	1644	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1133	025A	166.46	04/08/11	1930	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1134	025A	166.50	05/20/11	1040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1135	025A	166.50	06/15/14	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1136	025A	166.50	06/06/15	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1137	025A	166.50	10/08/11	0426	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1138	025A	166.50	09/18/11	1830	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1139	025A	166.50	09/05/13	1415	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1140	025A	166.50	12/18/15	1727	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1081	025A	165.80	10/03/15	1100	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
1082	025A	165.80	04/17/13	0700	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1083	025A	165.80	08/07/11	1030	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
1084	025A	165.90	04/17/13	1725	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1085	025A	165.90	01/19/13	1310	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1086	025A	165.90	05/19/13	1205	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1087	025A	165.90	01/17/14	1630	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1088	025A	165.90	08/02/14	1425	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1089	025A	165.90	06/01/14	1050	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
1090	025A	165.90	05/19/13	1205	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
1091	025A	165.90	06/21/14	1148	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1092	025A	165.94	04/15/15	1515	N	SIGN	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ILLNESS	035	GOING STRAIGHT
1093	025A	165.95	03/03/13	1400	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1094	025A	165.95	06/03/11	1520	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
1095	025A	165.96	08/24/13	0910	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1096	025A	166.00	08/31/12	1125	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1097	025A	166.00	08/31/12	1208	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1098	025A	166.00	01/21/13	1830	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
1099	025A	166.00	09/21/13	0915	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1100	025A	166.00	10/04/13	1530	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
1101	025A	166.00	05/09/15	1750	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
1102	025A	166.00	07/16/15	1423	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1103	025A	166.00	01/30/13	0520	N	PARKED MOTOR VEHICLE	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1104	025A	166.00	06/23/14	0428	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1105	025A	166.00	07/15/14	0305	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	ALCOHOL	NONE APPARENT	065	GOING STRAIGHT
1106	025A	166.00	07/01/15	0645	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1107	025A	166.00	04/17/15	1610	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
1108	025A	166.00	07/12/12	1523	N	DELINEATOR POST	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
1109	025A	166.00	12/29/12	1215	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	WEAVING
1110	025A	166.01	07/22/11	1615	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1111	025A	166.01	11/30/14	1320	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1112	025A	166.02	06/14/14	1030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1113	025A	166.10	08/09/15	1449	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
1114	025A	166.10	01/17/12	2055	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1115	025A	166.12	05/28/15	1844	N	WILD ANIMAL	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1116	025A	166.15	08/17/13	1415	N	OVERTURNING	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1117	025A	166.20	05/18/12	1415	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1118	025A	166.20	12/22/14	1510	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1119	025A	166.20	06/01/15	1815	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	083	GOING STRAIGHT
1120	025A	166.20	12/19/12	1910	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1121	025A	166.20	02/25/14	1645	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1122	025A	166.25	06/03/11	1635	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	048	CHANGING LANES
1123	025A	166.30	03/03/13	1337	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	080	GOING STRAIGHT
1124	025A	166.30	08/16/14	0835	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT
1125	025A	166.35	08/02/11	1130	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
1126	025A	166.40	10/08/11	0405	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1127	025A	166.40	05/14/11	1129	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
1128	025A	166.40	11/16/13	1722	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1129	025A	166.40	09/07/14	1545	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1130	025A	166.40	07/06/14	0615	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1131	025A	166.40	12/20/14	1844	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
1132	025A	166.43	09/20/13	1644	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1133	025A	166.46	04/08/11	1930	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1134	025A	166.50	05/20/11	1040	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1135	025A	166.50	06/15/14	1715	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1136	025A	166.50	06/06/15	1400	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
1137	025A	166.50	10/08/11	0426	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
1138	025A	166.50	09/18/11	1830	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1139	025A	166.50	09/05/13	1415	N	INVOLVING OTHER OBJECT	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1140	025A	166.50	12/18/15	1727	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1141	025A	166.50	12/18/15	1727	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1142	025A	166.60	11/12/11	1449	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
1143	025A	166.60	01/30/15	2215	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1144	025A	166.60	02/07/11	0930	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1145	025A	166.60	09/13/13	0400	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
1146	025A	166.60	09/26/15	1200	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1147	025A	166.62	02/07/11	0930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
1148	025A	166.63	08/09/13	1153	FAT	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1149	025A	166.65	06/02/15	1455	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1150	025A	166.70	09/20/13	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1151	025A	166.70	08/02/15	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1152	025A	166.70	01/20/11	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1153	025A	166.70	07/12/11	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1154	025A	166.70	01/31/14	1530	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1155	025A	166.75	04/07/12	2145	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1156	025A	166.80	08/05/12	2055	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1157	025A	166.80	07/24/13	1515	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1158	025A	166.80	10/22/14	1845	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1159	025A	166.80	10/11/15	1300	INJ	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1160	025A	166.80	07/17/11	1315	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1161	025A	166.80	06/17/12	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1162	025A	166.80	02/06/14	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1163	025A	166.80	08/07/15	1607	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1164	025A	166.80	05/13/11	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1165	025A	166.80	01/16/15	1530	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1166	025A	166.80	03/02/15	0720	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	WIND
1167	025A	166.80	09/13/13	0225	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1168	025A	166.80	07/15/15	1943	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1169	025A	166.80	12/11/12	1745	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1170	025A	166.80	03/02/15	0715	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	WIND
1171	025A	166.87	12/20/12	0003	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	WIND
1172	025A	166.88	05/14/15	1715	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1173	025A	166.90	06/19/11	1348	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1174	025A	166.90	03/19/11	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1175	025A	166.90	11/18/11	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1176	025A	166.90	05/31/14	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1177	025A	166.90	05/14/15	1725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1178	025A	166.90	07/11/15	1702	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1179	025A	166.90	08/02/15	1439	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1180	025A	166.90	08/15/15	1000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1181	025A	166.90	08/23/15	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1182	025A	166.90	08/07/11	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1183	025A	166.90	11/29/14	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1184	025A	166.90	06/16/14	0130	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1185	025A	166.90	05/31/12	1650	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1186	025A	166.90	08/02/15	1715	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1187	025A	166.91	03/31/15	1900	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1188	025A	166.93	08/02/14	2309	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1189	025A	166.94	08/16/15	1510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1190	025A	166.94	09/05/14	0105	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1191	025A	166.94	08/08/15	1500	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1192	025A	166.95	07/31/13	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1193	025A	166.95	09/15/12	0925	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1194	025A	166.95	07/23/14	1520	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1195	025A	166.95	01/19/15	1635	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1196	025A	166.96	03/07/15	1518	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1197	025A	166.96	12/25/12	2035	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1198	025A	166.97	10/23/11	1125	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1199	025A	166.98	10/11/15	1428	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1200	025A	166.98	12/28/12	1648	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1141	025A	166.50	12/18/15	1727	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1142	025A	166.60	11/12/11	1449	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
1143	025A	166.60	01/30/15	2215	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1144	025A	166.60	02/07/11	0930	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
1145	025A	166.60	09/13/13	0400	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	OTHER
1146	025A	166.60	09/26/15	1200	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1147	025A	166.62	02/07/11	0930	N	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1148	025A	166.63	08/09/13	1153	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
1149	025A	166.65	06/02/15	1455	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
1150	025A	166.70	09/20/13	1245	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1151	025A	166.70	08/02/15	1600	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	050	GOING STRAIGHT
1152	025A	166.70	01/20/11	0910	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1153	025A	166.70	07/12/11	1430	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	PASSING
1154	025A	166.70	01/31/14	1530	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1155	025A	166.75	04/07/12	2145	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1156	025A	166.80	08/05/12	2055	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1157	025A	166.80	07/24/13	1515	N	OVERTURNING	N	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
1158	025A	166.80	10/22/14	1845	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	080	OTHER
1159	025A	166.80	10/11/15	1300	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
1160	025A	166.80	07/17/11	1315	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1161	025A	166.80	06/17/12	1315	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1162	025A	166.80	02/06/14	1645	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1163	025A	166.80	08/07/15	1607	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1164	025A	166.80	05/13/11	1115	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1165	025A	166.80	01/16/15	1530	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1166	025A	166.80	03/02/15	0720	N	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1167	025A	166.80	09/13/13	0225	N	OVERTURNING	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	OTHER
1168	025A	166.80	07/15/15	1943	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1169	025A	166.80	12/11/12	1745	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1170	025A	166.80	03/02/15	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	AVOIDING OBJECT/VEHICLE IN ROAD
1171	025A	166.87	12/20/12	0003	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	CHANGING LANES
1172	025A	166.88	05/14/15	1715	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1173	025A	166.90	06/19/11	1348	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1174	025A	166.90	03/19/11	1750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1175	025A	166.90	11/18/11	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1176	025A	166.90	05/31/14	1430	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1177	025A	166.90	05/14/15	1725	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1178	025A	166.90	07/11/15	1702	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1179	025A	166.90	08/02/15	1439	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
1180	025A	166.90	08/15/15	1000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1181	025A	166.90	08/23/15	1250	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1182	025A	166.90	08/07/11	1250	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
1183	025A	166.90	11/29/14	1550	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1184	025A	166.90	06/16/14	0130	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
1185	025A	166.90	05/31/12	1650	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
1186	025A	166.90	08/02/15	1715	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	GOING STRAIGHT
1187	025A	166.91	03/31/15	1900	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1188	025A	166.93	08/02/14	2309	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	065	OTHER
1189	025A	166.94	08/16/15	1510	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1190	025A	166.94	09/05/14	0105	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1191	025A	166.94	08/08/15	1500	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1192	025A	166.95	07/31/13	1600	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1193	025A	166.95	09/15/12	0925	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1194	025A	166.95	07/23/14	1520	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
1195	025A	166.95	01/19/15	1635	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
1196	025A	166.96	03/07/15	1518	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1197	025A	166.96	12/25/12	2035	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
1198	025A	166.97	10/23/11	1125	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
1199	025A	166.98	10/11/15	1428	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
1200	025A	166.98	12/28/12	1648	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1201	025A	166.99	10/10/14	1115	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1202	025A	167.00	11/04/13	0448	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1203	025A	167.00	03/30/15	1055	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1204	025A	167.00	05/20/11	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1205	025A	167.00	03/15/12	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1206	025A	167.00	09/07/12	0000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1207	025A	167.00	10/19/13	1820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1208	025A	167.00	11/16/13	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1209	025A	167.00	01/12/14	2030	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
1210	025A	167.00	02/01/14	1619	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1211	025A	167.00	02/07/14	1902	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1212	025A	167.00	12/20/14	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1213	025A	167.00	04/18/15	1548	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
1214	025A	167.00	05/02/15	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1215	025A	167.00	09/24/15	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1216	025A	167.00	10/17/15	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1217	025A	167.00	10/24/15	1849	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1218	025A	167.00	11/06/15	1804	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1219	025A	167.00	04/05/13	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1220	025A	167.00	09/03/14	0856	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1221	025A	167.00	09/25/14	2045	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1222	025A	167.00	09/09/15	0620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1223	025A	167.00	12/31/15	1040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1224	025A	167.00	10/01/12	0435	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1225	025A	167.00	06/17/15	0220	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1226	025A	167.00	07/21/11	1440	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1227	025A	167.00	05/23/13	0116	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1228	025A	167.00	06/09/13	1215	PDO	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1229	025A	167.00	03/02/15	0720	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	WIND
1230	025A	167.00	06/17/12	1319	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1231	025A	167.00	11/22/12	1055	INJ	ON	NON-INTERSECTION	6	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1232	025A	167.00	06/02/13	1150	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1233	025A	167.00	06/15/14	1855	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1234	025A	167.00	08/05/15	1145	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1235	025A	167.00	08/05/15	1145	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1236	025A	167.00	09/24/15	1245	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1237	025A	167.00	12/18/15	1656	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1238	025A	167.02	07/18/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1239	025A	167.03	06/05/14	2215	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1240	025A	167.03	07/23/15	1200	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1241	025A	167.04	09/06/13	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1242	025A	167.04	10/07/14	1550	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1243	025A	167.05	04/07/11	2245	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1244	025A	167.06	09/01/15	1105	INJ	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1245	025A	167.06	10/17/14	1325	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1246	025A	167.08	11/21/12	1515	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1247	025A	167.10	01/15/12	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1248	025A	167.10	11/19/11	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1249	025A	167.10	06/13/12	1510	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1250	025A	167.10	08/26/13	0535	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1251	025A	167.10	09/01/14	1405	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1252	025A	167.10	09/26/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1253	025A	167.10	10/19/15	1635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1254	025A	167.10	05/27/11	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1255	025A	167.10	10/12/15	1852	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1256	025A	167.10	06/30/11	2015	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	RAIN
1257	025A	167.10	01/06/13	1735	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1258	025A	167.10	11/21/14	1415	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1259	025A	167.10	09/06/15	1055	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1260	025A	167.13	07/06/12	1530	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1201	025A	166.99	10/10/14	1115	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1202	025A	167.00	11/04/13	0448	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1203	025A	167.00	03/30/15	1055	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1204	025A	167.00	05/20/11	0945	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1205	025A	167.00	03/15/12	0920	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1206	025A	167.00	09/07/12	0000	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1207	025A	167.00	10/19/13	1820	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1208	025A	167.00	11/16/13	1130	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1209	025A	167.00	01/12/14	2030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	030	GOING STRAIGHT
1210	025A	167.00	02/01/14	1619	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1211	025A	167.00	02/07/14	1902	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1212	025A	167.00	12/20/14	1650	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1213	025A	167.00	04/18/15	1548	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
1214	025A	167.00	05/02/15	1650	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1215	025A	167.00	09/24/15	1245	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	068	GOING STRAIGHT
1216	025A	167.00	10/17/15	1320	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
1217	025A	167.00	10/24/15	1849	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
1218	025A	167.00	11/06/15	1804	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
1219	025A	167.00	04/05/13	1150	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1220	025A	167.00	09/03/14	0856	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1221	025A	167.00	09/25/14	2045	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1222	025A	167.00	09/09/15	0620	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1223	025A	167.00	12/31/15	1040	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1224	025A	167.00	10/01/12	0435	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1225	025A	167.00	06/17/15	0220	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1226	025A	167.00	07/21/11	1440	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
1227	025A	167.00	05/23/13	0116	N	GUARD RAIL	S	SUV	ALCOHOL	ASLEEP AT WHEEL	065	WEAVING
1228	025A	167.00	06/09/13	1215	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	030	OTHER
1229	025A	167.00	03/02/15	0720	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	OTHER
1230	025A	167.00	06/17/12	1319	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1231	025A	167.00	11/22/12	1055	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1232	025A	167.00	06/02/13	1150	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1233	025A	167.00	06/15/14	1855	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
1234	025A	167.00	08/05/15	1145	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1235	025A	167.00	08/05/15	1145	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1236	025A	167.00	09/24/15	1245	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1237	025A	167.00	12/18/15	1656	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1238	025A	167.02	07/18/14	1515	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1239	025A	167.03	06/05/14	2215	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1240	025A	167.03	07/23/15	1200	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
1241	025A	167.04	09/06/13	1400	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1242	025A	167.04	10/07/14	1550	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1243	025A	167.05	04/07/11	2245	N	OVERTURNING	N	SUV W/TRAILER	ALCOHOL	UNKNOWN	060	WEAVING
1244	025A	167.06	09/01/15	1105	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1245	025A	167.06	10/17/14	1325	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1246	025A	167.08	11/21/12	1515	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1247	025A	167.10	01/15/12	0945	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1248	025A	167.10	11/19/11	1500	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1249	025A	167.10	06/13/12	1510	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1250	025A	167.10	08/26/13	0535	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1251	025A	167.10	09/01/14	1405	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1252	025A	167.10	09/26/15	1330	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1253	025A	167.10	10/19/15	1635	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1254	025A	167.10	05/27/11	1610	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1255	025A	167.10	10/12/15	1852	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
1256	025A	167.10	06/30/11	2015	N	OVERTURNING	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1257	025A	167.10	01/06/13	1735	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1258	025A	167.10	11/21/14	1415	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1259	025A	167.10	09/06/15	1055	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1260	025A	167.13	07/06/12	1530	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1261	025A	167.20	06/30/12	2250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1262	025A	167.20	10/06/15	1338	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
1263	025A	167.20	09/27/11	2020	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1264	025A	167.21	06/20/12	0514	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1265	025A	167.25	08/09/12	1430	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1266	025A	167.29	03/05/14	1233	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1267	025A	167.30	03/17/12	0908	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1268	025A	167.30	07/20/14	1619	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1269	025A	167.30	05/09/15	1225	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1270	025A	167.30	12/21/15	1425	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1271	025A	167.30	07/17/15	0100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1272	025A	167.30	08/11/13	0425	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1273	025A	167.33	12/31/14	1455	INJ	ON	NON-INTERSECTION	9	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1274	025A	167.34	02/16/14	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1275	025A	167.39	05/11/13	1409	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1276	025A	167.40	02/15/15	1140	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1277	025A	167.40	07/26/15	1655	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1278	025A	167.40	08/10/11	2140	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1279	025A	167.43	09/11/15	0156	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1280	025A	167.43	04/01/15	0650	INJ	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1281	025A	167.46	08/13/15	1528	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1282	025A	167.46	05/11/11	1317	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
1283	025A	167.47	08/21/11	1806	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1284	025A	167.48	09/26/15	1210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1285	025A	167.48	05/23/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1286	025A	167.50	11/02/13	1215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1287	025A	167.50	06/20/14	1612	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1288	025A	167.50	07/11/14	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1289	025A	167.50	07/07/12	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1290	025A	167.50	11/30/14	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1291	025A	167.50	05/19/12	0330	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1292	025A	167.50	05/19/12	0330	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1293	025A	167.50	03/17/12	0820	PDO	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1294	025A	167.50	12/24/12	1830	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1295	025A	167.50	01/29/14	0118	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1296	025A	167.50	05/07/14	2220	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1297	025A	167.50	02/21/15	1601	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1298	025A	167.50	11/29/15	0315	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1299	025A	167.50	06/20/11	0545	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	WET	DAWN OR DUSK	RAIN
1300	025A	167.50	07/03/15	1020	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1301	025A	167.51	07/30/12	0420	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1302	025A	167.51	07/30/11	1725	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1303	025A	167.53	06/06/15	1143	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1304	025A	167.54	02/20/14	0650	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	NONE
1305	025A	167.57	09/02/11	2230	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1306	025A	167.59	11/10/15	0612	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1307	025A	167.60	06/20/15	1445	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1308	025A	167.60	08/03/12	1824	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1309	025A	167.60	02/20/14	0630	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAWN OR DUSK	NONE
1310	025A	167.60	05/29/15	2241	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
1311	025A	167.65	12/02/11	1310	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1312	025A	167.68	12/26/14	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
1313	025A	167.70	03/28/11	1350	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1314	025A	167.70	10/10/15	1558	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1315	025A	167.70	07/07/13	0935	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1316	025A	167.70	06/15/13	0840	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1317	025A	167.70	10/09/14	0202	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1318	025A	167.70	07/07/11	0640	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1319	025A	167.70	12/03/11	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1320	025A	167.71	06/19/11	1510	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1261	025A	167.20	06/30/12	2250	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
1262	025A	167.20	10/06/15	1338	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1263	025A	167.20	09/27/11	2020	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1264	025A	167.21	06/20/12	0514	Y (R)	FENCE	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
1265	025A	167.25	08/09/12	1430	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1266	025A	167.29	03/05/14	1233	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1267	025A	167.30	03/17/12	0908	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
1268	025A	167.30	07/20/14	1619	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1269	025A	167.30	05/09/15	1225	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1270	025A	167.30	12/21/15	1425	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	SLOWING
1271	025A	167.30	07/17/15	0100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1272	025A	167.30	08/11/13	0425	Y (R)	BRIDGE RAIL	N	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	050	GOING STRAIGHT
1273	025A	167.33	12/31/14	1455	N	APPROACH TURN	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	MAKING U-TURN
1274	025A	167.34	02/16/14	0850	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	SLOWING
1275	025A	167.39	05/11/13	1409	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1276	025A	167.40	02/15/15	1140	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1277	025A	167.40	07/26/15	1655	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1278	025A	167.40	08/10/11	2140	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1279	025A	167.43	09/11/15	0156	Y (B)	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	GOING STRAIGHT
1280	025A	167.43	04/01/15	0650	Y (B)	SIGN	N	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
1281	025A	167.46	08/13/15	1528	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	MAKING LEFT TURN
1282	025A	167.46	05/11/11	1317	Y (P)	REAR-END	W	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	BACKING
1283	025A	167.47	08/21/11	1806	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1284	025A	167.48	09/26/15	1210	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1285	025A	167.48	05/23/15	1330	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1286	025A	167.50	11/02/13	1215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1287	025A	167.50	06/20/14	1612	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1288	025A	167.50	07/11/14	1530	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1289	025A	167.50	07/07/12	1445	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1290	025A	167.50	11/30/14	1430	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
1291	025A	167.50	05/19/12	0330	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1292	025A	167.50	05/19/12	0330	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1293	025A	167.50	03/17/12	0820	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	WEAVING
1294	025A	167.50	12/24/12	1830	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1295	025A	167.50	01/29/14	0118	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1296	025A	167.50	05/07/14	2220	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1297	025A	167.50	02/21/15	1601	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
1298	025A	167.50	11/29/15	0315	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1299	025A	167.50	06/20/11	0545	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1300	025A	167.50	07/03/15	1020	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1301	025A	167.51	07/30/12	0420	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
1302	025A	167.51	07/30/11	1725	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1303	025A	167.53	06/06/15	1143	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	AVOIDING OBJECT/VEHICLE IN ROAD
1304	025A	167.54	02/20/14	0650	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1305	025A	167.57	09/02/11	2230	Y (D)	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1306	025A	167.59	11/10/15	0612	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1307	025A	167.60	06/20/15	1445	N	APPROACH TURN	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	MAKING U-TURN
1308	025A	167.60	08/03/12	1824	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
1309	025A	167.60	02/20/14	0630	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	WEAVING
1310	025A	167.60	05/29/15	2241	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT
1311	025A	167.65	12/02/11	1310	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1312	025A	167.68	12/26/14	1330	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1313	025A	167.70	03/28/11	1350	N	OVERTURNING	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1314	025A	167.70	10/10/15	1558	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1315	025A	167.70	07/07/13	0935	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1316	025A	167.70	06/15/13	0840	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1317	025A	167.70	10/09/14	0202	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1318	025A	167.70	07/07/11	0640	N	INVOLVING OTHER OBJECT	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1319	025A	167.70	12/03/11	1200	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1320	025A	167.71	06/19/11	1510	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1321	025A	167.75	12/29/14	0922	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1322	025A	167.80	09/23/11	1830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1323	025A	167.80	10/18/14	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1324	025A	167.80	06/16/11	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1325	025A	167.80	01/02/11	1735	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1326	025A	167.80	09/17/14	0030	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1327	025A	167.90	09/02/13	0935	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1328	025A	167.90	06/07/14	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1329	025A	167.90	11/15/14	2020	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1330	025A	167.90	11/24/15	0841	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1331	025A	167.91	07/13/12	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1332	025A	167.95	04/28/13	1615	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1333	025A	167.96	06/05/15	1512	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1334	025A	167.97	08/26/13	0545	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1335	025A	168.00	02/11/12	1640	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1336	025A	168.00	01/21/11	1838	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1337	025A	168.00	12/16/15	1753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1338	025A	168.00	10/08/12	0955	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1339	025A	168.00	08/03/13	1830	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1340	025A	168.00	09/22/15	1605	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1341	025A	168.00	03/07/15	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1342	025A	168.00	05/29/11	2000	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1343	025A	168.00	06/17/14	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	NONE
1344	025A	168.00	10/04/14	1045	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1345	025A	168.00	02/21/15	1421	PDO	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1346	025A	168.00	03/11/14	1451	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
1347	025A	168.00	05/26/14	1329	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1348	025A	168.00	11/24/15	1939	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1349	025A	168.10	11/24/11	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1350	025A	168.10	06/12/14	1105	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
1351	025A	168.10	08/29/14	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1352	025A	168.10	10/04/14	1020	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1353	025A	168.10	05/03/15	1605	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1354	025A	168.10	05/22/15	1101	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1355	025A	168.10	09/23/12	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1356	025A	168.10	05/22/15	1100	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1357	025A	168.10	11/23/13	1030	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1358	025A	168.10	11/06/11	1810	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1359	025A	168.10	05/09/13	1410	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1360	025A	168.10	12/23/12	1913	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1361	025A	168.10	09/28/14	1655	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1362	025A	168.10	01/02/15	0905	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1363	025A	168.12	04/29/11	0900	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1364	025A	168.20	04/04/11	2350	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1365	025A	168.20	09/07/15	1800	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1366	025A	168.20	07/22/13	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1367	025A	168.20	12/21/11	1115	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1368	025A	168.20	11/12/15	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1369	025A	168.20	06/22/11	2020	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1370	025A	168.20	01/13/13	1838	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1371	025A	168.20	01/02/11	1146	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1372	025A	168.20	04/27/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1373	025A	168.25	10/29/11	1840	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1374	025A	168.25	10/29/11	1856	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1375	025A	168.27	06/25/13	1530	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1376	025A	168.30	08/22/13	1021	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1377	025A	168.30	09/27/14	1220	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1378	025A	168.30	07/05/15	1453	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1379	025A	168.40	11/02/11	1349	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1380	025A	168.40	01/02/15	0900	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1321	025A	167.75	12/29/14	0922	N	SIDESWIPE OPPOSITE DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	060	OTHER
1322	025A	167.80	09/23/11	1830	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
1323	025A	167.80	10/18/14	1500	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1324	025A	167.80	06/16/11	1400	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1325	025A	167.80	01/02/11	1735	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1326	025A	167.80	09/17/14	0030	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
1327	025A	167.90	09/02/13	0935	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
1328	025A	167.90	06/07/14	1700	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1329	025A	167.90	11/15/14	2020	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
1330	025A	167.90	11/24/15	0841	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1331	025A	167.91	07/13/12	0735	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1332	025A	167.95	04/28/13	1615	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1333	025A	167.96	06/05/15	1512	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
1334	025A	167.97	08/26/13	0545	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1335	025A	168.00	02/11/12	1640	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1336	025A	168.00	01/21/11	1838	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1337	025A	168.00	12/16/15	1753	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1338	025A	168.00	10/08/12	0955	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
1339	025A	168.00	08/03/13	1830	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1340	025A	168.00	09/22/15	1605	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1341	025A	168.00	03/07/15	1800	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
1342	025A	168.00	05/29/11	2000	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1343	025A	168.00	06/17/14	2100	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1344	025A	168.00	10/04/14	1045	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1345	025A	168.00	02/21/15	1421	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1346	025A	168.00	03/11/14	1451	N	ROAD MAINTENANCE EQUIPMENT	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
1347	025A	168.00	05/26/14	1329	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1348	025A	168.00	11/24/15	1939	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
1349	025A	168.10	11/24/11	2000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1350	025A	168.10	06/12/14	1105	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
1351	025A	168.10	08/29/14	1540	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	GOING STRAIGHT
1352	025A	168.10	10/04/14	1020	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1353	025A	168.10	05/03/15	1605	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1354	025A	168.10	05/22/15	1101	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1355	025A	168.10	09/23/12	1125	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1356	025A	168.10	05/22/15	1100	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	PASSING
1357	025A	168.10	11/23/13	1030	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1358	025A	168.10	11/06/11	1810	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1359	025A	168.10	05/09/13	1410	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
1360	025A	168.10	12/23/12	1913	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1361	025A	168.10	09/28/14	1655	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1362	025A	168.10	01/02/15	0905	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1363	025A	168.12	04/29/11	0900	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1364	025A	168.20	04/04/11	2350	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
1365	025A	168.20	09/07/15	1800	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1366	025A	168.20	07/22/13	0945	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
1367	025A	168.20	12/21/11	1115	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1368	025A	168.20	11/12/15	1730	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1369	025A	168.20	06/22/11	2020	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1370	025A	168.20	01/13/13	1838	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
1371	025A	168.20	01/02/11	1146	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1372	025A	168.20	04/27/14	1515	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1373	025A	168.25	10/29/11	1840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1374	025A	168.25	10/29/11	1856	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
1375	025A	168.27	06/25/13	1530	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	OTHER
1376	025A	168.30	08/22/13	1021	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1377	025A	168.30	09/27/14	1220	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1378	025A	168.30	07/05/15	1453	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1379	025A	168.40	11/02/11	1349	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
1380	025A	168.40	01/02/15	0900	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1381	025A	168.40	12/23/15	2045	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1382	025A	168.40	01/02/15	0900	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
1383	025A	168.43	08/16/15	1800	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1384	025A	168.50	10/03/15	1150	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1385	025A	168.50	10/05/14	1230	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1386	025A	168.50	11/27/14	1215	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1387	025A	168.50	09/15/11	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1388	025A	168.50	04/02/14	1330	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1389	025A	168.50	02/13/15	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1390	025A	168.50	08/01/15	1437	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	UNKNOWN
1391	025A	168.50	05/09/15	0705	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
1392	025A	168.50	02/21/15	1411	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1393	025A	168.50	06/20/15	1615	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1394	025A	168.51	08/22/13	0842	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1395	025A	168.51	10/28/15	0725	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1396	025A	168.52	10/28/15	0800	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1397	025A	168.56	04/03/11	2245	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1398	025A	168.56	08/16/15	1145	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1399	025A	168.60	11/01/13	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1400	025A	168.60	11/08/14	1550	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1401	025A	168.60	01/01/13	1340	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1402	025A	168.60	05/09/13	1300	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
1403	025A	168.70	12/03/11	1200	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1404	025A	168.70	03/16/13	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1405	025A	168.70	02/17/13	0430	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1406	025A	168.70	10/22/15	1040	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
1407	025A	168.70	09/05/12	1340	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1408	025A	168.70	07/12/15	0755	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1409	025A	168.70	02/03/14	1445	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1410	025A	168.75	03/16/13	0400	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1411	025A	168.76	08/26/13	2245	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1412	025A	168.78	04/14/14	2105	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1413	025A	168.80	12/23/12	1210	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1414	025A	168.80	08/03/13	2230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1415	025A	168.80	11/30/14	1600	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1416	025A	168.80	11/30/14	1600	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1417	025A	168.80	08/18/14	0255	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1418	025A	168.80	09/19/11	0315	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1419	025A	168.80	01/28/13	1015	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1420	025A	168.80	10/04/14	1048	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1421	025A	168.80	10/01/15	1345	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1422	025A	168.85	05/24/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1423	025A	168.90	03/22/15	1435	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1424	025A	168.90	03/26/15	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1425	025A	168.90	08/31/13	0935	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1426	025A	168.90	04/04/15	1643	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1427	025A	168.90	04/18/12	1750	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1428	025A	168.90	12/14/12	0114	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1429	025A	168.90	01/20/14	1026	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1430	025A	168.90	05/09/13	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1431	025A	168.91	11/13/11	2355	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1432	025A	168.91	10/25/14	0018	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1433	025A	168.95	12/28/12	1920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1434	025A	168.96	08/22/13	0200	FAT	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1435	025A	168.98	11/10/11	0610	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1436	025A	168.99	12/24/13	0037	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1437	025A	169.00	07/25/11	1430	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1438	025A	169.00	04/18/15	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1439	025A	169.00	07/09/15	1830	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1440	025A	169.00	10/18/12	1155	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1381	025A	168.40	12/23/15	2045	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1382	025A	168.40	01/02/15	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1383	025A	168.43	08/16/15	1800	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
1384	025A	168.50	10/03/15	1150	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	SLOWING
1385	025A	168.50	10/05/14	1230	N	REAR-END	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	015	SLOWING
1386	025A	168.50	11/27/14	1215	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1387	025A	168.50	09/15/11	1500	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1388	025A	168.50	04/02/14	1330	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1389	025A	168.50	02/13/15	1350	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1390	025A	168.50	08/01/15	1437	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
1391	025A	168.50	05/09/15	0705	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1392	025A	168.50	02/21/15	1411	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1393	025A	168.50	06/20/15	1615	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1394	025A	168.51	08/22/13	0842	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
1395	025A	168.51	10/28/15	0725	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
1396	025A	168.52	10/28/15	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
1397	025A	168.56	04/03/11	2245	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1398	025A	168.56	08/16/15	1145	N	REAR-END	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1399	025A	168.60	11/01/13	1300	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	085	GOING STRAIGHT
1400	025A	168.60	11/08/14	1550	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1401	025A	168.60	01/01/13	1340	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	080	WEAVING
1402	025A	168.60	05/09/13	1300	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1403	025A	168.70	12/03/11	1200	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	AVOIDING OBJECT/VEHICLE IN ROAD
1404	025A	168.70	03/16/13	1245	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1405	025A	168.70	02/17/13	0430	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1406	025A	168.70	10/22/15	1040	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1407	025A	168.70	09/05/12	1340	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1408	025A	168.70	07/12/15	0755	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1409	025A	168.70	02/03/14	1445	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	077	GOING STRAIGHT
1410	025A	168.75	03/16/13	0400	N	TREE/SHRUBBERY	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	OTHER
1411	025A	168.76	08/26/13	2245	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	078	GOING STRAIGHT
1412	025A	168.78	04/14/14	2105	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
1413	025A	168.80	12/23/12	1210	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1414	025A	168.80	08/03/13	2230	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
1415	025A	168.80	11/30/14	1600	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
1416	025A	168.80	11/30/14	1600	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
1417	025A	168.80	08/18/14	0255	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	072	GOING STRAIGHT
1418	025A	168.80	09/19/11	0315	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
1419	025A	168.80	01/28/13	1015	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	075	WEAVING
1420	025A	168.80	10/04/14	1048	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	078	OTHER
1421	025A	168.80	10/01/15	1345	N	SIGN	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	GOING STRAIGHT
1422	025A	168.85	05/24/15	1315	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1423	025A	168.90	03/22/15	1435	N	OTHER NON-COLLISION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1424	025A	168.90	03/26/15	1500	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
1425	025A	168.90	08/31/13	0935	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1426	025A	168.90	04/04/15	1643	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1427	025A	168.90	04/18/12	1750	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1428	025A	168.90	12/14/12	0114	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	CHANGING LANES
1429	025A	168.90	01/20/14	1026	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	BACKING
1430	025A	168.90	05/09/13	1240	N	OVERTURNING	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	AVOIDING OBJECT/VEHICLE IN ROAD
1431	025A	168.91	11/13/11	2355	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1432	025A	168.91	10/25/14	0018	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1433	025A	168.95	12/28/12	1920	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1434	025A	168.96	08/22/13	0200	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
1435	025A	168.98	11/10/11	0610	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1436	025A	168.99	12/24/13	0037	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	OTHER
1437	025A	169.00	07/25/11	1430	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1438	025A	169.00	04/18/15	1450	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	072	GOING STRAIGHT
1439	025A	169.00	07/09/15	1830	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
1440	025A	169.00	10/18/12	1155	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1441	025A	169.00	11/21/14	0100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1442	025A	169.00	12/11/14	0620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1443	025A	169.00	03/09/15	2029	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1444	025A	169.00	10/07/13	0655	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1445	025A	169.00	07/25/11	1430	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1446	025A	169.00	11/16/12	1845	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1447	025A	169.03	11/07/14	1354	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1448	025A	169.10	02/11/12	2200	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1449	025A	169.10	12/21/15	1454	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1450	025A	169.10	06/02/13	1620	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1451	025A	169.10	07/03/15	1140	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1452	025A	169.10	12/21/15	1454	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1453	025A	169.12	11/13/12	0525	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1454	025A	169.15	05/21/11	1348	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1455	025A	169.20	07/02/12	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1456	025A	169.20	11/06/12	1830	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1457	025A	169.20	03/22/14	1545	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1458	025A	169.20	11/28/14	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1459	025A	169.20	05/03/15	1439	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1460	025A	169.25	07/19/12	0950	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1461	025A	169.25	06/06/15	1131	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1462	025A	169.30	06/20/12	0325	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1463	025A	169.30	10/24/15	1635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1464	025A	169.30	11/14/15	1505	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1465	025A	169.30	10/18/13	0440	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1466	025A	169.30	12/11/15	1330	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1467	025A	169.40	04/12/14	0230	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1468	025A	169.40	02/21/15	1405	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1469	025A	169.40	09/04/15	0740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1470	025A	169.40	10/04/14	0348	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1471	025A	169.40	10/26/11	0535	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1472	025A	169.40	07/19/15	1748	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1473	025A	169.50	06/10/14	1601	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1474	025A	169.50	05/25/15	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1475	025A	169.50	11/07/15	1710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1476	025A	169.50	04/03/14	0020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1477	025A	169.50	11/23/14	1100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1478	025A	169.50	11/09/15	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1479	025A	169.55	06/06/15	1740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1480	025A	169.60	11/25/15	1030	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1481	025A	169.60	01/04/13	0930	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1482	025A	169.60	06/28/11	0810	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1483	025A	169.68	04/18/11	0655	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1484	025A	169.70	11/24/12	0445	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1485	025A	169.70	10/20/13	1645	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1486	025A	169.70	04/17/13	1705	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1487	025A	169.70	06/20/15	0605	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1488	025A	169.75	11/13/13	0718	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1489	025A	169.75	12/27/14	2007	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1490	025A	169.78	06/02/13	2020	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1491	025A	169.80	06/07/13	1835	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1492	025A	169.80	07/23/11	0145	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1493	025A	169.80	11/10/14	1545	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	NONE
1494	025A	169.85	06/16/13	1140	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1495	025A	169.90	02/02/11	1755	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1496	025A	169.90	11/16/13	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1497	025A	169.90	04/20/14	1955	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
1498	025A	169.90	07/18/15	1422	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1499	025A	169.90	06/24/12	0915	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1500	025A	169.95	11/09/15	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1441	025A	169.00	11/21/14	0100	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	085	GOING STRAIGHT
1442	025A	169.00	12/11/14	0620	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	079	CHANGING LANES
1443	025A	169.00	03/09/15	2029	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
1444	025A	169.00	10/07/13	0655	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1445	025A	169.00	07/25/11	1430	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	040	GOING STRAIGHT
1446	025A	169.00	11/16/12	1845	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1447	025A	169.03	11/07/14	1354	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1448	025A	169.10	02/11/12	2200	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
1449	025A	169.10	12/21/15	1454	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1450	025A	169.10	06/02/13	1620	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1451	025A	169.10	07/03/15	1140	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1452	025A	169.10	12/21/15	1454	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
1453	025A	169.12	11/13/12	0525	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1454	025A	169.15	05/21/11	1348	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	060	GOING STRAIGHT
1455	025A	169.20	07/02/12	1600	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
1456	025A	169.20	11/06/12	1830	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
1457	025A	169.20	03/22/14	1545	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1458	025A	169.20	11/28/14	1700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1459	025A	169.20	05/03/15	1439	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
1460	025A	169.25	07/19/12	0950	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1461	025A	169.25	06/06/15	1131	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	025	AVOIDING OBJECT/VEHICLE IN ROAD
1462	025A	169.30	06/20/12	0325	N	OTHER NON-COLLISION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1463	025A	169.30	10/24/15	1635	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1464	025A	169.30	11/14/15	1505	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1465	025A	169.30	10/18/13	0440	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	045	OTHER
1466	025A	169.30	12/11/15	1330	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1467	025A	169.40	04/12/14	0230	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1468	025A	169.40	02/21/15	1405	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1469	025A	169.40	09/04/15	0740	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1470	025A	169.40	10/04/14	0348	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1471	025A	169.40	10/26/11	0535	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
1472	025A	169.40	07/19/15	1748	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
1473	025A	169.50	06/10/14	1601	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1474	025A	169.50	05/25/15	1150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1475	025A	169.50	11/07/15	1710	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1476	025A	169.50	04/03/14	0020	N	SIDESWIPE SAME DIRECTION	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	UK	PASSING
1477	025A	169.50	11/23/14	1100	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1478	025A	169.50	11/09/15	0700	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1479	025A	169.55	06/06/15	1740	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
1480	025A	169.60	11/25/15	1030	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1481	025A	169.60	01/04/13	0930	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	078	CHANGING LANES
1482	025A	169.60	06/28/11	0810	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1483	025A	169.68	04/18/11	0655	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1484	025A	169.70	11/24/12	0445	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1485	025A	169.70	10/20/13	1645	N	OVERTURNING	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
1486	025A	169.70	04/17/13	1705	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1487	025A	169.70	06/20/15	0605	N	GUARD RAIL	N	SUV	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
1488	025A	169.75	11/13/13	0718	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1489	025A	169.75	12/27/14	2007	N	PARKED MOTOR VEHICLE	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1490	025A	169.78	06/02/13	2020	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1491	025A	169.80	06/07/13	1835	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1492	025A	169.80	07/23/11	0145	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1493	025A	169.80	11/10/14	1545	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
1494	025A	169.85	06/16/13	1140	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1495	025A	169.90	02/02/11	1755	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1496	025A	169.90	11/16/13	1150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1497	025A	169.90	04/20/14	1955	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	SLOWING
1498	025A	169.90	07/18/15	1422	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1499	025A	169.90	06/24/12	0915	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1500	025A	169.95	11/09/15	0630	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1501	025A	170.00	07/12/14	1839	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1502	025A	170.00	02/15/11	2215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1503	025A	170.00	05/22/13	1155	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1504	025A	170.00	07/07/13	0015	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1505	025A	170.00	09/27/14	1635	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1506	025A	170.00	05/21/13	0625	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1507	025A	170.00	11/03/14	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1508	025A	170.00	03/05/15	1000	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1509	025A	170.00	09/04/15	1630	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1510	025A	170.00	07/09/13	2040	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1511	025A	170.00	06/28/15	0155	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1512	025A	170.00	12/29/11	0250	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1513	025A	170.00	09/11/13	2309	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1514	025A	170.00	07/23/15	1345	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1515	025A	170.03	01/04/14	1538	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1516	025A	170.10	07/20/11	1700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1517	025A	170.10	05/11/14	1220	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1518	025A	170.10	01/13/13	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	UNKNOWN	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1519	025A	170.20	10/28/15	2250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1520	025A	170.20	12/25/12	0825	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	NONE
1521	025A	170.20	04/21/13	0050	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1522	025A	170.20	01/01/15	2130	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1523	025A	170.20	05/16/15	0048	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1524	025A	170.25	08/22/13	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1525	025A	170.27	03/28/15	0120	PDO	OFF AT TEE	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1526	025A	170.30	12/19/12	0810	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1527	025A	170.30	10/10/11	0003	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1528	025A	170.30	11/03/13	1645	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1529	025A	170.30	09/01/14	0630	PDO	ON	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1530	025A	170.40	11/09/15	0748	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1531	025A	170.40	12/31/11	2000	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1532	025A	170.46	06/14/14	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1533	025A	170.50	07/09/12	1035	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1534	025A	170.50	08/29/14	1025	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
1535	025A	170.50	01/01/15	2220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1536	025A	170.50	10/11/15	1215	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
1537	025A	170.50	01/18/12	1930	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1538	025A	170.50	10/06/14	2200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1539	025A	170.50	04/16/12	2030	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1540	025A	170.50	08/16/13	0900	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1541	025A	170.50	07/15/11	1640	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1542	025A	170.50	04/14/14	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1543	025A	170.50	04/17/15	1253	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1544	025A	170.50	11/07/15	1245	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1545	025A	170.51	06/05/13	1602	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1546	025A	170.60	04/20/11	1230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1547	025A	170.60	09/28/12	1735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1548	025A	170.60	08/10/14	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1549	025A	170.60	11/07/15	1045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1550	025A	170.60	07/12/14	1120	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1551	025A	170.60	04/16/15	0800	PDO	OFF RIGHT	NON-INTERSECTION	1	HILLCREST	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1552	025A	170.60	07/13/12	1120	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1553	025A	170.70	02/01/15	0720	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
1554	025A	170.70	12/30/14	0820	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1555	025A	170.70	04/05/12	1600	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	WIND
1556	025A	170.70	12/30/14	0815	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
1557	025A	170.70	02/20/12	0645	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1558	025A	170.80	04/30/14	1137	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
1559	025A	170.80	11/27/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1560	025A	170.80	02/03/14	1745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1501	025A	170.00	07/12/14	1839	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
1502	025A	170.00	02/15/11	2215	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1503	025A	170.00	05/22/13	1155	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1504	025A	170.00	07/07/13	0015	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
1505	025A	170.00	09/27/14	1635	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1506	025A	170.00	05/21/13	0625	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
1507	025A	170.00	11/03/14	1900	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	PASSING
1508	025A	170.00	03/05/15	1000	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	CHANGING LANES
1509	025A	170.00	09/04/15	1630	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
1510	025A	170.00	07/09/13	2040	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1511	025A	170.00	06/28/15	0155	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
1512	025A	170.00	12/29/11	0250	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1513	025A	170.00	09/11/13	2309	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1514	025A	170.00	07/23/15	1345	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
1515	025A	170.03	01/04/14	1538	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
1516	025A	170.10	07/20/11	1700	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
1517	025A	170.10	05/11/14	1220	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
1518	025A	170.10	01/13/13	1720	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1519	025A	170.20	10/28/15	2250	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1520	025A	170.20	12/25/12	0825	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
1521	025A	170.20	04/21/13	0050	N	GUARD RAIL	S	SUV	ALCOHOL/DRUGS	UNKNOWN	075	PASSING
1522	025A	170.20	01/01/15	2130	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1523	025A	170.20	05/16/15	0048	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1524	025A	170.25	08/22/13	1730	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
1525	025A	170.27	03/28/15	0120	Y (T)	FENCE	N	SUV	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
1526	025A	170.30	12/19/12	0810	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1527	025A	170.30	10/10/11	0003	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT
1528	025A	170.30	11/03/13	1645	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	GOING STRAIGHT
1529	025A	170.30	09/01/14	0630	Y (T)	BARRICADE/TRAFFIC BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1530	025A	170.40	11/09/15	0748	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
1531	025A	170.40	12/31/11	2000	N	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1532	025A	170.46	06/14/14	1130	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	AVOIDING OBJECT/VEHICLE IN ROAD
1533	025A	170.50	07/09/12	1035	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
1534	025A	170.50	08/29/14	1025	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1535	025A	170.50	01/01/15	2220	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1536	025A	170.50	10/11/15	1215	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1537	025A	170.50	01/18/12	1930	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1538	025A	170.50	10/06/14	2200	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1539	025A	170.50	04/16/12	2030	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1540	025A	170.50	08/16/13	0900	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1541	025A	170.50	07/15/11	1640	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1542	025A	170.50	04/14/14	1130	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	GOING STRAIGHT
1543	025A	170.50	04/17/15	1253	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1544	025A	170.50	11/07/15	1245	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1545	025A	170.51	06/05/13	1602	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	WEAVING
1546	025A	170.60	04/20/11	1230	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1547	025A	170.60	09/28/12	1735	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1548	025A	170.60	08/10/14	1555	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1549	025A	170.60	11/07/15	1045	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1550	025A	170.60	07/12/14	1120	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1551	025A	170.60	04/16/15	0800	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1552	025A	170.60	07/13/12	1120	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1553	025A	170.70	02/01/15	0720	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1554	025A	170.70	12/30/14	0820	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1555	025A	170.70	04/05/12	1600	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1556	025A	170.70	12/30/14	0815	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1557	025A	170.70	02/20/12	0645	N	TREE/SHRUBBERY	N	SUV	NO IMPAIRMENT	ILLNESS	060	GOING STRAIGHT
1558	025A	170.80	04/30/14	1137	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1559	025A	170.80	11/27/14	1200	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1560	025A	170.80	02/03/14	1745	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	080	AVOIDING OBJECT/VEHICLE IN ROAD

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1561	025A	170.80	01/21/15	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1562	025A	170.80	12/26/11	0315	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1563	025A	170.80	09/27/15	1040	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1564	025A	170.80	04/20/15	0000	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1565	025A	170.83	09/19/15	1020	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1566	025A	170.87	11/08/15	1735	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1567	025A	170.90	05/05/13	1728	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1568	025A	170.90	08/10/14	1330	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1569	025A	170.90	02/07/15	1530	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1570	025A	170.90	05/11/14	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
1571	025A	170.90	11/16/14	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1572	025A	170.90	06/07/15	1720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1573	025A	170.90	02/07/11	1910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1574	025A	170.90	08/03/15	2035	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1575	025A	170.90	10/24/15	1805	PDO	ON	AT INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1576	025A	170.90	11/08/15	1331	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1577	025A	170.91	11/29/15	1820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1578	025A	170.93	04/03/12	0855	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1579	025A	170.95	09/18/13	0500	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
1580	025A	170.95	11/08/15	1858	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1581	025A	170.99	09/27/15	0019	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1582	025A	170.99	05/24/15	1704	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1583	025A	171.00	06/09/13	1510	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1584	025A	171.00	09/06/13	1640	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1585	025A	171.00	10/31/12	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1586	025A	171.00	07/07/12	0517	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
1587	025A	171.00	11/23/13	2224	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1588	025A	171.00	11/26/13	1724	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1589	025A	171.00	12/08/13	1755	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	UNKNOWN	DARK-UNLIGHTED	NONE
1590	025A	171.00	05/11/15	2030	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1591	025A	171.00	02/24/13	1615	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1592	025A	171.00	06/08/14	2345	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
1593	025A	171.00	04/18/12	1125	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1594	025A	171.00	11/16/12	1812	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1595	025A	171.02	02/24/13	1022	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1596	025A	171.04	07/21/11	1040	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1597	025A	171.06	08/10/15	1642	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1598	025A	171.10	04/20/11	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1599	025A	171.10	12/10/11	1345	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1600	025A	171.10	08/02/13	1010	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1601	025A	171.10	01/16/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1602	025A	171.10	11/10/11	2322	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1603	025A	171.10	12/01/11	0650	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1604	025A	171.10	11/29/13	2200	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1605	025A	171.20	05/05/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1606	025A	171.20	05/04/12	0020	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1607	025A	171.25	06/18/11	1505	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1608	025A	171.25	07/13/13	1550	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1609	025A	171.25	03/15/14	0810	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1610	025A	171.30	08/06/12	2245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1611	025A	171.30	10/16/11	1240	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1612	025A	171.30	01/17/12	0900	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1613	025A	171.36	09/13/13	1559	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1614	025A	171.40	08/30/12	1930	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1615	025A	171.40	04/20/14	0950	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1616	025A	171.40	05/03/13	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1617	025A	171.40	08/30/12	1930	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1618	025A	171.40	05/08/11	2000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1619	025A	171.40	07/11/13	1333	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1620	025A	171.44	11/29/15	1440	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1561	025A	170.80	01/21/15	1350	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
1562	025A	170.80	12/26/11	0315	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1563	025A	170.80	09/27/15	1040	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1564	025A	170.80	04/20/15	0000	N	TREE/SHRUBBERY	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1565	025A	170.83	09/19/15	1020	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1566	025A	170.87	11/08/15	1735	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	SLOWING
1567	025A	170.90	05/05/13	1728	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1568	025A	170.90	08/10/14	1330	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1569	025A	170.90	02/07/15	1530	N	OTHER NON-COLLISION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	015	ENTERING/LEAVING PARKED POSITION
1570	025A	170.90	05/11/14	1425	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1571	025A	170.90	11/16/14	1640	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1572	025A	170.90	06/07/15	1720	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
1573	025A	170.90	02/07/11	1910	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
1574	025A	170.90	08/03/15	2035	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1575	025A	170.90	10/24/15	1805	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1576	025A	170.90	11/08/15	1331	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	035	SLOWING
1577	025A	170.91	11/29/15	1820	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1578	025A	170.93	04/03/12	0855	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
1579	025A	170.95	09/18/13	0500	N	WILD ANIMAL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1580	025A	170.95	11/08/15	1858	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1581	025A	170.99	09/27/15	0019	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1582	025A	170.99	05/24/15	1704	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1583	025A	171.00	06/09/13	1510	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1584	025A	171.00	09/06/13	1640	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1585	025A	171.00	10/31/12	1720	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1586	025A	171.00	07/07/12	0517	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1587	025A	171.00	11/23/13	2224	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1588	025A	171.00	11/26/13	1724	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1589	025A	171.00	12/08/13	1755	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1590	025A	171.00	05/11/15	2030	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1591	025A	171.00	02/24/13	1615	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1592	025A	171.00	06/08/14	2345	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1593	025A	171.00	04/18/12	1125	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1594	025A	171.00	11/16/12	1812	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1595	025A	171.02	02/24/13	1022	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
1596	025A	171.04	07/21/11	1040	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1597	025A	171.06	08/10/15	1642	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	AVOIDING OBJECT/VEHICLE IN ROAD
1598	025A	171.10	04/20/11	1245	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1599	025A	171.10	12/10/11	1345	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1600	025A	171.10	08/02/13	1010	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
1601	025A	171.10	01/16/14	1515	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1602	025A	171.10	11/10/11	2322	N	SIGN	S	SUV	ALCOHOL	NONE APPARENT	025	GOING STRAIGHT
1603	025A	171.10	12/01/11	0650	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1604	025A	171.10	11/29/13	2200	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	OTHER
1605	025A	171.20	05/05/15	1130	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
1606	025A	171.20	05/04/12	0020	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1607	025A	171.25	06/18/11	1505	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1608	025A	171.25	07/13/13	1550	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1609	025A	171.25	03/15/14	0810	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1610	025A	171.30	08/06/12	2245	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1611	025A	171.30	10/16/11	1240	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1612	025A	171.30	01/17/12	0900	N	TRAFFIC SIGNAL POLE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1613	025A	171.36	09/13/13	1559	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1614	025A	171.40	08/30/12	1930	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	075	OTHER
1615	025A	171.40	04/20/14	0950	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1616	025A	171.40	05/03/13	1730	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1617	025A	171.40	08/30/12	1930	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
1618	025A	171.40	05/08/11	2000	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ILLNESS	070	GOING STRAIGHT
1619	025A	171.40	07/11/13	1333	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1620	025A	171.44	11/29/15	1440	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1621	025A	171.45	09/22/12	1315	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1622	025A	171.47	08/25/14	1000	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1623	025A	171.49	06/20/15	1451	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1624	025A	171.49	10/31/14	0250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1625	025A	171.50	02/15/12	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1626	025A	171.50	01/16/14	1429	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1627	025A	171.50	04/05/15	1215	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1628	025A	171.50	05/27/15	0634	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1629	025A	171.50	06/21/15	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1630	025A	171.50	07/26/15	1919	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1631	025A	171.50	11/28/15	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1632	025A	171.50	05/19/13	0115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1633	025A	171.50	07/24/11	1740	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1634	025A	171.50	09/11/14	0745	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1635	025A	171.50	11/30/14	1505	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1636	025A	171.50	08/16/15	1140	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1637	025A	171.51	10/02/15	1520	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1638	025A	171.53	09/17/11	0515	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1639	025A	171.56	05/25/12	1830	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1640	025A	171.56	11/12/13	0950	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1641	025A	171.59	05/27/15	0555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1642	025A	171.60	05/10/12	0245	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1643	025A	171.60	06/05/13	2105	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1644	025A	171.60	12/14/14	0100	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1645	025A	171.60	06/27/15	1415	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1646	025A	171.60	07/11/15	1302	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1647	025A	171.70	07/18/15	1015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1648	025A	171.70	10/07/13	0420	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1649	025A	171.70	06/17/15	0550	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1650	025A	171.70	09/12/12	0745	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
1651	025A	171.75	11/10/11	2320	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1652	025A	171.75	04/17/15	0805	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1653	025A	171.77	11/17/15	2313	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1654	025A	171.77	03/02/14	2155	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1655	025A	171.79	08/10/14	1410	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1656	025A	171.79	12/20/12	0015	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1657	025A	171.80	02/16/12	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1658	025A	171.80	07/14/13	1920	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1659	025A	171.80	06/22/14	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1660	025A	171.80	07/18/15	1020	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1661	025A	171.80	02/07/12	0705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1662	025A	171.80	01/04/13	0935	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1663	025A	171.80	08/01/13	0951	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1664	025A	171.80	06/21/11	0145	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1665	025A	171.80	06/23/11	2220	INJ	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1666	025A	171.80	09/21/14	0332	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1667	025A	171.80	02/23/15	1530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1668	025A	171.80	02/07/11	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1669	025A	171.80	03/15/14	0600	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1670	025A	171.80	06/14/14	1305	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1671	025A	171.80	07/10/15	1357	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1672	025A	171.81	02/23/12	0845	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1673	025A	171.81	03/05/14	0550	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1674	025A	171.82	03/26/15	0132	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1675	025A	171.82	03/15/14	0550	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1676	025A	171.82	09/14/11	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1677	025A	171.83	09/30/12	1040	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1678	025A	171.84	04/18/15	1926	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	WET	DAWN OR DUSK	RAIN
1679	025A	171.85	04/19/11	1840	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
1680	025A	171.85	05/28/12	0214	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1621	025A	171.45	09/22/12	1315	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
1622	025A	171.47	08/25/14	1000	N	GUARD RAIL	N	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1623	025A	171.49	06/20/15	1451	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1624	025A	171.49	10/31/14	0250	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	PASSING
1625	025A	171.50	02/15/12	1650	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1626	025A	171.50	01/16/14	1429	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1627	025A	171.50	04/05/15	1215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1628	025A	171.50	05/27/15	0634	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1629	025A	171.50	06/21/15	1540	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1630	025A	171.50	07/26/15	1919	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	GOING STRAIGHT
1631	025A	171.50	11/28/15	1020	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1632	025A	171.50	05/19/13	0115	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
1633	025A	171.50	07/24/11	1740	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1634	025A	171.50	09/11/14	0745	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1635	025A	171.50	11/30/14	1505	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1636	025A	171.50	08/16/15	1140	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1637	025A	171.51	10/02/15	1520	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1638	025A	171.53	09/17/11	0515	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1639	025A	171.56	05/25/12	1830	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	UK	SLOWING
1640	025A	171.56	11/12/13	0950	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	AVOIDING OBJECT/VEHICLE IN ROAD
1641	025A	171.59	05/27/15	0555	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1642	025A	171.60	05/10/12	0245	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1643	025A	171.60	06/05/13	2105	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1644	025A	171.60	12/14/14	0100	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1645	025A	171.60	06/27/15	1415	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1646	025A	171.60	07/11/15	1302	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1647	025A	171.70	07/18/15	1015	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1648	025A	171.70	10/07/13	0420	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1649	025A	171.70	06/17/15	0550	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1650	025A	171.70	09/12/12	0745	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1651	025A	171.75	11/10/11	2320	N	SIGN	S	SUV	ALCOHOL	DISTRACTED BY PASSENGER	UK	GOING STRAIGHT
1652	025A	171.75	04/17/15	0805	N	SIGN	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1653	025A	171.77	11/17/15	2313	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
1654	025A	171.77	03/02/14	2155	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
1655	025A	171.79	08/10/14	1410	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1656	025A	171.79	12/20/12	0015	Y (B)	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
1657	025A	171.80	02/16/12	0710	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
1658	025A	171.80	07/14/13	1920	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
1659	025A	171.80	06/22/14	1555	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	025	GOING STRAIGHT
1660	025A	171.80	07/18/15	1020	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1661	025A	171.80	02/07/12	0705	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	CHANGING LANES
1662	025A	171.80	01/04/13	0935	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	085	PASSING
1663	025A	171.80	08/01/13	0951	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1664	025A	171.80	06/21/11	0145	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1665	025A	171.80	06/23/11	2220	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1666	025A	171.80	09/21/14	0332	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1667	025A	171.80	02/23/15	1530	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	075	WEAVING
1668	025A	171.80	02/07/11	0630	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	AVOIDING OBJECT/VEHICLE IN ROAD
1669	025A	171.80	03/15/14	0600	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
1670	025A	171.80	06/14/14	1305	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1671	025A	171.80	07/10/15	1357	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	WEAVING
1672	025A	171.81	02/23/12	0845	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1673	025A	171.81	03/05/14	0550	Y (B)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1674	025A	171.82	03/26/15	0132	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	050	PASSING
1675	025A	171.82	03/15/14	0550	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1676	025A	171.82	09/14/11	1555	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1677	025A	171.83	09/30/12	1040	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
1678	025A	171.84	04/18/15	1926	Y (C)	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	RX/MEDICATION/DR	UNKNOWN	070	OTHER
1679	025A	171.85	04/19/11	1840	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
1680	025A	171.85	05/28/12	0214	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER FATIGUE	075	WEAVING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1681	025A	171.88	04/07/14	1800	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1682	025A	171.89	08/26/13	0105	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1683	025A	171.90	03/18/11	1520	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1684	025A	171.90	04/06/11	0445	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1685	025A	171.90	10/19/14	1535	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1686	025A	171.90	01/02/15	0350	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1687	025A	171.90	07/17/11	0230	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1688	025A	171.90	02/06/11	2000	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1689	025A	171.90	10/26/11	1721	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
1690	025A	171.90	02/04/12	0353	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1691	025A	171.90	12/15/12	0420	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1692	025A	171.90	07/05/13	1830	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
1693	025A	171.90	12/26/13	1805	INJ	OFF LEFT	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1694	025A	171.90	02/20/14	0520	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
1695	025A	171.90	10/26/11	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
1696	025A	171.93	10/03/15	0940	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1697	025A	171.94	01/24/11	1800	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1698	025A	171.95	07/14/11	1335	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1699	025A	171.95	07/29/11	1220	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1700	025A	171.95	02/16/12	0703	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1701	025A	171.97	07/20/14	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1702	025A	171.99	08/21/11	1310	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1703	025A	171.99	04/17/15	0139	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1704	025A	172.00	11/03/12	1930	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1705	025A	172.00	08/04/13	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1706	025A	172.00	01/02/15	0830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1707	025A	172.00	11/14/15	1730	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1708	025A	172.00	09/12/11	1650	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1709	025A	172.00	04/06/13	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1710	025A	172.00	09/02/13	0950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1711	025A	172.00	06/14/14	1325	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1712	025A	172.00	08/03/14	0320	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1713	025A	172.00	08/11/15	1701	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1714	025A	172.00	02/02/11	0612	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1715	025A	172.00	02/20/12	0345	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1716	025A	172.00	01/28/14	0525	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1717	025A	172.00	01/01/15	2045	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1718	025A	172.00	01/21/15	1930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1719	025A	172.00	02/02/11	0610	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
1720	025A	172.00	09/13/14	1155	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1721	025A	172.00	03/21/11	1240	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1722	025A	172.00	07/13/14	1630	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1723	025A	172.00	05/29/15	1434	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1724	025A	172.00	06/17/15	0750	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1725	025A	172.02	11/24/11	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1726	025A	172.02	09/29/14	1450	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1727	025A	172.04	11/11/15	2048	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1728	025A	172.10	05/20/12	1530	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1729	025A	172.10	06/20/15	1140	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1730	025A	172.10	04/27/11	0845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1731	025A	172.10	07/02/11	1115	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1732	025A	172.10	10/28/11	1840	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
1733	025A	172.10	04/22/12	1215	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1734	025A	172.10	09/02/13	1605	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1735	025A	172.10	12/26/15	1259	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1736	025A	172.10	07/07/13	0440	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1737	025A	172.10	11/03/12	1845	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1738	025A	172.10	12/14/14	0825	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1739	025A	172.10	01/03/15	1855	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1740	025A	172.10	03/29/11	0645	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1681	025A	171.88	04/07/14	1800	Y (D)	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1682	025A	171.89	08/26/13	0105	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	080	OTHER
1683	025A	171.90	03/18/11	1520	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1684	025A	171.90	04/06/11	0445	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1685	025A	171.90	10/19/14	1535	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1686	025A	171.90	01/02/15	0350	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1687	025A	171.90	07/17/11	0230	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1688	025A	171.90	02/06/11	2000	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1689	025A	171.90	10/26/11	1721	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1690	025A	171.90	02/04/12	0353	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
1691	025A	171.90	12/15/12	0420	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
1692	025A	171.90	07/05/13	1830	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1693	025A	171.90	12/26/13	1805	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1694	025A	171.90	02/20/14	0520	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
1695	025A	171.90	10/26/11	1715	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1696	025A	171.93	10/03/15	0940	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1697	025A	171.94	01/24/11	1800	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	080	GOING STRAIGHT
1698	025A	171.95	07/14/11	1335	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1699	025A	171.95	07/29/11	1220	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1700	025A	171.95	02/16/12	0703	N	INVOLVING OTHER OBJECT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1701	025A	171.97	07/20/14	1400	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
1702	025A	171.99	08/21/11	1310	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1703	025A	171.99	04/17/15	0139	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
1704	025A	172.00	11/03/12	1930	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1705	025A	172.00	08/04/13	1200	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1706	025A	172.00	01/02/15	0830	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1707	025A	172.00	11/14/15	1730	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1708	025A	172.00	09/12/11	1650	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	CHANGING LANES
1709	025A	172.00	04/06/13	1115	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1710	025A	172.00	09/02/13	0950	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
1711	025A	172.00	06/14/14	1325	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1712	025A	172.00	08/03/14	0320	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1713	025A	172.00	08/11/15	1701	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1714	025A	172.00	02/02/11	0612	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1715	025A	172.00	02/20/12	0345	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
1716	025A	172.00	01/28/14	0525	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1717	025A	172.00	01/01/15	2045	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1718	025A	172.00	01/21/15	1930	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
1719	025A	172.00	02/02/11	0610	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
1720	025A	172.00	09/13/14	1155	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	AVOIDING OBJECT/VEHICLE IN ROAD
1721	025A	172.00	03/21/11	1240	N	PARKED MOTOR VEHICLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1722	025A	172.00	07/13/14	1630	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
1723	025A	172.00	05/29/15	1434	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1724	025A	172.00	06/17/15	0750	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1725	025A	172.02	11/24/11	1300	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
1726	025A	172.02	09/29/14	1450	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1727	025A	172.04	11/11/15	2048	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1728	025A	172.10	05/20/12	1530	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1729	025A	172.10	06/20/15	1140	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1730	025A	172.10	04/27/11	0845	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1731	025A	172.10	07/02/11	1115	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
1732	025A	172.10	10/28/11	1840	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1733	025A	172.10	04/22/12	1215	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1734	025A	172.10	09/02/13	1605	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1735	025A	172.10	12/26/15	1259	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1736	025A	172.10	07/07/13	0440	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1737	025A	172.10	11/03/12	1845	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
1738	025A	172.10	12/14/14	0825	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	CHANGING LANES
1739	025A	172.10	01/03/15	1855	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1740	025A	172.10	03/29/11	0645	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1741	025A	172.10	12/09/12	0940	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	MUDDY	DAYLIGHT	NONE
1742	025A	172.10	10/30/13	0620	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	NONE
1743	025A	172.10	06/21/14	1100	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1744	025A	172.10	07/26/15	1110	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1745	025A	172.11	10/25/12	0625	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1746	025A	172.18	08/22/15	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1747	025A	172.20	10/29/13	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1748	025A	172.20	07/27/13	1115	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1749	025A	172.20	08/05/14	1559	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1750	025A	172.20	08/29/14	1120	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1751	025A	172.20	06/21/15	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1752	025A	172.20	01/03/15	2018	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1753	025A	172.20	10/17/15	0212	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1754	025A	172.20	10/17/15	0212	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1755	025A	172.20	10/17/15	0215	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1756	025A	172.20	10/18/13	0630	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
1757	025A	172.20	08/15/15	1445	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1758	025A	172.30	11/16/13	1045	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1759	025A	172.30	08/29/13	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1760	025A	172.30	04/17/15	1101	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1761	025A	172.30	07/29/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1762	025A	172.30	10/11/15	0745	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1763	025A	172.30	09/21/13	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1764	025A	172.30	04/03/14	2100	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1765	025A	172.30	08/07/13	0630	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1766	025A	172.30	11/10/13	0400	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1767	025A	172.30	02/04/11	0454	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1768	025A	172.30	12/15/14	1635	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1769	025A	172.30	12/29/14	1240	PDO	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
1770	025A	172.31	05/11/14	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1771	025A	172.32	02/02/11	0855	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
1772	025A	172.33	07/03/13	1658	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1773	025A	172.34	06/21/15	1850	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1774	025A	172.35	02/10/13	0330	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1775	025A	172.40	07/02/11	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1776	025A	172.40	06/15/12	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1777	025A	172.40	07/11/14	1005	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1778	025A	172.40	11/24/15	0745	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1779	025A	172.40	06/17/15	0710	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1780	025A	172.40	04/17/15	1105	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
1781	025A	172.40	05/22/15	1455	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1782	025A	172.40	10/11/15	1345	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1783	025A	172.45	06/28/15	1000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1784	025A	172.46	05/15/14	2125	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1785	025A	172.50	10/24/13	1240	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1786	025A	172.50	07/02/11	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1787	025A	172.50	07/31/11	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1788	025A	172.50	07/31/11	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1789	025A	172.50	12/09/12	0945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
1790	025A	172.50	07/31/15	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1791	025A	172.50	12/28/15	1501	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1792	025A	172.50	11/13/11	1202	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1793	025A	172.50	07/11/11	1940	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1794	025A	172.50	10/02/11	2230	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1795	025A	172.50	11/02/12	0330	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1796	025A	172.50	05/11/13	1415	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1797	025A	172.50	11/02/11	1222	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1798	025A	172.50	08/09/15	1300	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1799	025A	172.60	09/16/14	1353	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1800	025A	172.60	10/14/15	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1741	025A	172.10	12/09/12	0940	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1742	025A	172.10	10/30/13	0620	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1743	025A	172.10	06/21/14	1100	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	WEAVING
1744	025A	172.10	07/26/15	1110	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1745	025A	172.11	10/25/12	0625	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1746	025A	172.18	08/22/15	1445	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1747	025A	172.20	10/29/13	1240	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
1748	025A	172.20	07/27/13	1115	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1749	025A	172.20	08/05/14	1559	N	REAR-END	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
1750	025A	172.20	08/29/14	1120	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
1751	025A	172.20	06/21/15	1610	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1752	025A	172.20	01/03/15	2018	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1753	025A	172.20	10/17/15	0212	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1754	025A	172.20	10/17/15	0212	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1755	025A	172.20	10/17/15	0215	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1756	025A	172.20	10/18/13	0630	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1757	025A	172.20	08/15/15	1445	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1758	025A	172.30	11/16/13	1045	N	OVERTURNING	N	SUV W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
1759	025A	172.30	08/29/13	1430	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1760	025A	172.30	04/17/15	1101	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1761	025A	172.30	07/29/15	1130	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1762	025A	172.30	10/11/15	0745	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1763	025A	172.30	09/21/13	0800	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1764	025A	172.30	04/03/14	2100	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1765	025A	172.30	08/07/13	0630	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1766	025A	172.30	11/10/13	0400	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1767	025A	172.30	02/04/11	0454	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1768	025A	172.30	12/15/14	1635	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1769	025A	172.30	12/29/14	1240	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
1770	025A	172.31	05/11/14	1300	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1771	025A	172.32	02/02/11	0855	Y (D)	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
1772	025A	172.33	07/03/13	1658	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1773	025A	172.34	06/21/15	1850	N	REAR-END	N	SUV	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
1774	025A	172.35	02/10/13	0330	N	BRIDGE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1775	025A	172.40	07/02/11	1200	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1776	025A	172.40	06/15/12	1400	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1777	025A	172.40	07/11/14	1005	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
1778	025A	172.40	11/24/15	0745	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
1779	025A	172.40	06/17/15	0710	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1780	025A	172.40	04/17/15	1105	N	OTHER FIXED OBJECT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1781	025A	172.40	05/22/15	1455	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
1782	025A	172.40	10/11/15	1345	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	010	GOING STRAIGHT
1783	025A	172.45	06/28/15	1000	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1784	025A	172.46	05/15/14	2125	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1785	025A	172.50	10/24/13	1240	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1786	025A	172.50	07/02/11	1200	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1787	025A	172.50	07/31/11	1320	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1788	025A	172.50	07/31/11	1320	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1789	025A	172.50	12/09/12	0945	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
1790	025A	172.50	07/31/15	1300	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1791	025A	172.50	12/28/15	1501	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	SLOWING
1792	025A	172.50	11/13/11	1202	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1793	025A	172.50	07/11/11	1940	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
1794	025A	172.50	10/02/11	2230	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1795	025A	172.50	11/02/12	0330	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1796	025A	172.50	05/11/13	1415	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1797	025A	172.50	11/02/11	1222	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
1798	025A	172.50	08/09/15	1300	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1799	025A	172.60	09/16/14	1353	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
1800	025A	172.60	10/14/15	1545	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1801	025A	172.60	07/05/12	1658	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
1802	025A	172.60	10/13/13	0900	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1803	025A	172.60	04/13/13	1135	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1804	025A	172.60	05/11/14	1250	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
1805	025A	172.64	08/19/15	1215	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1806	025A	172.70	01/02/15	0935	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1807	025A	172.70	01/10/15	1250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1808	025A	172.70	06/19/11	2200	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1809	025A	172.70	09/19/13	1954	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1810	025A	172.70	11/11/12	0800	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1811	025A	172.75	03/18/13	1725	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1812	025A	172.75	06/30/14	0830	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1813	025A	172.75	12/19/12	1435	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1814	025A	172.80	11/29/15	1527	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
1815	025A	172.80	11/13/13	2345	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1816	025A	172.80	03/27/12	1240	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1817	025A	172.80	04/06/13	0850	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1818	025A	172.80	04/03/14	2115	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1819	025A	172.80	06/20/14	1650	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1820	025A	172.90	09/24/13	1340	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1821	025A	172.90	06/14/14	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1822	025A	172.90	06/14/14	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1823	025A	172.90	06/18/12	0145	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1824	025A	172.90	09/26/14	0545	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1825	025A	172.90	09/27/13	1355	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1826	025A	172.99	10/03/15	1455	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1827	025A	172.99	03/19/11	1551	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1828	025A	173.00	09/24/13	1240	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1829	025A	173.00	07/05/14	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1830	025A	173.00	08/20/14	1444	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1831	025A	173.00	09/28/14	1720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1832	025A	173.00	10/09/14	1230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1833	025A	173.00	06/07/15	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1834	025A	173.00	07/01/15	0659	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1835	025A	173.00	12/06/13	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1836	025A	173.00	04/13/15	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1837	025A	173.00	01/06/15	0315	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	NONE
1838	025A	173.00	08/21/15	0655	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1839	025A	173.00	12/15/15	0900	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
1840	025A	173.08	03/29/14	0416	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1841	025A	173.09	06/21/15	1635	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1842	025A	173.10	03/06/15	1959	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1843	025A	173.10	06/05/14	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1844	025A	173.10	09/25/11	0120	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1845	025A	173.10	09/25/11	0121	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1846	025A	173.10	09/25/11	0122	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1847	025A	173.10	08/05/13	1845	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1848	025A	173.10	09/11/14	0620	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1849	025A	173.20	04/17/13	1700	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
1850	025A	173.20	09/28/15	1430	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1851	025A	173.20	04/29/15	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1852	025A	173.20	09/03/14	0820	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1853	025A	173.20	09/03/12	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1854	025A	173.20	12/31/14	1700	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1855	025A	173.25	11/16/13	1820	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1856	025A	173.30	08/04/15	1250	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1857	025A	173.30	08/04/15	1250	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1858	025A	173.30	04/06/15	1200	INJ	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
1859	025A	173.30	03/05/15	1425	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
1860	025A	173.40	06/28/14	1920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1801	025A	172.60	07/05/12	1658	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	055	GOING STRAIGHT
1802	025A	172.60	10/13/13	0900	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1803	025A	172.60	04/13/13	1135	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1804	025A	172.60	05/11/14	1250	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1805	025A	172.64	08/19/15	1215	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
1806	025A	172.70	01/02/15	0935	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1807	025A	172.70	01/10/15	1250	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1808	025A	172.70	06/19/11	2200	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1809	025A	172.70	09/19/13	1954	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1810	025A	172.70	11/11/12	0800	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1811	025A	172.75	03/18/13	1725	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1812	025A	172.75	06/30/14	0830	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1813	025A	172.75	12/19/12	1435	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	WEAVING
1814	025A	172.80	11/29/15	1527	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
1815	025A	172.80	11/13/13	2345	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1816	025A	172.80	03/27/12	1240	N	GUARD RAIL	S	MOTOR HOME	ALCOHOL/DRUGS	UNKNOWN	050	WEAVING
1817	025A	172.80	04/06/13	0850	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1818	025A	172.80	04/03/14	2115	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1819	025A	172.80	06/20/14	1650	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1820	025A	172.90	09/24/13	1340	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1821	025A	172.90	06/14/14	1730	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1822	025A	172.90	06/14/14	1730	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
1823	025A	172.90	06/18/12	0145	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1824	025A	172.90	09/26/14	0545	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1825	025A	172.90	09/27/13	1355	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
1826	025A	172.99	10/03/15	1455	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
1827	025A	172.99	03/19/11	1551	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1828	025A	173.00	09/24/13	1240	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1829	025A	173.00	07/05/14	1145	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1830	025A	173.00	08/20/14	1444	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
1831	025A	173.00	09/28/14	1720	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
1832	025A	173.00	10/09/14	1230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
1833	025A	173.00	06/07/15	1130	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1834	025A	173.00	07/01/15	0659	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1835	025A	173.00	12/06/13	1150	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1836	025A	173.00	04/13/15	1545	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1837	025A	173.00	01/06/15	0315	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1838	025A	173.00	08/21/15	0655	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1839	025A	173.00	12/15/15	0900	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
1840	025A	173.08	03/29/14	0416	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	000	STOPPED IN TRAFFIC
1841	025A	173.09	06/21/15	1635	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1842	025A	173.10	03/06/15	1959	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	068	GOING STRAIGHT
1843	025A	173.10	06/05/14	1220	N	SIDESWIPE SAME DIRECTION	S	SUV	RX/MEDICATION/DR	NONE APPARENT	075	WEAVING
1844	025A	173.10	09/25/11	0120	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1845	025A	173.10	09/25/11	0121	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1846	025A	173.10	09/25/11	0122	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1847	025A	173.10	08/05/13	1845	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1848	025A	173.10	09/11/14	0620	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1849	025A	173.20	04/17/13	1700	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
1850	025A	173.20	09/28/15	1430	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1851	025A	173.20	04/29/15	0715	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1852	025A	173.20	09/03/14	0820	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1853	025A	173.20	09/03/12	1200	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1854	025A	173.20	12/31/14	1700	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
1855	025A	173.25	11/16/13	1820	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1856	025A	173.30	08/04/15	1250	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
1857	025A	173.30	08/04/15	1250	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
1858	025A	173.30	04/06/15	1200	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
1859	025A	173.30	03/05/15	1425	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
1860	025A	173.40	06/28/14	1920	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1861	025A	173.40	01/19/14	0510	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1862	025A	173.40	12/26/14	0745	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
1863	025A	173.50	06/24/14	1624	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1864	025A	173.50	02/05/13	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1865	025A	173.50	07/09/11	1100	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1866	025A	173.50	07/06/13	0330	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1867	025A	173.50	06/07/14	0545	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1868	025A	173.50	08/04/11	0220	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1869	025A	173.50	11/21/15	0640	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1870	025A	173.50	12/10/12	1015	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1871	025A	173.50	07/31/15	1303	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1872	025A	173.50	08/26/12	1240	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1873	025A	173.51	02/07/15	1239	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1874	025A	173.60	07/16/12	1230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1875	025A	173.60	06/22/15	1920	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1876	025A	173.60	09/21/12	0550	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1877	025A	173.60	08/29/15	1516	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1878	025A	173.70	09/03/13	2200	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1879	025A	173.70	08/26/11	1250	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1880	025A	173.74	08/07/12	1110	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1881	025A	173.79	05/03/15	1930	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1882	025A	173.80	11/29/14	1729	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1883	025A	173.80	07/10/12	2150	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1884	025A	173.80	07/01/11	1345	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1885	025A	173.80	06/20/11	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1886	025A	173.83	08/04/13	2310	INJ	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1887	025A	173.84	02/09/12	1343	PDO	OFF LEFT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1888	025A	173.86	03/27/12	0650	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1889	025A	173.90	03/15/15	1616	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1890	025A	173.90	10/29/11	1845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1891	025A	173.90	05/11/14	0807	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1892	025A	173.90	08/16/15	2138	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1893	025A	173.90	05/17/15	0235	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1894	025A	173.90	03/14/14	1735	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1895	025A	173.91	11/10/12	0606	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1896	025A	173.94	01/16/11	2046	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1897	025A	173.96	07/30/14	1158	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1898	025A	173.99	11/12/11	1815	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
1899	025A	174.00	03/09/14	1315	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1900	025A	174.00	10/31/15	0607	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1901	025A	174.00	12/31/15	1232	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1902	025A	174.00	07/10/11	2245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1903	025A	174.00	05/11/14	0715	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1904	025A	174.00	10/12/14	2236	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
1905	025A	174.00	11/27/15	0100	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1906	025A	174.00	12/22/14	1915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1907	025A	174.00	06/21/14	0810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1908	025A	174.01	01/03/15	2130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1909	025A	174.04	01/09/15	0157	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	RAIN
1910	025A	174.07	10/29/15	1351	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1911	025A	174.10	12/19/14	1834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1912	025A	174.10	07/23/15	2015	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
1913	025A	174.10	04/27/11	0530	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	NONE
1914	025A	174.10	05/11/13	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1915	025A	174.20	07/13/11	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1916	025A	174.20	08/03/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1917	025A	174.20	11/29/14	1705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1918	025A	174.20	09/23/13	0630	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	FOG
1919	025A	174.20	07/14/14	0900	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1920	025A	174.20	12/27/15	1715	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1861	025A	173.40	01/19/14	0510	N	CABLE RAIL	N	SUV	ALCOHOL	UNKNOWN	045	WEAVING
1862	025A	173.40	12/26/14	0745	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
1863	025A	173.50	06/24/14	1624	N	OTHER NON-COLLISION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
1864	025A	173.50	02/05/13	0710	N	VEHICLE CARGO/DEBRIS	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
1865	025A	173.50	07/09/11	1100	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1866	025A	173.50	07/06/13	0330	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1867	025A	173.50	06/07/14	0545	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1868	025A	173.50	08/04/11	0220	N	GUARD RAIL	N	SUV	ALCOHOL	UNKNOWN	055	OTHER
1869	025A	173.50	11/21/15	0640	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
1870	025A	173.50	12/10/12	1015	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1871	025A	173.50	07/31/15	1303	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1872	025A	173.50	08/26/12	1240	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1873	025A	173.51	02/07/15	1239	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1874	025A	173.60	07/16/12	1230	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
1875	025A	173.60	06/22/15	1920	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1876	025A	173.60	09/21/12	0550	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1877	025A	173.60	08/29/15	1516	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
1878	025A	173.70	09/03/13	2200	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1879	025A	173.70	08/26/11	1250	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1880	025A	173.74	08/07/12	1110	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1881	025A	173.79	05/03/15	1930	Y (O)	SIGN	N	SUV	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
1882	025A	173.80	11/29/14	1729	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1883	025A	173.80	07/10/12	2150	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1884	025A	173.80	07/01/11	1345	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1885	025A	173.80	06/20/11	1600	Y (L)	PEDESTRIAN	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
1886	025A	173.83	08/04/13	2310	Y (C)	CABLE RAIL	N	SUV	ALCOHOL	UNKNOWN	055	WEAVING
1887	025A	173.84	02/09/12	1343	Y (L)	OTHER NON-COLLISION	S	MOTOR HOME	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
1888	025A	173.86	03/27/12	0650	Y (L)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	MAKING LEFT TURN
1889	025A	173.90	03/15/15	1616	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
1890	025A	173.90	10/29/11	1845	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
1891	025A	173.90	05/11/14	0807	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1892	025A	173.90	08/16/15	2138	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1893	025A	173.90	05/17/15	0235	N	CABLE RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	GOING STRAIGHT
1894	025A	173.90	03/14/14	1735	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
1895	025A	173.91	11/10/12	0606	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1896	025A	173.94	01/16/11	2046	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1897	025A	173.96	07/30/14	1158	N	REAR-END	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	040	GOING STRAIGHT
1898	025A	173.99	11/12/11	1815	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1899	025A	174.00	03/09/14	1315	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1900	025A	174.00	10/31/15	0607	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1901	025A	174.00	12/31/15	1232	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	WEAVING
1902	025A	174.00	07/10/11	2245	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1903	025A	174.00	05/11/14	0715	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1904	025A	174.00	10/12/14	2236	N	SIGN	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	075	OTHER
1905	025A	174.00	11/27/15	0100	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
1906	025A	174.00	12/22/14	1915	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	020	PASSING
1907	025A	174.00	06/21/14	0810	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	WEAVING
1908	025A	174.01	01/03/15	2130	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1909	025A	174.04	01/09/15	0157	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
1910	025A	174.07	10/29/15	1351	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	BACKING
1911	025A	174.10	12/19/14	1834	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
1912	025A	174.10	07/23/15	2015	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1913	025A	174.10	04/27/11	0530	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
1914	025A	174.10	05/11/13	0645	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1915	025A	174.20	07/13/11	1240	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	SLOWING
1916	025A	174.20	08/03/14	1515	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1917	025A	174.20	11/29/14	1705	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
1918	025A	174.20	09/23/13	0630	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1919	025A	174.20	07/14/14	0900	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
1920	025A	174.20	12/27/15	1715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1921	025A	174.22	12/03/15	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1922	025A	174.25	06/27/12	1000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1923	025A	174.25	11/01/12	1830	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1924	025A	174.25	10/12/14	0110	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1925	025A	174.25	10/12/14	0110	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1926	025A	174.30	08/23/14	1735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1927	025A	174.30	10/23/15	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1928	025A	174.30	05/05/15	1155	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1929	025A	174.35	05/23/11	2330	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1930	025A	174.35	06/22/14	0405	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1931	025A	174.44	06/23/15	1140	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1932	025A	174.47	05/30/11	0715	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1933	025A	174.47	07/03/12	0010	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1934	025A	174.50	12/01/11	0520	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
1935	025A	174.50	10/09/12	1510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1936	025A	174.50	08/14/15	2118	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	UNKNOWN
1937	025A	174.50	08/14/15	2130	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
1938	025A	174.50	08/31/12	2050	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1939	025A	174.50	08/05/13	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1940	025A	174.50	01/30/15	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1941	025A	174.50	11/02/15	1945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1942	025A	174.50	05/29/11	2110	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1943	025A	174.50	08/31/14	1858	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1944	025A	174.50	04/18/14	0000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1945	025A	174.50	09/19/15	1135	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1946	025A	174.51	03/06/14	2216	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1947	025A	174.51	01/22/15	0420	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
1948	025A	174.60	09/26/15	1020	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1949	025A	174.60	06/08/14	1615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
1950	025A	174.61	03/09/13	0526	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
1951	025A	174.62	10/09/15	1541	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1952	025A	174.67	12/19/14	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
1953	025A	174.69	07/30/11	1500	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1954	025A	174.70	12/27/14	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1955	025A	174.70	07/27/15	1653	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1956	025A	174.70	10/21/12	1821	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1957	025A	174.75	03/11/11	1820	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1958	025A	174.75	10/07/14	1657	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1959	025A	174.75	11/26/14	1950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1960	025A	174.75	12/27/15	1618	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1961	025A	174.75	06/25/14	0620	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1962	025A	174.75	05/21/12	0100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1963	025A	174.75	05/21/12	0100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1964	025A	174.75	05/21/12	0101	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1965	025A	174.75	01/14/12	1240	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1966	025A	174.75	06/01/13	1525	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1967	025A	174.75	10/07/14	1550	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1968	025A	174.80	06/16/15	2105	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1969	025A	174.90	08/31/14	1140	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1970	025A	174.90	08/22/15	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1971	025A	174.90	09/19/15	1340	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1972	025A	174.90	11/20/12	2035	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1973	025A	174.90	04/30/11	0215	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
1974	025A	174.90	07/01/11	1325	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1975	025A	174.90	10/30/12	0305	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1976	025A	174.90	07/30/15	2215	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1977	025A	174.90	08/29/14	1510	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1978	025A	174.90	06/04/15	0245	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1979	025A	174.90	11/25/11	1735	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1980	025A	174.95	11/03/13	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1921	025A	174.22	12/03/15	1555	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	SLOWING
1922	025A	174.25	06/27/12	1000	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1923	025A	174.25	11/01/12	1830	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1924	025A	174.25	10/12/14	0110	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1925	025A	174.25	10/12/14	0110	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1926	025A	174.30	08/23/14	1735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1927	025A	174.30	10/23/15	1500	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1928	025A	174.30	05/05/15	1155	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1929	025A	174.35	05/23/11	2330	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1930	025A	174.35	06/22/14	0405	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1931	025A	174.44	06/23/15	1140	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1932	025A	174.47	05/30/11	0715	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
1933	025A	174.47	07/03/12	0010	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1934	025A	174.50	12/01/11	0520	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
1935	025A	174.50	10/09/12	1510	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1936	025A	174.50	08/14/15	2118	N		N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
1937	025A	174.50	08/14/15	2130	N		N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
1938	025A	174.50	08/31/12	2050	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
1939	025A	174.50	08/05/13	0800	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
1940	025A	174.50	01/30/15	1520	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
1941	025A	174.50	11/02/15	1945	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1942	025A	174.50	05/29/11	2110	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1943	025A	174.50	08/31/14	1858	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	078	OTHER
1944	025A	174.50	04/18/14	0000	N	CABLE RAIL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	ASLEEP AT WHEEL	070	WEAVING
1945	025A	174.50	09/19/15	1135	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1946	025A	174.51	03/06/14	2216	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	100	OTHER
1947	025A	174.51	01/22/15	0420	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1948	025A	174.60	09/26/15	1020	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
1949	025A	174.60	06/08/14	1615	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	040	GOING STRAIGHT
1950	025A	174.61	03/09/13	0526	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1951	025A	174.62	10/09/15	1541	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1952	025A	174.67	12/19/14	1130	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1953	025A	174.69	07/30/11	1500	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1954	025A	174.70	12/27/14	1130	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1955	025A	174.70	07/27/15	1653	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
1956	025A	174.70	10/21/12	1821	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	EVADING LAW ENFORCEMENT OFFICER	010	OTHER
1957	025A	174.75	03/11/11	1820	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
1958	025A	174.75	10/07/14	1657	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
1959	025A	174.75	11/26/14	1950	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
1960	025A	174.75	12/27/15	1618	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
1961	025A	174.75	06/25/14	0620	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
1962	025A	174.75	05/21/12	0100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1963	025A	174.75	05/21/12	0100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1964	025A	174.75	05/21/12	0101	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
1965	025A	174.75	01/14/12	1240	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
1966	025A	174.75	06/01/13	1525	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
1967	025A	174.75	10/07/14	1550	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
1968	025A	174.80	06/16/15	2105	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
1969	025A	174.90	08/31/14	1140	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	060	AVOIDING OBJECT/VEHICLE IN ROAD
1970	025A	174.90	08/22/15	1030	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
1971	025A	174.90	09/19/15	1340	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1972	025A	174.90	11/20/12	2035	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1973	025A	174.90	04/30/11	0215	N	CABLE RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
1974	025A	174.90	07/01/11	1325	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	065	OTHER
1975	025A	174.90	10/30/12	0305	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
1976	025A	174.90	07/30/15	2215	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1977	025A	174.90	08/29/14	1510	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1978	025A	174.90	06/04/15	0245	Y (L)	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
1979	025A	174.90	11/25/11	1735	Y (R)	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
1980	025A	174.95	11/03/13	1815	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
1981	025A	174.97	01/22/15	0901	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
1982	025A	174.99	07/20/15	0700	PDO	OFF RIGHT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1983	025A	175.00	06/17/12	1602	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1984	025A	175.00	03/20/13	0243	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1985	025A	175.00	06/08/14	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1986	025A	175.00	07/25/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1987	025A	175.00	05/27/14	1745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
1988	025A	175.00	06/17/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1989	025A	175.00	12/19/14	0415	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
1990	025A	175.00	01/07/15	0410	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	FOG
1991	025A	175.00	08/14/15	0910	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
1992	025A	175.01	04/03/12	0740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
1993	025A	175.03	06/28/14	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1994	025A	175.03	09/26/12	1450	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
1995	025A	175.10	07/30/11	1810	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1996	025A	175.10	07/30/11	1810	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1997	025A	175.10	04/20/14	1625	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
1998	025A	175.10	12/13/15	1949	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
1999	025A	175.10	05/12/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2000	025A	175.10	05/07/15	1145	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
2001	025A	175.10	07/03/15	1356	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2002	025A	175.15	09/27/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2003	025A	175.20	11/18/12	1955	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2004	025A	175.20	09/19/12	2100	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2005	025A	175.20	09/27/15	1315	PDO	ON	NON-INTERSECTION	6	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2006	025A	175.20	05/09/15	0335	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2007	025A	175.20	05/09/12	1623	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2008	025A	175.23	01/11/11	0805	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2009	025A	175.25	05/24/15	1245	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2010	025A	175.25	08/22/15	1205	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2011	025A	175.25	10/26/13	1917	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2012	025A	175.25	07/19/13	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2013	025A	175.25	08/05/13	0705	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2014	025A	175.25	11/12/12	1155	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2015	025A	175.25	02/02/13	0930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2016	025A	175.25	04/09/13	0700	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2017	025A	175.25	11/24/13	1144	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2018	025A	175.25	07/05/13	2150	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2019	025A	175.25	08/29/14	0940	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2020	025A	175.27	10/19/13	1435	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2021	025A	175.29	09/27/11	1020	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2022	025A	175.30	10/17/15	1240	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2023	025A	175.31	05/29/13	0740	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2024	025A	175.31	10/31/15	0800	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2025	025A	175.34	08/09/15	0103	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2026	025A	175.40	07/15/14	0100	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2027	025A	175.40	03/01/13	1840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2028	025A	175.45	08/10/14	2344	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2029	025A	175.45	12/17/15	0645	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
2030	025A	175.46	08/19/15	2105	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2031	025A	175.48	07/27/14	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2032	025A	175.50	05/14/14	1638	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2033	025A	175.50	01/08/11	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2034	025A	175.50	12/21/12	1830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2035	025A	175.50	12/21/12	1830	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2036	025A	175.50	01/11/13	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2037	025A	175.50	06/02/14	0610	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2038	025A	175.50	12/27/14	1228	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2039	025A	175.50	06/07/15	1110	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2040	025A	175.50	06/13/15	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
1981	025A	174.97	01/22/15	0901	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	077	OTHER
1982	025A	174.99	07/20/15	0700	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
1983	025A	175.00	06/17/12	1602	N	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
1984	025A	175.00	03/20/13	0243	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
1985	025A	175.00	06/08/14	1530	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
1986	025A	175.00	07/25/15	1220	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1987	025A	175.00	05/27/14	1745	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
1988	025A	175.00	06/17/14	1200	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	075	WEAVING
1989	025A	175.00	12/19/14	0415	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
1990	025A	175.00	01/07/15	0410	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
1991	025A	175.00	08/14/15	0910	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
1992	025A	175.01	04/03/12	0740	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
1993	025A	175.03	06/28/14	1645	N	REAR-END	N	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
1994	025A	175.03	09/26/12	1450	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
1995	025A	175.10	07/30/11	1810	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
1996	025A	175.10	07/30/11	1810	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
1997	025A	175.10	04/20/14	1625	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
1998	025A	175.10	12/13/15	1949	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
1999	025A	175.10	05/12/15	1130	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2000	025A	175.10	05/07/15	1145	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2001	025A	175.10	07/03/15	1356	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
2002	025A	175.15	09/27/15	1315	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2003	025A	175.20	11/18/12	1955	N	VEHICLE CARGO/DEBRIS	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	GOING STRAIGHT
2004	025A	175.20	09/19/12	2100	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2005	025A	175.20	09/27/15	1315	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	WEAVING
2006	025A	175.20	05/09/15	0335	N	CABLE RAIL	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	NONE APPARENT	075	GOING STRAIGHT
2007	025A	175.20	05/09/12	1623	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2008	025A	175.23	01/11/11	0805	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
2009	025A	175.25	05/24/15	1245	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	STOPPED IN TRAFFIC
2010	025A	175.25	08/22/15	1205	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2011	025A	175.25	10/26/13	1917	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
2012	025A	175.25	07/19/13	2100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2013	025A	175.25	08/05/13	0705	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2014	025A	175.25	11/12/12	1155	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2015	025A	175.25	02/02/13	0930	N	CABLE RAIL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2016	025A	175.25	04/09/13	0700	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2017	025A	175.25	11/24/13	1144	N	OVERTURNING	N	SUV	NO IMPAIRMENT	EVADING LAW ENFORCEMENT OFFICER	060	OTHER
2018	025A	175.25	07/05/13	2150	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	MAKING LEFT TURN
2019	025A	175.25	08/29/14	0940	N	PEDESTRIAN	S	SUV	NO IMPAIRMENT	NONE APPARENT	002	STOPPED IN TRAFFIC
2020	025A	175.27	10/19/13	1435	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2021	025A	175.29	09/27/11	1020	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	ALCOHOL/DRUGS	ASLEEP AT WHEEL	075	WEAVING
2022	025A	175.30	10/17/15	1240	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
2023	025A	175.31	05/29/13	0740	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	MAKING U-TURN
2024	025A	175.31	10/31/15	0800	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	GOING STRAIGHT
2025	025A	175.34	08/09/15	0103	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2026	025A	175.40	07/15/14	0100	N	OVERTURNING	N	SUV	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2027	025A	175.40	03/01/13	1840	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	072	GOING STRAIGHT
2028	025A	175.45	08/10/14	2344	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
2029	025A	175.45	12/17/15	0645	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
2030	025A	175.46	08/19/15	2105	N	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2031	025A	175.48	07/27/14	1300	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2032	025A	175.50	05/14/14	1638	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	AVOIDING OBJECT/VEHICLE IN ROAD
2033	025A	175.50	01/08/11	0815	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2034	025A	175.50	12/21/12	1830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2035	025A	175.50	12/21/12	1830	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2036	025A	175.50	01/11/13	0700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2037	025A	175.50	06/02/14	0610	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
2038	025A	175.50	12/27/14	1228	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2039	025A	175.50	06/07/15	1110	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2040	025A	175.50	06/13/15	1545	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2041	025A	175.50	04/22/12	1210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2042	025A	175.50	12/19/14	1048	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2043	025A	175.50	09/25/15	1705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2044	025A	175.50	11/23/13	1957	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2045	025A	175.50	11/16/12	1615	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2046	025A	175.50	02/25/13	0335	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2047	025A	175.50	05/07/13	1440	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2048	025A	175.50	10/19/14	1500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2049	025A	175.50	12/27/14	1325	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2050	025A	175.51	01/31/15	1951	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2051	025A	175.53	11/03/11	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2052	025A	175.56	10/09/15	1500	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2053	025A	175.58	12/15/15	0025	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2054	025A	175.60	07/17/11	0133	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2055	025A	175.60	03/14/14	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2056	025A	175.60	08/08/15	1000	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2057	025A	175.67	08/11/14	1135	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2058	025A	175.68	07/12/13	0950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2059	025A	175.73	10/17/15	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2060	025A	175.75	05/24/15	1317	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2061	025A	175.75	12/26/12	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2062	025A	175.75	06/07/13	2150	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2063	025A	175.75	11/05/14	2050	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2064	025A	175.75	02/02/11	1030	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2065	025A	175.75	09/23/12	0345	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2066	025A	175.75	06/29/14	1345	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2067	025A	175.75	11/06/14	0645	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2068	025A	175.75	05/23/15	1359	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2069	025A	175.77	07/18/15	1842	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2070	025A	175.78	07/21/11	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2071	025A	175.79	02/23/12	0640	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2072	025A	175.79	06/28/15	1210	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2073	025A	175.80	07/18/15	1145	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2074	025A	175.80	08/30/15	1650	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2075	025A	175.80	10/04/15	1230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2076	025A	175.80	04/09/11	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2077	025A	175.80	05/17/13	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2078	025A	175.80	05/27/11	0222	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2079	025A	175.80	05/19/12	1430	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2080	025A	175.80	12/19/14	0518	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2081	025A	175.80	07/24/11	0910	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2082	025A	175.84	08/20/11	0500	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2083	025A	175.85	02/25/13	1050	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2084	025A	175.85	09/25/15	1550	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2085	025A	175.86	01/06/13	1715	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2086	025A	175.86	06/11/13	1635	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2087	025A	175.86	06/20/12	1504	PDO	OFF LEFT	INTERSECTION RELATED	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2088	025A	175.88	06/06/15	1545	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2089	025A	175.89	08/07/12	1350	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2090	025A	175.89	08/26/12	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2091	025A	175.90	03/20/15	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2092	025A	175.90	08/09/15	1251	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2093	025A	175.90	08/16/15	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2094	025A	175.90	07/21/13	1840	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2095	025A	175.93	06/20/15	1125	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2096	025A	175.95	10/29/15	1311	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2097	025A	175.96	05/30/15	1540	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2098	025A	175.96	05/07/14	0320	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2099	025A	175.96	07/01/11	0140	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DARK-UNLIGHTED	NONE
2100	025A	175.98	06/17/15	1459	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2041	025A	175.50	04/22/12	1210	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2042	025A	175.50	12/19/14	1048	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2043	025A	175.50	09/25/15	1705	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2044	025A	175.50	11/23/13	1957	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	078	GOING STRAIGHT
2045	025A	175.50	11/16/12	1615	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2046	025A	175.50	02/25/13	0335	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
2047	025A	175.50	05/07/13	1440	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2048	025A	175.50	10/19/14	1500	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	075	GOING STRAIGHT
2049	025A	175.50	12/27/14	1325	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2050	025A	175.51	01/31/15	1951	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2051	025A	175.53	11/03/11	1550	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
2052	025A	175.56	10/09/15	1500	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2053	025A	175.58	12/15/15	0025	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2054	025A	175.60	07/17/11	0133	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
2055	025A	175.60	03/14/14	1100	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	085	GOING STRAIGHT
2056	025A	175.60	08/08/15	1000	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2057	025A	175.67	08/11/14	1135	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2058	025A	175.68	07/12/13	0950	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
2059	025A	175.73	10/17/15	1130	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
2060	025A	175.75	05/24/15	1317	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
2061	025A	175.75	12/26/12	1600	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
2062	025A	175.75	06/07/13	2150	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2063	025A	175.75	11/05/14	2050	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2064	025A	175.75	02/02/11	1030	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2065	025A	175.75	09/23/12	0345	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
2066	025A	175.75	06/29/14	1345	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2067	025A	175.75	11/06/14	0645	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
2068	025A	175.75	05/23/15	1359	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2069	025A	175.77	07/18/15	1842	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
2070	025A	175.78	07/21/11	1545	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2071	025A	175.79	02/23/12	0640	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
2072	025A	175.79	06/28/15	1210	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2073	025A	175.80	07/18/15	1145	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2074	025A	175.80	08/30/15	1650	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	SLOWING
2075	025A	175.80	10/04/15	1230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
2076	025A	175.80	04/09/11	1400	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	WEAVING
2077	025A	175.80	05/17/13	1400	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2078	025A	175.80	05/27/11	0222	N	CABLE RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
2079	025A	175.80	05/19/12	1430	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2080	025A	175.80	12/19/14	0518	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2081	025A	175.80	07/24/11	0910	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
2082	025A	175.84	08/20/11	0500	Y (R)	LARGE BOULDERS OR ROCKS	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	GOING STRAIGHT
2083	025A	175.85	02/25/13	1050	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
2084	025A	175.85	09/25/15	1550	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2085	025A	175.86	01/06/13	1715	Y (L)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2086	025A	175.86	06/11/13	1635	Y (L)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
2087	025A	175.86	06/20/12	1504	Y (L)	FENCE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2088	025A	175.88	06/06/15	1545	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2089	025A	175.89	08/07/12	1350	N	OVERTURNING	S	MOTOR HOME	NO IMPAIRMENT	NONE APPARENT	063	GOING STRAIGHT
2090	025A	175.89	08/26/12	1200	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
2091	025A	175.90	03/20/15	1800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2092	025A	175.90	08/09/15	1251	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
2093	025A	175.90	08/16/15	1100	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
2094	025A	175.90	07/21/13	1840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
2095	025A	175.93	06/20/15	1125	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2096	025A	175.95	10/29/15	1311	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2097	025A	175.96	05/30/15	1540	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	052	GOING STRAIGHT
2098	025A	175.96	05/07/14	0320	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2099	025A	175.96	07/01/11	0140	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT
2100	025A	175.98	06/17/15	1459	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2101	025A	176.00	11/15/15	2050	FAT	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2102	025A	176.00	06/27/11	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2103	025A	176.00	07/26/13	1522	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2104	025A	176.00	08/31/14	0925	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2105	025A	176.00	05/27/13	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2106	025A	176.00	02/28/14	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2107	025A	176.00	10/18/14	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2108	025A	176.00	12/03/14	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2109	025A	176.00	03/18/15	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2110	025A	176.00	08/10/15	1354	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2111	025A	176.00	11/07/12	0655	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2112	025A	176.00	03/29/14	1535	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2113	025A	176.00	05/12/13	1625	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2114	025A	176.03	07/26/13	1522	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2115	025A	176.04	07/02/14	0905	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2116	025A	176.06	07/16/14	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2117	025A	176.07	08/29/14	2157	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2118	025A	176.08	08/14/14	0050	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2119	025A	176.10	08/11/13	1818	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2120	025A	176.10	09/26/13	0200	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2121	025A	176.10	05/07/15	0615	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	FOG
2122	025A	176.10	09/06/15	1657	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2123	025A	176.20	12/28/12	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2124	025A	176.20	04/26/15	0130	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2125	025A	176.21	02/25/12	0922	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2126	025A	176.25	12/15/11	1730	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2127	025A	176.25	06/16/11	1650	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2128	025A	176.25	07/19/13	0430	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2129	025A	176.29	01/06/15	1055	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2130	025A	176.30	10/24/15	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2131	025A	176.30	09/06/13	2215	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2132	025A	176.35	08/13/11	2345	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2133	025A	176.37	06/09/13	2234	FAT	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2134	025A	176.40	09/27/14	2140	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2135	025A	176.40	09/28/14	2016	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2136	025A	176.40	08/21/15	0517	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2137	025A	176.40	08/22/15	0114	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2138	025A	176.46	08/03/14	1358	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2139	025A	176.47	06/06/15	0951	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2140	025A	176.50	02/10/15	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2141	025A	176.50	10/17/14	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2142	025A	176.50	08/30/11	2250	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2143	025A	176.50	11/24/14	1645	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2144	025A	176.50	08/03/15	0140	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2145	025A	176.50	08/24/13	1420	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2146	025A	176.50	05/11/14	0530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2147	025A	176.50	03/04/15	1735	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2148	025A	176.50	08/07/12	1430	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2149	025A	176.60	07/03/11	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2150	025A	176.60	10/24/15	1420	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2151	025A	176.60	01/10/11	1630	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	NONE
2152	025A	176.60	07/07/12	1055	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2153	025A	176.60	04/13/15	0850	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2154	025A	176.70	03/11/14	1000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2155	025A	176.70	01/14/15	1820	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2156	025A	176.70	06/09/13	1000	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2157	025A	176.75	05/13/11	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2158	025A	176.75	02/20/15	1800	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2159	025A	176.75	05/23/11	0450	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2160	025A	176.75	12/22/14	1530	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2101	025A	176.00	11/15/15	2050	N	PEDESTRIAN	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
2102	025A	176.00	06/27/11	1630	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
2103	025A	176.00	07/26/13	1522	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2104	025A	176.00	08/31/14	0925	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2105	025A	176.00	05/27/13	1540	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2106	025A	176.00	02/28/14	0730	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2107	025A	176.00	10/18/14	1800	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2108	025A	176.00	12/03/14	1550	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2109	025A	176.00	03/18/15	1450	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	077	CHANGING LANES
2110	025A	176.00	08/10/15	1354	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	072	CHANGING LANES
2111	025A	176.00	11/07/12	0655	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
2112	025A	176.00	03/29/14	1535	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2113	025A	176.00	05/12/13	1625	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2114	025A	176.03	07/26/13	1522	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2115	025A	176.04	07/02/14	0905	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2116	025A	176.06	07/16/14	1515	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
2117	025A	176.07	08/29/14	2157	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2118	025A	176.08	08/14/14	0050	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	080	OTHER
2119	025A	176.10	08/11/13	1818	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2120	025A	176.10	09/26/13	0200	N	CABLE RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
2121	025A	176.10	05/07/15	0615	N	CABLE RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2122	025A	176.10	09/06/15	1657	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
2123	025A	176.20	12/28/12	1100	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
2124	025A	176.20	04/26/15	0130	N	CABLE RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2125	025A	176.21	02/25/12	0922	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	085	OTHER
2126	025A	176.25	12/15/11	1730	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2127	025A	176.25	06/16/11	1650	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2128	025A	176.25	07/19/13	0430	Y (L)	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2129	025A	176.29	01/06/15	1055	N	CABLE RAIL	N	PASS CAR/VAN	RX/MEDICATION/DR	ASLEEP AT WHEEL	075	OTHER
2130	025A	176.30	10/24/15	1445	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
2131	025A	176.30	09/06/13	2215	Y (L)	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2132	025A	176.35	08/13/11	2345	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2133	025A	176.37	06/09/13	2234	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2134	025A	176.40	09/27/14	2140	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
2135	025A	176.40	09/28/14	2016	N	CABLE RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	085	OTHER
2136	025A	176.40	08/21/15	0517	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2137	025A	176.40	08/22/15	0114	Y (L)	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2138	025A	176.46	08/03/14	1358	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2139	025A	176.47	06/06/15	0951	N	HEAD-ON	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2140	025A	176.50	02/10/15	0800	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2141	025A	176.50	10/17/14	0745	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2142	025A	176.50	08/30/11	2250	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2143	025A	176.50	11/24/14	1645	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2144	025A	176.50	08/03/15	0140	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2145	025A	176.50	08/24/13	1420	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2146	025A	176.50	05/11/14	0530	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
2147	025A	176.50	03/04/15	1735	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	PASSING
2148	025A	176.50	08/07/12	1430	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2149	025A	176.60	07/03/11	1700	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
2150	025A	176.60	10/24/15	1420	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2151	025A	176.60	01/10/11	1630	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
2152	025A	176.60	07/07/12	1055	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2153	025A	176.60	04/13/15	0850	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
2154	025A	176.70	03/11/14	1000	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2155	025A	176.70	01/14/15	1820	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2156	025A	176.70	06/09/13	1000	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2157	025A	176.75	05/13/11	1425	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
2158	025A	176.75	02/20/15	1800	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2159	025A	176.75	05/23/11	0450	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2160	025A	176.75	12/22/14	1530	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2161	025A	176.79	06/15/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2162	025A	176.80	10/06/15	1245	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2163	025A	176.80	12/05/11	1020	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2164	025A	176.80	05/18/14	1008	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2165	025A	176.81	07/25/15	1847	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2166	025A	176.85	08/22/15	0411	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2167	025A	176.87	08/22/15	0411	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2168	025A	176.89	11/07/15	1230	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2169	025A	176.90	08/02/15	1519	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2170	025A	177.00	07/17/15	1841	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2171	025A	177.00	12/19/15	1405	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2172	025A	177.00	09/25/11	1924	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2173	025A	177.00	05/22/15	1635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2174	025A	177.00	09/19/11	1920	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2175	025A	177.00	06/22/15	0505	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2176	025A	177.00	12/19/12	0650	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2177	025A	177.00	01/31/15	0340	PDO	OFF LEFT	AT DRIVEWAY ACCESS	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2178	025A	177.01	05/25/14	1355	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2179	025A	177.03	01/02/15	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2180	025A	177.03	09/13/14	1540	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2181	025A	177.09	12/15/11	0100	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2182	025A	177.10	12/26/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2183	025A	177.10	06/20/14	0530	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2184	025A	177.10	11/21/14	1720	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2185	025A	177.17	01/15/15	0150	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2186	025A	177.20	07/05/12	1345	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2187	025A	177.20	12/11/15	0722	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2188	025A	177.20	09/19/15	2040	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2189	025A	177.25	11/15/13	0815	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2190	025A	177.25	05/26/12	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2191	025A	177.25	02/07/11	0119	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2192	025A	177.25	10/10/15	2305	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2193	025A	177.30	08/14/11	2105	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2194	025A	177.30	03/18/12	0040	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2195	025A	177.30	01/30/14	1123	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2196	025A	177.30	06/17/15	1300	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2197	025A	177.40	06/15/14	0130	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2198	025A	177.40	02/01/14	1632	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2199	025A	177.42	12/10/15	1745	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2200	025A	177.46	06/04/15	1235	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2201	025A	177.50	10/25/12	0939	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2202	025A	177.50	04/10/11	1410	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2203	025A	177.50	10/24/15	1447	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2204	025A	177.50	05/03/13	0815	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2205	025A	177.50	07/26/11	2330	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2206	025A	177.50	07/05/15	1600	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2207	025A	177.50	04/20/15	1800	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2208	025A	177.50	03/19/11	0117	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2209	025A	177.50	05/22/12	1900	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2210	025A	177.50	02/13/15	2215	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2211	025A	177.60	04/03/12	0700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2212	025A	177.61	07/07/15	0245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
2213	025A	177.72	07/06/12	1548	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2214	025A	177.75	10/01/11	2048	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2215	025A	177.75	12/30/12	1235	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2216	025A	177.75	03/28/15	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2217	025A	177.75	09/15/14	0015	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2218	025A	177.75	05/19/11	1430	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2219	025A	177.80	01/03/15	1900	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY W/WIS ICY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2220	025A	177.80	10/20/15	1558	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2161	025A	176.79	06/15/14	1200	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2162	025A	176.80	10/06/15	1245	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2163	025A	176.80	12/05/11	1020	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2164	025A	176.80	05/18/14	1008	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
2165	025A	176.81	07/25/15	1847	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
2166	025A	176.85	08/22/15	0411	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2167	025A	176.87	08/22/15	0411	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2168	025A	176.89	11/07/15	1230	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
2169	025A	176.90	08/02/15	1519	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	SLOWING
2170	025A	177.00	07/17/15	1841	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
2171	025A	177.00	01/31/15	1405	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
2172	025A	177.00	09/25/11	1924	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	085	PASSING
2173	025A	177.00	05/22/15	1635	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	WEAVING
2174	025A	177.00	09/19/11	1920	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2175	025A	177.00	06/22/15	0505	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2176	025A	177.00	12/19/12	0650	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2177	025A	177.00	01/31/15	0340	Y (L)	OTHER FIXED OBJECT	S	SUV	ALCOHOL/DRUGS	UNKNOWN	055	GOING STRAIGHT
2178	025A	177.01	05/25/14	1355	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
2179	025A	177.03	01/02/15	1600	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2180	025A	177.03	09/13/14	1540	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2181	025A	177.09	12/15/11	0100	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	075	OTHER
2182	025A	177.10	12/26/15	1130	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
2183	025A	177.10	06/20/14	0530	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
2184	025A	177.10	11/21/14	1720	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2185	025A	177.17	01/15/15	0150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	090	GOING STRAIGHT
2186	025A	177.20	07/05/12	1345	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	ENTERING/LEAVING PARKED POSITION
2187	025A	177.20	12/11/15	0722	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2188	025A	177.20	09/19/15	2040	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2189	025A	177.25	11/15/13	0815	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2190	025A	177.25	05/26/12	1150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2191	025A	177.25	02/07/11	0119	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
2192	025A	177.25	10/10/15	2305	Y (L)	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
2193	025A	177.30	08/14/11	2105	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2194	025A	177.30	03/18/12	0040	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2195	025A	177.30	01/30/14	1123	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2196	025A	177.30	06/17/15	1300	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2197	025A	177.40	06/15/14	0130	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	080	AVOIDING OBJECT/VEHICLE IN ROAD
2198	025A	177.40	02/01/14	1632	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	038	OTHER
2199	025A	177.42	12/10/15	1745	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
2200	025A	177.46	06/04/15	1235	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2201	025A	177.50	10/25/12	0939	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2202	025A	177.50	04/10/11	1410	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2203	025A	177.50	10/24/15	1447	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	077	SLOWING
2204	025A	177.50	05/03/13	0815	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	072	CHANGING LANES
2205	025A	177.50	07/26/11	2330	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2206	025A	177.50	07/05/15	1600	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2207	025A	177.50	04/20/15	1800	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2208	025A	177.50	03/19/11	0117	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	WEAVING
2209	025A	177.50	05/22/12	1900	N	CABLE RAIL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	ALCOHOL	DRIVER PREOCCUPIED	062	GOING STRAIGHT
2210	025A	177.50	02/13/15	2215	Y (L)	PEDESTRIAN	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	UK	WEAVING
2211	025A	177.60	04/03/12	0700	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2212	025A	177.61	07/07/15	0245	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
2213	025A	177.72	07/06/12	1548	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2214	025A	177.75	10/01/11	2048	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2215	025A	177.75	12/30/12	1235	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
2216	025A	177.75	03/28/15	1520	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2217	025A	177.75	09/15/14	0015	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2218	025A	177.75	05/19/11	1430	Y (L)	DELINEATOR POST	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	055	WEAVING
2219	025A	177.80	01/03/15	1900	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
2220	025A	177.80	10/20/15	1558	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2221	025A	177.80	10/20/15	1558	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2222	025A	177.83	12/13/15	0245	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
2223	025A	177.86	06/06/13	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2224	025A	177.90	06/04/11	1604	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2225	025A	177.90	09/30/15	1503	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2226	025A	178.00	08/07/12	1726	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2227	025A	178.00	12/30/12	1225	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2228	025A	178.00	12/05/13	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2229	025A	178.00	03/09/14	2145	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2230	025A	178.00	10/25/12	0115	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-UNLIGHTED	NONE
2231	025A	178.00	12/22/14	0305	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2232	025A	178.00	05/11/14	1300	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2233	025A	178.00	01/14/13	1828	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2234	025A	178.01	09/02/11	1405	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2235	025A	178.10	08/16/11	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2236	025A	178.17	08/01/14	1040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2237	025A	178.24	09/19/15	1440	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2238	025A	178.25	11/05/14	1915	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2239	025A	178.25	04/03/12	0706	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2240	025A	178.25	12/28/12	1725	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2241	025A	178.25	01/16/14	1826	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2242	025A	178.30	04/24/11	1750	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2243	025A	178.32	09/07/14	1024	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2244	025A	178.39	01/29/11	0830	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2245	025A	178.39	11/16/14	1904	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2246	025A	178.40	01/01/15	1635	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
2247	025A	178.40	06/02/13	1810	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2248	025A	178.41	05/11/13	1617	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2249	025A	178.45	10/07/11	0045	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2250	025A	178.46	12/28/12	1530	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2251	025A	178.48	01/16/12	2015	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2252	025A	178.50	11/15/12	1905	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2253	025A	178.50	09/26/14	1444	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2254	025A	178.50	08/10/15	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2255	025A	178.50	09/08/14	1755	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2256	025A	178.50	04/14/13	0235	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2257	025A	178.50	05/04/14	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2258	025A	178.57	10/05/13	2307	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2259	025A	178.60	07/06/15	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2260	025A	178.60	01/16/12	2105	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2261	025A	178.60	10/26/12	0200	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2262	025A	178.60	11/22/12	1330	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2263	025A	178.60	06/03/11	0059	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2264	025A	178.65	07/11/14	1709	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2265	025A	178.70	09/24/14	0115	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2266	025A	178.70	09/24/14	0115	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2267	025A	178.70	10/21/15	0632	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
2268	025A	178.75	11/15/12	1910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2269	025A	178.75	06/13/11	2045	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2270	025A	178.75	04/25/11	2159	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2271	025A	178.75	01/19/13	0805	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2272	025A	178.75	05/28/15	2024	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
2273	025A	178.80	09/12/12	0755	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2274	025A	178.80	10/02/14	0120	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	NONE
2275	025A	178.80	05/18/15	0550	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
2276	025A	178.80	05/22/15	0550	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2277	025A	178.80	06/21/15	2220	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2278	025A	178.80	09/17/15	0624	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2279	025A	178.80	10/22/15	0910	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2280	025A	178.80	05/22/15	1715	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2221	025A	177.80	10/20/15	1558	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2222	025A	177.83	12/13/15	0245	N	CABLE RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
2223	025A	177.86	06/06/13	1700	Y (L)	SIDESWIPE OPPOSITE DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2224	025A	177.90	06/04/11	1604	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
2225	025A	177.90	09/30/15	1503	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
2226	025A	178.00	08/07/12	1726	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
2227	025A	178.00	12/30/12	1225	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2228	025A	178.00	12/05/13	1800	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
2229	025A	178.00	03/09/14	2145	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	WEAVING
2230	025A	178.00	10/25/12	0115	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2231	025A	178.00	12/22/14	0305	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2232	025A	178.00	05/11/14	1300	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
2233	025A	178.00	01/14/13	1828	Y (L)	SIDESWIPE OPPOSITE DIRECTION	N	SUV	ALCOHOL/DRUGS	UNKNOWN	060	WEAVING
2234	025A	178.01	09/02/11	1405	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
2235	025A	178.10	08/16/11	1640	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2236	025A	178.17	08/01/14	1040	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2237	025A	178.24	09/19/15	1440	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2238	025A	178.25	11/05/14	1915	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2239	025A	178.25	04/03/12	0706	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
2240	025A	178.25	12/28/12	1725	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2241	025A	178.25	01/16/14	1826	Y (L)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	070	GOING STRAIGHT
2242	025A	178.30	04/24/11	1750	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	GOING STRAIGHT
2243	025A	178.32	09/07/14	1024	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	CHANGING LANES
2244	025A	178.39	01/29/11	0830	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2245	025A	178.39	11/16/14	1904	Y (L)	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
2246	025A	178.40	01/01/15	1635	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2247	025A	178.40	06/02/13	1810	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2248	025A	178.41	05/11/13	1617	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2249	025A	178.45	10/07/11	0045	N	CABLE RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
2250	025A	178.46	12/28/12	1530	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2251	025A	178.48	01/16/12	2015	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2252	025A	178.50	11/15/12	1905	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2253	025A	178.50	09/26/14	1444	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
2254	025A	178.50	08/10/15	1715	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
2255	025A	178.50	09/08/14	1755	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
2256	025A	178.50	04/14/13	0235	N	CABLE RAIL	S	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2257	025A	178.50	05/04/14	1500	Y (R)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	ENTERING/LEAVING PARKED POSITION
2258	025A	178.57	10/05/13	2307	Y (L)	CABLE RAIL	S	SUV	ALCOHOL	UNKNOWN	055	WEAVING
2259	025A	178.60	07/06/15	1630	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2260	025A	178.60	01/16/12	2105	N	CABLE RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	WEAVING
2261	025A	178.60	10/26/12	0200	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	WEAVING
2262	025A	178.60	11/22/12	1330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
2263	025A	178.60	06/03/11	0059	Y (R)	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
2264	025A	178.65	07/11/14	1709	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
2265	025A	178.70	09/24/14	0115	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2266	025A	178.70	09/24/14	0115	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2267	025A	178.70	10/21/15	0632	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2268	025A	178.75	11/15/12	1910	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
2269	025A	178.75	06/13/11	2045	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2270	025A	178.75	04/25/11	2159	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2271	025A	178.75	01/19/13	0805	N	CABLE RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2272	025A	178.75	05/28/15	2024	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2273	025A	178.80	09/12/12	0755	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2274	025A	178.80	10/02/14	0120	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2275	025A	178.80	05/18/15	0550	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2276	025A	178.80	05/22/15	0550	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2277	025A	178.80	06/21/15	2220	N	CABLE RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
2278	025A	178.80	09/17/15	0624	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	GOING STRAIGHT
2279	025A	178.80	10/22/15	0910	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2280	025A	178.80	05/22/15	1715	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	UK	MAKING LEFT TURN

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2281	025A	178.90	07/21/12	1150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2282	025A	178.92	05/13/11	1730	INJ	ON	INTERSECTION RELATED	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2283	025A	178.93	01/29/11	0500	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2284	025A	178.93	01/23/14	0713	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2285	025A	178.93	07/09/12	1245	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2286	025A	178.93	09/27/14	1615	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2287	025A	178.93	05/31/11	1726	PDO	ON	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
2288	025A	178.96	09/13/13	0700	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2289	025A	179.00	01/11/14	1120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2290	025A	179.00	08/03/14	2112	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2291	025A	179.00	10/12/14	0010	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2292	025A	179.00	05/14/13	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2293	025A	179.00	06/15/14	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2294	025A	179.00	02/12/12	1505	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2295	025A	179.00	01/29/14	1105	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2296	025A	179.00	03/30/14	0015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2297	025A	179.02	12/07/13	0725	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2298	025A	179.05	02/07/15	2147	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2299	025A	179.05	02/01/11	0850	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2300	025A	179.10	08/15/14	1638	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2301	025A	179.10	12/19/14	1619	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2302	025A	179.10	01/07/12	2150	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
2303	025A	179.10	03/20/15	0304	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2304	025A	179.15	11/09/13	2358	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2305	025A	179.20	04/17/15	0604	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2306	025A	179.20	01/07/12	1935	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2307	025A	179.20	08/20/15	0943	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2308	025A	179.23	09/26/15	2327	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2309	025A	179.25	06/19/15	0508	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
2310	025A	179.26	11/26/14	2030	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2311	025A	179.30	02/20/12	0345	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2312	025A	179.40	08/04/13	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2313	025A	179.40	08/05/14	1900	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2314	025A	179.43	01/16/11	1024	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2315	025A	179.48	05/15/11	1855	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2316	025A	179.50	09/12/13	0800	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
2317	025A	179.50	10/21/15	0635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
2318	025A	179.50	11/24/11	0356	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2319	025A	179.50	05/04/14	1557	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2320	025A	179.50	04/15/12	1730	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2321	025A	179.50	08/07/12	2140	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2322	025A	179.50	12/29/14	0655	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
2323	025A	179.50	05/01/15	1430	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2324	025A	179.50	04/03/12	0905	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2325	025A	179.57	12/01/15	1745	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2326	025A	179.58	07/14/12	1436	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2327	025A	179.60	02/27/15	1005	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2328	025A	179.60	12/03/15	0655	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
2329	025A	179.60	10/04/13	0855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2330	025A	179.60	02/20/15	1510	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2331	025A	179.60	01/24/13	1335	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2332	025A	179.64	12/29/14	0719	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2333	025A	179.64	05/11/12	1523	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
2334	025A	179.65	09/12/13	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2335	025A	179.67	04/21/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2336	025A	179.69	11/01/13	1215	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2337	025A	179.71	08/23/15	0106	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2338	025A	179.75	05/26/11	2045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2339	025A	179.75	03/15/11	0455	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2340	025A	179.75	12/29/14	0717	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2281	025A	178.90	07/21/12	1150	Y (L)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	055	WEAVING
2282	025A	178.92	05/13/11	1730	Y (N)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2283	025A	178.93	01/29/11	0500	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	070	GOING STRAIGHT
2284	025A	178.93	01/23/14	0713	Y (L)	BROADSIDE	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	MAKING LEFT TURN
2285	025A	178.93	07/09/12	1245	Y (L)	OVERTAKING TURN	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2286	025A	178.93	09/27/14	1615	Y (M)	OVERTAKING TURN	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING U-TURN
2287	025A	178.93	05/31/11	1726	Y (P)	INVOLVING OTHER OBJECT	W	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	005	MAKING RIGHT TURN
2288	025A	178.96	09/13/13	0700	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2289	025A	179.00	01/11/14	1120	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2290	025A	179.00	08/03/14	2112	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2291	025A	179.00	10/12/14	0010	N	REAR-END	S	SUV	ALCOHOL	UNKNOWN	085	GOING STRAIGHT
2292	025A	179.00	05/14/13	1900	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2293	025A	179.00	06/15/14	1900	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
2294	025A	179.00	02/12/12	1505	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	050	GOING STRAIGHT
2295	025A	179.00	01/29/14	1105	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2296	025A	179.00	03/30/14	0015	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2297	025A	179.02	12/07/13	0725	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2298	025A	179.05	02/07/15	2147	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	010	GOING STRAIGHT
2299	025A	179.05	02/01/11	0850	N	CABLE RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
2300	025A	179.10	08/15/14	1638	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	025	GOING STRAIGHT
2301	025A	179.10	12/19/14	1619	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2302	025A	179.10	01/07/12	2150	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2303	025A	179.10	03/20/15	0304	N	CABLE RAIL	N	SUV W/TRAILER	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
2304	025A	179.15	11/09/13	2358	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2305	025A	179.20	04/17/15	0604	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	040	OTHER
2306	025A	179.20	01/07/12	1935	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2307	025A	179.20	08/20/15	0943	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
2308	025A	179.23	09/26/15	2327	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	060	GOING STRAIGHT
2309	025A	179.25	06/19/15	0508	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2310	025A	179.26	11/26/14	2030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
2311	025A	179.30	02/20/12	0345	N	CABLE RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2312	025A	179.40	08/04/13	1125	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2313	025A	179.40	08/05/14	1900	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL/DRUGS	ASLEEP AT WHEEL	065	GOING STRAIGHT
2314	025A	179.43	01/16/11	1024	N	GUARD RAIL	N	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	WEAVING
2315	025A	179.48	05/15/11	1855	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2316	025A	179.50	09/12/13	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2317	025A	179.50	10/21/15	0635	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	005	GOING STRAIGHT
2318	025A	179.50	11/24/11	0356	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2319	025A	179.50	05/04/14	1557	N	WILD ANIMAL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2320	025A	179.50	04/15/12	1730	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
2321	025A	179.50	08/07/12	2140	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
2322	025A	179.50	12/29/14	0655	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
2323	025A	179.50	05/01/15	1430	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2324	025A	179.50	04/03/12	0905	Y (L)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
2325	025A	179.57	12/01/15	1745	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
2326	025A	179.58	07/14/12	1436	Y (L)	SIDESWIPE OPPOSITE DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	WRONG WAY
2327	025A	179.60	02/27/15	1005	N	OTHER NON-COLLISION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	CHANGING LANES
2328	025A	179.60	12/03/15	0655	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2329	025A	179.60	10/04/13	0855	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2330	025A	179.60	02/20/15	1510	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2331	025A	179.60	01/24/13	1335	N	BARRICADE/TRAFFIC BARRIER	N	SUV	ALCOHOL	UNKNOWN	065	OTHER
2332	025A	179.64	12/29/14	0719	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
2333	025A	179.64	05/11/12	1523	N	CABLE RAIL	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	WEAVING
2334	025A	179.65	09/12/13	0735	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	065	GOING STRAIGHT
2335	025A	179.67	04/21/15	1130	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
2336	025A	179.69	11/01/13	1215	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	WEAVING
2337	025A	179.71	08/23/15	0106	N	CABLE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2338	025A	179.75	05/26/11	2045	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
2339	025A	179.75	03/15/11	0455	N	PARKED MOTOR VEHICLE	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2340	025A	179.75	12/29/14	0717	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2341	025A	179.78	11/10/15	0655	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2342	025A	179.78	06/25/15	1635	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2343	025A	179.80	07/12/13	0915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2344	025A	179.80	10/25/15	1445	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2345	025A	179.80	05/15/11	1745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2346	025A	179.80	01/08/12	0115	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2347	025A	179.90	05/31/14	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2348	025A	179.90	09/14/14	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2349	025A	179.94	08/21/13	0620	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2350	025A	179.97	07/02/15	1604	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2351	025A	180.00	08/29/14	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2352	025A	180.00	02/03/12	1025	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2353	025A	180.00	05/18/12	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2354	025A	180.00	07/26/14	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2355	025A	180.00	08/03/14	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2356	025A	180.00	08/01/15	1415	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2357	025A	180.00	06/04/11	0940	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2358	025A	180.00	06/26/15	1511	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2359	025A	180.00	12/10/15	1830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2360	025A	180.00	11/29/11	1756	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2361	025A	180.00	04/29/12	1945	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2362	025A	180.00	12/31/11	0415	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2363	025A	180.00	12/15/15	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2364	025A	180.01	03/28/14	0600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2365	025A	180.10	09/29/14	2330	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2366	025A	180.10	10/23/15	1830	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2367	025A	180.10	10/18/13	1115	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2368	025A	180.20	06/28/14	1538	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2369	025A	180.20	07/20/11	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2370	025A	180.20	01/03/15	2240	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2371	025A	180.23	07/30/14	1345	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2372	025A	180.25	03/15/11	1125	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2373	025A	180.25	12/18/15	0725	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
2374	025A	180.25	10/29/15	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2375	025A	180.25	04/17/15	2100	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
2376	025A	180.27	01/06/13	1535	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2377	025A	180.28	07/21/12	2322	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2378	025A	180.30	04/16/13	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2379	025A	180.30	06/08/14	1025	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2380	025A	180.37	12/22/14	1945	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
2381	025A	180.38	01/01/15	1357	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
2382	025A	180.40	10/04/13	0855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2383	025A	180.40	07/12/14	1834	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2384	025A	180.40	10/25/15	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2385	025A	180.42	01/18/11	1140	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2386	025A	180.46	05/15/15	1542	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2387	025A	180.50	02/02/15	1400	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2388	025A	180.50	07/26/15	1505	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2389	025A	180.50	04/25/13	2045	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2390	025A	180.50	02/15/11	0500	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
2391	025A	180.50	09/10/14	0730	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2392	025A	180.50	01/04/14	1245	INJ	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2393	025A	180.50	05/12/14	0555	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2394	025A	180.50	02/08/11	0025	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2395	025A	180.52	05/15/12	0912	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2396	025A	180.54	01/30/13	0915	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SNOWY	DAYLIGHT	NONE
2397	025A	180.54	01/29/13	1815	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
2398	025A	180.56	02/01/14	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2399	025A	180.58	03/06/15	1700	INJ	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2400	025A	180.60	06/29/11	1830	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2341	025A	179.78	11/10/15	0655	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2342	025A	179.78	06/25/15	1635	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
2343	025A	179.80	07/12/13	0915	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
2344	025A	179.80	10/25/15	1445	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	079	CHANGING LANES
2345	025A	179.80	05/15/11	1745	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2346	025A	179.80	01/08/12	0115	Y (L)	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2347	025A	179.90	05/31/14	1020	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	WEAVING
2348	025A	179.90	09/14/14	2000	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
2349	025A	179.94	08/21/13	0620	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
2350	025A	179.97	07/02/15	1604	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2351	025A	180.00	08/29/14	1700	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2352	025A	180.00	02/03/12	1025	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
2353	025A	180.00	05/18/12	1245	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2354	025A	180.00	07/26/14	1320	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	020	GOING STRAIGHT
2355	025A	180.00	08/03/14	1110	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
2356	025A	180.00	08/01/15	1415	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2357	025A	180.00	06/04/11	0940	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2358	025A	180.00	06/26/15	1511	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
2359	025A	180.00	12/10/15	1830	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	003	CHANGING LANES
2360	025A	180.00	11/29/11	1756	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2361	025A	180.00	04/29/12	1945	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2362	025A	180.00	12/31/11	0415	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2363	025A	180.00	12/15/15	1400	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
2364	025A	180.01	03/28/14	0600	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2365	025A	180.10	09/29/14	2330	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
2366	025A	180.10	10/23/15	1830	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2367	025A	180.10	10/18/13	1115	N	GUARD RAIL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2368	025A	180.20	06/28/14	1538	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2369	025A	180.20	07/20/11	1515	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2370	025A	180.20	01/03/15	2240	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
2371	025A	180.23	07/30/14	1345	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
2372	025A	180.25	03/15/11	1125	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
2373	025A	180.25	12/18/15	0725	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
2374	025A	180.25	10/29/15	1245	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2375	025A	180.25	04/17/15	2100	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2376	025A	180.27	01/06/13	1535	Y (L)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	SLOWING
2377	025A	180.28	07/21/12	2322	Y (L)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2378	025A	180.30	04/16/13	1030	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	ASLEEP AT WHEEL	070	WEAVING
2379	025A	180.30	06/08/14	1025	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	060	WEAVING
2380	025A	180.37	12/22/14	1945	Y (B)	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	015	OTHER
2381	025A	180.38	01/01/15	1357	Y (L)	LIGHT/UTILITY POLE	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2382	025A	180.40	10/04/13	0855	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2383	025A	180.40	07/12/14	1834	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
2384	025A	180.40	10/25/15	1400	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2385	025A	180.42	01/18/11	1140	Y (L)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
2386	025A	180.46	05/15/15	1542	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
2387	025A	180.50	02/02/15	1400	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2388	025A	180.50	07/26/15	1505	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2389	025A	180.50	04/25/13	2045	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	080	OTHER
2390	025A	180.50	02/15/11	0500	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	068	OTHER
2391	025A	180.50	09/10/14	0730	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2392	025A	180.50	01/04/14	1245	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
2393	025A	180.50	05/12/14	0555	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
2394	025A	180.50	02/08/11	0025	N	ROAD MAINTENANCE EQUIPMENT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	043	PASSING
2395	025A	180.52	05/15/12	0912	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2396	025A	180.54	01/30/13	0915	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	CHANGING LANES
2397	025A	180.54	01/29/13	1815	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	OTHER
2398	025A	180.56	02/01/14	1715	Y (L)	SIDESWIPE OPPOSITE DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2399	025A	180.58	03/06/15	1700	Y (E)	HEAD-ON	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	025	WRONG WAY
2400	025A	180.60	06/29/11	1830	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2401	025A	180.60	05/08/13	1240	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2402	025A	180.60	07/11/14	1548	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2403	025A	180.61	07/14/12	2200	FAT	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2404	025A	180.63	03/28/15	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2405	025A	180.64	12/23/14	0555	PDO	ON	RAMP	2	HILLCREST	SNOWY	DAWN OR DUSK	FOG
2406	025A	180.65	12/12/12	1405	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2407	025A	180.65	01/07/12	2100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
2408	025A	180.66	08/25/14	1647	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
2409	025A	180.70	06/20/15	1102	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2410	025A	180.70	07/03/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2411	025A	180.70	01/01/15	2126	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2412	025A	180.70	10/06/13	1225	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2413	025A	180.70	01/04/14	1251	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2414	025A	180.70	05/31/15	1238	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2415	025A	180.72	08/07/15	1448	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2416	025A	180.73	04/10/11	1630	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2417	025A	180.74	06/15/14	1230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2418	025A	180.74	06/25/11	1130	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2419	025A	180.75	07/01/15	2135	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2420	025A	180.75	06/05/14	0714	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2421	025A	180.76	08/11/15	1409	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2422	025A	180.77	07/26/15	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2423	025A	180.80	10/13/13	2350	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2424	025A	180.80	08/30/14	1055	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2425	025A	180.83	05/22/15	1354	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2426	025A	180.83	07/24/15	1538	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2427	025A	180.85	11/15/11	0810	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2428	025A	180.86	06/28/14	1246	PDO	OFF LEFT	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2429	025A	180.86	04/26/11	0455	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
2430	025A	180.86	01/29/14	1620	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2431	025A	180.86	08/26/14	0906	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2432	025A	180.86	08/05/14	0851	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2433	025A	180.86	01/19/14	1825	INJ	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2434	025A	180.86	01/29/14	1542	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2435	025A	180.86	12/02/11	1514	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2436	025A	180.86	01/15/14	1318	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2437	025A	180.86	06/04/14	0850	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2438	025A	180.86	08/07/14	1415	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2439	025A	180.86	09/18/11	1343	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2440	025A	180.86	09/24/14	1113	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2441	025A	180.87	04/25/13	1530	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2442	025A	180.87	06/27/13	0642	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2443	025A	180.87	02/25/14	1558	INJ	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	SNOW/SLEET/HAIL
2444	025A	180.87	11/06/12	0808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2445	025A	180.88	09/18/15	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2446	025A	180.88	12/08/12	2201	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2447	025A	180.88	01/03/15	2052	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2448	025A	180.88	05/21/12	1045	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2449	025A	180.88	09/23/15	1027	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2450	025A	180.88	08/31/15	1514	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2451	025A	180.88	09/29/15	1725	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2452	025A	180.88	12/14/15	1841	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2453	025A	180.88	03/22/14	1220	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2454	025A	180.88	01/09/15	1632	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2455	025A	180.88	02/09/12	1000	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2456	025A	180.88	08/06/12	1735	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2457	025A	180.88	02/06/14	1509	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2458	025A	180.88	04/16/15	1330	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
2459	025A	180.88	07/28/15	1345	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2460	025A	180.88	10/19/13	1045	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2401	025A	180.60	05/08/13	1240	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2402	025A	180.60	07/11/14	1548	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2403	025A	180.61	07/14/12	2200	N	PEDESTRIAN	W	OTHER - SEE REPORT	ALCOHOL	NONE APPARENT	UK	OTHER
2404	025A	180.63	03/28/15	0900	N	SIDESWIPE SAME DIRECTION	N	NON-SCHOOL BUS < 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2405	025A	180.64	12/23/14	0555	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	OTHER
2406	025A	180.65	12/12/12	1405	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
2407	025A	180.65	01/07/12	2100	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2408	025A	180.66	08/25/14	1647	Y (B)	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	OTHER
2409	025A	180.70	06/20/15	1102	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
2410	025A	180.70	07/03/15	1330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2411	025A	180.70	01/01/15	2126	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2412	025A	180.70	10/06/13	1225	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2413	025A	180.70	01/04/14	1251	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2414	025A	180.70	05/31/15	1238	N	REAR-END	N	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	090	GOING STRAIGHT
2415	025A	180.72	08/07/15	1448	Y (M)	APPROACH TURN	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
2416	025A	180.73	04/10/11	1630	N	APPROACH TURN	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	MAKING LEFT TURN
2417	025A	180.74	06/15/14	1230	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	040	GOING STRAIGHT
2418	025A	180.74	06/25/11	1130	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2419	025A	180.75	07/01/15	2135	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	091	CHANGING LANES
2420	025A	180.75	06/05/14	0714	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2421	025A	180.76	08/11/15	1409	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	090	GOING STRAIGHT
2422	025A	180.77	07/26/15	1115	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2423	025A	180.80	10/13/13	2350	N	CONCRETE BARRIER	N	PASS CAR/VAN W/TRAILER	ALCOHOL	UNKNOWN	065	WEAVING
2424	025A	180.80	08/30/14	1055	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2425	025A	180.83	05/22/15	1354	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2426	025A	180.83	07/24/15	1538	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
2427	025A	180.85	11/15/11	0810	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2428	025A	180.86	06/28/14	1246	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	AVOIDING OBJECT/VEHICLE IN ROAD
2429	025A	180.86	04/26/11	0455	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2430	025A	180.86	01/29/14	1620	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
2431	025A	180.86	08/26/14	0906	Y (L)	LARGE BOULDERS OR ROCKS	W	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	035	OTHER
2432	025A	180.86	08/05/14	0851	Y (M)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	GOING STRAIGHT
2433	025A	180.86	01/19/14	1825	Y (M)	APPROACH TURN	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	MAKING LEFT TURN
2434	025A	180.86	01/29/14	1542	Y (N)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
2435	025A	180.86	12/02/11	1514	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
2436	025A	180.86	01/15/14	1318	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2437	025A	180.86	06/04/14	0850	Y (O)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
2438	025A	180.86	08/07/14	1415	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2439	025A	180.86	09/18/11	1343	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	002	GOING STRAIGHT
2440	025A	180.86	09/24/14	1113	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
2441	025A	180.87	04/25/13	1530	Y (B)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	001	GOING STRAIGHT
2442	025A	180.87	06/27/13	0642	Y (B)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER FATIGUE	010	GOING STRAIGHT
2443	025A	180.87	02/25/14	1558	Y (D)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
2444	025A	180.87	11/06/12	0808	Y (L)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
2445	025A	180.88	09/18/15	1650	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	015	SLOWING
2446	025A	180.88	12/08/12	2201	N	CONCRETE BARRIER	N	SUV	ALCOHOL	UNKNOWN	065	WEAVING
2447	025A	180.88	01/03/15	2052	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2448	025A	180.88	05/21/12	1045	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2449	025A	180.88	09/23/15	1027	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	CHANGING LANES
2450	025A	180.88	08/31/15	1514	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	GOING STRAIGHT
2451	025A	180.88	09/29/15	1725	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
2452	025A	180.88	12/14/15	1841	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	CHANGING LANES
2453	025A	180.88	03/22/14	1220	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	002	GOING STRAIGHT
2454	025A	180.88	01/09/15	1632	Y (M)	BROADSIDE	E	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING RIGHT TURN
2455	025A	180.88	02/09/12	1000	Y (N)	APPROACH TURN	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2456	025A	180.88	08/06/12	1735	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
2457	025A	180.88	02/06/14	1509	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2458	025A	180.88	04/16/15	1330	Y (N)	REAR-END	W	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	005	BACKING
2459	025A	180.88	07/28/15	1345	Y (N)	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2460	025A	180.88	10/19/13	1045	Y (N)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	MAKING LEFT TURN

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2461	025A	180.88	05/30/14	1455	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2462	025A	180.88	07/09/15	1112	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2463	025A	180.88	10/06/15	0225	INJ	OFF LEFT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2464	025A	180.88	01/29/12	1945	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2465	025A	180.88	03/23/12	1850	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2466	025A	180.88	12/27/14	1206	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	NONE
2467	025A	180.88	06/06/13	0917	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2468	025A	180.88	01/22/13	0716	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2469	025A	180.88	06/18/13	0809	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2470	025A	180.88	11/14/13	2029	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2471	025A	180.88	04/30/15	1608	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2472	025A	180.88	10/03/15	1713	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2473	025A	180.88	10/08/11	0610	PDO	OFF RIGHT	INTERSECTION RELATED	1	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
2474	025A	180.90	06/24/15	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2475	025A	180.95	07/06/14	1640	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2476	025A	180.97	09/20/14	0331	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2477	025A	180.98	07/26/13	1615	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2478	025A	180.98	06/26/14	1500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2479	025A	181.00	10/03/14	0652	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2480	025A	181.00	09/02/11	1714	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2481	025A	181.00	07/21/12	1144	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
2482	025A	181.00	05/30/14	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2483	025A	181.00	06/26/14	1525	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2484	025A	181.00	06/30/14	1615	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2485	025A	181.00	03/07/15	1959	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2486	025A	181.00	06/13/15	1212	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2487	025A	181.00	10/09/15	1753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2488	025A	181.00	02/24/12	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2489	025A	181.00	06/13/12	1855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2490	025A	181.00	02/03/15	1944	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2491	025A	181.00	05/14/15	2104	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
2492	025A	181.00	07/19/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2493	025A	181.00	08/06/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2494	025A	181.00	09/29/15	0000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2495	025A	181.00	11/03/15	0200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2496	025A	181.00	12/29/15	1639	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2497	025A	181.00	04/27/11	0539	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
2498	025A	181.00	07/12/13	1124	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2499	025A	181.00	07/01/15	0703	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2500	025A	181.00	11/27/15	1922	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
2501	025A	181.00	04/27/11	0455	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
2502	025A	181.00	03/10/13	0938	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
2503	025A	181.00	05/01/15	1636	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2504	025A	181.00	01/30/14	1251	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2505	025A	181.00	07/20/15	1332	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2506	025A	181.07	07/06/12	1726	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2507	025A	181.07	12/26/15	2239	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2508	025A	181.08	06/02/12	0145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2509	025A	181.08	06/28/14	1230	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2510	025A	181.10	09/02/11	1710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2511	025A	181.10	07/11/15	1105	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2512	025A	181.10	09/20/14	0330	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2513	025A	181.10	07/11/15	1040	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
2514	025A	181.10	07/11/15	1110	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2515	025A	181.20	03/09/15	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2516	025A	181.20	07/25/12	0335	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2517	025A	181.20	03/07/14	1947	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2518	025A	181.20	10/14/13	0550	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2519	025A	181.20	10/15/13	0550	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2520	025A	181.20	10/09/11	0200	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2461	025A	180.88	05/30/14	1455	Y (N)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING RIGHT TURN
2462	025A	180.88	07/09/15	1112	Y (N)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
2463	025A	180.88	10/06/15	0225	Y (N)	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	EVADING LAW ENFORCEMENT OFFICER	070	OTHER
2464	025A	180.88	01/29/12	1945	Y (O)	BROADSIDE	N	SUV	NO IMPAIRMENT	UNKNOWN	UK	MAKING RIGHT TURN
2465	025A	180.88	03/23/12	1850	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2466	025A	180.88	12/27/14	1206	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	002	GOING STRAIGHT
2467	025A	180.88	06/06/13	0917	Y (O)	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	003	MAKING RIGHT TURN
2468	025A	180.88	01/22/13	0716	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING LEFT TURN
2469	025A	180.88	06/18/13	0809	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2470	025A	180.88	11/14/13	2029	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2471	025A	180.88	04/30/15	1608	Y (O)	APPROACH TURN	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
2472	025A	180.88	10/03/15	1713	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
2473	025A	180.88	10/08/11	0610	Y (O)	EMBANKMENT CUT/FILL SLOPE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	OTHER
2474	025A	180.90	06/24/15	0850	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2475	025A	180.95	07/06/14	1640	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2476	025A	180.97	09/20/14	0331	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2477	025A	180.98	07/26/13	1615	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	040	GOING STRAIGHT
2478	025A	180.98	06/26/14	1500	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	073	GOING STRAIGHT
2479	025A	181.00	10/03/14	0652	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2480	025A	181.00	09/02/11	1714	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
2481	025A	181.00	07/21/12	1144	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
2482	025A	181.00	05/30/14	1600	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	SLOWING
2483	025A	181.00	06/26/14	1525	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
2484	025A	181.00	06/30/14	1615	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
2485	025A	181.00	03/07/15	1959	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
2486	025A	181.00	06/13/15	1212	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2487	025A	181.00	10/09/15	1753	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
2488	025A	181.00	02/24/12	1640	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	CHANGING LANES
2489	025A	181.00	06/13/12	1855	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
2490	025A	181.00	02/03/15	1944	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2491	025A	181.00	05/14/15	2104	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	ALCOHOL	NONE APPARENT	065	CHANGING LANES
2492	025A	181.00	07/19/15	1220	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
2493	025A	181.00	08/06/15	1130	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
2494	025A	181.00	09/29/15	0000	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
2495	025A	181.00	11/03/15	0200	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2496	025A	181.00	12/29/15	1639	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	060	GOING STRAIGHT
2497	025A	181.00	04/27/11	0539	N	PARKED MOTOR VEHICLE	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2498	025A	181.00	07/12/13	1124	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2499	025A	181.00	07/01/15	0703	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2500	025A	181.00	11/27/15	1922	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2501	025A	181.00	04/27/11	0455	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2502	025A	181.00	03/10/13	0938	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2503	025A	181.00	05/01/15	1636	N	BARRICADE/TRAFFIC BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2504	025A	181.00	01/30/14	1251	Y (C)	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	035	GOING STRAIGHT
2505	025A	181.00	07/20/15	1332	Y (D)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	025	GOING STRAIGHT
2506	025A	181.07	07/06/12	1726	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2507	025A	181.07	12/26/15	2239	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	065	GOING STRAIGHT
2508	025A	181.08	06/02/12	0145	N	SIDESWIPE OPPOSITE DIRECTION	N	SUV	ALCOHOL	UNKNOWN	065	WRONG WAY
2509	025A	181.08	06/28/14	1230	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
2510	025A	181.10	09/02/11	1710	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	SLOWING
2511	025A	181.10	07/11/15	1105	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
2512	025A	181.10	09/20/14	0330	N	REAR-END	N	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
2513	025A	181.10	07/11/15	1040	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
2514	025A	181.10	07/11/15	1110	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2515	025A	181.20	03/09/15	1700	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
2516	025A	181.20	07/25/12	0335	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2517	025A	181.20	03/07/14	1947	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	OTHER
2518	025A	181.20	10/14/13	0550	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2519	025A	181.20	10/15/13	0550	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2520	025A	181.20	10/09/11	0200	Y (C)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2521	025A	181.22	03/09/15	1700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2522	025A	181.25	04/27/11	0626	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	FOG
2523	025A	181.25	10/26/13	1522	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2524	025A	181.28	06/26/15	1624	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2525	025A	181.30	04/03/15	0610	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
2526	025A	181.40	04/30/12	2230	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2527	025A	181.40	12/27/12	1930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2528	025A	181.50	04/19/15	1213	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2529	025A	181.50	10/09/15	1753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2530	025A	181.50	11/08/12	1605	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2531	025A	181.50	08/02/11	0542	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2532	025A	181.50	06/27/14	0240	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2533	025A	181.50	06/20/15	2214	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2534	025A	181.50	08/10/12	1743	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2535	025A	181.50	12/22/11	0440	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2536	025A	181.50	02/10/15	0500	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2537	025A	181.50	07/14/12	1345	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2538	025A	181.60	09/11/11	0759	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2539	025A	181.60	12/25/12	1103	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
2540	025A	181.67	07/24/13	1952	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2541	025A	181.75	12/11/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2542	025A	181.75	12/24/11	1023	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2543	025A	181.75	06/16/12	2308	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2544	025A	181.79	07/14/12	1346	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2545	025A	181.80	02/16/15	0303	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2546	025A	181.80	04/20/15	1021	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2547	025A	181.80	07/16/12	0943	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2548	025A	181.83	02/23/12	0810	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2549	025A	181.83	10/21/15	1451	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2550	025A	181.83	10/05/14	0217	INJ	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2551	025A	181.85	05/31/15	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2552	025A	181.85	10/21/14	1214	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2553	025A	181.85	02/27/12	1335	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2554	025A	181.85	07/24/13	1825	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2555	025A	181.85	08/29/13	1141	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2556	025A	181.85	10/22/13	1426	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2557	025A	181.85	04/05/14	0938	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2558	025A	181.85	05/17/14	1723	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2559	025A	181.85	08/22/14	2012	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2560	025A	181.85	08/22/11	1345	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2561	025A	181.85	10/02/11	1128	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2562	025A	181.85	02/15/12	1312	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2563	025A	181.85	11/04/14	0755	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2564	025A	181.85	04/18/15	1509	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2565	025A	181.85	03/17/14	1545	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2566	025A	181.85	02/06/15	1324	PDO	ON	INTERSECTION RELATED	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2567	025A	181.85	12/20/12	1714	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2568	025A	181.85	06/24/14	1735	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2569	025A	181.85	02/25/15	0843	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2570	025A	181.85	03/14/12	2137	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2571	025A	181.85	07/03/12	1700	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2572	025A	181.85	11/09/12	1804	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2573	025A	181.85	01/16/14	1904	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2574	025A	181.85	08/14/15	1949	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2575	025A	181.85	11/02/15	1450	INJ	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2576	025A	181.85	06/08/12	1810	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2577	025A	181.85	11/09/12	1804	INJ	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2578	025A	181.85	06/01/13	1630	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2579	025A	181.85	04/03/14	0917	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2580	025A	181.85	09/03/14	0550	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2521	025A	181.22	03/09/15	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2522	025A	181.25	04/27/11	0626	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
2523	025A	181.25	10/26/13	1522	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
2524	025A	181.28	06/26/15	1624	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	070	CHANGING LANES
2525	025A	181.30	04/03/15	0610	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2526	025A	181.40	04/30/12	2230	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2527	025A	181.40	12/27/12	1930	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
2528	025A	181.50	04/19/15	1213	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
2529	025A	181.50	10/09/15	1753	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	040	SLOWING
2530	025A	181.50	11/08/12	1605	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2531	025A	181.50	08/02/11	0542	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2532	025A	181.50	06/27/14	0240	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2533	025A	181.50	06/20/15	2214	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2534	025A	181.50	08/10/12	1743	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
2535	025A	181.50	12/22/11	0440	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2536	025A	181.50	02/10/15	0500	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2537	025A	181.50	07/14/12	1345	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	070	GOING STRAIGHT
2538	025A	181.60	09/11/11	0759	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2539	025A	181.60	12/25/12	1103	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
2540	025A	181.67	07/24/13	1952	N	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	OTHER
2541	025A	181.75	12/11/12	0730	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2542	025A	181.75	12/24/11	1023	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2543	025A	181.75	06/16/12	2308	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2544	025A	181.79	07/14/12	1346	N	OVERTURNING	N	SUV	ALCOHOL	DISTRACTED BY PASSENGER	066	GOING STRAIGHT
2545	025A	181.80	02/16/15	0303	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2546	025A	181.80	04/20/15	1021	N	BARRICADE/TRAFFIC BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	OTHER
2547	025A	181.80	07/16/12	0943	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING RIGHT TURN
2548	025A	181.83	02/23/12	0810	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
2549	025A	181.83	10/21/15	1451	Y (B)	LARGE BOULDERS OR ROCKS	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
2550	025A	181.83	10/05/14	0217	Y (B)	OTHER FIXED OBJECT	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	060	GOING STRAIGHT
2551	025A	181.85	05/31/15	1110	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
2552	025A	181.85	10/21/14	1214	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
2553	025A	181.85	02/27/12	1335	Y (N)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
2554	025A	181.85	07/24/13	1825	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
2555	025A	181.85	08/29/13	1141	Y (N)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	GOING STRAIGHT
2556	025A	181.85	10/22/13	1426	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	GOING STRAIGHT
2557	025A	181.85	04/05/14	0938	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2558	025A	181.85	05/17/14	1723	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	006	MAKING RIGHT TURN
2559	025A	181.85	08/22/14	2012	Y (N)	BROADSIDE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2560	025A	181.85	08/22/11	1345	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
2561	025A	181.85	10/02/11	1128	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	008	GOING STRAIGHT
2562	025A	181.85	02/15/12	1312	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	MAKING U-TURN
2563	025A	181.85	11/04/14	0755	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
2564	025A	181.85	04/18/15	1509	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2565	025A	181.85	03/17/14	1545	Y (N)	BROADSIDE	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
2566	025A	181.85	02/06/15	1324	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	PASSING
2567	025A	181.85	12/20/12	1714	Y (N)	APPROACH TURN	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	MAKING LEFT TURN
2568	025A	181.85	06/24/14	1735	Y (N)	APPROACH TURN	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2569	025A	181.85	02/25/15	0843	Y (N)	APPROACH TURN	E	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2570	025A	181.85	03/14/12	2137	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2571	025A	181.85	07/03/12	1700	Y (O)	BROADSIDE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	MAKING LEFT TURN
2572	025A	181.85	11/09/12	1804	Y (O)	BROADSIDE	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	MAKING LEFT TURN
2573	025A	181.85	01/16/14	1904	Y (O)	BROADSIDE	E	SUV	NO IMPAIRMENT	UNKNOWN	025	MAKING LEFT TURN
2574	025A	181.85	08/14/15	1949	Y (O)	BROADSIDE	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2575	025A	181.85	11/02/15	1450	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2576	025A	181.85	06/08/12	1810	Y (O)	REAR-END	E	PASS CAR/VAN	ALCOHOL	UNKNOWN	020	GOING STRAIGHT
2577	025A	181.85	11/09/12	1804	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	025	STOPPED IN TRAFFIC
2578	025A	181.85	06/01/13	1630	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	003	GOING STRAIGHT
2579	025A	181.85	04/03/14	0917	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
2580	025A	181.85	09/03/14	0550	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2581	025A	181.85	11/12/14	1738	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2582	025A	181.85	12/05/14	1507	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2583	025A	181.85	04/03/15	1105	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2584	025A	181.85	06/19/12	0851	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2585	025A	181.85	07/24/12	1533	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2586	025A	181.85	03/22/13	1740	INJ	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2587	025A	181.85	11/28/13	2303	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2588	025A	181.85	01/27/14	1707	PDO	ON	INTERSECTION RELATED	2	CURVE ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2589	025A	181.85	05/31/12	0640	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2590	025A	181.85	05/20/14	1503	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2591	025A	181.85	03/31/12	2225	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2592	025A	181.85	04/03/12	1641	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
2593	025A	181.85	09/12/12	1016	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
2594	025A	181.85	01/01/13	1607	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
2595	025A	181.85	01/18/13	1555	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2596	025A	181.85	03/15/13	1725	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2597	025A	181.85	02/22/14	1920	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2598	025A	181.85	04/08/14	0616	PDO	ON	INTERSECTION RELATED	2	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
2599	025A	181.85	11/14/14	1834	INJ	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2600	025A	181.85	11/21/14	2010	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2601	025A	181.85	01/28/15	1813	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2602	025A	181.85	02/19/15	0714	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2603	025A	181.85	03/20/15	1815	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2604	025A	181.85	12/02/15	0630	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2605	025A	181.85	10/13/12	1448	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2606	025A	181.85	03/21/15	2338	PDO	OFF RIGHT	INTERSECTION RELATED	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2607	025A	181.85	08/07/13	1725	PDO	OFF LEFT	INTERSECTION RELATED	1	CURVE ON-GRADE	FOREIGN MATERIAL	DAYLIGHT	NONE
2608	025A	181.85	07/21/15	1457	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
2609	025A	181.86	05/04/13	1252	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2610	025A	181.87	02/09/15	1553	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2611	025A	181.90	08/02/13	0510	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
2612	025A	181.90	08/07/15	1100	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2613	025A	181.90	09/13/12	1400	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2614	025A	181.90	12/01/13	1745	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2615	025A	181.91	08/15/12	1522	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2616	025A	181.92	07/16/12	1445	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2617	025A	181.92	03/03/13	1315	INJ	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2618	025A	181.94	04/03/11	0731	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2619	025A	181.94	04/13/14	1635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2620	025A	181.95	02/05/11	0248	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2621	025A	181.95	10/01/14	0905	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2622	025A	181.97	07/06/12	1702	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2623	025A	182.00	07/01/13	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2624	025A	182.00	07/27/13	2224	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2625	025A	182.00	02/05/11	1516	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2626	025A	182.00	07/02/11	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2627	025A	182.00	12/20/12	1505	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2628	025A	182.00	06/21/13	1845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2629	025A	182.00	09/10/14	0656	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2630	025A	182.00	11/22/14	1741	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2631	025A	182.00	12/30/11	1648	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2632	025A	182.00	03/23/12	0748	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2633	025A	182.00	10/06/12	2130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2634	025A	182.00	03/29/13	2056	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2635	025A	182.00	11/22/13	1736	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2636	025A	182.00	03/11/14	2100	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2637	025A	182.00	08/29/15	0858	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2638	025A	182.00	06/20/15	2350	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2639	025A	182.00	08/20/15	1100	PDO	ON	NON-INTERSECTION	4	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2640	025A	182.00	05/17/11	0500	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2581	025A	181.85	11/12/14	1738	Y (O)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
2582	025A	181.85	12/05/14	1507	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
2583	025A	181.85	04/03/15	1105	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	UK	GOING STRAIGHT
2584	025A	181.85	06/19/12	0851	Y (O)	SIDESWIPE SAME DIRECTION	NE	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2585	025A	181.85	07/24/12	1533	Y (O)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
2586	025A	181.85	03/22/13	1740	Y (O)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	MAKING LEFT TURN
2587	025A	181.85	11/28/13	2303	Y (O)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	MAKING LEFT TURN
2588	025A	181.85	01/27/14	1707	Y (O)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	020	MAKING RIGHT TURN
2589	025A	181.85	05/31/12	0640	Y (O)	SIDESWIPE OPPOSITE DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	005	MAKING LEFT TURN
2590	025A	181.85	05/20/14	1503	Y (O)	BROADSIDE	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2591	025A	181.85	03/31/12	2225	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	MAKING LEFT TURN
2592	025A	181.85	04/03/12	1641	Y (O)	APPROACH TURN	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	MAKING LEFT TURN
2593	025A	181.85	09/12/12	1016	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING LEFT TURN
2594	025A	181.85	01/01/13	1607	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	NONE APPARENT	020	MAKING LEFT TURN
2595	025A	181.85	01/18/13	1555	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING LEFT TURN
2596	025A	181.85	03/15/13	1725	Y (O)	APPROACH TURN	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	MAKING LEFT TURN
2597	025A	181.85	02/22/14	1920	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	MAKING LEFT TURN
2598	025A	181.85	04/08/14	0616	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2599	025A	181.85	11/14/14	1834	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	UNKNOWN	010	MAKING LEFT TURN
2600	025A	181.85	11/21/14	2010	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	MAKING LEFT TURN
2601	025A	181.85	01/28/15	1813	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
2602	025A	181.85	02/19/15	0714	Y (O)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	MAKING LEFT TURN
2603	025A	181.85	03/20/15	1815	Y (O)	APPROACH TURN	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	MAKING LEFT TURN
2604	025A	181.85	12/02/15	0630	Y (O)	APPROACH TURN	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2605	025A	181.85	10/13/12	1448	Y (O)	OVERTAKING TURN	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING U-TURN
2606	025A	181.85	03/21/15	2338	Y (O)	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	MAKING LEFT TURN
2607	025A	181.85	08/07/13	1725	Y (O)	CURB/RAISED MEDIAN	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	010	MAKING RIGHT TURN
2608	025A	181.85	07/21/15	1457	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
2609	025A	181.86	05/04/13	1252	Y (D)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	005	BACKING
2610	025A	181.87	02/09/15	1553	Y (D)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
2611	025A	181.90	08/02/13	0510	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2612	025A	181.90	08/07/15	1100	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	095	OTHER
2613	025A	181.90	09/13/12	1400	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
2614	025A	181.90	12/01/13	1745	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	STOPPED IN TRAFFIC
2615	025A	181.91	08/15/12	1522	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	WEAVING
2616	025A	181.92	07/16/12	1445	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	100	GOING STRAIGHT
2617	025A	181.92	03/03/13	1315	Y (D)	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
2618	025A	181.94	04/03/11	0731	N	SIDESWIPE SAME DIRECTION	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	025	OTHER
2619	025A	181.94	04/13/14	1635	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	PASSING
2620	025A	181.95	02/05/11	0248	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
2621	025A	181.95	10/01/14	0905	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
2622	025A	181.97	07/06/12	1702	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2623	025A	182.00	07/01/13	2100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	067	AVOIDING OBJECT/VEHICLE IN ROAD
2624	025A	182.00	07/27/13	2224	N	SIDESWIPE SAME DIRECTION	N	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2625	025A	182.00	02/05/11	1516	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
2626	025A	182.00	07/02/11	1100	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2627	025A	182.00	12/20/12	1505	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	CHANGING LANES
2628	025A	182.00	06/21/13	1845	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2629	025A	182.00	09/10/14	0656	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2630	025A	182.00	11/22/14	1741	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2631	025A	182.00	12/30/11	1648	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	063	CHANGING LANES
2632	025A	182.00	03/23/12	0748	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
2633	025A	182.00	10/06/12	2130	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
2634	025A	182.00	03/29/13	2056	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	068	GOING STRAIGHT
2635	025A	182.00	11/22/13	1736	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2636	025A	182.00	03/11/14	2100	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2637	025A	182.00	08/29/15	0858	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	065	CHANGING LANES
2638	025A	182.00	06/20/15	2350	N	PARKED MOTOR VEHICLE	N	SUV	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2639	025A	182.00	08/20/15	1100	N	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2640	025A	182.00	05/17/11	0500	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2641	025A	182.00	06/04/12	1426	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2642	025A	182.00	01/18/13	2052	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2643	025A	182.00	12/21/13	2154	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2644	025A	182.00	03/07/14	1712	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2645	025A	182.00	06/06/14	1400	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
2646	025A	182.00	07/12/14	2013	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
2647	025A	182.00	09/28/14	1634	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2648	025A	182.00	10/12/14	0229	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2649	025A	182.00	03/15/13	0904	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2650	025A	182.00	03/23/13	1300	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2651	025A	182.04	05/09/15	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2652	025A	182.04	09/04/14	2242	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
2653	025A	182.05	09/16/14	0743	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2654	025A	182.07	02/16/15	1738	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2655	025A	182.10	04/29/11	0145	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2656	025A	182.10	03/31/11	0650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2657	025A	182.10	05/11/11	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2658	025A	182.10	07/03/13	2315	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2659	025A	182.10	07/19/11	1815	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2660	025A	182.10	05/19/12	2340	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2661	025A	182.10	03/23/13	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2662	025A	182.10	08/17/15	1527	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2663	025A	182.19	09/05/12	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2664	025A	182.19	01/16/12	1910	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2665	025A	182.20	10/13/14	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2666	025A	182.20	03/03/11	1505	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2667	025A	182.20	11/10/15	2350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2668	025A	182.20	08/16/12	0750	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2669	025A	182.20	09/12/13	1630	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2670	025A	182.27	09/29/14	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2671	025A	182.30	10/23/14	0528	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2672	025A	182.30	11/22/14	1751	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2673	025A	182.30	10/29/11	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2674	025A	182.30	03/22/14	1625	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2675	025A	182.30	11/30/13	1700	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
2676	025A	182.34	10/22/14	1500	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2677	025A	182.35	05/30/12	0356	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2678	025A	182.49	06/15/14	0805	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2679	025A	182.50	09/15/11	1417	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2680	025A	182.50	11/13/12	0802	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2681	025A	182.50	06/18/14	1230	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2682	025A	182.50	12/05/12	1600	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2683	025A	182.55	11/09/13	2338	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2684	025A	182.60	05/27/15	0230	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2685	025A	182.60	07/06/14	0255	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2686	025A	182.70	07/25/12	1726	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2687	025A	182.70	09/19/12	0838	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2688	025A	182.70	08/02/14	1907	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2689	025A	182.75	07/07/12	1900	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
2690	025A	182.80	08/13/15	0610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2691	025A	182.80	08/15/14	1600	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2692	025A	182.80	06/17/14	2115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2693	025A	182.80	07/16/12	2051	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
2694	025A	182.80	04/17/15	2049	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2695	025A	182.87	10/20/15	0320	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2696	025A	182.89	08/21/15	1320	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2697	025A	182.90	10/24/15	1020	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2698	025A	182.95	02/28/15	2300	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2699	025A	182.99	07/27/12	1058	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2700	025A	183.00	05/30/11	1522	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2641	025A	182.00	06/04/12	1426	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2642	025A	182.00	01/18/13	2052	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	080	OTHER
2643	025A	182.00	12/21/13	2154	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2644	025A	182.00	03/07/14	1712	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2645	025A	182.00	06/06/14	1400	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2646	025A	182.00	07/12/14	2013	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
2647	025A	182.00	09/28/14	1634	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	OTHER
2648	025A	182.00	10/12/14	0229	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2649	025A	182.00	03/15/13	0904	N	INVOLVING OTHER OBJECT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2650	025A	182.00	03/23/13	1300	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2651	025A	182.04	05/09/15	1540	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
2652	025A	182.04	09/04/14	2242	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
2653	025A	182.05	09/16/14	0743	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	085	GOING STRAIGHT
2654	025A	182.07	02/16/15	1738	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	OTHER
2655	025A	182.10	04/29/11	0145	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2656	025A	182.10	03/31/11	0650	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2657	025A	182.10	05/11/11	1300	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	CHANGING LANES
2658	025A	182.10	07/03/13	2315	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2659	025A	182.10	07/19/11	1815	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2660	025A	182.10	05/19/12	2340	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
2661	025A	182.10	03/23/13	1245	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
2662	025A	182.10	08/17/15	1527	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
2663	025A	182.19	09/05/12	1600	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2664	025A	182.19	01/16/12	1910	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2665	025A	182.20	10/13/14	0630	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	072	GOING STRAIGHT
2666	025A	182.20	03/03/11	1505	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
2667	025A	182.20	11/10/15	2350	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	WEAVING
2668	025A	182.20	08/16/12	0750	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2669	025A	182.20	09/12/13	1630	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2670	025A	182.27	09/29/14	1555	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	075	PASSING
2671	025A	182.30	10/23/14	0528	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2672	025A	182.30	11/22/14	1751	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
2673	025A	182.30	10/29/11	2100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2674	025A	182.30	03/22/14	1625	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2675	025A	182.30	11/30/13	1700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
2676	025A	182.34	10/22/14	1500	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	060	WEAVING
2677	025A	182.35	05/30/12	0356	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2678	025A	182.49	06/15/14	0805	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	069	GOING STRAIGHT
2679	025A	182.50	09/15/11	1417	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2680	025A	182.50	11/13/12	0802	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2681	025A	182.50	06/18/14	1230	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2682	025A	182.50	12/05/12	1600	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2683	025A	182.55	11/09/13	2338	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	062	GOING STRAIGHT
2684	025A	182.60	05/27/15	0230	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	063	GOING STRAIGHT
2685	025A	182.60	07/06/14	0255	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
2686	025A	182.70	07/25/12	1726	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2687	025A	182.70	09/19/12	0838	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	CHANGING LANES
2688	025A	182.70	08/02/14	1907	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	CHANGING LANES
2689	025A	182.75	07/07/12	1900	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
2690	025A	182.80	08/13/15	0610	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
2691	025A	182.80	08/15/14	1600	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2692	025A	182.80	06/17/14	2115	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2693	025A	182.80	07/16/12	2051	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2694	025A	182.80	04/17/15	2049	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2695	025A	182.87	10/20/15	0320	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2696	025A	182.89	08/21/15	1320	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2697	025A	182.90	10/24/15	1020	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2698	025A	182.95	02/28/15	2300	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2699	025A	182.99	07/27/12	1058	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2700	025A	183.00	05/30/11	1522	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2701	025A	183.00	01/18/14	1257	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2702	025A	183.00	10/13/14	1813	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2703	025A	183.00	03/22/12	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2704	025A	183.00	10/13/14	0633	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2705	025A	183.00	01/29/13	2016	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2706	025A	183.00	03/11/14	1823	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2707	025A	183.00	07/20/12	0030	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2708	025A	183.01	03/22/13	0500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2709	025A	183.02	04/17/15	0140	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
2710	025A	183.06	11/09/11	0020	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2711	025A	183.10	02/16/11	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2712	025A	183.10	07/02/11	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2713	025A	183.10	03/30/12	1445	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2714	025A	183.10	12/14/11	1955	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2715	025A	183.20	01/18/12	1145	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2716	025A	183.20	11/27/14	1120	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2717	025A	183.30	02/20/14	0015	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2718	025A	183.32	02/25/12	0200	FAT	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2719	025A	183.40	06/01/11	2222	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2720	025A	183.50	04/20/14	1217	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2721	025A	183.50	12/22/14	1228	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2722	025A	183.50	01/20/15	0628	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2723	025A	183.50	03/20/15	1424	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2724	025A	183.50	10/10/15	1923	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2725	025A	183.50	01/11/12	0856	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2726	025A	183.50	01/16/12	1903	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2727	025A	183.50	10/08/12	2104	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2728	025A	183.50	02/13/13	1940	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2729	025A	183.50	12/16/13	0001	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2730	025A	183.50	05/13/14	1950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2731	025A	183.50	10/17/14	1334	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2732	025A	183.50	05/15/15	1538	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2733	025A	183.50	03/10/12	0541	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2734	025A	183.50	11/02/13	0415	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2735	025A	183.50	05/24/15	1507	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2736	025A	183.50	08/26/11	2300	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2737	025A	183.50	02/24/13	0745	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2738	025A	183.50	12/24/12	2205	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2739	025A	183.50	09/13/13	0807	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2740	025A	183.50	05/15/15	1407	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2741	025A	183.50	09/09/14	0729	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2742	025A	183.50	04/21/13	0300	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2743	025A	183.60	08/15/11	1833	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2744	025A	183.60	07/24/12	1305	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2745	025A	183.71	08/28/11	2130	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2746	025A	183.75	09/20/12	0134	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2747	025A	183.75	10/07/13	0753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2748	025A	183.78	05/13/14	1906	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2749	025A	183.80	01/01/12	1130	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2750	025A	183.80	01/01/12	1130	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2751	025A	183.80	01/11/12	0920	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2752	025A	183.80	02/03/12	1337	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2753	025A	183.80	02/21/13	0755	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2754	025A	183.89	06/11/13	1224	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2755	025A	183.90	02/20/15	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2756	025A	183.91	01/19/15	1045	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2757	025A	183.92	02/20/13	1751	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2758	025A	183.92	05/22/15	1553	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2759	025A	183.96	03/30/12	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2760	025A	183.99	03/02/13	1430	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2701	025A	183.00	01/18/14	1257	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2702	025A	183.00	10/13/14	1813	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2703	025A	183.00	03/22/12	1030	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	067	GOING STRAIGHT
2704	025A	183.00	10/13/14	0633	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2705	025A	183.00	01/29/13	2016	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2706	025A	183.00	03/11/14	1823	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
2707	025A	183.00	07/20/12	0030	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
2708	025A	183.01	03/22/13	0500	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	080	GOING STRAIGHT
2709	025A	183.02	04/17/15	0140	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	055	WEAVING
2710	025A	183.06	11/09/11	0020	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2711	025A	183.10	02/16/11	0920	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2712	025A	183.10	07/02/11	1200	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
2713	025A	183.10	03/30/12	1445	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2714	025A	183.10	12/14/11	1955	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
2715	025A	183.20	01/18/12	1145	N	DOMESTIC ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2716	025A	183.20	11/27/14	1120	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2717	025A	183.30	02/20/14	0015	N	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2718	025A	183.32	02/25/12	0200	N	PARKED MOTOR VEHICLE	S	SUV	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
2719	025A	183.40	06/01/11	2222	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	WEAVING
2720	025A	183.50	04/20/14	1217	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2721	025A	183.50	12/22/14	1228	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
2722	025A	183.50	01/20/15	0628	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
2723	025A	183.50	03/20/15	1424	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2724	025A	183.50	10/10/15	1923	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	030	SLOWING
2725	025A	183.50	01/11/12	0856	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2726	025A	183.50	01/16/12	1903	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
2727	025A	183.50	10/08/12	2104	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	ENTERING/LEAVING PARKED POSITION
2728	025A	183.50	02/13/13	1940	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
2729	025A	183.50	12/16/13	0001	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	WEAVING
2730	025A	183.50	05/13/14	1950	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2731	025A	183.50	10/17/14	1334	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2732	025A	183.50	05/15/15	1538	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2733	025A	183.50	03/10/12	0541	N	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	001	BACKING
2734	025A	183.50	11/02/13	0415	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2735	025A	183.50	05/24/15	1507	N	SIGN	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	050	GOING STRAIGHT
2736	025A	183.50	08/26/11	2300	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
2737	025A	183.50	02/24/13	0745	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
2738	025A	183.50	12/24/12	2205	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
2739	025A	183.50	09/13/13	0807	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	OTHER
2740	025A	183.50	05/15/15	1407	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2741	025A	183.50	09/09/14	0729	N	EMBANKMENT CUT/FILL SLOPE	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2742	025A	183.50	04/21/13	0300	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2743	025A	183.60	08/15/11	1833	N	DELINEATOR POST	S	PASS CAR/VAN W/TRAILER	ALCOHOL	DRIVER PREOCCUPIED	003	BACKING
2744	025A	183.60	07/24/12	1305	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	ENTERING/LEAVING PARKED POSITION
2745	025A	183.71	08/28/11	2130	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
2746	025A	183.75	09/20/12	0134	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2747	025A	183.75	10/07/13	0753	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2748	025A	183.78	05/13/14	1906	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
2749	025A	183.80	01/01/12	1130	N	REAR-END	N	SUV	ALCOHOL	UNKNOWN	000	STOPPED IN TRAFFIC
2750	025A	183.80	01/01/12	1130	N	REAR-END	N	SUV	ALCOHOL	UNKNOWN	000	STOPPED IN TRAFFIC
2751	025A	183.80	01/11/12	0920	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2752	025A	183.80	02/03/12	1337	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2753	025A	183.80	02/21/13	0755	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
2754	025A	183.89	06/11/13	1224	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2755	025A	183.90	02/20/15	1425	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
2756	025A	183.91	01/19/15	1045	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	SLOWING
2757	025A	183.92	02/20/13	1751	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	035	OTHER
2758	025A	183.92	05/22/15	1553	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
2759	025A	183.96	03/30/12	1450	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	PASSING
2760	025A	183.99	03/02/13	1430	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	BACKING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2761	025A	183.99	03/18/14	1022	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
2762	025A	183.99	03/23/15	0000	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2763	025A	184.00	04/30/14	1923	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2764	025A	184.00	06/04/14	1209	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2765	025A	184.00	08/28/12	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2766	025A	184.00	04/07/14	1636	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2767	025A	184.00	07/18/14	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2768	025A	184.00	08/09/15	1738	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2769	025A	184.00	08/24/12	1828	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2770	025A	184.00	05/01/13	1230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
2771	025A	184.00	07/06/13	1450	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2772	025A	184.00	07/20/13	1304	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2773	025A	184.00	09/30/13	1340	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2774	025A	184.00	02/13/15	1404	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2775	025A	184.00	03/26/15	0000	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2776	025A	184.00	10/16/15	1836	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2777	025A	184.00	10/31/15	1310	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2778	025A	184.00	02/22/14	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2779	025A	184.00	04/13/14	1724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2780	025A	184.00	06/16/14	2135	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2781	025A	184.00	06/26/14	1253	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2782	025A	184.00	02/20/15	0949	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2783	025A	184.00	08/06/15	1130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2784	025A	184.00	03/02/15	0247	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2785	025A	184.00	07/05/13	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2786	025A	184.00	06/05/14	0902	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2787	025A	184.00	09/11/13	1721	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2788	025A	184.00	08/24/14	1838	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2789	025A	184.00	10/19/14	1717	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2790	025A	184.00	03/20/15	1410	PDO	ON	NON-INTERSECTION	6	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2791	025A	184.00	04/10/15	1510	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2792	025A	184.00	12/05/14	1358	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2793	025A	184.00	03/22/12	1630	PDO	OFF LEFT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2794	025A	184.02	11/27/15	1325	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2795	025A	184.06	05/31/12	2102	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2796	025A	184.09	06/29/11	1705	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2797	025A	184.10	03/01/12	2050	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
2798	025A	184.10	03/03/15	1814	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY W/WIS ICY ROAD TREATMENT	DAWN OR DUSK	SNOW/SLEET/HAIL
2799	025A	184.11	01/20/15	0916	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2800	025A	184.11	12/12/15	2055	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2801	025A	184.15	08/01/15	1947	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2802	025A	184.16	11/01/15	1802	PDO	ON	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2803	025A	184.17	04/22/14	1430	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2804	025A	184.17	11/03/11	1649	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2805	025A	184.18	08/12/12	1425	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2806	025A	184.19	05/05/14	1407	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2807	025A	184.19	02/09/14	1620	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2808	025A	184.20	11/22/14	1820	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2809	025A	184.20	07/11/14	2337	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2810	025A	184.20	12/12/14	1435	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2811	025A	184.20	03/05/12	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2812	025A	184.20	08/08/14	1050	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2813	025A	184.20	02/06/11	0800	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2814	025A	184.20	09/15/13	0130	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2815	025A	184.20	10/18/14	2045	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2816	025A	184.20	10/19/11	1036	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2817	025A	184.20	10/28/11	1559	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2818	025A	184.20	10/05/15	1226	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2819	025A	184.20	09/24/11	0203	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2820	025A	184.20	10/31/15	1253	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2761	025A	183.99	03/18/14	1022	Y (B)	REAR-END	N	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	005	STOPPED IN TRAFFIC
2762	025A	183.99	03/23/15	0000	Y (B)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	CHANGING LANES
2763	025A	184.00	04/30/14	1923	N	OTHER NON-COLLISION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2764	025A	184.00	06/04/14	1209	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
2765	025A	184.00	08/28/12	1545	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2766	025A	184.00	04/07/14	1636	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2767	025A	184.00	07/18/14	1600	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2768	025A	184.00	08/09/15	1738	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2769	025A	184.00	08/24/12	1828	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
2770	025A	184.00	05/01/13	1230	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2771	025A	184.00	07/06/13	1450	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	067	PASSING
2772	025A	184.00	07/20/13	1304	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
2773	025A	184.00	09/30/13	1340	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	MAKING RIGHT TURN
2774	025A	184.00	02/13/15	1404	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	GOING STRAIGHT
2775	025A	184.00	03/26/15	0000	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
2776	025A	184.00	10/16/15	1836	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2777	025A	184.00	10/31/15	1310	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2778	025A	184.00	02/22/14	0840	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2779	025A	184.00	04/13/14	1724	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
2780	025A	184.00	06/16/14	2135	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	070	PASSING
2781	025A	184.00	06/26/14	1253	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
2782	025A	184.00	02/20/15	0949	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	CHANGING LANES
2783	025A	184.00	08/06/15	1130	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
2784	025A	184.00	03/02/15	0247	N	DOMESTIC ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2785	025A	184.00	07/05/13	2100	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2786	025A	184.00	06/05/14	0902	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2787	025A	184.00	09/11/13	1721	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2788	025A	184.00	08/24/14	1838	N	ROAD MAINTENANCE EQUIPMENT	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
2789	025A	184.00	10/19/14	1717	N	REAR-END	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
2790	025A	184.00	03/20/15	1410	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
2791	025A	184.00	04/10/15	1510	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	GOING STRAIGHT
2792	025A	184.00	12/05/14	1358	Y (B)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	PASSING
2793	025A	184.00	03/22/12	1630	Y (B)	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	OTHER
2794	025A	184.02	11/27/15	1325	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2795	025A	184.06	05/31/12	2102	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2796	025A	184.09	06/29/11	1705	Y (B)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
2797	025A	184.10	03/01/12	2050	N	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2798	025A	184.10	03/03/15	1814	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2799	025A	184.11	01/20/15	0916	N	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	WEAVING
2800	025A	184.11	12/12/15	2055	Y (B)	SIGN	N	SUV	NO IMPAIRMENT	UNKNOWN	045	OTHER
2801	025A	184.15	08/01/15	1947	Y (B)	LIGHT/UTILITY POLE	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2802	025A	184.16	11/01/15	1802	Y (B)	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
2803	025A	184.17	04/22/14	1430	Y (B)	CONCRETE BARRIER	NE	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
2804	025A	184.17	11/03/11	1649	Y (D)	REAR-END	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	040	GOING STRAIGHT
2805	025A	184.18	08/12/12	1425	Y (E)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	010	GOING STRAIGHT
2806	025A	184.19	05/05/14	1407	Y (B)	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
2807	025A	184.19	02/09/14	1620	Y (B)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
2808	025A	184.20	11/22/14	1820	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2809	025A	184.20	07/11/14	2337	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
2810	025A	184.20	12/12/14	1435	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2811	025A	184.20	03/05/12	0900	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
2812	025A	184.20	08/08/14	1050	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
2813	025A	184.20	02/06/11	0800	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2814	025A	184.20	09/15/13	0130	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	065	WEAVING
2815	025A	184.20	10/18/14	2045	N	CRASH CUSHION	S	SUV	ALCOHOL	UNKNOWN	010	GOING STRAIGHT
2816	025A	184.20	10/19/11	1036	Y (B)	REAR-END	NE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	GOING STRAIGHT
2817	025A	184.20	10/28/11	1559	Y (B)	REAR-END	NE	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2818	025A	184.20	10/05/15	1226	Y (B)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	MAKING RIGHT TURN
2819	025A	184.20	09/24/11	0203	Y (B)	LIGHT/UTILITY POLE	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
2820	025A	184.20	10/31/15	1253	Y (B)	REAR-END	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	050	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2821	025A	184.21	06/18/12	0957	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2822	025A	184.21	01/04/15	1439	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2823	025A	184.21	02/05/11	1520	PDO	ON	RAMP	2	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2824	025A	184.21	12/22/11	1100	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2825	025A	184.21	05/14/15	1016	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2826	025A	184.21	12/24/11	1428	PDO	OFF AT TEE	AT INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2827	025A	184.21	08/30/11	0615	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2828	025A	184.22	09/11/14	0728	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2829	025A	184.22	07/29/11	1410	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2830	025A	184.22	09/19/11	1645	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2831	025A	184.22	10/29/11	1314	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2832	025A	184.22	12/30/11	1606	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
2833	025A	184.22	11/13/12	1534	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2834	025A	184.22	01/03/14	1330	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2835	025A	184.22	08/21/13	1605	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2836	025A	184.22	02/26/15	1622	PDO	ON	RAMP	2	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2837	025A	184.23	11/16/15	1706	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2838	025A	184.23	08/09/12	0830	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2839	025A	184.24	11/28/13	2112	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2840	025A	184.24	06/26/13	0000	INJ	ON	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2841	025A	184.24	08/05/14	1542	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2842	025A	184.24	08/07/14	1803	INJ	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2843	025A	184.24	09/12/14	1418	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2844	025A	184.24	03/08/14	0200	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
2845	025A	184.24	03/20/14	1931	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2846	025A	184.25	01/03/14	1855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2847	025A	184.25	02/03/11	1500	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2848	025A	184.25	11/26/15	1555	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2849	025A	184.25	04/06/15	0950	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2850	025A	184.28	03/26/11	1945	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2851	025A	184.29	03/08/14	0720	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	NONE
2852	025A	184.30	02/04/15	1413	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2853	025A	184.30	12/22/14	1615	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2854	025A	184.30	08/24/11	1955	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
2855	025A	184.30	12/19/12	1250	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2856	025A	184.30	10/08/14	2211	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2857	025A	184.30	01/24/14	2305	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2858	025A	184.30	02/04/14	0933	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
2859	025A	184.30	04/17/13	2037	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2860	025A	184.30	07/25/15	0030	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2861	025A	184.31	10/09/14	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2862	025A	184.31	11/13/14	0706	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2863	025A	184.31	03/23/11	0230	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2864	025A	184.31	12/16/15	1035	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
2865	025A	184.34	03/27/15	1230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2866	025A	184.36	02/04/11	0450	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2867	025A	184.37	08/16/11	1555	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2868	025A	184.40	07/20/13	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2869	025A	184.40	03/20/15	1015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2870	025A	184.40	12/04/13	0530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2871	025A	184.40	02/09/11	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
2872	025A	184.40	02/15/15	2111	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DARK-LIGHTED	SNOW/SLEET/HAIL
2873	025A	184.43	06/20/12	0200	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2874	025A	184.45	07/21/15	0003	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2875	025A	184.50	11/17/12	2225	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2876	025A	184.50	04/22/12	2103	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2877	025A	184.50	12/09/14	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2878	025A	184.50	01/09/11	1505	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2879	025A	184.50	04/30/11	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2880	025A	184.50	05/04/13	0943	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2821	025A	184.21	06/18/12	0957	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2822	025A	184.21	01/04/15	1439	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
2823	025A	184.21	02/05/11	1520	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
2824	025A	184.21	12/22/11	1100	Y (N)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	OTHER
2825	025A	184.21	05/14/15	1016	Y (N)	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
2826	025A	184.21	12/24/11	1428	Y (N)	CONCRETE BARRIER	S	SUV	ALCOHOL	UNKNOWN	010	MAKING LEFT TURN
2827	025A	184.21	08/30/11	0615	Y (O)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2828	025A	184.22	09/11/14	0728	Y (C)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	AVOIDING OBJECT/VEHICLE IN ROAD
2829	025A	184.22	07/29/11	1410	Y (D)	REAR-END	SW	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
2830	025A	184.22	09/19/11	1645	Y (D)	REAR-END	SW	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
2831	025A	184.22	10/29/11	1314	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2832	025A	184.22	12/30/11	1606	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	MAKING RIGHT TURN
2833	025A	184.22	11/13/12	1534	Y (D)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
2834	025A	184.22	01/03/14	1330	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	MAKING LEFT TURN
2835	025A	184.22	08/21/13	1605	Y (D)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
2836	025A	184.22	02/26/15	1622	Y (D)	SIDESWIPE SAME DIRECTION	SW	SCHOOL BUS < 15 PEOPLE	RX/MEDICATION/DR	NONE APPARENT	020	OTHER
2837	025A	184.23	11/16/15	1706	Y (C)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
2838	025A	184.23	08/09/12	0830	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	UNKNOWN	020	MAKING RIGHT TURN
2839	025A	184.24	11/28/13	2112	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
2840	025A	184.24	06/26/13	0000	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	045	OTHER
2841	025A	184.24	08/05/14	1542	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
2842	025A	184.24	08/07/14	1803	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
2843	025A	184.24	09/12/14	1418	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	012	GOING STRAIGHT
2844	025A	184.24	03/08/14	0200	Y (D)	LIGHT/UTILITY POLE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2845	025A	184.24	03/20/14	1931	Y (D)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
2846	025A	184.25	01/03/14	1855	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
2847	025A	184.25	02/03/11	1500	Y (C)	LIGHT/UTILITY POLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	OTHER
2848	025A	184.25	11/26/15	1555	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
2849	025A	184.25	04/06/15	0950	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
2850	025A	184.28	03/26/11	1945	Y (C)	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
2851	025A	184.29	03/08/14	0720	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2852	025A	184.30	02/04/15	1413	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	075	GOING STRAIGHT
2853	025A	184.30	12/22/14	1615	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
2854	025A	184.30	08/24/11	1955	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	BACKING
2855	025A	184.30	12/19/12	1250	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2856	025A	184.30	10/08/14	2211	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	GOING STRAIGHT
2857	025A	184.30	01/24/14	2305	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
2858	025A	184.30	02/04/14	0933	Y (C)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
2859	025A	184.30	04/17/13	2037	Y (C)	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
2860	025A	184.30	07/25/15	0030	Y (C)	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
2861	025A	184.31	10/09/14	0910	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
2862	025A	184.31	11/13/14	0706	Y (C)	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	BACKING
2863	025A	184.31	03/23/11	0230	Y (D)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	OTHER
2864	025A	184.31	12/16/15	1035	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2865	025A	184.34	03/27/15	1230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2866	025A	184.36	02/04/11	0450	Y (D)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2867	025A	184.37	08/16/11	1555	Y (D)	DELINEATOR POST	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
2868	025A	184.40	07/20/13	1730	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
2869	025A	184.40	03/20/15	1015	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
2870	025A	184.40	12/04/13	0530	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2871	025A	184.40	02/09/11	1730	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2872	025A	184.40	02/15/15	2111	Y (C)	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
2873	025A	184.43	06/20/12	0200	Y (C)	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
2874	025A	184.45	07/21/15	0003	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
2875	025A	184.50	11/17/12	2225	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2876	025A	184.50	04/22/12	2103	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	080	GOING STRAIGHT
2877	025A	184.50	12/09/14	1730	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
2878	025A	184.50	01/09/11	1505	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
2879	025A	184.50	04/30/11	0700	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	070	WEAVING
2880	025A	184.50	05/04/13	0943	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	WEAVING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2881	025A	184.50	01/03/15	1500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2882	025A	184.50	08/27/15	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2883	025A	184.50	07/17/11	1810	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2884	025A	184.50	04/06/15	1550	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND
2885	025A	184.50	01/15/15	0650	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
2886	025A	184.50	10/13/15	0930	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2887	025A	184.50	07/20/13	1705	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2888	025A	184.50	02/06/12	0918	INJ	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2889	025A	184.50	12/26/15	0637	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	WET W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
2890	025A	184.55	09/17/15	2348	INJ	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
2891	025A	184.58	05/15/15	1521	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2892	025A	184.60	07/28/15	1155	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2893	025A	184.60	10/04/12	1526	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
2894	025A	184.60	08/07/14	0642	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2895	025A	184.60	04/17/15	2105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2896	025A	184.60	07/23/15	1510	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2897	025A	184.60	03/14/13	0945	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2898	025A	184.60	02/10/11	0832	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2899	025A	184.60	11/25/15	1750	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2900	025A	184.65	06/23/15	2005	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2901	025A	184.69	05/23/15	0326	PDO	ON	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2902	025A	184.70	08/24/11	1500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2903	025A	184.70	07/08/14	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2904	025A	184.70	01/12/15	1005	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2905	025A	184.70	12/09/12	2332	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2906	025A	184.70	05/29/15	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2907	025A	184.70	02/02/12	2155	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2908	025A	184.70	03/05/14	0805	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2909	025A	184.70	12/15/14	1800	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2910	025A	184.75	02/05/14	0920	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
2911	025A	184.79	06/21/12	2150	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2912	025A	184.80	06/18/11	1005	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2913	025A	184.80	04/13/12	0438	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
2914	025A	184.80	07/31/14	1758	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2915	025A	184.80	03/22/13	2257	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2916	025A	184.80	11/05/15	2230	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2917	025A	184.90	08/11/12	0030	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2918	025A	184.90	05/22/15	0625	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
2919	025A	184.90	11/28/15	1430	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2920	025A	184.90	01/25/15	0630	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	NONE
2921	025A	184.95	04/03/14	1420	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
2922	025A	185.00	05/29/11	1400	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2923	025A	185.00	06/16/11	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2924	025A	185.00	09/23/15	0510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2925	025A	185.00	01/11/12	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2926	025A	185.00	12/08/12	2216	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
2927	025A	185.00	12/23/12	1749	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2928	025A	185.00	11/10/14	1400	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
2929	025A	185.00	09/04/15	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
2930	025A	185.00	11/08/15	0940	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2931	025A	185.00	12/21/11	2015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2932	025A	185.00	03/06/12	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2933	025A	185.00	09/25/12	0555	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2934	025A	185.00	03/02/13	1505	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2935	025A	185.00	05/25/14	1130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2936	025A	185.00	07/19/14	0855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2937	025A	185.00	12/30/14	0040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
2938	025A	185.00	08/02/15	1709	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2939	025A	185.00	08/05/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2940	025A	185.00	08/05/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2881	025A	184.50	01/03/15	1500	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
2882	025A	184.50	08/27/15	0715	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2883	025A	184.50	07/17/11	1810	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	060	GOING STRAIGHT
2884	025A	184.50	04/06/15	1550	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2885	025A	184.50	01/15/15	0650	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	WEAVING
2886	025A	184.50	10/13/15	0930	N	LARGE BOULDERS OR ROCKS	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	055	GOING STRAIGHT
2887	025A	184.50	07/20/13	1705	N	INVOLVING OTHER OBJECT	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
2888	025A	184.50	02/06/12	0918	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
2889	025A	184.50	12/26/15	0637	Y (C)	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
2890	025A	184.55	09/17/15	2348	Y (D)	GUARD RAIL	SW	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	065	OTHER
2891	025A	184.58	05/15/15	1521	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
2892	025A	184.60	07/28/15	1155	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2893	025A	184.60	10/04/12	1526	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2894	025A	184.60	08/07/14	0642	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2895	025A	184.60	04/17/15	2105	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
2896	025A	184.60	07/23/15	1510	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2897	025A	184.60	03/14/13	0945	N	SIGN	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	068	WEAVING
2898	025A	184.60	02/10/11	0832	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2899	025A	184.60	11/25/15	1750	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
2900	025A	184.65	06/23/15	2005	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2901	025A	184.69	05/23/15	0326	Y (D)	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
2902	025A	184.70	08/24/11	1500	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
2903	025A	184.70	07/08/14	1250	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT
2904	025A	184.70	01/12/15	1005	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
2905	025A	184.70	12/09/12	2332	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	053	GOING STRAIGHT
2906	025A	184.70	05/29/15	0645	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	077	CHANGING LANES
2907	025A	184.70	02/02/12	2155	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
2908	025A	184.70	03/05/14	0805	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	OTHER
2909	025A	184.70	12/15/14	1800	Y (D)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
2910	025A	184.75	02/05/14	0920	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	CHANGING LANES
2911	025A	184.79	06/21/12	2150	N	SIGN	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	055	OTHER
2912	025A	184.80	06/18/11	1005	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2913	025A	184.80	04/13/12	0438	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	ASLEEP AT WHEEL	075	WEAVING
2914	025A	184.80	07/31/14	1758	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
2915	025A	184.80	03/22/13	2257	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	045	OTHER
2916	025A	184.80	11/05/15	2230	N	CONCRETE BARRIER	N	SUV	ALCOHOL	UNKNOWN	065	WEAVING
2917	025A	184.90	08/11/12	0030	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
2918	025A	184.90	05/22/15	0625	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2919	025A	184.90	11/28/15	1430	N	CONCRETE BARRIER	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2920	025A	184.90	01/25/15	0630	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
2921	025A	184.95	04/03/14	1420	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2922	025A	185.00	05/29/11	1400	N	VEHICLE CARGO/DEBRIS	S	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
2923	025A	185.00	06/16/11	1720	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2924	025A	185.00	09/23/15	0510	N	VEHICLE CARGO/DEBRIS	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
2925	025A	185.00	01/11/12	1030	N	REAR-END	S	FARM EQUIPMENT	NO IMPAIRMENT	NONE APPARENT	UK	WEAVING
2926	025A	185.00	12/08/12	2216	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2927	025A	185.00	12/23/12	1749	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
2928	025A	185.00	11/10/14	1400	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
2929	025A	185.00	09/04/15	2000	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
2930	025A	185.00	11/08/15	0940	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	000	STOPPED IN TRAFFIC
2931	025A	185.00	12/21/11	2015	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2932	025A	185.00	03/06/12	0710	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
2933	025A	185.00	09/25/12	0555	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	WEAVING
2934	025A	185.00	03/02/13	1505	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	CHANGING LANES
2935	025A	185.00	05/25/14	1130	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
2936	025A	185.00	07/19/14	0855	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2937	025A	185.00	12/30/14	0040	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2938	025A	185.00	08/02/15	1709	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2939	025A	185.00	08/05/15	1315	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
2940	025A	185.00	08/05/15	1315	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	AVOIDING OBJECT/VEHICLE IN ROAD

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
2941	025A	185.00	08/20/15	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2942	025A	185.00	07/03/11	2045	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2943	025A	185.00	11/10/12	0030	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2944	025A	185.00	11/09/13	0215	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2945	025A	185.00	08/24/13	1140	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2946	025A	185.00	02/06/11	0025	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2947	025A	185.00	12/26/14	0825	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2948	025A	185.00	12/12/15	1520	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2949	025A	185.00	10/25/13	1915	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
2950	025A	185.00	12/04/13	0530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
2951	025A	185.00	04/13/14	1630	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2952	025A	185.00	01/15/15	0650	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
2953	025A	185.10	01/01/13	1213	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2954	025A	185.10	08/16/11	1515	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
2955	025A	185.10	05/30/14	1813	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2956	025A	185.10	11/28/14	1740	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2957	025A	185.10	03/13/11	0115	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2958	025A	185.10	01/01/15	1410	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
2959	025A	185.10	11/11/13	1510	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2960	025A	185.10	12/27/12	1750	INJ	ON	NON-INTERSECTION	4	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
2961	025A	185.11	04/10/13	0300	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	NONE
2962	025A	185.13	10/26/14	1955	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2963	025A	185.20	05/31/13	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2964	025A	185.20	12/04/13	0725	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
2965	025A	185.20	03/27/15	0640	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
2966	025A	185.20	04/16/15	0914	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
2967	025A	185.20	05/09/13	1545	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
2968	025A	185.20	11/22/14	2345	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2969	025A	185.21	06/17/14	0950	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2970	025A	185.25	12/27/12	2015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2971	025A	185.25	11/08/13	1825	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2972	025A	185.25	09/15/13	0055	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
2973	025A	185.30	11/26/11	0414	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
2974	025A	185.30	02/25/14	0000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2975	025A	185.30	10/28/14	0315	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2976	025A	185.30	04/17/15	0615	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	NONE
2977	025A	185.30	09/24/11	1323	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2978	025A	185.40	06/13/12	1235	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2979	025A	185.40	08/26/12	2023	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2980	025A	185.40	01/10/13	1820	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2981	025A	185.40	12/08/13	0920	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
2982	025A	185.40	02/02/11	0945	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	NONE
2983	025A	185.40	03/20/11	1237	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
2984	025A	185.40	07/20/11	1320	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2985	025A	185.40	08/28/11	1900	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
2986	025A	185.50	10/15/15	1115	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2987	025A	185.50	10/11/14	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
2988	025A	185.50	02/01/11	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2989	025A	185.50	05/29/13	1650	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2990	025A	185.50	12/06/14	0020	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2991	025A	185.50	06/01/15	0820	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2992	025A	185.50	12/06/14	0030	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2993	025A	185.50	06/03/11	0900	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2994	025A	185.50	12/03/11	0200	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
2995	025A	185.50	03/23/13	0600	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
2996	025A	185.50	03/30/15	2227	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
2997	025A	185.50	09/07/15	1424	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
2998	025A	185.50	01/23/14	0725	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
2999	025A	185.50	06/06/13	1445	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3000	025A	185.53	07/21/14	2232	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
2941	025A	185.00	08/20/15	1110	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	WEAVING
2942	025A	185.00	07/03/11	2045	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2943	025A	185.00	11/10/12	0030	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2944	025A	185.00	11/09/13	0215	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2945	025A	185.00	08/24/13	1140	N	SIGN	S	SUV	NO IMPAIRMENT	ILLNESS	075	OTHER
2946	025A	185.00	02/06/11	0025	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2947	025A	185.00	12/26/14	0825	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
2948	025A	185.00	12/12/15	1520	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2949	025A	185.00	10/25/13	1915	N	REAR-END	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2950	025A	185.00	12/04/13	0530	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2951	025A	185.00	04/13/14	1630	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2952	025A	185.00	01/15/15	0650	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
2953	025A	185.10	01/01/13	1213	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2954	025A	185.10	08/16/11	1515	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
2955	025A	185.10	05/30/14	1813	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
2956	025A	185.10	11/28/14	1740	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
2957	025A	185.10	03/13/11	0115	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
2958	025A	185.10	01/01/15	1410	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
2959	025A	185.10	11/11/13	1510	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
2960	025A	185.10	12/27/12	1750	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
2961	025A	185.11	04/10/13	0300	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2962	025A	185.13	10/26/14	1955	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
2963	025A	185.20	05/31/13	1425	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
2964	025A	185.20	12/04/13	0725	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
2965	025A	185.20	03/27/15	0640	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
2966	025A	185.20	04/16/15	0914	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
2967	025A	185.20	05/09/13	1545	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
2968	025A	185.20	11/22/14	2345	N	CRASH CUSHION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	090	GOING STRAIGHT
2969	025A	185.21	06/17/14	0950	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
2970	025A	185.25	12/27/12	2015	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
2971	025A	185.25	11/08/13	1825	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
2972	025A	185.25	09/15/13	0055	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	WEAVING
2973	025A	185.30	11/26/11	0414	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
2974	025A	185.30	02/25/14	0000	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2975	025A	185.30	10/28/14	0315	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	060	WEAVING
2976	025A	185.30	04/17/15	0615	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2977	025A	185.30	09/24/11	1323	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
2978	025A	185.40	06/13/12	1235	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
2979	025A	185.40	08/26/12	2023	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
2980	025A	185.40	01/10/13	1820	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2981	025A	185.40	12/08/13	0920	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2982	025A	185.40	02/02/11	0945	N	WALL/BUILDING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
2983	025A	185.40	03/20/11	1237	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	090	OTHER
2984	025A	185.40	07/20/11	1320	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	WEAVING
2985	025A	185.40	08/28/11	1900	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
2986	025A	185.50	10/15/15	1115	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2987	025A	185.50	10/11/14	1700	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
2988	025A	185.50	02/01/11	2000	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
2989	025A	185.50	05/29/13	1650	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	WEAVING
2990	025A	185.50	12/06/14	0020	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
2991	025A	185.50	06/01/15	0820	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	WEAVING
2992	025A	185.50	12/06/14	0030	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	PARKED
2993	025A	185.50	06/03/11	0900	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
2994	025A	185.50	12/03/11	0200	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
2995	025A	185.50	03/23/13	0600	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
2996	025A	185.50	03/30/15	2227	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	WEAVING
2997	025A	185.50	09/07/15	1424	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
2998	025A	185.50	01/23/14	0725	N	FENCE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	073	CHANGING LANES
2999	025A	185.50	06/06/13	1445	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	OTHER
3000	025A	185.53	07/21/14	2232	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3001	025A	185.54	05/02/11	0606	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3002	025A	185.55	06/01/11	1345	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3003	025A	185.60	05/30/15	1609	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3004	025A	185.60	11/28/14	1710	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3005	025A	185.60	06/19/15	0859	INJ	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3006	025A	185.70	02/04/14	0800	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3007	025A	185.70	11/25/13	2310	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3008	025A	185.70	02/04/14	0800	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3009	025A	185.70	04/03/15	0225	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3010	025A	185.75	10/18/14	2210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3011	025A	185.75	07/02/11	1514	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3012	025A	185.75	11/11/14	1900	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
3013	025A	185.80	12/04/14	2052	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3014	025A	185.80	03/05/14	0440	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3015	025A	185.80	03/30/14	1100	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3016	025A	185.80	01/03/15	1716	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3017	025A	185.80	03/07/13	0645	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3018	025A	185.90	10/21/14	2125	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3019	025A	185.90	12/23/12	1825	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3020	025A	185.90	12/11/12	0740	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
3021	025A	185.90	05/11/13	1340	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3022	025A	185.90	08/22/13	2345	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3023	025A	185.90	11/26/15	1320	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3024	025A	185.90	10/29/15	1350	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3025	025A	185.95	06/08/12	0600	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3026	025A	185.97	09/24/11	1315	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3027	025A	185.98	11/26/15	2340	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3028	025A	186.00	12/22/11	1428	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
3029	025A	186.00	07/30/13	1450	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3030	025A	186.00	07/12/14	1310	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3031	025A	186.00	09/14/15	1513	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3032	025A	186.00	07/08/12	1935	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3033	025A	186.00	10/25/12	2145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3034	025A	186.00	03/01/13	1910	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3035	025A	186.00	10/03/13	1255	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3036	025A	186.00	10/15/14	0430	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3037	025A	186.00	10/23/11	0430	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3038	025A	186.00	05/21/11	0445	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3039	025A	186.00	01/01/15	1455	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3040	025A	186.00	07/21/15	1500	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3041	025A	186.00	08/21/15	1440	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3042	025A	186.02	09/29/11	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3043	025A	186.05	05/13/12	1735	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3044	025A	186.06	03/19/15	1649	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3045	025A	186.10	01/16/12	1910	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3046	025A	186.10	02/19/14	2115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3047	025A	186.10	04/04/13	0700	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3048	025A	186.10	07/26/13	2240	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3049	025A	186.10	02/20/12	0245	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
3050	025A	186.10	09/14/13	1555	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3051	025A	186.10	12/04/13	0510	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3052	025A	186.10	01/01/14	1830	PDO	OFF RIGHT	NON-INTERSECTION	3	CURVE ON-GRADE	SLUSHY	DARK-UNLIGHTED	NONE
3053	025A	186.10	07/20/15	1500	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3054	025A	186.16	11/29/15	0046	INJ	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3055	025A	186.20	10/10/15	0300	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3056	025A	186.20	06/01/11	1430	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3057	025A	186.20	04/05/15	0441	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3058	025A	186.20	09/11/14	1545	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3059	025A	186.20	09/26/11	1545	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3060	025A	186.20	12/20/11	0540	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3001	025A	185.54	05/02/11	0606	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	075	OTHER
3002	025A	185.55	06/01/11	1345	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
3003	025A	185.60	05/30/15	1609	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3004	025A	185.60	11/28/14	1710	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3005	025A	185.60	06/19/15	0859	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3006	025A	185.70	02/04/14	0800	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3007	025A	185.70	11/25/13	2310	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3008	025A	185.70	02/04/14	0800	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3009	025A	185.70	04/03/15	0225	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3010	025A	185.75	10/18/14	2210	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3011	025A	185.75	07/02/11	1514	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	005	OTHER
3012	025A	185.75	11/11/14	1900	N	CULVERT/HEADWALL	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3013	025A	185.80	12/04/14	2052	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3014	025A	185.80	03/05/14	0440	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3015	025A	185.80	03/30/14	1100	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
3016	025A	185.80	01/03/15	1716	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3017	025A	185.80	03/07/13	0645	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3018	025A	185.90	10/21/14	2125	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3019	025A	185.90	12/23/12	1825	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3020	025A	185.90	12/11/12	0740	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3021	025A	185.90	05/11/13	1340	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3022	025A	185.90	08/22/13	2345	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3023	025A	185.90	11/26/15	1320	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3024	025A	185.90	10/29/15	1350	N	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
3025	025A	185.95	06/08/12	0600	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
3026	025A	185.97	09/24/11	1315	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3027	025A	185.98	11/26/15	2340	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3028	025A	186.00	12/22/11	1428	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3029	025A	186.00	07/30/13	1450	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3030	025A	186.00	07/12/14	1310	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	055	GOING STRAIGHT
3031	025A	186.00	09/14/15	1513	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3032	025A	186.00	07/08/12	1935	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3033	025A	186.00	10/25/12	2145	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	010	BACKING
3034	025A	186.00	03/01/13	1910	N	SIDESWIPE SAME DIRECTION	S	SUV	RX/MEDICATION/DR	UNKNOWN	070	WEAVING
3035	025A	186.00	10/03/13	1255	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3036	025A	186.00	10/15/14	0430	N	WILD ANIMAL	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3037	025A	186.00	10/23/11	0430	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3038	025A	186.00	05/21/11	0445	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	074	GOING STRAIGHT
3039	025A	186.00	01/01/15	1455	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
3040	025A	186.00	07/21/15	1500	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3041	025A	186.00	08/21/15	1440	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3042	025A	186.02	09/29/11	1415	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	WEAVING
3043	025A	186.05	05/13/12	1735	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	CHANGING LANES
3044	025A	186.06	03/19/15	1649	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
3045	025A	186.10	01/16/12	1910	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3046	025A	186.10	02/19/14	2115	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	010	BACKING
3047	025A	186.10	04/04/13	0700	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3048	025A	186.10	07/26/13	2240	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3049	025A	186.10	02/20/12	0245	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3050	025A	186.10	09/14/13	1555	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
3051	025A	186.10	12/04/13	0510	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
3052	025A	186.10	01/01/14	1830	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
3053	025A	186.10	07/20/15	1500	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
3054	025A	186.16	11/29/15	0046	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	OTHER
3055	025A	186.20	10/10/15	0300	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	050	OTHER
3056	025A	186.20	06/01/11	1430	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3057	025A	186.20	04/05/15	0441	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
3058	025A	186.20	09/11/14	1545	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	070	WEAVING
3059	025A	186.20	09/26/11	1545	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3060	025A	186.20	12/20/11	0540	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3061	025A	186.20	01/29/13	2350	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3062	025A	186.20	12/25/15	1228	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3063	025A	186.20	03/03/15	1710	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3064	025A	186.21	04/14/14	0848	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
3065	025A	186.22	11/29/15	0045	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3066	025A	186.25	11/25/14	1605	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3067	025A	186.25	05/26/12	1526	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3068	025A	186.25	05/04/14	2134	FAT	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3069	025A	186.30	03/04/14	2355	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3070	025A	186.30	01/31/11	0845	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3071	025A	186.30	12/25/11	0400	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3072	025A	186.30	11/25/12	1335	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3073	025A	186.30	06/30/14	1430	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3074	025A	186.30	01/22/12	2000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3075	025A	186.40	06/28/15	0615	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3076	025A	186.40	06/19/13	1843	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3077	025A	186.40	11/15/14	1703	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	UNKNOWN	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3078	025A	186.40	12/01/15	0530	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3079	025A	186.47	06/24/14	1610	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3080	025A	186.47	06/14/14	1502	INJ	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3081	025A	186.47	05/08/15	1453	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3082	025A	186.47	06/19/14	1050	PDO	OFF LEFT	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3083	025A	186.50	10/25/15	1651	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3084	025A	186.50	05/21/13	0915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3085	025A	186.50	06/20/12	0500	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3086	025A	186.50	10/28/12	1020	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3087	025A	186.50	11/06/15	1521	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3088	025A	186.50	02/21/15	1430	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3089	025A	186.50	06/01/15	1157	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3090	025A	186.51	06/03/11	1347	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3091	025A	186.54	07/17/11	1400	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3092	025A	186.55	04/27/11	0515	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3093	025A	186.56	02/23/15	0800	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3094	025A	186.57	10/05/14	0245	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3095	025A	186.60	04/01/11	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3096	025A	186.60	06/20/11	0414	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3097	025A	186.60	02/09/14	2040	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3098	025A	186.60	07/21/14	1400	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3099	025A	186.66	07/06/12	1713	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3100	025A	186.69	01/14/14	1850	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3101	025A	186.70	03/29/12	1403	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
3102	025A	186.70	07/12/13	1115	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3103	025A	186.70	02/21/15	1435	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3104	025A	186.70	09/16/15	1520	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3105	025A	186.70	04/20/15	1341	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3106	025A	186.70	06/11/15	1940	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3107	025A	186.74	04/08/13	2212	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
3108	025A	186.74	01/21/15	1307	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
3109	025A	186.75	10/17/14	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3110	025A	186.75	02/02/12	2200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3111	025A	186.80	07/31/15	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3112	025A	186.80	02/02/12	2030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3113	025A	186.80	11/25/13	2125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3114	025A	186.80	05/27/13	1600	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3115	025A	186.80	03/09/13	0831	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3116	025A	186.80	02/04/14	0938	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3117	025A	186.80	09/07/15	0825	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3118	025A	186.90	04/26/13	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3119	025A	186.90	11/26/11	0055	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3120	025A	186.90	11/18/12	1804	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3061	025A	186.20	01/29/13	2350	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3062	025A	186.20	12/25/15	1228	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3063	025A	186.20	03/03/15	1710	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
3064	025A	186.21	04/14/14	0848	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
3065	025A	186.22	11/29/15	0045	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
3066	025A	186.25	11/25/14	1605	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3067	025A	186.25	05/26/12	1526	N	CONCRETE BARRIER	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	070	WEAVING
3068	025A	186.25	05/04/14	2134	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
3069	025A	186.30	03/04/14	2355	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
3070	025A	186.30	01/31/11	0845	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3071	025A	186.30	12/25/11	0400	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	WEAVING
3072	025A	186.30	11/25/12	1335	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3073	025A	186.30	06/30/14	1430	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
3074	025A	186.30	01/22/12	2000	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	090	CHANGING LANES
3075	025A	186.40	06/28/15	0615	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3076	025A	186.40	06/19/13	1843	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3077	025A	186.40	11/15/14	1703	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
3078	025A	186.40	12/01/15	0530	N	CONCRETE BARRIER	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	WEAVING
3079	025A	186.47	06/24/14	1610	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	065	AVOIDING OBJECT/VEHICLE IN ROAD
3080	025A	186.47	06/14/14	1502	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3081	025A	186.47	05/08/15	1453	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	WEAVING
3082	025A	186.47	06/19/14	1050	N	CONCRETE BARRIER	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3083	025A	186.50	10/25/15	1651	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3084	025A	186.50	05/21/13	0915	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
3085	025A	186.50	06/20/12	0500	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3086	025A	186.50	10/28/12	1020	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3087	025A	186.50	11/06/15	1521	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3088	025A	186.50	02/21/15	1430	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3089	025A	186.50	06/01/15	1157	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	MAKING RIGHT TURN
3090	025A	186.51	06/03/11	1347	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3091	025A	186.54	07/17/11	1400	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3092	025A	186.55	04/27/11	0515	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3093	025A	186.56	02/23/15	0800	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3094	025A	186.57	10/05/14	0245	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	GOING STRAIGHT
3095	025A	186.60	04/01/11	1545	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
3096	025A	186.60	06/20/11	0414	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3097	025A	186.60	02/09/14	2040	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
3098	025A	186.60	07/21/14	1400	N	REAR-END	S	MOTOR HOME	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3099	025A	186.66	07/06/12	1713	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3100	025A	186.69	01/14/14	1850	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3101	025A	186.70	03/29/12	1403	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3102	025A	186.70	07/12/13	1115	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3103	025A	186.70	02/21/15	1435	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3104	025A	186.70	09/16/15	1520	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3105	025A	186.70	04/20/15	1341	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
3106	025A	186.70	06/11/15	1940	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3107	025A	186.74	04/08/13	2212	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
3108	025A	186.74	01/21/15	1307	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3109	025A	186.75	10/17/14	1900	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
3110	025A	186.75	02/02/12	2200	N	ROAD MAINTENANCE EQUIPMENT	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3111	025A	186.80	07/31/15	0710	N	REAR-END	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3112	025A	186.80	02/02/12	2030	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
3113	025A	186.80	11/25/13	2125	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
3114	025A	186.80	05/27/13	1600	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3115	025A	186.80	03/09/13	0831	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3116	025A	186.80	02/04/14	0938	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3117	025A	186.80	09/07/15	0825	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
3118	025A	186.90	04/26/13	1800	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	072	CHANGING LANES
3119	025A	186.90	11/26/11	0055	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3120	025A	186.90	11/18/12	1804	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	078	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3121	025A	186.90	06/06/13	2210	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3122	025A	186.90	06/04/14	0645	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3123	025A	186.90	01/17/12	0300	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3124	025A	186.90	12/04/13	0815	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3125	025A	186.90	11/18/12	1844	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3126	025A	186.92	08/01/12	1331	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3127	025A	186.94	01/21/15	1250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3128	025A	186.94	01/10/11	1130	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3129	025A	186.94	02/25/13	1000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3130	025A	186.94	02/14/14	0445	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3131	025A	186.94	01/21/15	1250	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3132	025A	186.94	07/15/12	1640	PDO	OFF RIGHT	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3133	025A	186.94	11/22/14	1830	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3134	025A	186.94	04/23/12	0855	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3135	025A	186.94	03/29/13	1025	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3136	025A	186.94	06/19/11	1830	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3137	025A	186.96	11/10/12	2105	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3138	025A	186.98	05/18/15	1815	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3139	025A	186.99	06/17/11	2344	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3140	025A	187.00	08/07/12	0600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3141	025A	187.00	07/10/15	1305	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3142	025A	187.00	11/24/15	1905	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3143	025A	187.00	01/25/11	0640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3144	025A	187.00	05/12/12	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3145	025A	187.00	05/31/12	1100	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3146	025A	187.00	12/08/12	2123	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3147	025A	187.00	06/11/15	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3148	025A	187.00	09/19/15	1843	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
3149	025A	187.00	09/17/14	0335	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3150	025A	187.00	10/15/12	0615	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3151	025A	187.00	06/13/13	2348	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3152	025A	187.00	06/21/14	1840	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3153	025A	187.00	08/23/14	0645	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3154	025A	187.00	12/22/14	0611	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
3155	025A	187.02	11/24/13	1925	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3156	025A	187.04	11/20/11	1945	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3157	025A	187.10	07/19/11	1355	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3158	025A	187.10	01/07/12	0940	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3159	025A	187.10	10/02/15	0933	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3160	025A	187.14	03/09/13	1745	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
3161	025A	187.14	04/12/15	1600	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3162	025A	187.19	12/10/15	0527	PDO	OFF RIGHT	IN ALLEY	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	NONE
3163	025A	187.20	01/14/14	0734	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3164	025A	187.20	03/07/15	1356	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3165	025A	187.20	10/08/14	0955	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3166	025A	187.20	11/29/15	0150	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3167	025A	187.20	11/06/15	0230	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3168	025A	187.20	02/23/12	1100	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3169	025A	187.20	11/01/15	0050	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3170	025A	187.20	12/29/14	0955	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3171	025A	187.20	10/18/13	1030	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3172	025A	187.20	12/09/14	0820	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3173	025A	187.24	12/08/12	2230	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3174	025A	187.25	08/31/13	1950	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3175	025A	187.28	07/07/12	1645	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3176	025A	187.30	01/27/12	1100	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3177	025A	187.30	09/09/13	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3178	025A	187.30	12/22/14	0600	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
3179	025A	187.30	06/23/14	0920	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3180	025A	187.30	03/26/13	0030	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3121	025A	186.90	06/06/13	2210	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3122	025A	186.90	06/04/14	0645	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3123	025A	186.90	01/17/12	0300	N	BRIDGE RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3124	025A	186.90	12/04/13	0815	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3125	025A	186.90	11/18/12	1844	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3126	025A	186.92	08/01/12	1331	Y (A)	HEAD-ON	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	035	GOING STRAIGHT
3127	025A	186.94	01/21/15	1250	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3128	025A	186.94	01/10/11	1130	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
3129	025A	186.94	02/25/13	1000	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3130	025A	186.94	02/14/14	0445	N	FENCE	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3131	025A	186.94	01/21/15	1250	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
3132	025A	186.94	07/15/12	1640	Y (N)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3133	025A	186.94	11/22/14	1830	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3134	025A	186.94	04/23/12	0855	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
3135	025A	186.94	03/29/13	1025	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3136	025A	186.94	06/19/11	1830	Y (O)	GUARD RAIL	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	MAKING LEFT TURN
3137	025A	186.96	11/10/12	2105	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3138	025A	186.98	05/18/15	1815	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3139	025A	186.99	06/17/11	2344	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	OTHER
3140	025A	187.00	08/07/12	0600	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3141	025A	187.00	07/10/15	1305	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
3142	025A	187.00	11/24/15	1905	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
3143	025A	187.00	01/25/11	0640	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	080	WEAVING
3144	025A	187.00	05/12/12	0920	N	SIDESWIPE SAME DIRECTION	N	SUV	ALCOHOL	UNKNOWN	075	WEAVING
3145	025A	187.00	05/31/12	1100	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
3146	025A	187.00	12/08/12	2123	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3147	025A	187.00	06/11/15	1730	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
3148	025A	187.00	09/19/15	1843	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3149	025A	187.00	09/17/14	0335	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3150	025A	187.00	10/15/12	0615	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3151	025A	187.00	06/13/13	2348	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	080	OTHER
3152	025A	187.00	06/21/14	1840	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3153	025A	187.00	08/23/14	0645	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	055	WEAVING
3154	025A	187.00	12/22/14	0611	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	040	OTHER
3155	025A	187.02	11/24/13	1925	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3156	025A	187.04	11/20/11	1945	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3157	025A	187.10	07/19/11	1355	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3158	025A	187.10	01/07/12	0940	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3159	025A	187.10	10/02/15	0933	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
3160	025A	187.14	03/09/13	1745	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	OTHER
3161	025A	187.14	04/12/15	1600	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3162	025A	187.19	12/10/15	0527	N	CULVERT/HEADWALL	S	SUV	ALCOHOL	UNKNOWN	070	WEAVING
3163	025A	187.20	01/14/14	0734	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
3164	025A	187.20	03/07/15	1356	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3165	025A	187.20	10/08/14	0955	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3166	025A	187.20	11/29/15	0150	N	PARKED MOTOR VEHICLE	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
3167	025A	187.20	11/06/15	0230	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3168	025A	187.20	02/23/12	1100	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3169	025A	187.20	11/01/15	0050	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	085	GOING STRAIGHT
3170	025A	187.20	12/29/14	0955	N	ROAD MAINTENANCE EQUIPMENT	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3171	025A	187.20	10/18/13	1030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3172	025A	187.20	12/09/14	0820	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3173	025A	187.24	12/08/12	2230	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	CHANGING LANES
3174	025A	187.25	08/31/13	1950	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
3175	025A	187.28	07/07/12	1645	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3176	025A	187.30	01/27/12	1100	N	OTHER NON-COLLISION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
3177	025A	187.30	09/09/13	0750	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3178	025A	187.30	12/22/14	0600	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
3179	025A	187.30	06/23/14	0920	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3180	025A	187.30	03/26/13	0030	N	DELINEATOR POST	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3181	025A	187.40	08/20/15	2018	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3182	025A	187.40	09/16/13	0715	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3183	025A	187.40	03/22/14	1555	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3184	025A	187.40	02/13/15	0609	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3185	025A	187.40	08/10/11	1330	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3186	025A	187.40	11/10/12	2035	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3187	025A	187.40	11/26/11	1000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3188	025A	187.40	05/01/15	2230	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3189	025A	187.41	12/22/14	0045	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
3190	025A	187.50	09/11/12	2005	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3191	025A	187.50	01/07/12	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3192	025A	187.50	07/17/13	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3193	025A	187.50	09/04/13	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3194	025A	187.50	09/17/13	0720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3195	025A	187.50	01/16/14	1225	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3196	025A	187.50	12/08/15	1810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3197	025A	187.50	01/07/12	1946	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3198	025A	187.50	01/07/12	1946	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3199	025A	187.50	01/27/12	1105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3200	025A	187.50	06/14/13	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3201	025A	187.50	10/19/13	2033	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3202	025A	187.50	04/25/15	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3203	025A	187.50	05/05/15	1025	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3204	025A	187.50	09/08/11	0345	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3205	025A	187.50	04/29/14	2100	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	WIND
3206	025A	187.50	12/11/12	0625	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
3207	025A	187.50	03/08/13	2310	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3208	025A	187.50	03/30/12	1258	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3209	025A	187.50	05/08/14	1535	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3210	025A	187.50	12/09/14	0825	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3211	025A	187.51	08/24/14	2320	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3212	025A	187.54	05/30/15	1429	INJ	OFF RIGHT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3213	025A	187.57	04/04/11	1500	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3214	025A	187.60	12/12/15	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3215	025A	187.70	03/23/14	1320	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3216	025A	187.70	05/07/12	1604	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3217	025A	187.70	01/24/11	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3218	025A	187.70	11/04/13	0125	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3219	025A	187.70	05/02/15	2330	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3220	025A	187.70	02/09/11	0353	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
3221	025A	187.70	12/22/14	0615	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3222	025A	187.70	12/22/14	0615	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
3223	025A	187.76	05/25/12	1417	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3224	025A	187.80	06/12/13	1310	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3225	025A	187.80	05/05/12	0249	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3226	025A	187.80	12/05/15	0600	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3227	025A	187.80	10/15/14	2230	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3228	025A	187.80	02/16/15	0840	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE
3229	025A	187.80	12/06/12	0745	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3230	025A	187.80	02/24/15	1515	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3231	025A	187.84	04/05/14	2006	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3232	025A	187.90	04/25/12	2250	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3233	025A	187.90	12/16/11	1235	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3234	025A	187.90	07/19/12	0820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3235	025A	187.90	05/11/13	0450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3236	025A	187.90	07/30/14	0755	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3237	025A	187.90	02/12/15	0400	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3238	025A	187.90	12/25/15	2047	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3239	025A	187.91	12/17/14	0105	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3240	025A	187.99	08/08/15	0135	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3181	025A	187.40	08/20/15	2018	N	OTHER NON-COLLISION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3182	025A	187.40	09/16/13	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3183	025A	187.40	03/22/14	1555	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3184	025A	187.40	02/13/15	0609	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	010	CHANGING LANES
3185	025A	187.40	08/10/11	1330	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3186	025A	187.40	11/10/12	2035	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
3187	025A	187.40	11/26/11	1000	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3188	025A	187.40	05/01/15	2230	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	085	GOING STRAIGHT
3189	025A	187.41	12/22/14	0045	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3190	025A	187.50	09/11/12	2005	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3191	025A	187.50	01/07/12	1620	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	NONE APPARENT	055	GOING STRAIGHT
3192	025A	187.50	07/17/13	0850	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
3193	025A	187.50	09/04/13	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3194	025A	187.50	09/17/13	0720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3195	025A	187.50	01/16/14	1225	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3196	025A	187.50	12/08/15	1810	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3197	025A	187.50	01/07/12	1946	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3198	025A	187.50	01/07/12	1946	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3199	025A	187.50	01/27/12	1105	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3200	025A	187.50	06/14/13	0900	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	AVOIDING OBJECT/VEHICLE IN ROAD
3201	025A	187.50	10/19/13	2033	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	CHANGING LANES
3202	025A	187.50	04/25/15	0920	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3203	025A	187.50	05/05/15	1025	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
3204	025A	187.50	09/08/11	0345	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3205	025A	187.50	04/29/14	2100	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3206	025A	187.50	12/11/12	0625	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3207	025A	187.50	03/08/13	2310	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3208	025A	187.50	03/30/12	1258	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	020	CHANGING LANES
3209	025A	187.50	05/08/14	1535	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3210	025A	187.50	12/09/14	0825	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
3211	025A	187.51	08/24/14	2320	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3212	025A	187.54	05/30/15	1429	N	PARKED MOTOR VEHICLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
3213	025A	187.57	04/04/11	1500	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3214	025A	187.60	12/12/15	1415	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	CHANGING LANES
3215	025A	187.70	03/23/14	1320	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	AVOIDING OBJECT/VEHICLE IN ROAD
3216	025A	187.70	05/07/12	1604	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3217	025A	187.70	01/24/11	0750	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3218	025A	187.70	11/04/13	0125	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3219	025A	187.70	05/02/15	2330	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3220	025A	187.70	02/09/11	0353	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
3221	025A	187.70	12/22/14	0615	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
3222	025A	187.70	12/22/14	0615	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3223	025A	187.76	05/25/12	1417	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
3224	025A	187.80	06/12/13	1310	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	075	CHANGING LANES
3225	025A	187.80	05/05/12	0249	N	PARKED MOTOR VEHICLE	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3226	025A	187.80	12/05/15	0600	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3227	025A	187.80	10/15/14	2230	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3228	025A	187.80	02/16/15	0840	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3229	025A	187.80	12/06/12	0745	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3230	025A	187.80	02/24/15	1515	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3231	025A	187.84	04/05/14	2006	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3232	025A	187.90	04/25/12	2250	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	PARKED
3233	025A	187.90	12/16/11	1235	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3234	025A	187.90	07/19/12	0820	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	090	GOING STRAIGHT
3235	025A	187.90	05/11/13	0450	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
3236	025A	187.90	07/30/14	0755	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3237	025A	187.90	02/12/15	0400	N	WILD ANIMAL	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3238	025A	187.90	12/25/15	2047	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
3239	025A	187.91	12/17/14	0105	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3240	025A	187.99	08/08/15	0135	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3241	025A	187.99	02/17/15	0034	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3242	025A	188.00	04/05/15	0357	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3243	025A	188.00	09/06/13	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3244	025A	188.00	06/27/15	2130	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3245	025A	188.00	08/17/12	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3246	025A	188.00	01/03/13	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3247	025A	188.00	03/29/13	1250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3248	025A	188.00	09/30/13	0135	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3249	025A	188.00	12/14/14	1540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3250	025A	188.00	06/21/15	0500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3251	025A	188.00	12/26/15	0610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
3252	025A	188.00	01/16/12	2130	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3253	025A	188.00	08/04/12	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3254	025A	188.00	08/30/13	2055	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3255	025A	188.00	02/01/14	1554	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
3256	025A	188.00	05/11/14	2200	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
3257	025A	188.00	08/25/14	1710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3258	025A	188.00	04/14/15	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3259	025A	188.00	05/09/15	2015	INJ	OFF IN MIDDLE	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3260	025A	188.00	11/08/12	0534	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3261	025A	188.00	03/01/12	2120	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3262	025A	188.00	04/09/12	2120	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3263	025A	188.00	05/23/12	1927	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3264	025A	188.00	04/16/15	2240	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3265	025A	188.00	12/06/12	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3266	025A	188.00	09/08/13	1500	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3267	025A	188.00	02/20/15	1045	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3268	025A	188.00	05/26/11	0810	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3269	025A	188.00	04/13/14	1626	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3270	025A	188.02	03/24/13	1115	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3271	025A	188.02	08/17/12	0420	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3272	025A	188.04	02/06/13	1948	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3273	025A	188.05	02/13/11	0445	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3274	025A	188.05	03/06/14	0200	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3275	025A	188.06	11/21/15	0655	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
3276	025A	188.07	11/11/15	0725	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3277	025A	188.10	01/25/13	1617	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3278	025A	188.10	12/19/11	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3279	025A	188.10	12/28/11	0150	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3280	025A	188.10	03/22/14	1530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3281	025A	188.10	02/21/15	1600	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3282	025A	188.10	06/24/11	1225	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3283	025A	188.10	02/28/14	0144	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3284	025A	188.19	03/07/13	1755	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3285	025A	188.20	01/03/13	1050	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3286	025A	188.20	07/31/15	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3287	025A	188.20	07/05/12	0500	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3288	025A	188.20	08/29/12	2123	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3289	025A	188.20	01/17/15	0415	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3290	025A	188.20	11/17/15	0130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3291	025A	188.20	12/21/11	1930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3292	025A	188.20	05/11/14	2145	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3293	025A	188.20	07/28/11	2237	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3294	025A	188.20	05/11/14	2150	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3295	025A	188.24	02/03/12	1530	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3296	025A	188.24	12/25/15	1304	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3297	025A	188.26	03/07/12	0010	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3298	025A	188.26	12/22/14	1720	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3299	025A	188.30	03/05/11	1401	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3300	025A	188.30	12/10/15	0600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3241	025A	187.99	02/17/15	0034	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3242	025A	188.00	04/05/15	0357	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3243	025A	188.00	09/06/13	0800	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3244	025A	188.00	06/27/15	2130	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3245	025A	188.00	08/17/12	1125	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3246	025A	188.00	01/03/13	1220	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3247	025A	188.00	03/29/13	1250	N	REAR-END	S	SUV	RX/MEDICATION/DR	UNKNOWN	085	GOING STRAIGHT
3248	025A	188.00	09/30/13	0135	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT
3249	025A	188.00	12/14/14	1540	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
3250	025A	188.00	06/21/15	0500	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3251	025A	188.00	12/26/15	0610	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
3252	025A	188.00	01/16/12	2130	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
3253	025A	188.00	08/04/12	0900	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	WEAVING
3254	025A	188.00	08/30/13	2055	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3255	025A	188.00	02/01/14	1554	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	OTHER
3256	025A	188.00	05/11/14	2200	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
3257	025A	188.00	08/25/14	1710	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3258	025A	188.00	04/14/15	0900	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
3259	025A	188.00	05/09/15	2015	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3260	025A	188.00	11/08/12	0534	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3261	025A	188.00	03/01/12	2120	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
3262	025A	188.00	04/09/12	2120	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	070	OTHER
3263	025A	188.00	05/23/12	1927	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3264	025A	188.00	04/16/15	2240	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
3265	025A	188.00	12/06/12	0745	N	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3266	025A	188.00	09/08/13	1500	N	INVOLVING OTHER OBJECT	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3267	025A	188.00	02/20/15	1045	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3268	025A	188.00	05/26/11	0810	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3269	025A	188.00	04/13/14	1626	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
3270	025A	188.02	03/24/13	1115	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
3271	025A	188.02	08/17/12	0420	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	ASLEEP AT WHEEL	075	GOING STRAIGHT
3272	025A	188.04	02/06/13	1948	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	AVOIDING OBJECT/VEHICLE IN ROAD
3273	025A	188.05	02/13/11	0445	N	OVERTURNING	S	PASS CAR/VAN	ALCOHOL/DRUGS	ASLEEP AT WHEEL	075	WEAVING
3274	025A	188.05	03/06/14	0200	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	OTHER
3275	025A	188.06	11/21/15	0655	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
3276	025A	188.07	11/11/15	0725	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
3277	025A	188.10	01/25/13	1617	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3278	025A	188.10	12/19/11	1545	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	PASSING
3279	025A	188.10	12/28/11	0150	N	LIGHT/UTILITY POLE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	060	GOING STRAIGHT
3280	025A	188.10	03/22/14	1530	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3281	025A	188.10	02/21/15	1600	N	TREE/SHRUBBERY	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3282	025A	188.10	06/24/11	1225	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3283	025A	188.10	02/28/14	0144	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	CHANGING LANES
3284	025A	188.19	03/07/13	1755	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER FATIGUE	060	CHANGING LANES
3285	025A	188.20	01/03/13	1050	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	068	GOING STRAIGHT
3286	025A	188.20	07/31/15	1430	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
3287	025A	188.20	07/05/12	0500	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3288	025A	188.20	08/29/12	2123	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3289	025A	188.20	01/17/15	0415	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3290	025A	188.20	11/17/15	0130	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
3291	025A	188.20	12/21/11	1930	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3292	025A	188.20	05/11/14	2145	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	OTHER
3293	025A	188.20	07/28/11	2237	N	TREE/SHRUBBERY	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	ASLEEP AT WHEEL	055	WEAVING
3294	025A	188.20	05/11/14	2150	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
3295	025A	188.24	02/03/12	1530	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3296	025A	188.24	12/25/15	1304	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	GOING STRAIGHT
3297	025A	188.26	03/07/12	0010	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
3298	025A	188.26	12/22/14	1720	Y (E)	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3299	025A	188.30	03/05/11	1401	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3300	025A	188.30	12/10/15	0600	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3301	025A	188.30	01/29/13	2050	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3302	025A	188.30	01/30/14	2115	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3303	025A	188.30	12/30/12	1120	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3304	025A	188.40	05/07/11	2344	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3305	025A	188.40	05/12/11	0540	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3306	025A	188.40	12/26/14	0615	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3307	025A	188.40	07/10/15	1254	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3308	025A	188.40	11/30/11	1000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3309	025A	188.40	08/12/15	0745	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3310	025A	188.40	11/21/13	0450	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3311	025A	188.42	05/17/14	2239	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
3312	025A	188.44	05/12/11	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
3313	025A	188.44	11/18/15	2045	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
3314	025A	188.45	06/26/11	2045	PDO	ON	RAMP	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3315	025A	188.48	02/15/15	1805	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3316	025A	188.48	11/30/13	1445	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3317	025A	188.49	10/18/15	1049	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3318	025A	188.49	08/10/13	1445	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3319	025A	188.49	06/17/11	0925	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3320	025A	188.49	09/25/11	2019	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3321	025A	188.49	09/25/13	1210	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3322	025A	188.49	02/10/11	1540	INJ	ON	RAMP	2	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
3323	025A	188.49	04/25/11	0950	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3324	025A	188.49	08/10/11	1745	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3325	025A	188.49	09/04/12	1525	INJ	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3326	025A	188.49	11/29/12	1900	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
3327	025A	188.49	03/09/15	1740	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3328	025A	188.49	05/21/15	1503	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3329	025A	188.49	10/08/11	0801	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3330	025A	188.49	11/29/12	1515	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3331	025A	188.49	10/07/13	0845	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3332	025A	188.49	12/14/14	1315	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3333	025A	188.49	07/18/14	1846	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3334	025A	188.49	09/11/11	1145	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3335	025A	188.49	10/07/12	1100	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3336	025A	188.49	04/26/11	1100	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3337	025A	188.49	12/23/11	0945	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
3338	025A	188.50	07/21/15	0740	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3339	025A	188.50	02/04/12	1039	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	NONE
3340	025A	188.50	07/08/12	1928	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3341	025A	188.50	04/30/14	1533	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3342	025A	188.50	12/14/14	1540	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3343	025A	188.50	06/17/15	1535	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3344	025A	188.50	02/06/13	2120	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3345	025A	188.50	08/10/13	1845	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3346	025A	188.50	07/21/12	1520	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3347	025A	188.50	11/27/13	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3348	025A	188.50	02/04/12	1650	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
3349	025A	188.50	03/09/11	0745	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3350	025A	188.50	03/17/11	1345	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3351	025A	188.50	04/20/15	0710	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3352	025A	188.51	02/16/15	1655	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
3353	025A	188.51	04/01/14	1731	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3354	025A	188.52	02/26/15	0925	INJ	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	WET W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
3355	025A	188.53	12/26/14	1420	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3356	025A	188.56	11/17/15	1135	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	NONE
3357	025A	188.59	01/24/14	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3358	025A	188.59	07/15/12	1610	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3359	025A	188.59	07/21/15	0820	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3360	025A	188.60	05/11/11	2330	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3301	025A	188.30	01/29/13	2050	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3302	025A	188.30	01/30/14	2115	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3303	025A	188.30	12/30/12	1120	Y (B)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3304	025A	188.40	05/07/11	2344	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	090	GOING STRAIGHT
3305	025A	188.40	05/12/11	0540	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3306	025A	188.40	12/26/14	0615	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
3307	025A	188.40	07/10/15	1254	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3308	025A	188.40	11/30/11	1000	N	DELINEATOR POST	S	SUV W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
3309	025A	188.40	08/12/15	0745	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
3310	025A	188.40	11/21/13	0450	Y (G)	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	OTHER
3311	025A	188.42	05/17/14	2239	Y (B)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
3312	025A	188.44	05/12/11	0900	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3313	025A	188.44	11/18/15	2045	Y (E)	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
3314	025A	188.45	06/26/11	2045	Y (E)	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3315	025A	188.48	02/15/15	1805	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
3316	025A	188.48	11/30/13	1445	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3317	025A	188.49	10/18/15	1049	N	SIDESWIPE SAME DIRECTION	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	085	GOING STRAIGHT
3318	025A	188.49	08/10/13	1445	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	085	OTHER
3319	025A	188.49	06/17/11	0925	Y (N)	BROADSIDE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	030	MAKING LEFT TURN
3320	025A	188.49	09/25/11	2019	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
3321	025A	188.49	09/25/13	1210	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3322	025A	188.49	02/10/11	1540	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3323	025A	188.49	04/25/11	0950	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	MAKING RIGHT TURN
3324	025A	188.49	08/10/11	1745	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3325	025A	188.49	09/04/12	1525	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	MAKING RIGHT TURN
3326	025A	188.49	11/29/12	1900	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	MAKING RIGHT TURN
3327	025A	188.49	03/09/15	1740	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	030	GOING STRAIGHT
3328	025A	188.49	05/21/15	1503	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	005	MAKING RIGHT TURN
3329	025A	188.49	10/08/11	0801	Y (N)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	CHANGING LANES
3330	025A	188.49	11/29/12	1515	Y (N)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	050	CHANGING LANES
3331	025A	188.49	10/07/13	0845	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3332	025A	188.49	12/14/14	1315	Y (N)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	CHANGING LANES
3333	025A	188.49	07/18/14	1846	Y (N)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	030	GOING STRAIGHT
3334	025A	188.49	09/11/11	1145	Y (N)	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3335	025A	188.49	10/07/12	1100	Y (N)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
3336	025A	188.49	04/26/11	1100	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	MAKING LEFT TURN
3337	025A	188.49	12/23/11	0945	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
3338	025A	188.50	07/21/15	0740	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
3339	025A	188.50	02/04/12	1039	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3340	025A	188.50	07/08/12	1928	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
3341	025A	188.50	04/30/14	1533	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
3342	025A	188.50	12/14/14	1540	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	PASSING
3343	025A	188.50	06/17/15	1535	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
3344	025A	188.50	02/06/13	2120	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	OTHER
3345	025A	188.50	08/10/13	1845	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	OTHER
3346	025A	188.50	07/21/12	1520	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
3347	025A	188.50	11/27/13	1600	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3348	025A	188.50	02/04/12	1650	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	035	OTHER
3349	025A	188.50	03/09/11	0745	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
3350	025A	188.50	03/17/11	1345	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3351	025A	188.50	04/20/15	0710	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	001	GOING STRAIGHT
3352	025A	188.51	02/16/15	1655	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
3353	025A	188.51	04/01/14	1731	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3354	025A	188.52	02/26/15	0925	Y (C)	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3355	025A	188.53	12/26/14	1420	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3356	025A	188.56	11/17/15	1135	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	PASSING
3357	025A	188.59	01/24/14	1545	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3358	025A	188.59	07/15/12	1610	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3359	025A	188.59	07/21/15	0820	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3360	025A	188.60	05/11/11	2330	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3361	025A	188.60	03/25/15	1105	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3362	025A	188.60	09/14/15	1520	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3363	025A	188.60	01/01/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3364	025A	188.64	10/26/11	0735	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3365	025A	188.69	04/23/12	1436	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3366	025A	188.70	10/28/13	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	FOG
3367	025A	188.70	06/19/14	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3368	025A	188.70	05/05/12	0230	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3369	025A	188.70	12/22/11	2100	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
3370	025A	188.75	06/05/11	0500	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3371	025A	188.75	03/24/13	1110	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3372	025A	188.80	04/23/14	2015	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3373	025A	188.80	11/26/12	1940	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3374	025A	188.80	09/25/13	0655	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3375	025A	188.80	10/23/13	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3376	025A	188.80	12/04/11	2230	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3377	025A	188.80	08/08/13	2020	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3378	025A	188.80	12/14/14	1500	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3379	025A	188.80	10/08/11	1000	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3380	025A	188.89	08/10/13	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3381	025A	188.90	07/23/11	1005	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3382	025A	188.90	05/17/11	1730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3383	025A	188.90	05/23/11	2225	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3384	025A	188.90	10/01/14	0648	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
3385	025A	188.90	01/31/11	1335	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3386	025A	188.90	05/12/11	0224	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3387	025A	188.90	04/03/15	0316	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3388	025A	188.90	09/29/12	1703	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3389	025A	188.90	03/12/13	0600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3390	025A	188.96	03/12/13	0610	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
3391	025A	189.00	12/08/12	2052	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3392	025A	189.00	03/03/11	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3393	025A	189.00	09/17/14	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3394	025A	189.00	12/22/14	0530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
3395	025A	189.00	02/05/15	1025	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3396	025A	189.00	07/08/15	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3397	025A	189.00	05/03/13	1435	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3398	025A	189.00	08/18/14	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3399	025A	189.00	05/19/15	0500	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
3400	025A	189.00	05/20/15	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3401	025A	189.00	06/17/15	0350	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3402	025A	189.00	06/04/13	2000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3403	025A	189.00	11/15/14	2210	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3404	025A	189.00	12/22/14	0530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
3405	025A	189.00	12/25/14	2130	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3406	025A	189.00	02/26/15	1530	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3407	025A	189.00	12/15/15	2052	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3408	025A	189.00	10/16/13	1510	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3409	025A	189.00	08/10/13	2130	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3410	025A	189.00	08/26/15	0728	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3411	025A	189.01	03/22/14	1740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3412	025A	189.02	12/09/12	0750	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
3413	025A	189.04	02/28/15	0245	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3414	025A	189.10	12/19/11	1415	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3415	025A	189.10	06/17/11	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3416	025A	189.10	08/27/13	0640	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3417	025A	189.10	10/03/13	0650	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3418	025A	189.10	10/09/13	0709	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3419	025A	189.10	08/23/13	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3420	025A	189.10	11/26/13	1816	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3361	025A	188.60	03/25/15	1105	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3362	025A	188.60	09/14/15	1520	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	085	CHANGING LANES
3363	025A	188.60	01/01/15	1220	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3364	025A	188.64	10/26/11	0735	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3365	025A	188.69	04/23/12	1436	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
3366	025A	188.70	10/28/13	0920	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3367	025A	188.70	06/19/14	1450	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3368	025A	188.70	05/05/12	0230	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	085	GOING STRAIGHT
3369	025A	188.70	12/22/11	2100	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
3370	025A	188.75	06/05/11	0500	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3371	025A	188.75	03/24/13	1110	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3372	025A	188.80	04/23/14	2015	N	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3373	025A	188.80	11/26/12	1940	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	CHANGING LANES
3374	025A	188.80	09/25/13	0655	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
3375	025A	188.80	10/23/13	0735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3376	025A	188.80	12/04/11	2230	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	OTHER
3377	025A	188.80	08/08/13	2020	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3378	025A	188.80	12/14/14	1500	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3379	025A	188.80	10/08/11	1000	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3380	025A	188.89	08/10/13	1545	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3381	025A	188.90	07/23/11	1005	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
3382	025A	188.90	05/17/11	1730	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
3383	025A	188.90	05/23/11	2225	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
3384	025A	188.90	10/01/14	0648	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3385	025A	188.90	01/31/11	1335	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3386	025A	188.90	05/12/11	0224	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3387	025A	188.90	04/03/15	0316	N	CONCRETE BARRIER	N	SUV	ALCOHOL	UNKNOWN	075	OTHER
3388	025A	188.90	09/29/12	1703	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3389	025A	188.90	03/12/13	0600	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3390	025A	188.96	03/12/13	0610	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3391	025A	189.00	12/08/12	2052	N	PEDESTRIAN	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3392	025A	189.00	03/03/11	1430	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3393	025A	189.00	09/17/14	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3394	025A	189.00	12/22/14	0530	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3395	025A	189.00	02/05/15	1025	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3396	025A	189.00	07/08/15	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3397	025A	189.00	05/03/13	1435	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3398	025A	189.00	08/18/14	1220	N	SIDESWIPE SAME DIRECTION	S	SUV	ALCOHOL	UNKNOWN	070	CHANGING LANES
3399	025A	189.00	05/19/15	0500	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
3400	025A	189.00	05/20/15	1020	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3401	025A	189.00	06/17/15	0350	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3402	025A	189.00	06/04/13	2000	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3403	025A	189.00	11/15/14	2210	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
3404	025A	189.00	12/22/14	0530	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3405	025A	189.00	12/25/14	2130	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3406	025A	189.00	02/26/15	1530	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
3407	025A	189.00	12/15/15	2052	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	065	OTHER
3408	025A	189.00	10/16/13	1510	N	CRASH CUSHION	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	055	WEAVING
3409	025A	189.00	08/10/13	2130	N	INVOLVING OTHER OBJECT	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3410	025A	189.00	08/26/15	0728	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3411	025A	189.01	03/22/14	1740	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
3412	025A	189.02	12/09/12	0750	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	CHANGING LANES
3413	025A	189.04	02/28/15	0245	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
3414	025A	189.10	12/19/11	1415	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
3415	025A	189.10	06/17/11	0850	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3416	025A	189.10	08/27/13	0640	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
3417	025A	189.10	10/03/13	0650	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3418	025A	189.10	10/09/13	0709	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
3419	025A	189.10	08/23/13	1330	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3420	025A	189.10	11/26/13	1816	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3421	025A	189.10	12/01/11	1100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3422	025A	189.10	05/12/12	2105	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3423	025A	189.10	07/14/13	1935	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3424	025A	189.10	02/24/12	1220	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3425	025A	189.13	07/03/13	0740	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3426	025A	189.17	10/01/11	0730	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3427	025A	189.20	04/28/11	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3428	025A	189.20	06/21/12	1240	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3429	025A	189.20	10/14/11	1445	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3430	025A	189.20	08/30/13	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3431	025A	189.20	09/29/13	1250	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3432	025A	189.20	11/03/15	0800	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3433	025A	189.20	05/19/13	0052	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3434	025A	189.20	07/21/11	1720	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3435	025A	189.20	11/30/11	0705	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3436	025A	189.20	07/17/12	1810	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3437	025A	189.20	05/07/13	1930	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
3438	025A	189.20	01/30/13	0850	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	NONE
3439	025A	189.20	03/05/14	1010	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3440	025A	189.25	09/08/14	0725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3441	025A	189.25	12/21/11	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3442	025A	189.25	09/05/14	1055	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3443	025A	189.25	08/10/13	1905	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3444	025A	189.26	07/31/14	1435	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3445	025A	189.26	02/07/15	1715	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3446	025A	189.30	05/20/11	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3447	025A	189.30	09/23/13	0945	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3448	025A	189.30	03/14/14	2030	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3449	025A	189.30	03/07/14	1534	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
3450	025A	189.30	04/16/15	0813	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3451	025A	189.30	12/22/14	0645	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3452	025A	189.31	05/22/15	1744	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3453	025A	189.40	07/16/12	2040	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
3454	025A	189.40	09/11/13	0640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3455	025A	189.40	10/06/13	2045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3456	025A	189.40	02/26/15	2040	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3457	025A	189.40	07/16/12	2045	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3458	025A	189.40	10/01/13	1912	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3459	025A	189.40	07/19/11	1820	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3460	025A	189.40	05/23/12	2210	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
3461	025A	189.46	05/15/11	0010	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3462	025A	189.46	09/21/15	0720	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3463	025A	189.48	09/06/13	1408	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3464	025A	189.48	05/11/14	0730	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3465	025A	189.49	07/27/13	2000	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
3466	025A	189.50	01/05/13	1725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3467	025A	189.50	10/08/11	1000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3468	025A	189.50	10/08/11	1000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3469	025A	189.50	08/06/13	0935	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3470	025A	189.50	08/06/13	0936	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3471	025A	189.50	08/25/13	1120	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3472	025A	189.50	12/10/13	0840	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3473	025A	189.50	07/30/14	1701	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3474	025A	189.50	10/07/14	0835	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3475	025A	189.50	02/07/15	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3476	025A	189.50	09/17/15	0704	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3477	025A	189.50	03/13/11	0550	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3478	025A	189.50	04/22/13	2040	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3479	025A	189.50	05/08/14	0000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3480	025A	189.50	07/30/14	1635	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3421	025A	189.10	12/01/11	1100	N	SIGN	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
3422	025A	189.10	05/12/12	2105	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
3423	025A	189.10	07/14/13	1935	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3424	025A	189.10	02/24/12	1220	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3425	025A	189.13	07/03/13	0740	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3426	025A	189.17	10/01/11	0730	N	PARKED MOTOR VEHICLE	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	070	GOING STRAIGHT
3427	025A	189.20	04/28/11	0745	N	VEHICLE CARGO/DEBRIS	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3428	025A	189.20	06/21/12	1240	N	VEHICLE CARGO/DEBRIS	S	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3429	025A	189.20	10/14/11	1445	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
3430	025A	189.20	08/30/13	1550	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
3431	025A	189.20	09/29/13	1250	N	REAR-END	N	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3432	025A	189.20	11/03/15	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
3433	025A	189.20	05/19/13	0052	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	OTHER
3434	025A	189.20	08/10/11	1720	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	OTHER
3435	025A	189.20	11/30/11	0705	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	WEAVING
3436	025A	189.20	07/17/12	1810	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	AVOIDING OBJECT/VEHICLE IN ROAD
3437	025A	189.20	05/07/13	1930	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
3438	025A	189.20	01/30/13	0850	N	DOMESTIC ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
3439	025A	189.20	03/05/14	1010	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3440	025A	189.25	09/08/14	0725	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3441	025A	189.25	12/21/11	1900	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	OTHER
3442	025A	189.25	09/05/14	1055	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
3443	025A	189.25	08/10/13	1905	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3444	025A	189.26	07/31/14	1435	N	LARGE BOULDERS OR ROCKS	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3445	025A	189.26	02/07/15	1715	N	SIDESWIPE SAME DIRECTION	S	SUV	ALCOHOL	UNKNOWN	075	OTHER
3446	025A	189.30	05/20/11	1620	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	030	GOING STRAIGHT
3447	025A	189.30	09/23/13	0945	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
3448	025A	189.30	03/14/14	2030	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3449	025A	189.30	03/07/14	1534	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
3450	025A	189.30	04/16/15	0813	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3451	025A	189.30	12/22/14	0645	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3452	025A	189.31	05/22/15	1744	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3453	025A	189.40	07/16/12	2040	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3454	025A	189.40	09/11/13	0640	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3455	025A	189.40	10/06/13	2045	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
3456	025A	189.40	02/26/15	2040	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
3457	025A	189.40	07/16/12	2045	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3458	025A	189.40	10/01/13	1912	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	OTHER
3459	025A	189.40	07/19/11	1820	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3460	025A	189.40	05/23/12	2210	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
3461	025A	189.46	05/15/11	0010	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
3462	025A	189.46	09/21/15	0720	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
3463	025A	189.48	09/06/13	1408	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	030	GOING STRAIGHT
3464	025A	189.48	05/11/14	0730	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3465	025A	189.49	07/27/13	2000	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3466	025A	189.50	01/05/13	1725	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3467	025A	189.50	10/08/11	1000	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
3468	025A	189.50	10/08/11	1000	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3469	025A	189.50	08/06/13	0935	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
3470	025A	189.50	08/06/13	0936	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3471	025A	189.50	08/25/13	1120	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3472	025A	189.50	12/10/13	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
3473	025A	189.50	07/30/14	1701	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3474	025A	189.50	10/07/14	0835	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
3475	025A	189.50	02/07/15	1730	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
3476	025A	189.50	09/17/15	0704	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
3477	025A	189.50	03/13/11	0550	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	OTHER
3478	025A	189.50	04/22/13	2040	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3479	025A	189.50	05/08/14	0000	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	OTHER
3480	025A	189.50	07/30/14	1635	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3481	025A	189.50	01/15/15	1835	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3482	025A	189.50	06/30/15	0945	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
3483	025A	189.50	07/03/12	1035	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3484	025A	189.50	06/19/12	2105	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3485	025A	189.50	07/23/13	0445	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3486	025A	189.50	11/22/13	2052	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3487	025A	189.50	11/25/13	0325	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3488	025A	189.50	01/13/11	0725	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3489	025A	189.50	01/29/13	0715	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3490	025A	189.50	05/08/13	1545	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3491	025A	189.50	01/30/14	2154	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3492	025A	189.50	07/30/14	1942	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
3493	025A	189.50	09/29/14	1700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3494	025A	189.50	06/11/15	2015	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3495	025A	189.50	12/25/15	1335	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3496	025A	189.50	08/27/13	0840	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3497	025A	189.50	09/04/13	0800	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3498	025A	189.50	12/19/11	1410	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3499	025A	189.50	05/20/11	1605	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3500	025A	189.50	04/14/14	1005	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3501	025A	189.50	01/21/15	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3502	025A	189.51	09/06/13	1355	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3503	025A	189.54	05/02/14	2116	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3504	025A	189.55	05/11/11	2300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3505	025A	189.56	08/29/12	1500	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3506	025A	189.60	08/05/15	1911	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3507	025A	189.60	08/05/15	1911	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3508	025A	189.60	08/05/15	1911	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3509	025A	189.60	12/31/11	1550	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
3510	025A	189.60	06/09/13	1255	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3511	025A	189.60	09/11/13	0707	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3512	025A	189.60	07/10/14	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3513	025A	189.60	01/27/15	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3514	025A	189.60	07/27/11	1600	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3515	025A	189.60	12/11/12	0050	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3516	025A	189.60	08/22/11	1935	PDO	OFF LEFT	NON-INTERSECTION	1	HILLCREST	WET	DAWN OR DUSK	RAIN
3517	025A	189.60	05/12/14	0620	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3518	025A	189.60	08/02/14	1938	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3519	025A	189.60	01/31/15	2315	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3520	025A	189.64	04/12/11	2025	FAT	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3521	025A	189.70	08/18/11	0805	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3522	025A	189.70	12/22/11	1045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3523	025A	189.70	08/30/13	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3524	025A	189.70	10/09/13	1244	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3525	025A	189.70	11/25/13	0605	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3526	025A	189.70	05/12/12	2130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3527	025A	189.70	11/11/14	1949	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3528	025A	189.70	02/15/15	2055	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3529	025A	189.70	04/15/13	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3530	025A	189.73	08/21/12	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3531	025A	189.75	11/28/12	1325	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3532	025A	189.75	01/07/12	0140	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3533	025A	189.75	11/27/15	0305	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3534	025A	189.75	06/02/11	1430	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3535	025A	189.80	02/09/13	2225	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
3536	025A	189.80	10/18/13	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3537	025A	189.80	11/07/13	1845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3538	025A	189.80	01/27/14	1742	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3539	025A	189.80	01/21/15	1148	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3540	025A	189.80	07/27/15	0830	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3481	025A	189.50	01/15/15	1835	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
3482	025A	189.50	06/30/15	0945	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
3483	025A	189.50	07/03/12	1035	N	PARKED MOTOR VEHICLE	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3484	025A	189.50	06/19/12	2105	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3485	025A	189.50	07/23/13	0445	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3486	025A	189.50	11/22/13	2052	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3487	025A	189.50	11/25/13	0325	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3488	025A	189.50	01/13/11	0725	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3489	025A	189.50	01/29/13	0715	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
3490	025A	189.50	05/08/13	1545	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
3491	025A	189.50	01/30/14	2154	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
3492	025A	189.50	07/30/14	1942	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3493	025A	189.50	09/29/14	1700	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
3494	025A	189.50	06/11/15	2015	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3495	025A	189.50	12/25/15	1335	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3496	025A	189.50	08/27/13	0840	N	CRASH CUSHION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	OTHER
3497	025A	189.50	09/04/13	0800	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
3498	025A	189.50	12/19/11	1410	N	INVOLVING OTHER OBJECT	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	SLOWING
3499	025A	189.50	05/20/11	1605	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3500	025A	189.50	04/14/14	1005	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3501	025A	189.50	01/21/15	1550	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3502	025A	189.51	09/06/13	1355	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
3503	025A	189.54	05/02/14	2116	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3504	025A	189.55	05/11/11	2300	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3505	025A	189.56	08/29/12	1500	N	CONCRETE BARRIER	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3506	025A	189.60	08/05/15	1911	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3507	025A	189.60	08/05/15	1911	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3508	025A	189.60	08/05/15	1911	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3509	025A	189.60	12/31/11	1550	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3510	025A	189.60	06/09/13	1255	N	VEHICLE CARGO/DEBRIS	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3511	025A	189.60	09/11/13	0707	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3512	025A	189.60	07/10/14	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3513	025A	189.60	01/27/15	0745	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
3514	025A	189.60	07/27/11	1600	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
3515	025A	189.60	12/11/12	0050	N	PARKED MOTOR VEHICLE	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
3516	025A	189.60	08/22/11	1935	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3517	025A	189.60	05/12/14	0620	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3518	025A	189.60	08/02/14	1938	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3519	025A	189.60	01/31/15	2315	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	000	STOPPED IN TRAFFIC
3520	025A	189.64	04/12/11	2025	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3521	025A	189.70	08/18/11	0805	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
3522	025A	189.70	12/22/11	1045	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3523	025A	189.70	08/30/13	0630	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	WEAVING
3524	025A	189.70	10/09/13	1244	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3525	025A	189.70	11/25/13	0605	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3526	025A	189.70	05/12/12	2130	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	070	GOING STRAIGHT
3527	025A	189.70	11/11/14	1949	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3528	025A	189.70	02/15/15	2055	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	OTHER
3529	025A	189.70	04/15/13	1800	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3530	025A	189.73	08/21/12	0745	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3531	025A	189.75	11/28/12	1325	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3532	025A	189.75	01/07/12	0140	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	GOING STRAIGHT
3533	025A	189.75	11/27/15	0305	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3534	025A	189.75	06/02/11	1430	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3535	025A	189.80	02/09/13	2225	N	OTHER NON-COLLISION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3536	025A	189.80	10/18/13	1030	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	SLOWING
3537	025A	189.80	11/07/13	1845	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3538	025A	189.80	01/27/14	1742	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3539	025A	189.80	01/21/15	1148	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3540	025A	189.80	07/27/15	0830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	050	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3541	025A	189.80	10/18/11	0510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3542	025A	189.80	02/20/13	1720	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3543	025A	189.80	02/06/11	0745	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3544	025A	189.80	02/07/12	0348	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3545	025A	189.80	09/12/12	1015	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3546	025A	189.80	10/09/14	1650	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
3547	025A	189.80	01/03/15	1536	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3548	025A	189.80	04/15/12	0244	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	NONE
3549	025A	189.80	09/24/12	0730	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3550	025A	189.80	12/08/13	0830	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
3551	025A	189.81	01/02/11	1833	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3552	025A	189.81	08/10/15	1600	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3553	025A	189.90	06/11/13	1214	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3554	025A	189.90	07/31/15	1840	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3555	025A	189.90	01/28/15	1400	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3556	025A	189.90	04/24/14	0745	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3557	025A	189.90	07/30/14	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3558	025A	189.90	02/03/11	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3559	025A	189.90	11/02/11	1239	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	NONE
3560	025A	189.90	02/11/13	0830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
3561	025A	189.90	04/15/13	1800	INJ	ON	NON-INTERSECTION	9	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3562	025A	189.90	04/22/13	2120	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3563	025A	189.90	11/29/14	2150	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3564	025A	189.90	01/02/11	1830	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3565	025A	189.90	10/26/11	0945	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3566	025A	189.90	04/04/13	1130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3567	025A	189.90	12/26/14	1025	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3568	025A	189.90	01/26/11	0845	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3569	025A	189.90	04/23/15	0639	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3570	025A	189.90	10/26/11	0530	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3571	025A	189.90	04/15/13	1801	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3572	025A	189.90	10/12/13	1856	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3573	025A	189.90	11/04/13	0900	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3574	025A	189.90	04/24/14	0745	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3575	025A	189.93	10/03/12	1531	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3576	025A	189.94	01/11/12	1200	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3577	025A	189.94	03/25/15	1150	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3578	025A	189.96	11/24/15	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3579	025A	189.96	11/24/15	1650	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3580	025A	189.99	12/03/13	0230	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	NONE
3581	025A	190.00	06/25/13	2007	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3582	025A	190.00	08/30/11	0845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3583	025A	190.00	10/04/12	0720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3584	025A	190.00	10/04/12	0720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3585	025A	190.00	01/11/13	1203	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3586	025A	190.00	05/13/14	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3587	025A	190.00	01/14/15	0850	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3588	025A	190.00	06/10/15	0802	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3589	025A	190.00	11/24/15	1813	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3590	025A	190.00	01/07/11	1110	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3591	025A	190.00	05/19/11	1450	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3592	025A	190.00	07/14/11	1710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3593	025A	190.00	11/15/11	1445	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3594	025A	190.00	05/31/14	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3595	025A	190.00	01/25/15	1805	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3596	025A	190.00	03/29/11	1725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3597	025A	190.00	04/23/15	0310	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3598	025A	190.00	04/24/15	0316	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3599	025A	190.00	08/09/12	0523	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3600	025A	190.00	02/14/12	1820	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3541	025A	189.80	10/18/11	0510	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3542	025A	189.80	02/20/13	1720	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	045	WEAVING
3543	025A	189.80	02/06/11	0745	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3544	025A	189.80	02/07/12	0348	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3545	025A	189.80	09/12/12	1015	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3546	025A	189.80	10/09/14	1650	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3547	025A	189.80	01/03/15	1536	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3548	025A	189.80	04/15/12	0244	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
3549	025A	189.80	09/24/12	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3550	025A	189.80	12/08/13	0830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	AVOIDING OBJECT/VEHICLE IN ROAD
3551	025A	189.81	01/02/11	1833	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
3552	025A	189.81	08/10/15	1600	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	OTHER
3553	025A	189.90	06/11/13	1214	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
3554	025A	189.90	07/31/15	1840	N	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	SLOWING
3555	025A	189.90	01/28/15	1400	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	ENTERING/LEAVING PARKED POSITION
3556	025A	189.90	04/24/14	0745	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3557	025A	189.90	07/30/14	0920	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3558	025A	189.90	02/03/11	1415	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3559	025A	189.90	11/02/11	1239	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3560	025A	189.90	02/11/13	0830	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3561	025A	189.90	04/15/13	1800	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
3562	025A	189.90	04/22/13	2120	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	OTHER
3563	025A	189.90	11/29/14	2150	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3564	025A	189.90	01/02/11	1830	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	OTHER
3565	025A	189.90	10/26/11	0945	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3566	025A	189.90	04/04/13	1130	N	CONCRETE BARRIER	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3567	025A	189.90	12/26/14	1025	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3568	025A	189.90	01/26/11	0845	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3569	025A	189.90	04/23/15	0639	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	CHANGING LANES
3570	025A	189.90	10/26/11	0530	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3571	025A	189.90	04/15/13	1801	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3572	025A	189.90	10/12/13	1856	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3573	025A	189.90	11/04/13	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3574	025A	189.90	04/24/14	0745	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3575	025A	189.93	10/03/12	1531	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
3576	025A	189.94	01/11/12	1200	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3577	025A	189.94	03/25/15	1150	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3578	025A	189.96	11/24/15	1650	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	070	GOING STRAIGHT
3579	025A	189.96	11/24/15	1650	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	GOING STRAIGHT
3580	025A	189.99	12/03/13	0230	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3581	025A	190.00	06/25/13	2007	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3582	025A	190.00	08/30/11	0845	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3583	025A	190.00	10/04/12	0720	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3584	025A	190.00	10/04/12	0720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3585	025A	190.00	01/11/13	1203	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
3586	025A	190.00	05/13/14	0724	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
3587	025A	190.00	01/14/15	0850	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3588	025A	190.00	06/10/15	0802	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	010	CHANGING LANES
3589	025A	190.00	11/24/15	1813	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	005	BACKING
3590	025A	190.00	01/07/11	1110	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
3591	025A	190.00	05/19/11	1450	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3592	025A	190.00	07/14/11	1710	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	WEAVING
3593	025A	190.00	11/15/11	1445	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3594	025A	190.00	05/31/14	1200	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
3595	025A	190.00	01/25/15	1805	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	010	GOING STRAIGHT
3596	025A	190.00	03/29/11	1725	N	PARKED MOTOR VEHICLE	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3597	025A	190.00	04/23/15	0310	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3598	025A	190.00	04/24/15	0316	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3599	025A	190.00	08/09/12	0523	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	WEAVING
3600	025A	190.00	02/14/12	1820	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3601	025A	190.00	04/09/13	0746	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
3602	025A	190.00	08/03/13	2020	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3603	025A	190.00	07/16/14	2110	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3604	025A	190.00	12/25/14	2050	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3605	025A	190.00	06/09/14	1144	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3606	025A	190.00	03/25/15	1140	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3607	025A	190.00	06/29/15	0830	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3608	025A	190.00	07/30/15	0852	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3609	025A	190.05	07/21/11	1515	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3610	025A	190.08	02/20/13	1657	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3611	025A	190.09	10/05/15	0750	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	FOG
3612	025A	190.10	08/23/11	0715	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3613	025A	190.10	03/05/14	1000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3614	025A	190.10	12/25/14	1830	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3615	025A	190.10	02/20/13	2000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3616	025A	190.10	07/31/13	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3617	025A	190.10	11/25/15	0920	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3618	025A	190.10	11/22/11	0020	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3619	025A	190.10	03/02/12	1840	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3620	025A	190.10	04/29/12	0645	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
3621	025A	190.10	08/16/12	1214	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3622	025A	190.10	11/17/12	1735	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3623	025A	190.10	01/03/15	1424	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DAYLIGHT	NONE
3624	025A	190.10	06/19/12	0720	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3625	025A	190.10	06/05/13	0730	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3626	025A	190.10	12/08/13	0825	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3627	025A	190.10	10/07/15	0715	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3628	025A	190.20	04/28/11	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3629	025A	190.20	06/19/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3630	025A	190.20	12/03/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3631	025A	190.20	02/26/13	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3632	025A	190.20	08/29/13	0738	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3633	025A	190.20	07/03/14	1726	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3634	025A	190.20	07/24/14	2105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3635	025A	190.20	07/30/15	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3636	025A	190.20	11/22/11	0015	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3637	025A	190.20	12/09/11	0930	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3638	025A	190.20	06/20/12	0820	INJ	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3639	025A	190.20	02/16/15	1015	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3640	025A	190.25	11/13/12	0855	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3641	025A	190.25	11/28/12	0419	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3642	025A	190.25	12/04/13	1155	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
3643	025A	190.25	09/24/14	0710	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3644	025A	190.30	04/11/11	0800	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3645	025A	190.30	10/14/11	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3646	025A	190.30	04/23/12	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3647	025A	190.30	08/29/13	0650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3648	025A	190.30	05/22/14	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3649	025A	190.30	10/20/14	0730	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3650	025A	190.30	04/15/15	0725	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3651	025A	190.30	02/20/11	2328	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	FOG
3652	025A	190.30	07/19/13	2105	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3653	025A	190.30	05/22/14	0800	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3654	025A	190.30	10/08/11	0910	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	FOG
3655	025A	190.30	01/26/15	0645	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
3656	025A	190.30	07/25/15	1445	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3657	025A	190.30	07/28/15	0650	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3658	025A	190.35	05/08/15	1000	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3659	025A	190.35	10/20/15	0550	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3660	025A	190.40	10/05/11	0710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3601	025A	190.00	04/09/13	0746	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	OTHER
3602	025A	190.00	08/03/13	2020	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3603	025A	190.00	07/16/14	2110	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3604	025A	190.00	12/25/14	2050	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
3605	025A	190.00	06/09/14	1144	N	INVOLVING OTHER OBJECT	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
3606	025A	190.00	03/25/15	1140	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	AVOIDING OBJECT/VEHICLE IN ROAD
3607	025A	190.00	06/29/15	0830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3608	025A	190.00	07/30/15	0852	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3609	025A	190.05	07/21/11	1515	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
3610	025A	190.08	02/20/13	1657	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
3611	025A	190.09	10/05/15	0750	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3612	025A	190.10	08/23/11	0715	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3613	025A	190.10	03/05/14	1000	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3614	025A	190.10	12/25/14	1830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	OTHER
3615	025A	190.10	02/20/13	2000	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3616	025A	190.10	07/31/13	1245	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
3617	025A	190.10	11/25/15	0920	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3618	025A	190.10	11/22/11	0020	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3619	025A	190.10	03/02/12	1840	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3620	025A	190.10	04/29/12	0645	N	GUARD RAIL	N	SUV	RX/MEDICATION/DR	ASLEEP AT WHEEL	075	OTHER
3621	025A	190.10	08/16/12	1214	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	AVOIDING OBJECT/VEHICLE IN ROAD
3622	025A	190.10	11/17/12	1735	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3623	025A	190.10	01/03/15	1424	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3624	025A	190.10	06/19/12	0720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	SLOWING
3625	025A	190.10	06/05/13	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3626	025A	190.10	12/08/13	0825	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
3627	025A	190.10	10/07/15	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
3628	025A	190.20	04/28/11	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3629	025A	190.20	06/19/12	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
3630	025A	190.20	12/03/12	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
3631	025A	190.20	02/26/13	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3632	025A	190.20	08/29/13	0738	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
3633	025A	190.20	07/03/14	1726	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3634	025A	190.20	07/24/14	2105	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
3635	025A	190.20	07/30/15	1250	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
3636	025A	190.20	11/22/11	0015	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3637	025A	190.20	12/09/11	0930	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3638	025A	190.20	06/20/12	0820	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3639	025A	190.20	02/16/15	1015	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3640	025A	190.25	11/13/12	0855	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3641	025A	190.25	11/28/12	0419	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3642	025A	190.25	12/04/13	1155	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	CHANGING LANES
3643	025A	190.25	09/24/14	0710	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
3644	025A	190.30	04/11/11	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3645	025A	190.30	10/14/11	1425	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3646	025A	190.30	04/23/12	0735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3647	025A	190.30	08/29/13	0650	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	SLOWING
3648	025A	190.30	05/22/14	0815	N	REAR-END	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3649	025A	190.30	10/20/14	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3650	025A	190.30	04/15/15	0725	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
3651	025A	190.30	02/20/11	2328	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3652	025A	190.30	07/19/13	2105	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	OTHER
3653	025A	190.30	05/22/14	0800	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
3654	025A	190.30	10/08/11	0910	N	REAR-END	N	NON-SCHOOL BUS < 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3655	025A	190.30	01/26/15	0645	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3656	025A	190.30	07/25/15	1445	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3657	025A	190.30	07/28/15	0650	N	REAR-END	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
3658	025A	190.35	05/08/15	1000	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3659	025A	190.35	10/20/15	0550	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	SLOWING
3660	025A	190.40	10/05/11	0710	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3661	025A	190.40	05/27/14	0650	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3662	025A	190.40	08/02/11	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3663	025A	190.40	03/30/12	1458	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3664	025A	190.40	01/09/13	0745	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3665	025A	190.40	08/20/13	1110	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3666	025A	190.40	09/18/13	0740	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3667	025A	190.40	08/16/14	1340	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3668	025A	190.40	06/02/15	0720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3669	025A	190.40	06/18/13	1147	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3670	025A	190.40	09/18/13	2129	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3671	025A	190.40	11/11/13	2025	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3672	025A	190.40	02/15/11	0705	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3673	025A	190.40	07/29/15	0910	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3674	025A	190.42	11/12/13	0735	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3675	025A	190.43	03/14/15	1300	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3676	025A	190.46	10/07/15	0700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3677	025A	190.48	06/02/15	0755	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3678	025A	190.49	10/11/14	0958	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3679	025A	190.50	03/04/13	0615	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3680	025A	190.50	03/03/11	0802	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3681	025A	190.50	05/18/11	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3682	025A	190.50	05/06/12	0030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3683	025A	190.50	08/07/12	0710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3684	025A	190.50	05/09/13	1920	INJ	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
3685	025A	190.50	09/27/13	1634	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3686	025A	190.50	12/10/13	0820	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3687	025A	190.50	06/20/14	1135	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3688	025A	190.50	09/02/14	1015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3689	025A	190.50	04/18/11	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3690	025A	190.50	12/19/12	2055	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3691	025A	190.50	03/12/13	0459	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	RAIN
3692	025A	190.50	07/12/13	1555	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3693	025A	190.50	04/10/14	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3694	025A	190.50	01/29/15	0615	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3695	025A	190.50	11/09/13	2210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3696	025A	190.50	07/24/11	0400	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3697	025A	190.50	02/15/11	0822	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3698	025A	190.50	09/22/12	1625	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3699	025A	190.50	03/09/13	1745	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3700	025A	190.50	03/09/13	1846	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3701	025A	190.50	10/09/14	2230	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3702	025A	190.50	01/01/15	1550	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3703	025A	190.50	01/30/14	1850	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3704	025A	190.50	10/03/13	0515	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3705	025A	190.50	06/24/11	2035	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
3706	025A	190.50	10/08/11	1000	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3707	025A	190.50	02/23/12	1105	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
3708	025A	190.50	09/04/12	0850	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3709	025A	190.50	11/13/12	0830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3710	025A	190.50	09/19/14	0750	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3711	025A	190.50	06/22/15	2347	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3712	025A	190.55	09/24/12	0815	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3713	025A	190.56	11/22/13	1410	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3714	025A	190.58	02/09/13	1847	FAT	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3715	025A	190.60	08/06/13	0800	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3716	025A	190.60	08/20/13	1130	INJ	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3717	025A	190.60	04/21/11	0915	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3718	025A	190.60	08/02/12	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3719	025A	190.60	12/02/13	0750	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3720	025A	190.60	01/23/14	0840	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3661	025A	190.40	05/27/14	0650	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3662	025A	190.40	08/02/11	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
3663	025A	190.40	03/30/12	1458	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	GOING STRAIGHT
3664	025A	190.40	01/09/13	0745	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3665	025A	190.40	08/20/13	1110	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3666	025A	190.40	09/18/13	0740	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3667	025A	190.40	08/16/14	1340	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
3668	025A	190.40	06/02/15	0720	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3669	025A	190.40	06/18/13	1147	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3670	025A	190.40	09/18/13	2129	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
3671	025A	190.40	11/11/13	2025	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3672	025A	190.40	02/15/11	0705	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
3673	025A	190.40	07/29/15	0910	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3674	025A	190.42	11/12/13	0735	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3675	025A	190.43	03/14/15	1300	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3676	025A	190.46	10/07/15	0700	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3677	025A	190.48	06/02/15	0755	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3678	025A	190.49	10/11/14	0958	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3679	025A	190.50	03/04/13	0615	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3680	025A	190.50	03/03/11	0802	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
3681	025A	190.50	05/18/11	0745	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3682	025A	190.50	05/06/12	0030	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3683	025A	190.50	08/07/12	0710	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3684	025A	190.50	05/09/13	1920	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3685	025A	190.50	09/27/13	1634	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3686	025A	190.50	12/10/13	0820	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3687	025A	190.50	06/20/14	1135	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
3688	025A	190.50	09/02/14	1015	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
3689	025A	190.50	04/18/11	1245	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	WEAVING
3690	025A	190.50	12/19/12	2055	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3691	025A	190.50	03/12/13	0459	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	055	WEAVING
3692	025A	190.50	07/12/13	1555	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	WEAVING
3693	025A	190.50	04/10/14	0800	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
3694	025A	190.50	01/29/15	0615	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3695	025A	190.50	11/09/13	2210	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3696	025A	190.50	07/24/11	0400	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3697	025A	190.50	02/15/11	0822	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
3698	025A	190.50	09/22/12	1625	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3699	025A	190.50	03/09/13	1745	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3700	025A	190.50	03/09/13	1846	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3701	025A	190.50	10/09/14	2230	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	080	OTHER
3702	025A	190.50	01/01/15	1550	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3703	025A	190.50	01/30/14	1850	N	TREE/SHRUBBERY	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3704	025A	190.50	10/03/13	0515	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3705	025A	190.50	06/24/11	2035	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	AVOIDING OBJECT/VEHICLE IN ROAD
3706	025A	190.50	10/08/11	1000	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
3707	025A	190.50	02/23/12	1105	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3708	025A	190.50	09/04/12	0850	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	CHANGING LANES
3709	025A	190.50	11/13/12	0830	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3710	025A	190.50	09/19/14	0750	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
3711	025A	190.50	06/22/15	2347	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	GOING STRAIGHT
3712	025A	190.55	09/24/12	0815	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3713	025A	190.56	11/22/13	1410	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	063	CHANGING LANES
3714	025A	190.58	02/09/13	1847	N	OVERTURNING	N	SUV	ALCOHOL/DRUGS	UNKNOWN	095	OTHER
3715	025A	190.60	08/06/13	0800	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	070	CHANGING LANES
3716	025A	190.60	08/20/13	1130	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
3717	025A	190.60	04/21/11	0915	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	SLOWING
3718	025A	190.60	08/02/12	0750	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
3719	025A	190.60	12/02/13	0750	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
3720	025A	190.60	01/23/14	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3721	025A	190.60	09/18/14	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3722	025A	190.60	10/23/14	0740	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3723	025A	190.60	01/10/11	1000	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE
3724	025A	190.60	10/08/11	0930	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3725	025A	190.60	03/04/13	1645	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
3726	025A	190.60	09/12/12	0630	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAWN OR DUSK	RAIN
3727	025A	190.60	09/12/12	0900	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
3728	025A	190.60	08/17/14	2200	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3729	025A	190.60	02/03/11	1730	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3730	025A	190.60	02/23/12	1050	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
3731	025A	190.60	09/12/12	0720	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
3732	025A	190.60	07/12/13	1500	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3733	025A	190.60	08/29/14	1215	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3734	025A	190.62	08/31/14	0238	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3735	025A	190.62	05/07/15	2125	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3736	025A	190.70	10/22/14	0650	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
3737	025A	190.70	05/29/15	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3738	025A	190.70	10/11/11	0510	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3739	025A	190.70	10/11/11	0510	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3740	025A	190.70	01/04/14	0610	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	UNKNOWN	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3741	025A	190.70	10/08/11	0510	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	RAIN
3742	025A	190.70	01/08/12	0645	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	NONE
3743	025A	190.70	09/12/12	0915	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3744	025A	190.70	03/09/13	0744	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3745	025A	190.70	01/01/15	1436	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3746	025A	190.70	03/07/15	1535	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3747	025A	190.70	07/02/15	2030	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3748	025A	190.74	04/03/14	1058	PDO	OFF LEFT	NON-INTERSECTION	3	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
3749	025A	190.75	07/24/14	1935	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3750	025A	190.79	01/10/12	0710	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3751	025A	190.80	07/18/13	1050	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3752	025A	190.80	10/05/13	2310	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3753	025A	190.80	12/03/13	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3754	025A	190.80	07/22/11	0900	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3755	025A	190.80	10/07/11	1353	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3756	025A	190.80	03/31/14	1245	INJ	ON	NON-INTERSECTION	5	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3757	025A	190.80	08/12/14	1045	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3758	025A	190.80	11/03/14	0640	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
3759	025A	190.80	02/13/15	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3760	025A	190.80	05/07/15	2130	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3761	025A	190.80	05/07/15	2135	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3762	025A	190.80	10/08/11	0930	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3763	025A	190.80	01/17/12	0035	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE
3764	025A	190.80	09/12/12	1025	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3765	025A	190.80	10/23/13	0155	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3766	025A	190.80	07/30/14	0505	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
3767	025A	190.80	04/16/15	1045	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3768	025A	190.80	06/12/15	0830	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3769	025A	190.80	07/09/15	2300	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3770	025A	190.80	12/25/15	1907	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SLUSHY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3771	025A	190.80	11/04/13	0830	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3772	025A	190.80	03/18/15	0745	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3773	025A	190.80	03/18/15	2257	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3774	025A	190.80	05/07/15	2138	INJ	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
3775	025A	190.85	04/23/14	2145	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3776	025A	190.90	06/06/11	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3777	025A	190.90	09/20/11	0725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3778	025A	190.90	06/12/15	1001	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	RAIN
3779	025A	190.90	07/24/15	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3780	025A	190.90	09/28/15	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3721	025A	190.60	09/18/14	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3722	025A	190.60	10/23/14	0740	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	OTHER
3723	025A	190.60	01/10/11	1000	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	050	WEAVING
3724	025A	190.60	10/08/11	0930	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3725	025A	190.60	03/04/13	1645	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3726	025A	190.60	09/12/12	0630	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3727	025A	190.60	09/12/12	0900	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3728	025A	190.60	08/17/14	2200	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	080	GOING STRAIGHT
3729	025A	190.60	02/03/11	1730	N	FENCE	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3730	025A	190.60	02/23/12	1050	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3731	025A	190.60	09/12/12	0720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
3732	025A	190.60	07/12/13	1500	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3733	025A	190.60	08/29/14	1215	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3734	025A	190.62	08/31/14	0238	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	060	GOING STRAIGHT
3735	025A	190.62	05/07/15	2125	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
3736	025A	190.70	10/22/14	0650	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3737	025A	190.70	05/29/15	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
3738	025A	190.70	10/11/11	0510	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3739	025A	190.70	10/11/11	0510	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3740	025A	190.70	01/04/14	0610	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	OTHER
3741	025A	190.70	10/08/11	0510	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3742	025A	190.70	01/08/12	0645	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
3743	025A	190.70	09/12/12	0915	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3744	025A	190.70	03/09/13	0744	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3745	025A	190.70	01/01/15	1436	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
3746	025A	190.70	03/07/15	1535	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	AVOIDING OBJECT/VEHICLE IN ROAD
3747	025A	190.70	07/02/15	2030	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3748	025A	190.74	04/03/14	1058	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3749	025A	190.75	07/24/14	1935	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	OTHER
3750	025A	190.79	01/10/12	0710	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
3751	025A	190.80	07/18/13	1050	N	OTHER NON-COLLISION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3752	025A	190.80	10/05/13	2310	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
3753	025A	190.80	12/03/13	0750	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3754	025A	190.80	07/22/11	0900	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3755	025A	190.80	10/07/11	1353	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	WEAVING
3756	025A	190.80	03/31/14	1245	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3757	025A	190.80	08/12/14	1045	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3758	025A	190.80	11/03/14	0640	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3759	025A	190.80	02/13/15	1750	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	PASSING
3760	025A	190.80	05/07/15	2130	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3761	025A	190.80	05/07/15	2135	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
3762	025A	190.80	10/08/11	0930	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3763	025A	190.80	01/17/12	0035	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3764	025A	190.80	09/12/12	1025	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3765	025A	190.80	10/23/13	0155	N	CONCRETE BARRIER	N	SUV	ALCOHOL	UNKNOWN	060	WEAVING
3766	025A	190.80	07/30/14	0505	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	077	OTHER
3767	025A	190.80	04/16/15	1045	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
3768	025A	190.80	06/12/15	0830	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3769	025A	190.80	07/09/15	2300	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3770	025A	190.80	12/25/15	1907	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
3771	025A	190.80	11/04/13	0830	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3772	025A	190.80	03/18/15	0745	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	080	GOING STRAIGHT
3773	025A	190.80	03/18/15	2257	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3774	025A	190.80	05/07/15	2138	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3775	025A	190.85	04/23/14	2145	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
3776	025A	190.90	06/06/11	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3777	025A	190.90	09/20/11	0725	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3778	025A	190.90	06/12/15	1001	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3779	025A	190.90	07/24/15	1545	N	REAR-END	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	045	GOING STRAIGHT
3780	025A	190.90	09/28/15	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3781	025A	190.90	01/23/14	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
3782	025A	190.90	11/12/12	2220	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3783	025A	190.90	11/10/14	1550	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3784	025A	190.90	07/06/12	2245	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3785	025A	190.90	01/19/14	0505	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3786	025A	190.90	01/23/14	0830	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
3787	025A	190.90	12/14/14	1430	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3788	025A	190.90	02/16/15	1030	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3789	025A	190.90	06/14/12	0725	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3790	025A	190.93	11/12/12	2231	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3791	025A	190.96	01/17/13	1200	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3792	025A	190.96	09/15/13	2327	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3793	025A	190.98	02/20/13	1556	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3794	025A	191.00	08/15/15	1228	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3795	025A	191.00	02/11/11	1830	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3796	025A	191.00	11/09/12	0931	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
3797	025A	191.00	09/17/15	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3798	025A	191.00	01/13/11	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3799	025A	191.00	01/13/11	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3800	025A	191.00	03/29/11	1753	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3801	025A	191.00	05/22/11	1544	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3802	025A	191.00	04/18/12	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3803	025A	191.00	02/20/13	2226	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3804	025A	191.00	05/02/13	0729	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3805	025A	191.00	07/08/13	0756	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3806	025A	191.00	07/12/13	1511	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3807	025A	191.00	07/31/14	0745	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3808	025A	191.00	12/25/14	0030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3809	025A	191.00	08/28/15	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3810	025A	191.00	12/21/15	1600	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3811	025A	191.00	03/28/11	2245	FAT	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3812	025A	191.00	05/22/11	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3813	025A	191.00	04/23/13	0509	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3814	025A	191.00	09/29/13	1856	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3815	025A	191.00	04/23/13	0509	PDO	OFF RIGHT	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3816	025A	191.00	12/01/12	0400	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3817	025A	191.00	01/19/11	1730	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3818	025A	191.00	08/16/13	0056	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3819	025A	191.00	08/20/11	2127	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
3820	025A	191.00	03/13/13	0558	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3821	025A	191.00	11/21/13	0150	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3822	025A	191.00	10/12/13	1710	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3823	025A	191.00	08/08/14	1840	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3824	025A	191.00	07/18/15	1000	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3825	025A	191.00	08/15/12	1146	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3826	025A	191.00	03/05/14	0931	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
3827	025A	191.00	04/09/14	0735	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3828	025A	191.00	06/13/14	0447	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3829	025A	191.02	12/03/11	0305	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
3830	025A	191.06	06/17/12	0300	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3831	025A	191.08	04/26/11	2206	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3832	025A	191.08	01/19/11	2002	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3833	025A	191.08	09/14/11	1824	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
3834	025A	191.08	01/05/14	0911	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
3835	025A	191.09	12/08/11	0448	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3836	025A	191.09	04/04/12	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3837	025A	191.10	06/22/11	0745	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3838	025A	191.20	05/26/11	0738	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3839	025A	191.20	02/05/11	1500	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3840	025A	191.25	06/02/11	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3781	025A	190.90	01/23/14	0910	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3782	025A	190.90	11/12/12	2220	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3783	025A	190.90	11/10/14	1550	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
3784	025A	190.90	07/06/12	2245	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3785	025A	190.90	01/19/14	0505	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	OTHER
3786	025A	190.90	01/23/14	0830	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
3787	025A	190.90	12/14/14	1430	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	CHANGING LANES
3788	025A	190.90	02/16/15	1030	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
3789	025A	190.90	06/14/12	0725	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3790	025A	190.93	11/12/12	2231	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3791	025A	190.96	01/17/13	1200	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	WEAVING
3792	025A	190.96	09/15/13	2327	N	CONCRETE BARRIER	S	SUV	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
3793	025A	190.98	02/20/13	1556	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
3794	025A	191.00	08/15/15	1228	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3795	025A	191.00	02/11/11	1830	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3796	025A	191.00	11/09/12	0931	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3797	025A	191.00	09/17/15	1545	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3798	025A	191.00	01/13/11	0745	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3799	025A	191.00	01/13/11	0750	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3800	025A	191.00	03/29/11	1753	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
3801	025A	191.00	05/22/11	1544	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
3802	025A	191.00	04/18/12	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3803	025A	191.00	02/20/13	2226	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3804	025A	191.00	05/02/13	0729	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
3805	025A	191.00	07/08/13	0756	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
3806	025A	191.00	07/12/13	1511	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
3807	025A	191.00	07/31/14	0745	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
3808	025A	191.00	12/25/14	0030	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	055	OTHER
3809	025A	191.00	08/28/15	1700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3810	025A	191.00	12/21/15	1600	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
3811	025A	191.00	03/28/11	2245	N	OVERTURNING	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3812	025A	191.00	05/22/11	1330	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	025	GOING STRAIGHT
3813	025A	191.00	04/23/13	0509	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
3814	025A	191.00	09/29/13	1856	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3815	025A	191.00	04/23/13	0509	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
3816	025A	191.00	12/01/12	0400	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3817	025A	191.00	01/19/11	1730	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
3818	025A	191.00	08/16/13	0056	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	075	WEAVING
3819	025A	191.00	08/20/11	2127	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	DRIVER INEXPERIENCE	075	OTHER
3820	025A	191.00	03/13/13	0558	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
3821	025A	191.00	11/21/13	0150	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3822	025A	191.00	10/12/13	1710	N	INVOLVING OTHER OBJECT	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
3823	025A	191.00	08/08/14	1840	N	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3824	025A	191.00	07/18/15	1000	N	INVOLVING OTHER OBJECT	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3825	025A	191.00	08/15/12	1146	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	AVOIDING OBJECT/VEHICLE IN ROAD
3826	025A	191.00	03/05/14	0931	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
3827	025A	191.00	04/09/14	0735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
3828	025A	191.00	06/13/14	0447	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3829	025A	191.02	12/03/11	0305	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3830	025A	191.06	06/17/12	0300	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3831	025A	191.08	04/26/11	2206	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
3832	025A	191.08	01/19/11	2002	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
3833	025A	191.08	09/14/11	1824	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
3834	025A	191.08	01/05/14	0911	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
3835	025A	191.09	12/08/11	0448	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3836	025A	191.09	04/04/12	1200	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3837	025A	191.10	06/22/11	0745	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
3838	025A	191.20	05/26/11	0738	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3839	025A	191.20	02/05/11	1500	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
3840	025A	191.25	06/02/11	1450	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3841	025A	191.25	05/19/11	0803	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3842	025A	191.25	07/19/11	0853	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3843	025A	191.25	05/10/12	0316	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3844	025A	191.25	01/30/13	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3845	025A	191.25	06/18/12	0911	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3846	025A	191.30	05/28/15	0930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3847	025A	191.30	08/18/14	0710	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3848	025A	191.30	08/18/14	0807	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3849	025A	191.30	10/16/15	1000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3850	025A	191.30	04/16/15	0750	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
3851	025A	191.35	03/25/15	1153	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3852	025A	191.36	05/22/11	1431	PDO	ON	NON-INTERSECTION	5	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3853	025A	191.41	09/22/13	1241	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3854	025A	191.41	12/14/15	1740	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3855	025A	191.41	03/18/14	1503	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3856	025A	191.47	06/16/14	0747	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3857	025A	191.48	06/24/13	0809	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3858	025A	191.50	04/04/11	0806	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3859	025A	191.50	04/18/11	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3860	025A	191.50	10/07/11	1523	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3861	025A	191.50	06/13/12	0925	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3862	025A	191.50	06/13/12	1204	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3863	025A	191.50	11/06/12	0709	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3864	025A	191.50	04/04/13	0857	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3865	025A	191.50	09/03/13	0815	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3866	025A	191.50	09/03/13	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3867	025A	191.50	09/23/13	2030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3868	025A	191.50	02/13/14	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3869	025A	191.50	02/20/14	0921	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3870	025A	191.50	07/16/14	0834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3871	025A	191.50	09/02/14	0806	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3872	025A	191.50	09/09/14	0745	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3873	025A	191.50	09/29/14	0909	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3874	025A	191.50	01/26/15	0642	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3875	025A	191.50	07/05/15	0036	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3876	025A	191.50	11/20/15	1528	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3877	025A	191.50	03/30/11	1805	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3878	025A	191.50	11/02/11	1215	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
3879	025A	191.50	01/11/12	1920	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3880	025A	191.50	04/03/12	0921	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3881	025A	191.50	09/26/12	2124	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
3882	025A	191.50	09/17/13	1127	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3883	025A	191.50	04/14/14	0557	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3884	025A	191.50	11/12/14	0049	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3885	025A	191.50	06/07/13	0350	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3886	025A	191.50	09/18/14	0228	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3887	025A	191.50	11/05/14	2220	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3888	025A	191.50	09/02/12	0345	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3889	025A	191.50	09/25/12	2218	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3890	025A	191.50	03/22/13	2122	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3891	025A	191.50	08/18/15	2137	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3892	025A	191.50	04/26/11	2239	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3893	025A	191.50	09/12/12	1045	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
3894	025A	191.50	01/30/13	2337	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3895	025A	191.50	02/20/13	2238	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3896	025A	191.50	06/24/14	2005	PDO	OFF LEFT	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3897	025A	191.50	07/14/14	2254	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3898	025A	191.50	11/05/15	0128	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3899	025A	191.50	11/05/15	0109	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3900	025A	191.50	02/17/11	0535	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3841	025A	191.25	05/19/11	0803	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3842	025A	191.25	07/19/11	0853	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3843	025A	191.25	05/10/12	0316	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	CHANGING LANES
3844	025A	191.25	01/30/13	0920	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
3845	025A	191.25	06/18/12	0911	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3846	025A	191.30	05/28/15	0930	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3847	025A	191.30	08/18/14	0710	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3848	025A	191.30	08/18/14	0807	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3849	025A	191.30	10/16/15	1000	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	CHANGING LANES
3850	025A	191.30	04/16/15	0750	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
3851	025A	191.35	03/25/15	1153	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	CHANGING LANES
3852	025A	191.36	05/22/11	1431	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
3853	025A	191.41	09/22/13	1241	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
3854	025A	191.41	12/14/15	1740	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
3855	025A	191.41	03/18/14	1503	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
3856	025A	191.47	06/16/14	0747	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3857	025A	191.48	06/24/13	0809	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
3858	025A	191.50	04/04/11	0806	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3859	025A	191.50	04/18/11	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
3860	025A	191.50	10/07/11	1523	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
3861	025A	191.50	06/13/12	0925	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3862	025A	191.50	06/13/12	1204	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3863	025A	191.50	11/06/12	0709	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3864	025A	191.50	04/04/13	0857	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
3865	025A	191.50	09/03/13	0815	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3866	025A	191.50	09/03/13	0850	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3867	025A	191.50	09/23/13	2030	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
3868	025A	191.50	02/13/14	0700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
3869	025A	191.50	02/20/14	0921	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
3870	025A	191.50	07/16/14	0834	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	SLOWING
3871	025A	191.50	09/02/14	0806	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
3872	025A	191.50	09/09/14	0745	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
3873	025A	191.50	09/29/14	0909	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3874	025A	191.50	01/26/15	0642	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3875	025A	191.50	07/05/15	0036	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
3876	025A	191.50	11/20/15	1528	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	SLOWING
3877	025A	191.50	03/30/11	1805	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	090	WEAVING
3878	025A	191.50	11/02/11	1215	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	OTHER
3879	025A	191.50	01/11/12	1920	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
3880	025A	191.50	04/03/12	0921	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	PASSING
3881	025A	191.50	09/26/12	2124	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3882	025A	191.50	09/17/13	1127	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3883	025A	191.50	04/14/14	0557	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	OTHER
3884	025A	191.50	11/12/14	0049	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	EVADING LAW ENFORCEMENT OFFICER	070	OTHER
3885	025A	191.50	06/07/13	0350	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3886	025A	191.50	09/18/14	0228	N	WILD ANIMAL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3887	025A	191.50	11/05/14	2220	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3888	025A	191.50	09/02/12	0345	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	SLOWING
3889	025A	191.50	09/25/12	2218	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
3890	025A	191.50	03/22/13	2122	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	UNKNOWN	040	OTHER
3891	025A	191.50	08/18/15	2137	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	085	OTHER
3892	025A	191.50	04/26/11	2239	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3893	025A	191.50	09/12/12	1045	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3894	025A	191.50	01/30/13	2337	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
3895	025A	191.50	02/20/13	2238	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
3896	025A	191.50	06/24/14	2005	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	UNKNOWN	060	OTHER
3897	025A	191.50	07/14/14	2254	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	UNKNOWN	060	OTHER
3898	025A	191.50	11/05/15	0128	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
3899	025A	191.50	11/05/15	0109	N	EMBANKMENT CUT/FILL SLOPE	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3900	025A	191.50	02/17/11	0535	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	075	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3901	025A	191.50	11/26/13	0305	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3902	025A	191.50	07/31/15	2257	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3903	025A	191.50	07/07/12	1539	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3904	025A	191.50	12/11/12	0557	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
3905	025A	191.50	07/08/14	0835	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3906	025A	191.50	07/10/14	0733	INJ	ON	NON-INTERSECTION	6	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3907	025A	191.50	03/25/15	1126	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
3908	025A	191.50	07/28/15	0830	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3909	025A	191.50	10/26/15	0654	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
3910	025A	191.52	09/25/13	0800	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3911	025A	191.53	06/13/12	0926	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3912	025A	191.54	11/09/13	1300	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3913	025A	191.54	06/13/12	0926	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3914	025A	191.58	02/01/13	2355	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3915	025A	191.58	11/05/15	1220	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3916	025A	191.58	02/25/11	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3917	025A	191.58	05/30/13	0830	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3918	025A	191.58	06/25/14	0834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3919	025A	191.58	11/06/14	0905	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
3920	025A	191.58	12/28/14	1338	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3921	025A	191.58	09/03/15	0645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3922	025A	191.58	09/26/14	2150	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3923	025A	191.58	12/09/12	0830	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	WIND
3924	025A	191.58	01/31/14	2317	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3925	025A	191.58	02/21/13	2226	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3926	025A	191.58	08/24/13	1333	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3927	025A	191.58	09/02/14	0912	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3928	025A	191.58	11/21/15	1057	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
3929	025A	191.60	06/02/14	2103	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3930	025A	191.61	12/22/14	0809	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
3931	025A	191.66	02/01/14	1741	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
3932	025A	191.69	05/22/11	1206	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3933	025A	191.69	09/12/12	0838	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
3934	025A	191.69	06/22/14	1900	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3935	025A	191.69	07/19/15	0119	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3936	025A	191.70	12/20/13	1028	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3937	025A	191.70	08/25/15	0719	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3938	025A	191.70	07/21/12	1615	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3939	025A	191.75	07/25/15	1423	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
3940	025A	191.75	01/12/11	0835	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3941	025A	191.75	09/27/11	0707	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3942	025A	191.75	01/07/11	1628	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3943	025A	191.75	12/21/15	0053	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3944	025A	191.75	01/29/13	2059	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
3945	025A	191.75	10/03/13	1742	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3946	025A	191.75	12/23/14	1242	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3947	025A	191.76	06/21/15	0419	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3948	025A	191.80	04/17/15	1150	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
3949	025A	191.80	11/02/12	0330	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3950	025A	191.80	06/30/13	2017	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-UNLIGHTED	RAIN
3951	025A	191.82	08/21/12	0709	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3952	025A	191.83	05/02/15	0240	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3953	025A	191.83	02/17/11	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3954	025A	191.83	10/25/11	0703	INJ	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3955	025A	191.83	04/04/12	0706	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3956	025A	191.83	07/18/12	0758	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3957	025A	191.83	07/25/12	0818	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3958	025A	191.83	08/28/13	0740	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3959	025A	191.83	04/28/15	0659	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3960	025A	191.83	12/25/15	2045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3901	025A	191.50	11/26/13	0305	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3902	025A	191.50	07/31/15	2257	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
3903	025A	191.50	07/07/12	1539	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
3904	025A	191.50	12/11/12	0557	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
3905	025A	191.50	07/08/14	0835	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
3906	025A	191.50	07/10/14	0733	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	060	SLOWING
3907	025A	191.50	03/25/15	1126	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
3908	025A	191.50	07/28/15	0830	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
3909	025A	191.50	10/26/15	0654	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
3910	025A	191.52	09/25/13	0800	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
3911	025A	191.53	06/13/12	0926	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
3912	025A	191.54	11/09/13	1300	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
3913	025A	191.54	06/13/12	0926	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	045	SLOWING
3914	025A	191.58	02/01/13	2355	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	090	CHANGING LANES
3915	025A	191.58	11/05/15	1220	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
3916	025A	191.58	02/25/11	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	003	GOING STRAIGHT
3917	025A	191.58	05/30/13	0830	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
3918	025A	191.58	06/25/14	0834	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3919	025A	191.58	11/06/14	0905	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
3920	025A	191.58	12/28/14	1338	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3921	025A	191.58	09/03/15	0645	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
3922	025A	191.58	09/26/14	2150	N	PARKED MOTOR VEHICLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3923	025A	191.58	12/09/12	0830	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
3924	025A	191.58	01/31/14	2317	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
3925	025A	191.58	02/21/13	2226	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3926	025A	191.58	08/24/13	1333	N	BARRICADE/TRAFFIC BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3927	025A	191.58	09/02/14	0912	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
3928	025A	191.58	11/21/15	1057	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	STOPPED IN TRAFFIC
3929	025A	191.60	06/02/14	2103	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3930	025A	191.61	12/22/14	0809	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
3931	025A	191.66	02/01/14	1741	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
3932	025A	191.69	05/22/11	1206	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	070	OTHER
3933	025A	191.69	09/12/12	0838	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	005	PASSING
3934	025A	191.69	06/22/14	1900	N	CONCRETE BARRIER	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
3935	025A	191.69	07/19/15	0119	N	CRASH CUSHION	N	SUV	ALCOHOL/DRUGS	UNKNOWN	090	GOING STRAIGHT
3936	025A	191.70	12/20/13	1028	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	CHANGING LANES
3937	025A	191.70	08/25/15	0719	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
3938	025A	191.70	07/21/12	1615	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
3939	025A	191.75	07/25/15	1423	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	070	AVOIDING OBJECT/VEHICLE IN ROAD
3940	025A	191.75	01/12/11	0835	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
3941	025A	191.75	09/27/11	0707	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
3942	025A	191.75	01/07/11	1628	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
3943	025A	191.75	12/21/15	0053	N	DELINEATOR POST	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
3944	025A	191.75	01/29/13	2059	N	HEAD-ON	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
3945	025A	191.75	10/03/13	1742	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
3946	025A	191.75	12/23/14	1242	Y (B)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
3947	025A	191.76	06/21/15	0419	N	INVOLVING OTHER OBJECT	N	SUV	ALCOHOL	UNKNOWN	055	GOING STRAIGHT
3948	025A	191.80	04/17/15	1150	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3949	025A	191.80	11/02/12	0330	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3950	025A	191.80	06/30/13	2017	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
3951	025A	191.82	08/21/12	0709	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	SLOWING
3952	025A	191.83	05/02/15	0240	N	OVERTURNING	N	SUV	ALCOHOL	UNKNOWN	055	AVOIDING OBJECT/VEHICLE IN ROAD
3953	025A	191.83	02/17/11	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
3954	025A	191.83	10/25/11	0703	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
3955	025A	191.83	04/04/12	0706	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3956	025A	191.83	07/18/12	0758	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
3957	025A	191.83	07/25/12	0818	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
3958	025A	191.83	08/28/13	0740	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
3959	025A	191.83	04/28/15	0659	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
3960	025A	191.83	12/25/15	2045	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
3961	025A	191.83	03/14/12	0554	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3962	025A	191.83	09/10/15	1343	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3963	025A	191.83	09/10/15	0255	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3964	025A	191.83	08/16/14	2109	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
3965	025A	191.83	02/13/15	1707	INJ	OFF LEFT	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3966	025A	191.83	09/01/15	1125	INJ	OFF RIGHT	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3967	025A	191.83	12/25/15	2045	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
3968	025A	191.83	12/20/14	1344	PDO	ON	NON-INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
3969	025A	191.87	05/29/12	0706	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3970	025A	191.88	02/03/11	0717	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3971	025A	191.88	07/26/11	0234	INJ	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3972	025A	191.89	10/20/15	0652	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3973	025A	191.91	01/08/14	0756	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3974	025A	191.91	03/01/14	2258	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3975	025A	191.98	10/14/11	0700	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
3976	025A	191.98	12/08/14	0754	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3977	025A	191.98	02/06/11	0940	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	NONE
3978	025A	191.98	11/28/14	1731	PDO	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
3979	025A	191.99	06/24/13	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3980	025A	191.99	02/24/12	0810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3981	025A	191.99	11/13/14	1432	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3982	025A	191.99	12/23/13	0604	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	UNKNOWN	DARK-LIGHTED	NONE
3983	025A	192.00	05/07/13	0637	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3984	025A	192.00	09/23/13	0703	PDO	ON	NON-INTERSECTION	4	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
3985	025A	192.00	10/25/13	1920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3986	025A	192.00	10/26/15	0545	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
3987	025A	192.00	01/05/11	0826	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
3988	025A	192.00	09/04/12	1511	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3989	025A	192.00	09/24/12	0737	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3990	025A	192.00	07/15/13	0655	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3991	025A	192.00	09/24/13	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3992	025A	192.00	09/26/13	1546	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3993	025A	192.00	05/07/14	0942	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3994	025A	192.00	08/27/14	0659	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
3995	025A	192.00	09/22/14	0917	PDO	ON	NON-INTERSECTION	2	HILLCREST	WET	DAYLIGHT	RAIN
3996	025A	192.00	05/30/15	0933	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3997	025A	192.00	11/04/15	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3998	025A	192.00	04/28/14	1218	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
3999	025A	192.00	05/11/14	1526	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4000	025A	192.00	08/27/15	1050	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4001	025A	192.00	01/06/12	0944	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4002	025A	192.00	09/22/12	0225	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4003	025A	192.00	05/19/12	0621	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4004	025A	192.00	12/05/11	0404	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4005	025A	192.00	08/10/13	1934	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4006	025A	192.00	11/07/13	1728	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4007	025A	192.00	06/07/14	1315	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4008	025A	192.00	02/16/15	1712	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4009	025A	192.00	06/24/15	1647	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4010	025A	192.00	02/24/13	0956	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4011	025A	192.00	02/16/15	1050	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4012	025A	192.00	06/17/15	2349	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4013	025A	192.00	08/14/13	2056	INJ	OFF RIGHT	RAMP	5	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4014	025A	192.01	02/11/11	1643	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4015	025A	192.02	02/07/12	2214	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4016	025A	192.02	02/24/13	1134	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4017	025A	192.03	02/10/12	0724	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4018	025A	192.03	11/27/12	1517	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4019	025A	192.04	03/25/15	1157	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4020	025A	192.04	04/11/15	1229	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
3961	025A	191.83	03/14/12	0554	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
3962	025A	191.83	09/10/15	1343	N	SIDESWIPE SAME DIRECTION	N	SUV W/TRAILER	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	070	CHANGING LANES
3963	025A	191.83	09/10/15	0255	N	WILD ANIMAL	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	062	GOING STRAIGHT
3964	025A	191.83	08/16/14	2109	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
3965	025A	191.83	02/13/15	1707	N	CONCRETE BARRIER	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	075	OTHER
3966	025A	191.83	09/01/15	1125	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	OTHER
3967	025A	191.83	12/25/15	2045	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
3968	025A	191.83	12/20/14	1344	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
3969	025A	191.87	05/29/12	0706	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	CHANGING LANES
3970	025A	191.88	02/03/11	0717	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3971	025A	191.88	07/26/11	0234	Y (B)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	065	OTHER
3972	025A	191.89	10/20/15	0652	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
3973	025A	191.91	01/08/14	0756	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
3974	025A	191.91	03/01/14	2258	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
3975	025A	191.98	10/14/11	0700	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ILLNESS	030	GOING STRAIGHT
3976	025A	191.98	12/08/14	0754	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
3977	025A	191.98	02/06/11	0940	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
3978	025A	191.98	11/28/14	1731	Y (E)	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
3979	025A	191.99	06/24/13	0850	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
3980	025A	191.99	02/24/12	0810	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
3981	025A	191.99	11/13/14	1432	N	GUARD RAIL	N	SUV	ALCOHOL/DRUGS	UNKNOWN	005	ENTERING/LEAVING PARKED POSITION
3982	025A	191.99	12/23/13	0604	N	FENCE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
3983	025A	192.00	05/07/13	0637	N	OVERTURNING	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
3984	025A	192.00	09/23/13	0703	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
3985	025A	192.00	10/25/13	1920	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
3986	025A	192.00	10/26/15	0545	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
3987	025A	192.00	01/05/11	0826	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
3988	025A	192.00	09/04/12	1511	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
3989	025A	192.00	09/24/12	0737	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
3990	025A	192.00	07/15/13	0655	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
3991	025A	192.00	09/24/13	1645	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
3992	025A	192.00	09/26/13	1546	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
3993	025A	192.00	05/07/14	0942	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
3994	025A	192.00	08/27/14	0659	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
3995	025A	192.00	09/22/14	0917	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
3996	025A	192.00	05/30/15	0933	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
3997	025A	192.00	11/04/15	0840	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
3998	025A	192.00	04/28/14	1218	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
3999	025A	192.00	05/11/14	1526	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
4000	025A	192.00	08/27/15	1050	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	060	PASSING
4001	025A	192.00	01/06/12	0944	N	WILD ANIMAL	S	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4002	025A	192.00	09/22/12	0225	N	SIGN	N	SUV	ALCOHOL	UNKNOWN	050	GOING STRAIGHT
4003	025A	192.00	05/19/12	0621	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
4004	025A	192.00	12/05/11	0404	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
4005	025A	192.00	08/10/13	1934	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
4006	025A	192.00	11/07/13	1728	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	WEAVING
4007	025A	192.00	06/07/14	1315	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	065	OTHER
4008	025A	192.00	02/16/15	1712	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4009	025A	192.00	06/24/15	1647	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4010	025A	192.00	02/24/13	0956	N	WALL/BUILDING	S	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
4011	025A	192.00	02/16/15	1050	N	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
4012	025A	192.00	06/17/15	2349	N	CRASH CUSHION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
4013	025A	192.00	08/14/13	2056	Y (E)	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
4014	025A	192.01	02/11/11	1643	Y (B)	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	PHYSICAL DISABILITY	050	OTHER
4015	025A	192.02	02/07/12	2214	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	CHANGING LANES
4016	025A	192.02	02/24/13	1134	N	SIDESWIPE OPPOSITE DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
4017	025A	192.03	02/10/12	0724	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4018	025A	192.03	11/27/12	1517	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
4019	025A	192.04	03/25/15	1157	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	SLOWING
4020	025A	192.04	04/11/15	1229	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4021	025A	192.05	08/16/13	0658	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4022	025A	192.05	11/06/12	0854	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4023	025A	192.06	12/03/11	0636	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4024	025A	192.06	04/30/14	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4025	025A	192.06	11/30/11	0612	PDO	OFF LEFT	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4026	025A	192.06	02/03/12	2335	PDO	OFF LEFT	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4027	025A	192.06	02/09/15	0723	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4028	025A	192.06	01/02/14	0023	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	NONE
4029	025A	192.06	04/23/13	0733	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4030	025A	192.07	11/21/15	1234	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4031	025A	192.07	01/16/13	1658	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4032	025A	192.07	05/15/14	1836	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4033	025A	192.07	02/16/11	1418	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4034	025A	192.08	02/09/11	0538	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
4035	025A	192.08	03/05/14	0459	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	NONE
4036	025A	192.08	03/23/11	0554	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4037	025A	192.08	04/16/13	1113	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4038	025A	192.08	11/07/14	2351	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	NONE
4039	025A	192.08	11/26/14	1442	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4040	025A	192.08	03/31/15	1834	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4041	025A	192.08	06/08/12	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4042	025A	192.08	02/20/13	1617	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4043	025A	192.08	02/22/14	1054	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4044	025A	192.08	02/22/14	1054	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4045	025A	192.08	02/22/14	1100	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4046	025A	192.08	03/21/14	1538	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4047	025A	192.08	05/26/11	0915	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4048	025A	192.08	08/29/11	0704	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4049	025A	192.08	11/07/11	0839	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4050	025A	192.08	01/03/12	0727	PDO	ON	NON-INTERSECTION	2	UNKNOWN	UNKNOWN	DAYLIGHT	NONE
4051	025A	192.08	01/26/12	0659	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4052	025A	192.08	04/20/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4053	025A	192.08	09/26/12	1340	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4054	025A	192.08	12/19/12	1024	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4055	025A	192.08	04/08/13	0713	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4056	025A	192.08	04/09/13	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4057	025A	192.08	05/09/13	0822	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4058	025A	192.08	08/24/13	1453	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4059	025A	192.08	08/28/13	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4060	025A	192.08	09/05/13	0716	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4061	025A	192.08	09/23/13	0706	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4062	025A	192.08	10/16/13	1802	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4063	025A	192.08	10/16/13	1833	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4064	025A	192.08	04/13/14	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4065	025A	192.08	05/16/14	0732	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4066	025A	192.08	06/04/14	1847	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4067	025A	192.08	06/20/14	1156	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4068	025A	192.08	08/13/14	0841	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4069	025A	192.08	08/14/14	0902	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4070	025A	192.08	09/12/14	0801	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
4071	025A	192.08	10/01/14	0647	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4072	025A	192.08	10/11/14	0920	INJ	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4073	025A	192.08	10/11/14	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4074	025A	192.08	10/28/14	0635	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4075	025A	192.08	10/28/14	0647	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4076	025A	192.08	10/30/14	1603	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4077	025A	192.08	12/27/14	1449	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4078	025A	192.08	01/08/15	1459	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4079	025A	192.08	03/14/15	1805	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4080	025A	192.08	03/20/15	0832	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4021	025A	192.05	08/16/13	0658	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	SLOWING
4022	025A	192.05	11/06/12	0854	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
4023	025A	192.06	12/03/11	0636	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4024	025A	192.06	04/30/14	1550	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
4025	025A	192.06	11/30/11	0612	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4026	025A	192.06	02/03/12	2335	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
4027	025A	192.06	02/09/15	0723	Y (B)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	010	CHANGING LANES
4028	025A	192.06	01/02/14	0023	Y (B)	CULVERT/HEADWALL	N	PASS CAR/VAN	ALCOHOL	EVADING LAW ENFORCEMENT OFFICER	065	OTHER
4029	025A	192.06	04/23/13	0733	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4030	025A	192.07	11/21/15	1234	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	STOPPED IN TRAFFIC
4031	025A	192.07	01/16/13	1658	Y (B)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4032	025A	192.07	05/15/14	1836	Y (C)	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	050	GOING STRAIGHT
4033	025A	192.07	02/16/11	1418	Y (E)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
4034	025A	192.08	02/09/11	0538	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	OTHER
4035	025A	192.08	03/05/14	0459	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4036	025A	192.08	03/23/11	0554	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
4037	025A	192.08	04/16/13	1113	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4038	025A	192.08	11/07/14	2351	N	BARRICADE/TRAFFIC BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	OTHER
4039	025A	192.08	11/26/14	1442	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	035	OTHER
4040	025A	192.08	03/31/15	1834	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	000	STOPPED IN TRAFFIC
4041	025A	192.08	06/08/12	0900	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4042	025A	192.08	02/20/13	1617	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4043	025A	192.08	02/22/14	1054	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
4044	025A	192.08	02/22/14	1054	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
4045	025A	192.08	02/22/14	1100	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
4046	025A	192.08	03/21/14	1538	N	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4047	025A	192.08	05/26/11	0915	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4048	025A	192.08	08/29/11	0704	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
4049	025A	192.08	11/07/11	0839	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	020	GOING STRAIGHT
4050	025A	192.08	01/03/12	0727	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	CHANGING LANES
4051	025A	192.08	01/26/12	0659	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4052	025A	192.08	04/20/12	0730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4053	025A	192.08	09/26/12	1340	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4054	025A	192.08	12/19/12	1024	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	GOING STRAIGHT
4055	025A	192.08	04/08/13	0713	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4056	025A	192.08	04/09/13	1030	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4057	025A	192.08	05/09/13	0822	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4058	025A	192.08	08/24/13	1453	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4059	025A	192.08	08/28/13	0850	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
4060	025A	192.08	09/05/13	0716	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
4061	025A	192.08	09/23/13	0706	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
4062	025A	192.08	10/16/13	1802	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4063	025A	192.08	10/16/13	1833	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4064	025A	192.08	04/13/14	1640	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	STOPPED IN TRAFFIC
4065	025A	192.08	05/16/14	0732	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
4066	025A	192.08	06/04/14	1847	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4067	025A	192.08	06/20/14	1156	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4068	025A	192.08	08/13/14	0841	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4069	025A	192.08	08/14/14	0902	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
4070	025A	192.08	09/12/14	0801	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
4071	025A	192.08	10/01/14	0647	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4072	025A	192.08	10/11/14	0920	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
4073	025A	192.08	10/11/14	1125	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4074	025A	192.08	10/28/14	0635	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4075	025A	192.08	10/28/14	0647	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
4076	025A	192.08	10/30/14	1603	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	SLOWING
4077	025A	192.08	12/27/14	1449	N	REAR-END	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	085	GOING STRAIGHT
4078	025A	192.08	01/08/15	1459	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4079	025A	192.08	03/14/15	1805	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
4080	025A	192.08	03/20/15	0832	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	SLOWING

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4081	025A	192.08	03/25/15	0856	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
4082	025A	192.08	03/31/15	1834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4083	025A	192.08	05/27/15	0751	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4084	025A	192.08	06/19/15	1317	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4085	025A	192.08	07/03/15	1145	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4086	025A	192.08	07/17/15	1604	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4087	025A	192.08	07/22/15	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4088	025A	192.08	08/07/15	1401	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4089	025A	192.08	08/07/15	1401	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4090	025A	192.08	08/10/15	1404	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	WET	DAYLIGHT	RAIN
4091	025A	192.08	08/24/15	0908	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4092	025A	192.08	01/18/11	1703	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4093	025A	192.08	09/13/11	0729	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4094	025A	192.08	02/01/12	2035	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4095	025A	192.08	02/14/13	1453	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
4096	025A	192.08	02/17/13	2010	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4097	025A	192.08	03/24/13	2201	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4098	025A	192.08	04/11/13	0947	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4099	025A	192.08	05/14/13	0810	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4100	025A	192.08	08/28/13	0737	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4101	025A	192.08	10/18/13	0831	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4102	025A	192.08	01/22/14	1842	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4103	025A	192.08	05/06/14	1627	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4104	025A	192.08	09/09/14	1638	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4105	025A	192.08	09/15/14	1634	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4106	025A	192.08	12/24/14	1616	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4107	025A	192.08	01/12/15	1235	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4108	025A	192.08	03/19/15	0940	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4109	025A	192.08	06/29/15	0809	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4110	025A	192.08	11/30/15	0023	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4111	025A	192.08	08/12/13	0121	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4112	025A	192.08	07/14/14	1212	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4113	025A	192.08	06/27/13	0451	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4114	025A	192.08	11/17/13	0551	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4115	025A	192.08	07/06/15	0225	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4116	025A	192.08	06/18/11	1611	INJ	OFF LEFT	NON-INTERSECTION	1	HILLCREST	DRY	DAYLIGHT	NONE
4117	025A	192.08	12/04/11	2131	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4118	025A	192.08	02/21/13	1752	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4119	025A	192.08	01/04/14	0837	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4120	025A	192.08	04/30/15	2100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
4121	025A	192.08	09/29/15	1826	PDO	OFF RIGHT	AT INTERSECTION	2	CURVE ON-GRADE	WET	DAWN OR DUSK	RAIN
4122	025A	192.08	01/19/11	2250	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4123	025A	192.08	02/01/11	2111	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
4124	025A	192.08	06/20/11	0258	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	WET	DARK-LIGHTED	RAIN
4125	025A	192.08	09/12/12	0500	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4126	025A	192.08	01/29/13	2022	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4127	025A	192.08	04/17/13	1814	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE
4128	025A	192.08	07/27/13	2014	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
4129	025A	192.08	12/28/13	2157	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4130	025A	192.08	01/30/14	2214	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4131	025A	192.08	01/31/14	0802	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4132	025A	192.08	12/15/14	1440	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4133	025A	192.08	12/30/14	0011	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4134	025A	192.08	02/22/15	1450	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4135	025A	192.08	12/25/15	1405	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4136	025A	192.08	08/23/15	0134	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4137	025A	192.08	07/03/13	2213	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4138	025A	192.08	12/25/12	2154	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4139	025A	192.08	01/17/13	0720	INJ	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4140	025A	192.08	05/01/13	0839	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4081	025A	192.08	03/25/15	0856	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4082	025A	192.08	03/31/15	1834	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4083	025A	192.08	05/27/15	0751	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	GOING STRAIGHT
4084	025A	192.08	06/19/15	1317	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4085	025A	192.08	07/03/15	1145	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	005	BACKING
4086	025A	192.08	07/17/15	1604	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4087	025A	192.08	07/22/15	0724	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	SLOWING
4088	025A	192.08	08/07/15	1401	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4089	025A	192.08	08/07/15	1401	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4090	025A	192.08	08/10/15	1404	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4091	025A	192.08	08/24/15	0908	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4092	025A	192.08	01/18/11	1703	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4093	025A	192.08	09/13/11	0729	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	075	CHANGING LANES
4094	025A	192.08	02/01/12	2035	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
4095	025A	192.08	02/14/13	1453	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	CHANGING LANES
4096	025A	192.08	02/17/13	2010	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4097	025A	192.08	03/24/13	2201	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4098	025A	192.08	04/11/13	0947	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4099	025A	192.08	05/14/13	0810	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	CHANGING LANES
4100	025A	192.08	08/28/13	0737	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4101	025A	192.08	10/18/13	0831	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
4102	025A	192.08	01/22/14	1842	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
4103	025A	192.08	05/06/14	1627	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
4104	025A	192.08	09/09/14	1638	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4105	025A	192.08	09/15/14	1634	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4106	025A	192.08	12/24/14	1616	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	AVOIDING OBJECT/VEHICLE IN ROAD
4107	025A	192.08	01/12/15	1235	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4108	025A	192.08	03/19/15	0940	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4109	025A	192.08	06/29/15	0809	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	CHANGING LANES
4110	025A	192.08	11/30/15	0023	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4111	025A	192.08	08/12/13	0121	N	SIDESWIPE OPPOSITE DIRECTION	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	065	AVOIDING OBJECT/VEHICLE IN ROAD
4112	025A	192.08	07/14/14	1212	N	PARKED MOTOR VEHICLE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	045	AVOIDING OBJECT/VEHICLE IN ROAD
4113	025A	192.08	06/27/13	0451	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4114	025A	192.08	11/17/13	0551	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4115	025A	192.08	07/06/15	0225	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	AVOIDING OBJECT/VEHICLE IN ROAD
4116	025A	192.08	06/18/11	1611	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	AVOIDING OBJECT/VEHICLE IN ROAD
4117	025A	192.08	12/04/11	2131	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
4118	025A	192.08	02/21/13	1752	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4119	025A	192.08	01/04/14	0837	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	CHANGING LANES
4120	025A	192.08	04/30/15	2100	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
4121	025A	192.08	09/29/15	1826	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	UNKNOWN	075	OTHER
4122	025A	192.08	01/19/11	2250	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
4123	025A	192.08	02/01/11	2111	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4124	025A	192.08	06/20/11	0258	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	055	OTHER
4125	025A	192.08	09/12/12	0500	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
4126	025A	192.08	01/29/13	2022	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
4127	025A	192.08	04/17/13	1814	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4128	025A	192.08	07/27/13	2014	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4129	025A	192.08	12/28/13	2157	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	OTHER
4130	025A	192.08	01/30/14	2214	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4131	025A	192.08	01/31/14	0802	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4132	025A	192.08	12/15/14	1440	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	070	OTHER
4133	025A	192.08	12/30/14	0011	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4134	025A	192.08	02/22/15	1450	N	CONCRETE BARRIER	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
4135	025A	192.08	12/25/15	1405	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
4136	025A	192.08	08/23/15	0134	N	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	085	GOING STRAIGHT
4137	025A	192.08	07/03/13	2213	N	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
4138	025A	192.08	12/25/12	2154	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
4139	025A	192.08	01/17/13	0720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
4140	025A	192.08	05/01/13	0839	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4141	025A	192.08	12/11/13	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4142	025A	192.08	06/05/14	0830	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4143	025A	192.08	07/30/14	0723	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4144	025A	192.08	07/31/14	0734	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4145	025A	192.08	08/29/14	1156	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4146	025A	192.08	09/09/14	1433	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4147	025A	192.08	11/10/14	1546	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4148	025A	192.08	11/19/14	0818	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4149	025A	192.08	11/24/14	1442	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4150	025A	192.08	12/13/14	1025	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	WIND
4151	025A	192.08	01/01/15	1520	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4152	025A	192.08	12/29/15	1549	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
4153	025A	192.08	10/09/12	1411	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4154	025A	192.08	10/17/11	1712	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4155	025A	192.08	01/18/12	1624	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4156	025A	192.08	11/28/12	1811	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4157	025A	192.08	03/25/15	1807	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4158	025A	192.08	04/16/15	1621	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
4159	025A	192.08	07/29/15	1736	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4160	025A	192.08	08/30/15	1327	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4161	025A	192.08	09/29/15	1752	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4162	025A	192.08	05/08/13	1556	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4163	025A	192.08	05/11/13	1549	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4164	025A	192.08	02/11/14	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4165	025A	192.08	06/25/14	1749	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4166	025A	192.08	06/24/15	1536	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4167	025A	192.08	08/02/14	2153	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4168	025A	192.08	01/23/14	1709	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	NONE
4169	025A	192.08	03/03/14	1836	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4170	025A	192.08	09/10/14	2039	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4171	025A	192.08	10/21/14	1823	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4172	025A	192.08	12/11/12	1125	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
4173	025A	192.08	02/21/12	1406	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4174	025A	192.08	01/18/13	1452	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4175	025A	192.08	04/03/14	1600	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4176	025A	192.08	06/21/12	1851	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4177	025A	192.08	05/20/14	1311	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4178	025A	192.08	12/04/14	1802	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4179	025A	192.08	01/02/15	0815	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
4180	025A	192.08	06/24/12	2153	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4181	025A	192.08	11/04/14	1748	PDO	ON	AT INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4182	025A	192.08	03/02/14	0513	PDO	OFF AT TEE	AT INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	NONE
4183	025A	192.08	07/25/14	2259	PDO	OFF LEFT	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4184	025A	192.08	10/23/14	2205	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4185	025A	192.08	12/23/14	0704	PDO	OFF LEFT	INTERSECTION RELATED	1	CURVE ON-LEVEL	ICY	DAWN OR DUSK	NONE
4186	025A	192.08	11/29/15	0157	PDO	OFF RIGHT	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4187	025A	192.08	05/31/13	1841	PDO	ON	AT INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4188	025A	192.08	12/14/11	0920	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4189	025A	192.08	10/04/12	1308	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4190	025A	192.08	04/13/11	1520	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4191	025A	192.08	04/21/11	1045	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4192	025A	192.08	01/19/12	1145	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4193	025A	192.08	05/29/12	0909	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4194	025A	192.08	06/13/12	0958	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4195	025A	192.08	09/09/13	1222	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4196	025A	192.08	11/21/13	0642	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
4197	025A	192.08	03/21/14	1318	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4198	025A	192.08	07/30/14	0131	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
4199	025A	192.09	12/13/12	0738	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4200	025A	192.09	10/15/12	1911	PDO	ON	RAMP	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4141	025A	192.08	12/11/13	0900	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4142	025A	192.08	06/05/14	0830	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4143	025A	192.08	07/30/14	0723	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
4144	025A	192.08	07/31/14	0734	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
4145	025A	192.08	08/29/14	1156	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
4146	025A	192.08	09/09/14	1433	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4147	025A	192.08	11/10/14	1546	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4148	025A	192.08	11/19/14	0818	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4149	025A	192.08	11/24/14	1442	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	035	OTHER
4150	025A	192.08	12/13/14	1025	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	007	SLOWING
4151	025A	192.08	01/01/15	1520	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
4152	025A	192.08	12/29/15	1549	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
4153	025A	192.08	10/09/12	1411	Y (A)	SIGN	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4154	025A	192.08	10/17/11	1712	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
4155	025A	192.08	01/18/12	1624	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4156	025A	192.08	11/28/12	1811	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	GOING STRAIGHT
4157	025A	192.08	03/25/15	1807	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4158	025A	192.08	04/16/15	1621	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	STOPPED IN TRAFFIC
4159	025A	192.08	07/29/15	1736	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4160	025A	192.08	08/30/15	1327	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4161	025A	192.08	09/29/15	1752	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4162	025A	192.08	05/08/13	1556	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	015	CHANGING LANES
4163	025A	192.08	05/11/13	1549	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4164	025A	192.08	02/11/14	1815	Y (A)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	CHANGING LANES
4165	025A	192.08	06/25/14	1749	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4166	025A	192.08	06/24/15	1536	Y (A)	SIDESWIPE SAME DIRECTION	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4167	025A	192.08	08/02/14	2153	Y (A)	CURB/RAISED MEDIAN	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4168	025A	192.08	01/23/14	1709	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4169	025A	192.08	03/03/14	1836	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4170	025A	192.08	09/10/14	2039	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
4171	025A	192.08	10/21/14	1823	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4172	025A	192.08	12/11/12	1125	Y (N)	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	015	OTHER
4173	025A	192.08	02/21/12	1406	Y (N)	REAR-END	W	SUV	RX/MEDICATION/DR	UNKNOWN	050	GOING STRAIGHT
4174	025A	192.08	01/18/13	1452	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4175	025A	192.08	04/03/14	1600	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	030	GOING STRAIGHT
4176	025A	192.08	06/21/12	1851	Y (N)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
4177	025A	192.08	05/20/14	1311	Y (N)	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
4178	025A	192.08	12/04/14	1802	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	025	MAKING LEFT TURN
4179	025A	192.08	01/02/15	0815	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	MAKING LEFT TURN
4180	025A	192.08	06/24/12	2153	Y (N)	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
4181	025A	192.08	11/04/14	1748	Y (N)	WILD ANIMAL	E	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4182	025A	192.08	03/02/14	0513	Y (N)	TRAFFIC SIGNAL POLE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
4183	025A	192.08	07/25/14	2259	Y (N)	CURB/RAISED MEDIAN	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	MAKING LEFT TURN
4184	025A	192.08	10/23/14	2205	Y (N)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	MAKING LEFT TURN
4185	025A	192.08	12/23/14	0704	Y (N)	CURB/RAISED MEDIAN	NW	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	MAKING RIGHT TURN
4186	025A	192.08	11/29/15	0157	Y (N)	CURB/RAISED MEDIAN	S	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	MAKING LEFT TURN
4187	025A	192.08	05/31/13	1841	Y (O)	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4188	025A	192.08	12/14/11	0920	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4189	025A	192.08	10/04/12	1308	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	MAKING LEFT TURN
4190	025A	192.08	04/13/11	1520	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
4191	025A	192.08	04/21/11	1045	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	GOING STRAIGHT
4192	025A	192.08	01/19/12	1145	Y (O)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	OTHER
4193	025A	192.08	05/29/12	0909	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	015	GOING STRAIGHT
4194	025A	192.08	06/13/12	0958	Y (O)	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4195	025A	192.08	09/09/13	1222	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
4196	025A	192.08	11/21/13	0642	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	SLOWING
4197	025A	192.08	03/21/14	1318	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4198	025A	192.08	07/30/14	0131	Y (O)	CONCRETE BARRIER	N	PASS CAR/VAN	RX/MEDICATION/DR	UNKNOWN	065	GOING STRAIGHT
4199	025A	192.09	12/13/12	0738	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4200	025A	192.09	10/15/12	1911	Y (D)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4201	025A	192.09	01/09/15	0122	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4202	025A	192.10	09/11/13	1622	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4203	025A	192.10	01/16/15	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4204	025A	192.10	01/11/12	0728	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4205	025A	192.10	06/20/14	2103	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4206	025A	192.10	11/21/13	0152	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4207	025A	192.10	04/12/14	1426	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4208	025A	192.10	03/05/14	0512	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4209	025A	192.10	11/14/15	1310	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4210	025A	192.10	10/24/14	1153	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4211	025A	192.10	02/17/15	1427	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4212	025A	192.10	10/15/12	1910	PDO	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4213	025A	192.10	05/17/13	0001	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4214	025A	192.11	05/16/14	0911	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4215	025A	192.12	03/23/11	0617	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4216	025A	192.12	05/16/11	0726	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4217	025A	192.12	04/24/11	1542	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4218	025A	192.12	06/08/13	1627	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4219	025A	192.13	12/04/11	2226	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4220	025A	192.14	07/24/15	1520	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	UNKNOWN
4221	025A	192.14	11/09/15	0906	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4222	025A	192.14	10/05/11	0756	PDO	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4223	025A	192.14	02/20/15	1030	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4224	025A	192.15	03/27/13	1353	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4225	025A	192.15	08/02/13	1910	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4226	025A	192.15	03/11/12	2000	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4227	025A	192.17	08/07/15	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4228	025A	192.17	08/07/15	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4229	025A	192.17	11/27/15	0219	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4230	025A	192.17	02/02/12	2136	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4231	025A	192.20	08/18/15	0724	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4232	025A	192.21	11/19/14	0923	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4233	025A	192.23	11/21/13	0532	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4234	025A	192.25	08/21/13	2010	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4235	025A	192.25	10/15/11	1255	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4236	025A	192.27	09/23/15	0704	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4237	025A	192.28	06/26/11	1920	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4238	025A	192.30	03/05/15	1323	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4239	025A	192.33	06/18/12	0815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4240	025A	192.33	10/01/12	0924	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4241	025A	192.33	09/20/13	0805	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4242	025A	192.33	11/08/14	1417	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4243	025A	192.33	02/01/14	2018	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4244	025A	192.33	12/28/14	1229	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4245	025A	192.33	02/25/15	2008	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
4246	025A	192.33	09/06/13	0748	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4247	025A	192.34	12/27/14	1027	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4248	025A	192.35	10/27/13	0853	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4249	025A	192.35	03/15/13	1035	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	NONE
4250	025A	192.35	01/14/14	1501	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4251	025A	192.35	12/03/14	0549	PDO	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4252	025A	192.35	05/21/15	1358	PDO	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4253	025A	192.38	08/24/13	1236	INJ	ON	NON-INTERSECTION	4	UNKNOWN	DRY	DAYLIGHT	NONE
4254	025A	192.38	12/19/14	2217	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4255	025A	192.39	11/10/13	1735	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4256	025A	192.45	03/22/11	0801	PDO	ON	NON-INTERSECTION	5	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4257	025A	192.47	10/31/13	0716	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4258	025A	192.48	08/06/15	1333	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4259	025A	192.48	08/06/15	1333	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4260	025A	192.49	04/27/11	1719	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4201	025A	192.09	01/09/15	0122	Y (D)	CURB/RAISED MEDIAN	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	MAKING RIGHT TURN
4202	025A	192.10	09/11/13	1622	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
4203	025A	192.10	01/16/15	1300	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	SLOWING
4204	025A	192.10	01/11/12	0728	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
4205	025A	192.10	06/20/14	2103	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	070	GOING STRAIGHT
4206	025A	192.10	11/21/13	0152	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
4207	025A	192.10	04/12/14	1426	N	GUARD RAIL	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	065	GOING STRAIGHT
4208	025A	192.10	03/05/14	0512	N	LARGE BOULDERS OR ROCKS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4209	025A	192.10	11/14/15	1310	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
4210	025A	192.10	10/24/14	1153	Y (C)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	CHANGING LANES
4211	025A	192.10	02/17/15	1427	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4212	025A	192.10	10/15/12	1910	Y (D)	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4213	025A	192.10	05/17/13	0001	Y (D)	OVERTURNING	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	GOING STRAIGHT
4214	025A	192.11	05/16/14	0911	Y (C)	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4215	025A	192.12	03/23/11	0617	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
4216	025A	192.12	05/16/11	0726	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	008	GOING STRAIGHT
4217	025A	192.12	04/24/11	1542	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	080	CHANGING LANES
4218	025A	192.12	06/08/13	1627	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
4219	025A	192.13	12/04/11	2226	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
4220	025A	192.14	07/24/15	1520	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	CHANGING LANES
4221	025A	192.14	11/09/15	0906	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	055	GOING STRAIGHT
4222	025A	192.14	10/05/11	0756	Y (C)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4223	025A	192.14	02/20/15	1030	Y (C)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4224	025A	192.15	03/27/13	1353	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	WEAVING
4225	025A	192.15	08/02/13	1910	Y (D)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	CHANGING LANES
4226	025A	192.15	03/11/12	2000	Y (D)	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
4227	025A	192.17	08/07/15	1200	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4228	025A	192.17	08/07/15	1200	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4229	025A	192.17	11/27/15	0219	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4230	025A	192.17	02/02/12	2136	Y (C)	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
4231	025A	192.20	08/18/15	0724	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	MAKING RIGHT TURN
4232	025A	192.21	11/19/14	0923	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4233	025A	192.23	11/21/13	0532	Y (C)	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	015	OTHER
4234	025A	192.25	08/21/13	2010	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
4235	025A	192.25	10/15/11	1255	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
4236	025A	192.27	09/23/15	0704	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4237	025A	192.28	06/26/11	1920	N	CONCRETE BARRIER	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	080	WEAVING
4238	025A	192.30	03/05/15	1323	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	SLOWING
4239	025A	192.33	06/18/12	0815	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4240	025A	192.33	10/01/12	0924	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4241	025A	192.33	09/20/13	0805	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4242	025A	192.33	11/08/14	1417	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	GOING STRAIGHT
4243	025A	192.33	02/01/14	2018	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	055	OTHER
4244	025A	192.33	12/28/14	1229	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	OTHER
4245	025A	192.33	02/25/15	2008	N	BARRICADE/TRAFFIC BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
4246	025A	192.33	09/06/13	0748	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4247	025A	192.34	12/27/14	1027	Y (C)	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
4248	025A	192.35	10/27/13	0853	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
4249	025A	192.35	03/15/13	1035	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4250	025A	192.35	01/14/14	1501	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	CHANGING LANES
4251	025A	192.35	12/03/14	0549	Y (C)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
4252	025A	192.35	05/21/15	1358	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4253	025A	192.38	08/24/13	1236	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	SLOWING
4254	025A	192.38	12/19/14	2217	Y (D)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
4255	025A	192.39	11/10/13	1735	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	063	OTHER
4256	025A	192.45	03/22/11	0801	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4257	025A	192.47	10/31/13	0716	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4258	025A	192.48	08/06/15	1333	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4259	025A	192.48	08/06/15	1333	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4260	025A	192.49	04/27/11	1719	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4261	025A	192.49	11/22/11	1615	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4262	025A	192.49	07/17/12	0840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4263	025A	192.49	10/17/13	0821	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4264	025A	192.49	01/03/14	1913	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4265	025A	192.49	01/08/14	0913	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4266	025A	192.49	09/05/14	0736	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	RAIN
4267	025A	192.49	06/12/15	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4268	025A	192.49	07/29/15	0934	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4269	025A	192.49	12/04/11	2023	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4270	025A	192.49	06/30/12	2205	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4271	025A	192.49	07/22/12	2108	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4272	025A	192.49	08/30/14	1414	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4273	025A	192.49	11/27/15	1135	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
4274	025A	192.49	04/23/12	0031	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4275	025A	192.49	12/24/13	1057	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4276	025A	192.49	12/24/13	0935	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4277	025A	192.50	07/02/12	1831	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4278	025A	192.50	08/21/12	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4279	025A	192.50	03/07/13	2125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4280	025A	192.50	11/01/14	0956	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4281	025A	192.50	06/20/15	1053	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4282	025A	192.50	12/24/15	1215	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4283	025A	192.50	07/02/11	1826	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4284	025A	192.50	08/10/13	1928	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4285	025A	192.50	05/08/15	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4286	025A	192.50	08/07/15	1145	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4287	025A	192.50	08/07/15	1145	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4288	025A	192.50	11/04/13	0600	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4289	025A	192.50	07/15/14	2310	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
4290	025A	192.50	08/10/13	1928	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4291	025A	192.50	02/21/15	0632	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	NONE
4292	025A	192.50	03/22/15	2213	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4293	025A	192.50	03/25/15	1100	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4294	025A	192.50	01/16/12	1942	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4295	025A	192.50	06/01/15	2155	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4296	025A	192.50	03/04/14	0759	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4297	025A	192.50	11/05/15	0702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4298	025A	192.50	12/24/15	1215	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4299	025A	192.58	01/12/15	1219	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4300	025A	192.58	03/22/12	0737	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4301	025A	192.58	05/23/14	1843	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4302	025A	192.58	06/14/14	1643	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4303	025A	192.58	10/22/14	0651	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4304	025A	192.58	11/04/15	1733	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
4305	025A	192.58	07/12/13	1404	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4306	025A	192.58	01/07/14	0006	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
4307	025A	192.59	08/03/13	1524	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4308	025A	192.63	05/16/13	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4309	025A	192.63	05/14/14	1051	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4310	025A	192.64	01/03/15	1620	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4311	025A	192.65	11/22/15	0751	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
4312	025A	192.65	12/25/14	1800	INJ	ON	NON-INTERSECTION	2	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4313	025A	192.65	04/04/15	1037	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4314	025A	192.65	08/16/15	0931	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4315	025A	192.66	01/21/15	1600	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4316	025A	192.66	04/05/14	1324	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4317	025A	192.68	01/31/14	0735	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
4318	025A	192.71	07/24/13	0952	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4319	025A	192.74	04/14/11	0908	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4320	025A	192.74	03/16/13	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4261	025A	192.49	11/22/11	1615	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	OTHER
4262	025A	192.49	07/17/12	0840	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	015	CHANGING LANES
4263	025A	192.49	10/17/13	0821	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
4264	025A	192.49	01/03/14	1913	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4265	025A	192.49	01/08/14	0913	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4266	025A	192.49	09/05/14	0736	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4267	025A	192.49	06/12/15	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4268	025A	192.49	07/29/15	0934	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4269	025A	192.49	12/04/11	2023	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
4270	025A	192.49	06/30/12	2205	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	CHANGING LANES
4271	025A	192.49	07/22/12	2108	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	015	CHANGING LANES
4272	025A	192.49	08/30/14	1414	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4273	025A	192.49	11/27/15	1135	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4274	025A	192.49	04/23/12	0031	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	025	GOING STRAIGHT
4275	025A	192.49	12/24/13	1057	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4276	025A	192.49	12/24/13	0935	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4277	025A	192.50	07/02/12	1831	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4278	025A	192.50	08/21/12	0700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4279	025A	192.50	03/07/13	2125	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
4280	025A	192.50	11/01/14	0956	N	REAR-END	N	SCHOOL BUS < 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4281	025A	192.50	06/20/15	1053	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	SLOWING
4282	025A	192.50	12/24/15	1215	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4283	025A	192.50	07/02/11	1826	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	075	GOING STRAIGHT
4284	025A	192.50	08/10/13	1928	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	060	OTHER
4285	025A	192.50	05/08/15	1350	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4286	025A	192.50	08/07/15	1145	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4287	025A	192.50	08/07/15	1145	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4288	025A	192.50	11/04/13	0600	N	WILD ANIMAL	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4289	025A	192.50	07/15/14	2310	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4290	025A	192.50	08/10/13	1928	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	OTHER
4291	025A	192.50	02/21/15	0632	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	PASSING
4292	025A	192.50	03/22/15	2213	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	065	WEAVING
4293	025A	192.50	03/25/15	1100	N	CONCRETE BARRIER	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4294	025A	192.50	01/16/12	1942	N	FENCE	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
4295	025A	192.50	06/01/15	2155	N	INVOLVING OTHER OBJECT	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	AVOIDING OBJECT/VEHICLE IN ROAD
4296	025A	192.50	03/04/14	0759	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4297	025A	192.50	11/05/15	0702	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4298	025A	192.50	12/24/15	1215	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	STOPPED IN TRAFFIC
4299	025A	192.58	01/12/15	1219	N	OTHER NON-COLLISION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
4300	025A	192.58	03/22/12	0737	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
4301	025A	192.58	05/23/14	1843	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4302	025A	192.58	06/14/14	1643	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	AVOIDING OBJECT/VEHICLE IN ROAD
4303	025A	192.58	10/22/14	0651	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4304	025A	192.58	11/04/15	1733	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4305	025A	192.58	07/12/13	1404	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	CHANGING LANES
4306	025A	192.58	01/07/14	0006	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	OTHER
4307	025A	192.59	08/03/13	1524	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4308	025A	192.63	05/16/13	0710	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	CHANGING LANES
4309	025A	192.63	05/14/14	1051	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	055	OTHER
4310	025A	192.64	01/03/15	1620	Y (B)	TRAFFIC SIGNAL POLE	N	SUV	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4311	025A	192.65	11/22/15	0751	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	OTHER
4312	025A	192.65	12/25/14	1800	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	025	OTHER
4313	025A	192.65	04/04/15	1037	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4314	025A	192.65	08/16/15	0931	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4315	025A	192.66	01/21/15	1600	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	OTHER
4316	025A	192.66	04/05/14	1324	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4317	025A	192.68	01/31/14	0735	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
4318	025A	192.71	07/24/13	0952	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4319	025A	192.74	04/14/11	0908	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4320	025A	192.74	03/16/13	1350	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4321	025A	192.74	11/21/14	1537	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4322	025A	192.74	10/31/15	2045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4323	025A	192.74	05/18/11	1450	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4324	025A	192.74	11/23/14	1520	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	SNOW/SLEET/HAIL
4325	025A	192.74	11/05/15	0114	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4326	025A	192.74	01/16/12	1924	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4327	025A	192.74	07/14/12	1930	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4328	025A	192.74	11/26/12	0704	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4329	025A	192.74	07/25/14	1932	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4330	025A	192.74	11/21/14	1537	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4331	025A	192.74	06/15/15	0844	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4332	025A	192.74	08/13/15	1604	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4333	025A	192.74	10/21/15	0720	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4334	025A	192.74	09/18/14	0445	PDO	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4335	025A	192.75	09/24/11	1149	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4336	025A	192.75	10/13/14	1033	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4337	025A	192.75	02/19/14	2032	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4338	025A	192.75	11/11/12	0112	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4339	025A	192.75	08/05/13	1503	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4340	025A	192.76	04/09/13	0927	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4341	025A	192.79	01/19/11	1754	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4342	025A	192.79	06/25/14	0733	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4343	025A	192.79	08/16/15	1334	INJ	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
4344	025A	192.80	04/23/13	1020	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
4345	025A	192.83	08/01/11	1542	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4346	025A	192.84	08/28/13	0851	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4347	025A	192.84	12/18/14	1748	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4348	025A	192.87	03/02/12	2030	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4349	025A	192.88	01/04/15	1825	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	SLUSHY	DARK-LIGHTED	NONE
4350	025A	192.89	10/22/15	0625	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
4351	025A	192.89	02/06/11	2148	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4352	025A	192.90	06/12/14	1158	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4353	025A	192.90	05/25/11	0726	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4354	025A	192.90	06/27/14	1720	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4355	025A	192.90	01/31/11	1715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4356	025A	192.90	04/11/13	0913	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4357	025A	192.90	04/01/14	1728	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4358	025A	192.91	12/19/11	1517	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4359	025A	192.92	08/01/13	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4360	025A	192.92	11/27/15	1750	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
4361	025A	192.93	05/20/13	0701	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4362	025A	192.93	10/01/14	0825	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4363	025A	192.93	01/07/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4364	025A	192.93	04/09/13	0257	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4365	025A	192.93	04/13/14	1700	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
4366	025A	192.93	05/11/15	0647	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4367	025A	192.93	05/11/15	0647	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4368	025A	192.94	07/22/14	0713	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4369	025A	192.95	03/25/12	1200	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4370	025A	192.95	12/15/12	1507	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4371	025A	192.95	09/23/13	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4372	025A	192.95	09/20/14	1212	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4373	025A	192.95	07/21/15	1009	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4374	025A	192.95	09/17/15	0742	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4375	025A	192.95	01/02/15	0808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
4376	025A	192.95	07/17/15	0647	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4377	025A	192.95	01/02/14	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4378	025A	192.95	09/30/13	1400	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4379	025A	192.95	10/23/13	0654	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4380	025A	192.95	07/20/15	1225	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4321	025A	192.74	11/21/14	1537	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
4322	025A	192.74	10/31/15	2045	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
4323	025A	192.74	05/18/11	1450	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	OTHER
4324	025A	192.74	11/23/14	1520	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4325	025A	192.74	11/05/15	0114	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4326	025A	192.74	01/16/12	1924	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
4327	025A	192.74	07/14/12	1930	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	SLOWING
4328	025A	192.74	11/26/12	0704	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4329	025A	192.74	07/25/14	1932	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
4330	025A	192.74	11/21/14	1537	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4331	025A	192.74	06/15/15	0844	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	025	GOING STRAIGHT
4332	025A	192.74	08/13/15	1604	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4333	025A	192.74	10/21/15	0720	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	SLOWING
4334	025A	192.74	09/18/14	0445	Y (D)	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4335	025A	192.75	09/24/11	1149	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4336	025A	192.75	10/13/14	1033	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4337	025A	192.75	02/19/14	2032	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4338	025A	192.75	11/11/12	0112	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
4339	025A	192.75	08/05/13	1503	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
4340	025A	192.76	04/09/13	0927	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4341	025A	192.79	01/19/11	1754	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	OTHER
4342	025A	192.79	06/25/14	0733	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4343	025A	192.79	08/16/15	1334	Y (B)	CRASH CUSHION	N	SUV	ALCOHOL	UNKNOWN	055	GOING STRAIGHT
4344	025A	192.80	04/23/13	1020	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
4345	025A	192.83	08/01/11	1542	N	LARGE BOULDERS OR ROCKS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	075	GOING STRAIGHT
4346	025A	192.84	08/28/13	0851	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4347	025A	192.84	12/18/14	1748	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	SLOWING
4348	025A	192.87	03/02/12	2030	Y (B)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4349	025A	192.88	01/04/15	1825	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4350	025A	192.89	10/22/15	0625	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4351	025A	192.89	02/06/11	2148	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
4352	025A	192.90	06/12/14	1158	N	CONCRETE BARRIER	N	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4353	025A	192.90	05/25/11	0726	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
4354	025A	192.90	06/27/14	1720	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4355	025A	192.90	01/31/11	1715	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4356	025A	192.90	04/11/13	0913	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	015	AVOIDING OBJECT/VEHICLE IN ROAD
4357	025A	192.90	04/01/14	1728	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4358	025A	192.91	12/19/11	1517	Y (B)	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4359	025A	192.92	08/01/13	0800	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4360	025A	192.92	11/27/15	1750	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	GOING STRAIGHT
4361	025A	192.93	05/20/13	0701	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4362	025A	192.93	10/01/14	0825	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
4363	025A	192.93	01/07/15	1330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	SLOWING
4364	025A	192.93	04/09/13	0257	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	OTHER
4365	025A	192.93	04/13/14	1700	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4366	025A	192.93	05/11/15	0647	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	SLOWING
4367	025A	192.93	05/11/15	0647	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4368	025A	192.94	07/22/14	0713	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	SLOWING
4369	025A	192.95	03/25/12	1200	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4370	025A	192.95	12/15/12	1507	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4371	025A	192.95	09/23/13	0715	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
4372	025A	192.95	09/20/14	1212	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
4373	025A	192.95	07/21/15	1009	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4374	025A	192.95	09/17/15	0742	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4375	025A	192.95	01/02/15	0808	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
4376	025A	192.95	07/17/15	0647	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	CHANGING LANES
4377	025A	192.95	01/02/14	0750	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	OTHER
4378	025A	192.95	09/30/13	1400	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4379	025A	192.95	10/23/13	0654	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
4380	025A	192.95	07/20/15	1225	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4381	025A	192.95	11/09/12	0756	INJ	ON	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4382	025A	192.95	01/31/13	1637	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4383	025A	192.95	09/20/15	0927	PDO	OFF RIGHT	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4384	025A	192.95	12/24/15	1531	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4385	025A	192.96	01/08/14	0647	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4386	025A	192.96	01/08/14	0647	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4387	025A	192.96	01/13/14	0707	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4388	025A	192.96	12/13/14	1829	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4389	025A	192.96	05/31/11	0659	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4390	025A	192.96	04/11/13	0918	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4391	025A	192.96	06/08/14	0018	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4392	025A	192.97	01/29/13	0643	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4393	025A	192.97	09/23/13	0900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4394	025A	192.97	11/08/13	1707	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4395	025A	192.97	01/17/12	0452	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
4396	025A	192.97	05/01/14	0917	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4397	025A	192.97	11/02/11	0319	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4398	025A	192.97	03/24/13	1105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
4399	025A	192.97	07/30/14	1236	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4400	025A	192.97	07/22/12	1029	PDO	ON	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4401	025A	192.97	01/24/12	0649	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
4402	025A	192.97	02/08/14	1243	PDO	ON	RAMP	2	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
4403	025A	192.97	11/28/15	0752	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4404	025A	192.98	11/03/14	0901	PDO	ON	NON-INTERSECTION	3	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
4405	025A	192.98	05/11/15	0834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4406	025A	192.98	01/11/12	1045	INJ	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4407	025A	192.98	04/16/13	0816	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
4408	025A	192.98	05/06/15	0702	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4409	025A	192.98	05/22/15	0938	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4410	025A	192.98	12/27/14	1355	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
4411	025A	192.98	04/27/15	1506	PDO	ON	RAMP	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4412	025A	192.98	03/16/14	1046	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4413	025A	192.98	02/25/12	0732	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
4414	025A	192.98	11/05/15	0103	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4415	025A	192.98	05/21/14	0735	PDO	ON	RAMP	4	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4416	025A	192.99	12/15/15	1440	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4417	025A	192.99	10/18/11	0609	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4418	025A	192.99	03/01/14	0015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	FOG
4419	025A	192.99	12/13/11	2141	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4420	025A	192.99	12/13/12	0649	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4421	025A	192.99	07/09/13	1808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4422	025A	192.99	06/03/14	1535	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4423	025A	192.99	06/30/14	1550	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4424	025A	192.99	06/23/15	1315	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4425	025A	192.99	02/04/11	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4426	025A	192.99	04/06/11	0820	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4427	025A	192.99	03/26/12	0727	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4428	025A	192.99	05/23/12	0734	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4429	025A	192.99	05/23/12	1658	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4430	025A	192.99	07/30/12	0919	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4431	025A	192.99	07/30/12	1644	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4432	025A	192.99	08/24/12	0731	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4433	025A	192.99	09/13/12	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4434	025A	192.99	09/18/12	0852	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4435	025A	192.99	09/22/12	1708	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4436	025A	192.99	10/01/12	0656	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4437	025A	192.99	10/26/12	1535	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4438	025A	192.99	12/09/12	2345	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4439	025A	192.99	01/03/13	1543	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4440	025A	192.99	03/09/13	0749	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4381	025A	192.95	11/09/12	0756	Y (B)	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4382	025A	192.95	01/31/13	1637	Y (B)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4383	025A	192.95	09/20/15	0927	Y (G)	PARKED MOTOR VEHICLE	N	SUV	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4384	025A	192.95	12/24/15	1531	Y (O)	SIDESWIPE SAME DIRECTION	NW	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	MAKING LEFT TURN
4385	025A	192.96	01/08/14	0647	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4386	025A	192.96	01/08/14	0647	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4387	025A	192.96	01/13/14	0707	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4388	025A	192.96	12/13/14	1829	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER FATIGUE	050	SLOWING
4389	025A	192.96	05/31/11	0659	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
4390	025A	192.96	04/11/13	0918	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4391	025A	192.96	06/08/14	0018	Y (B)	SIGN	N	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	060	OTHER
4392	025A	192.97	01/29/13	0643	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4393	025A	192.97	09/23/13	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4394	025A	192.97	11/08/13	1707	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4395	025A	192.97	01/17/12	0452	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	055	OTHER
4396	025A	192.97	05/01/14	0917	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	CHANGING LANES
4397	025A	192.97	11/02/11	0319	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
4398	025A	192.97	03/24/13	1105	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	035	AVOIDING OBJECT/VEHICLE IN ROAD
4399	025A	192.97	07/30/14	1236	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
4400	025A	192.97	07/22/12	1029	Y (B)	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4401	025A	192.97	01/24/12	0649	Y (B)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
4402	025A	192.97	02/08/14	1243	Y (G)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4403	025A	192.97	11/28/15	0752	Y (G)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	OTHER
4404	025A	192.98	11/03/14	0901	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4405	025A	192.98	05/11/15	0834	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4406	025A	192.98	01/11/12	1045	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
4407	025A	192.98	04/16/13	0816	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4408	025A	192.98	05/06/15	0702	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4409	025A	192.98	05/22/15	0938	Y (E)	TREE/SHRUBBERY	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4410	025A	192.98	12/27/14	1355	Y (E)	TREE/SHRUBBERY	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	MAKING RIGHT TURN
4411	025A	192.98	04/27/15	1506	Y (G)	PEDESTRIAN	E	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
4412	025A	192.98	03/16/14	1046	Y (G)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
4413	025A	192.98	02/25/12	0732	Y (G)	LIGHT/UTILITY POLE	SE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	OTHER
4414	025A	192.98	11/05/15	0103	Y (G)	GUARD RAIL	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4415	025A	192.98	05/21/14	0735	Y (G)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4416	025A	192.99	12/15/15	1440	N	OVERTURNING	N	SUV	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4417	025A	192.99	10/18/11	0609	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	AVOIDING OBJECT/VEHICLE IN ROAD
4418	025A	192.99	03/01/14	0015	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
4419	025A	192.99	12/13/11	2141	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4420	025A	192.99	12/13/12	0649	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	OTHER
4421	025A	192.99	07/09/13	1808	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4422	025A	192.99	06/03/14	1535	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4423	025A	192.99	06/30/14	1550	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4424	025A	192.99	06/23/15	1315	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4425	025A	192.99	02/04/11	1645	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4426	025A	192.99	04/06/11	0820	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4427	025A	192.99	03/26/12	0727	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4428	025A	192.99	05/23/12	0734	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4429	025A	192.99	05/23/12	1658	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4430	025A	192.99	07/30/12	0919	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4431	025A	192.99	07/30/12	1644	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4432	025A	192.99	08/24/12	0731	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	000	STOPPED IN TRAFFIC
4433	025A	192.99	09/13/12	0710	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	SLOWING
4434	025A	192.99	09/18/12	0852	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	UK	GOING STRAIGHT
4435	025A	192.99	09/22/12	1708	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	STOPPED IN TRAFFIC
4436	025A	192.99	10/01/12	0656	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
4437	025A	192.99	10/26/12	1535	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
4438	025A	192.99	12/09/12	2345	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4439	025A	192.99	01/03/13	1543	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4440	025A	192.99	03/09/13	0749	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4441	025A	192.99	03/13/13	0854	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4442	025A	192.99	04/18/13	0940	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4443	025A	192.99	04/23/13	0826	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4444	025A	192.99	05/13/13	0741	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4445	025A	192.99	06/28/13	0857	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4446	025A	192.99	08/06/13	0830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4447	025A	192.99	08/19/13	0710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4448	025A	192.99	09/03/13	0838	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4449	025A	192.99	09/09/13	1741	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4450	025A	192.99	09/14/13	1602	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAWN OR DUSK	RAIN
4451	025A	192.99	09/18/13	1847	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4452	025A	192.99	11/26/13	1555	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4453	025A	192.99	02/06/14	0736	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	NONE
4454	025A	192.99	02/12/14	0721	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4455	025A	192.99	02/13/14	0742	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4456	025A	192.99	03/21/14	1450	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4457	025A	192.99	06/13/14	0819	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4458	025A	192.99	07/09/14	0743	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4459	025A	192.99	10/07/14	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4460	025A	192.99	10/14/14	0851	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4461	025A	192.99	12/17/14	0700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4462	025A	192.99	01/06/15	1712	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4463	025A	192.99	03/07/15	1217	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4464	025A	192.99	04/26/15	2013	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
4465	025A	192.99	05/19/15	0631	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4466	025A	192.99	05/30/15	1722	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4467	025A	192.99	05/30/15	2246	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4468	025A	192.99	07/07/15	1649	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4469	025A	192.99	07/25/15	1434	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	UNKNOWN
4470	025A	192.99	08/13/15	1701	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4471	025A	192.99	08/23/15	1236	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4472	025A	192.99	08/23/15	1236	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4473	025A	192.99	09/14/15	1556	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4474	025A	192.99	09/16/15	1628	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4475	025A	192.99	09/18/15	1211	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4476	025A	192.99	09/18/15	1234	PDO	ON	NON-INTERSECTION	2	UNKNOWN	DRY	DAYLIGHT	NONE
4477	025A	192.99	09/19/15	1608	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4478	025A	192.99	12/02/15	0948	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4479	025A	192.99	12/10/15	0752	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4480	025A	192.99	12/31/15	1606	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4481	025A	192.99	12/20/11	1603	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4482	025A	192.99	02/07/12	0618	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4483	025A	192.99	07/28/12	0122	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4484	025A	192.99	09/12/12	1152	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4485	025A	192.99	12/12/12	1848	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4486	025A	192.99	01/11/13	1251	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4487	025A	192.99	03/18/13	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4488	025A	192.99	04/17/13	0853	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4489	025A	192.99	07/11/13	2003	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4490	025A	192.99	08/17/13	0114	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4491	025A	192.99	01/03/14	1443	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4492	025A	192.99	01/25/14	1402	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4493	025A	192.99	03/01/14	1401	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4494	025A	192.99	04/13/14	1639	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4495	025A	192.99	08/11/14	1828	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4496	025A	192.99	09/26/14	2123	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4497	025A	192.99	09/29/14	2219	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4498	025A	192.99	03/13/15	1154	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4499	025A	192.99	04/08/15	1352	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4500	025A	192.99	04/17/15	0227	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	SNOW/SLEET/HAIL

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4441	025A	192.99	03/13/13	0854	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4442	025A	192.99	04/18/13	0940	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4443	025A	192.99	04/23/13	0826	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4444	025A	192.99	05/13/13	0741	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4445	025A	192.99	06/28/13	0857	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4446	025A	192.99	08/06/13	0830	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4447	025A	192.99	08/19/13	0710	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
4448	025A	192.99	09/03/13	0838	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4449	025A	192.99	09/09/13	1741	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4450	025A	192.99	09/14/13	1602	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4451	025A	192.99	09/18/13	1847	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4452	025A	192.99	11/26/13	1555	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
4453	025A	192.99	02/06/14	0736	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	SLOWING
4454	025A	192.99	02/12/14	0721	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	GOING STRAIGHT
4455	025A	192.99	02/13/14	0742	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
4456	025A	192.99	03/21/14	1450	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4457	025A	192.99	06/13/14	0819	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4458	025A	192.99	07/09/14	0743	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
4459	025A	192.99	10/07/14	0715	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	SLOWING
4460	025A	192.99	10/14/14	0851	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
4461	025A	192.99	12/17/14	0700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	SLOWING
4462	025A	192.99	01/06/15	1712	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4463	025A	192.99	03/07/15	1217	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	SLOWING
4464	025A	192.99	04/26/15	2013	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	055	WEAVING
4465	025A	192.99	05/19/15	0631	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4466	025A	192.99	05/30/15	1722	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4467	025A	192.99	05/30/15	2246	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER FATIGUE	065	GOING STRAIGHT
4468	025A	192.99	07/07/15	1649	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4469	025A	192.99	07/25/15	1434	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	STOPPED IN TRAFFIC
4470	025A	192.99	08/13/15	1701	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
4471	025A	192.99	08/23/15	1236	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4472	025A	192.99	08/23/15	1236	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4473	025A	192.99	09/14/15	1556	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4474	025A	192.99	09/16/15	1628	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4475	025A	192.99	09/18/15	1211	N	REAR-END	S	MOTOR HOME	NO IMPAIRMENT	NONE APPARENT	045	SLOWING
4476	025A	192.99	09/18/15	1234	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4477	025A	192.99	09/19/15	1608	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4478	025A	192.99	12/02/15	0948	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4479	025A	192.99	12/10/15	0752	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4480	025A	192.99	12/31/15	1606	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
4481	025A	192.99	12/20/11	1603	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4482	025A	192.99	02/07/12	0618	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
4483	025A	192.99	07/28/12	0122	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	PASSING
4484	025A	192.99	09/12/12	1152	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	OTHER
4485	025A	192.99	12/12/12	1848	N	SIDESWIPE SAME DIRECTION	S	OTHER - SEE REPORT	NO IMPAIRMENT	NONE APPARENT	001	OTHER
4486	025A	192.99	01/11/13	1251	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
4487	025A	192.99	03/18/13	1620	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
4488	025A	192.99	04/17/13	0853	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
4489	025A	192.99	07/11/13	2003	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4490	025A	192.99	08/17/13	0114	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	065	WEAVING
4491	025A	192.99	01/03/14	1443	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	WEAVING
4492	025A	192.99	01/25/14	1402	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	CHANGING LANES
4493	025A	192.99	03/01/14	1401	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	070	PASSING
4494	025A	192.99	04/13/14	1639	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	040	OTHER
4495	025A	192.99	08/11/14	1828	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4496	025A	192.99	09/26/14	2123	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4497	025A	192.99	09/29/14	2219	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4498	025A	192.99	03/13/15	1154	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	CHANGING LANES
4499	025A	192.99	04/08/15	1352	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4500	025A	192.99	04/17/15	0227	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	035	OTHER

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4501	025A	192.99	04/24/15	0633	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4502	025A	192.99	06/05/15	1537	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4503	025A	192.99	06/14/15	1227	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4504	025A	192.99	06/22/15	1923	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4505	025A	192.99	07/21/15	0141	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4506	025A	192.99	08/20/15	1234	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4507	025A	192.99	09/02/15	1353	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4508	025A	192.99	09/17/15	0558	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4509	025A	192.99	09/18/15	1135	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4510	025A	192.99	09/21/15	1819	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
4511	025A	192.99	09/26/15	1716	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4512	025A	192.99	09/26/15	1839	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4513	025A	192.99	11/02/15	1434	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4514	025A	192.99	11/10/15	2047	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4515	025A	192.99	01/07/15	2131	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4516	025A	192.99	07/09/14	0423	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4517	025A	192.99	04/04/13	0303	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
4518	025A	192.99	01/04/14	0925	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4519	025A	192.99	12/30/12	1857	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4520	025A	192.99	02/07/12	0607	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4521	025A	192.99	01/23/14	0420	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4522	025A	192.99	12/01/11	0806	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4523	025A	192.99	08/08/13	2017	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4524	025A	192.99	03/05/14	0704	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
4525	025A	192.99	12/01/11	0711	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4526	025A	192.99	02/03/12	1155	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4527	025A	192.99	09/12/12	1354	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4528	025A	192.99	10/24/12	2250	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	DUST
4529	025A	192.99	12/09/12	0437	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4530	025A	192.99	12/11/12	0150	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	FOREIGN MATERIAL	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4531	025A	192.99	12/15/12	1727	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4532	025A	192.99	02/21/13	1745	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4533	025A	192.99	04/29/13	2248	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4534	025A	192.99	12/04/13	0506	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4535	025A	192.99	04/03/14	0033	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4536	025A	192.99	08/29/14	1549	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4537	025A	192.99	11/12/14	0244	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4538	025A	192.99	01/18/15	0338	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4539	025A	192.99	02/01/15	0734	INJ	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY W/VIS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/HAIL
4540	025A	192.99	11/29/15	0610	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4541	025A	192.99	12/18/15	2326	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
4542	025A	192.99	12/26/15	0243	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4543	025A	192.99	03/16/13	1155	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4544	025A	192.99	08/03/12	1445	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4545	025A	192.99	05/29/15	1200	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4546	025A	192.99	06/16/15	1034	PDO	ON	NON-INTERSECTION	1	UNKNOWN	DRY	DAYLIGHT	NONE
4547	025A	192.99	04/14/12	1150	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4548	025A	192.99	09/26/12	1306	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4549	025A	192.99	09/27/12	0739	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4550	025A	192.99	11/10/12	1952	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4551	025A	192.99	11/14/12	0847	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4552	025A	192.99	12/18/12	0843	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4553	025A	192.99	03/16/13	1434	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4554	025A	192.99	03/24/13	2120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4555	025A	192.99	03/31/14	0900	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4556	025A	192.99	04/13/14	1600	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4557	025A	192.99	06/06/14	1410	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4558	025A	192.99	06/16/14	1708	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4559	025A	192.99	10/01/14	1553	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4560	025A	192.99	04/22/15	1529	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4501	025A	192.99	04/24/15	0633	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4502	025A	192.99	06/05/15	1537	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4503	025A	192.99	06/14/15	1227	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4504	025A	192.99	06/22/15	1923	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4505	025A	192.99	07/21/15	0141	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	055	OTHER
4506	025A	192.99	08/20/15	1234	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4507	025A	192.99	09/02/15	1353	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4508	025A	192.99	09/17/15	0558	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4509	025A	192.99	09/18/15	1135	N	SIDESWIPE SAME DIRECTION	S	MOTOR HOME	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4510	025A	192.99	09/21/15	1819	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4511	025A	192.99	09/26/15	1716	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	CHANGING LANES
4512	025A	192.99	09/26/15	1839	N	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4513	025A	192.99	11/02/15	1434	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4514	025A	192.99	11/10/15	2047	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4515	025A	192.99	01/07/15	2131	N	PARKED MOTOR VEHICLE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4516	025A	192.99	07/09/14	0423	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4517	025A	192.99	04/04/13	0303	N	LIGHT/UTILITY POLE	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
4518	025A	192.99	01/04/14	0925	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	CHANGING LANES
4519	025A	192.99	12/30/12	1857	N	TRAFFIC SIGNAL POLE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
4520	025A	192.99	02/07/12	0607	N	SIGN	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	OTHER
4521	025A	192.99	01/23/14	0420	N	SIGN	N	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4522	025A	192.99	12/01/11	0806	N	GUARD RAIL	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
4523	025A	192.99	08/08/13	2017	N	GUARD RAIL	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	060	GOING STRAIGHT
4524	025A	192.99	03/05/14	0704	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4525	025A	192.99	12/01/11	0711	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4526	025A	192.99	02/03/12	1155	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4527	025A	192.99	09/12/12	1354	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4528	025A	192.99	10/24/12	2250	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
4529	025A	192.99	12/09/12	0437	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
4530	025A	192.99	12/11/12	0150	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
4531	025A	192.99	12/15/12	1727	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4532	025A	192.99	02/21/13	1745	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4533	025A	192.99	04/29/13	2248	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	CHANGING LANES
4534	025A	192.99	12/04/13	0506	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
4535	025A	192.99	04/03/14	0033	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
4536	025A	192.99	08/29/14	1549	N	CONCRETE BARRIER	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	055	OTHER
4537	025A	192.99	11/12/14	0244	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4538	025A	192.99	01/18/15	0338	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4539	025A	192.99	02/01/15	0734	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
4540	025A	192.99	11/29/15	0610	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
4541	025A	192.99	12/18/15	2326	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4542	025A	192.99	12/26/15	0243	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4543	025A	192.99	03/16/13	1155	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4544	025A	192.99	08/03/12	1445	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	SLOWING
4545	025A	192.99	05/29/15	1200	N	INVOLVING OTHER OBJECT	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4546	025A	192.99	06/16/15	1034	N	INVOLVING OTHER OBJECT	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	AVOIDING OBJECT/VEHICLE IN ROAD
4547	025A	192.99	04/14/12	1150	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4548	025A	192.99	09/26/12	1306	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4549	025A	192.99	09/27/12	0739	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	022	GOING STRAIGHT
4550	025A	192.99	11/10/12	1952	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4551	025A	192.99	11/14/12	0847	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4552	025A	192.99	12/18/12	0843	N	REAR-END	N	PASS CAR/VAN W/TRAILER	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
4553	025A	192.99	03/16/13	1434	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
4554	025A	192.99	03/24/13	2120	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4555	025A	192.99	03/31/14	0900	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4556	025A	192.99	04/13/14	1600	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
4557	025A	192.99	06/06/14	1410	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
4558	025A	192.99	06/16/14	1708	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4559	025A	192.99	10/01/14	1553	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4560	025A	192.99	04/22/15	1529	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4561	025A	192.99	07/08/15	0740	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4562	025A	192.99	07/20/15	1425	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4563	025A	192.99	09/01/15	1237	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4564	025A	192.99	09/08/15	1739	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4565	025A	192.99	09/20/15	0946	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4566	025A	192.99	10/08/15	0657	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4567	025A	192.99	10/10/15	1812	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4568	025A	192.99	10/14/15	1353	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4569	025A	192.99	11/11/15	0728	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4570	025A	192.99	12/06/15	1655	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4571	025A	192.99	07/10/12	0923	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4572	025A	192.99	01/06/13	2140	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4573	025A	192.99	03/02/11	0754	PDO	ON	NON-INTERSECTION	3	HILLCREST	DRY	DAYLIGHT	NONE
4574	025A	192.99	03/29/11	0802	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4575	025A	192.99	03/29/11	0807	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4576	025A	192.99	05/24/11	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4577	025A	192.99	06/06/11	1454	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4578	025A	192.99	09/18/11	1058	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4579	025A	192.99	09/24/11	1902	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4580	025A	192.99	09/25/11	1151	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4581	025A	192.99	10/05/11	1617	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4582	025A	192.99	10/18/11	1232	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4583	025A	192.99	05/06/12	1033	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4584	025A	192.99	10/04/12	1056	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4585	025A	192.99	12/07/12	1402	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4586	025A	192.99	02/14/13	0747	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4587	025A	192.99	02/14/13	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4588	025A	192.99	02/26/13	0800	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4589	025A	192.99	02/26/13	1830	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4590	025A	192.99	03/05/13	0833	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4591	025A	192.99	03/08/13	1538	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4592	025A	192.99	03/13/13	0834	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4593	025A	192.99	03/13/13	1724	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4594	025A	192.99	03/26/13	0757	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4595	025A	192.99	03/26/13	0757	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4596	025A	192.99	05/29/13	1624	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4597	025A	192.99	05/29/13	1645	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4598	025A	192.99	07/06/13	1650	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4599	025A	192.99	07/18/13	1726	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4600	025A	192.99	07/23/13	0845	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4601	025A	192.99	07/24/13	1930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4602	025A	192.99	09/13/13	0912	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4603	025A	192.99	09/20/13	0702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4604	025A	192.99	12/04/13	1245	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4605	025A	192.99	12/05/13	1417	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE
4606	025A	192.99	12/12/13	1056	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4607	025A	192.99	12/26/13	1358	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4608	025A	192.99	01/30/14	1530	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4609	025A	192.99	02/03/14	0752	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4610	025A	192.99	02/03/14	0754	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4611	025A	192.99	08/23/14	1624	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4612	025A	192.99	08/26/14	1341	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4613	025A	192.99	08/27/14	1737	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4614	025A	192.99	09/04/14	1512	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
4615	025A	192.99	09/05/14	1724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4616	025A	192.99	10/04/14	1808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4617	025A	192.99	10/09/14	1811	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
4618	025A	192.99	10/23/14	1555	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4619	025A	192.99	10/30/14	1922	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4620	025A	192.99	12/05/14	1247	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4561	025A	192.99	07/08/15	0740	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	SLOWING
4562	025A	192.99	07/20/15	1425	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4563	025A	192.99	09/01/15	1237	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
4564	025A	192.99	09/08/15	1739	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4565	025A	192.99	09/20/15	0946	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
4566	025A	192.99	10/08/15	0657	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4567	025A	192.99	10/10/15	1812	N	REAR-END	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4568	025A	192.99	10/14/15	1353	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
4569	025A	192.99	11/11/15	0728	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4570	025A	192.99	12/06/15	1655	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4571	025A	192.99	07/10/12	0923	Y (A)	OVERTURNING	W	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	010	OTHER
4572	025A	192.99	01/06/13	2140	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4573	025A	192.99	03/02/11	0754	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4574	025A	192.99	03/29/11	0802	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4575	025A	192.99	03/29/11	0807	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4576	025A	192.99	05/24/11	1620	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4577	025A	192.99	06/06/11	1454	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4578	025A	192.99	09/18/11	1058	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	015	SLOWING
4579	025A	192.99	09/24/11	1902	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4580	025A	192.99	09/25/11	1151	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
4581	025A	192.99	10/05/11	1617	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
4582	025A	192.99	10/18/11	1232	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4583	025A	192.99	05/06/12	1033	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4584	025A	192.99	10/04/12	1056	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	UK	GOING STRAIGHT
4585	025A	192.99	12/07/12	1402	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	002	STOPPED IN TRAFFIC
4586	025A	192.99	02/14/13	0747	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
4587	025A	192.99	02/14/13	0800	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	035	CHANGING LANES
4588	025A	192.99	02/26/13	0800	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4589	025A	192.99	02/26/13	1830	Y (A)	REAR-END	W	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4590	025A	192.99	03/05/13	0833	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4591	025A	192.99	03/08/13	1538	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	SLOWING
4592	025A	192.99	03/13/13	0834	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4593	025A	192.99	03/13/13	1724	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
4594	025A	192.99	03/26/13	0757	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
4595	025A	192.99	03/26/13	0757	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4596	025A	192.99	05/29/13	1624	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
4597	025A	192.99	05/29/13	1645	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4598	025A	192.99	07/06/13	1650	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4599	025A	192.99	07/18/13	1726	Y (A)	REAR-END	E	SUV	ALCOHOL	NONE APPARENT	040	GOING STRAIGHT
4600	025A	192.99	07/23/13	0845	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4601	025A	192.99	07/24/13	1930	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4602	025A	192.99	09/13/13	0912	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
4603	025A	192.99	09/20/13	0702	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4604	025A	192.99	12/04/13	1245	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
4605	025A	192.99	12/05/13	1417	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4606	025A	192.99	12/12/13	1056	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4607	025A	192.99	12/26/13	1358	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4608	025A	192.99	01/30/14	1530	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4609	025A	192.99	02/03/14	0752	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4610	025A	192.99	02/03/14	0754	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4611	025A	192.99	08/23/14	1624	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
4612	025A	192.99	08/26/14	1341	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4613	025A	192.99	08/27/14	1737	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4614	025A	192.99	09/04/14	1512	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4615	025A	192.99	09/05/14	1724	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4616	025A	192.99	10/04/14	1808	Y (A)	REAR-END	W	PASS CAR/VAN	ALCOHOL	EVADING LAW ENFORCEMENT OFFICER	020	GOING STRAIGHT
4617	025A	192.99	10/09/14	1811	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	SLOWING
4618	025A	192.99	10/23/14	1555	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4619	025A	192.99	10/30/14	1922	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4620	025A	192.99	12/05/14	1247	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4621	025A	192.99	01/06/15	0942	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4622	025A	192.99	01/13/15	1821	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
4623	025A	192.99	02/03/15	1253	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4624	025A	192.99	08/12/15	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4625	025A	192.99	09/03/15	1149	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
4626	025A	192.99	11/02/15	1533	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4627	025A	192.99	11/08/15	1851	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4628	025A	192.99	12/29/15	1237	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4629	025A	192.99	01/02/11	1614	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4630	025A	192.99	06/09/11	1815	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4631	025A	192.99	10/25/11	1747	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4632	025A	192.99	12/21/11	1641	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	SNOW/SLEET/HAIL
4633	025A	192.99	12/22/11	1812	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4634	025A	192.99	03/23/12	0729	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4635	025A	192.99	11/23/12	1710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4636	025A	192.99	01/18/13	1005	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4637	025A	192.99	01/28/13	1622	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
4638	025A	192.99	03/29/13	1125	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4639	025A	192.99	04/17/13	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4640	025A	192.99	06/25/13	1721	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4641	025A	192.99	09/28/13	1910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4642	025A	192.99	10/11/13	1905	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4643	025A	192.99	02/07/14	1056	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4644	025A	192.99	02/12/14	0748	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4645	025A	192.99	03/17/14	1510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4646	025A	192.99	08/07/14	2150	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4647	025A	192.99	08/30/14	2037	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4648	025A	192.99	01/22/15	1727	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4649	025A	192.99	11/23/15	0820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4650	025A	192.99	12/03/11	1600	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	NONE
4651	025A	192.99	09/20/14	2210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4652	025A	192.99	04/05/15	1847	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4653	025A	192.99	01/29/13	1904	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4654	025A	192.99	07/22/12	2121	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4655	025A	192.99	07/14/11	1422	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4656	025A	192.99	08/18/12	1959	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4657	025A	192.99	10/22/12	0744	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4658	025A	192.99	01/03/13	1712	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4659	025A	192.99	02/04/13	0845	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4660	025A	192.99	02/19/13	0704	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4661	025A	192.99	03/10/13	1817	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4662	025A	192.99	03/03/14	1714	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4663	025A	192.99	04/14/14	1744	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4664	025A	192.99	07/20/14	1550	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4665	025A	192.99	08/28/14	1720	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4666	025A	192.99	04/29/15	1652	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4667	025A	192.99	04/29/15	1836	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4668	025A	192.99	01/06/14	0830	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
4669	025A	192.99	10/23/12	2155	PDO	ON	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4670	025A	192.99	11/19/15	1108	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4671	025A	192.99	01/09/11	1731	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4672	025A	192.99	08/02/13	0900	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4673	025A	192.99	08/09/14	1550	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4674	025A	192.99	04/18/11	1331	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4675	025A	192.99	05/18/11	1720	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	WET	DAYLIGHT	NONE
4676	025A	192.99	05/26/11	1301	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4677	025A	192.99	06/09/11	0735	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4678	025A	192.99	06/21/11	1431	PDO	ON	AT INTERSECTION	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4679	025A	192.99	06/21/11	1453	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4680	025A	192.99	07/25/11	0815	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4621	025A	192.99	01/06/15	0942	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	SLOWING
4622	025A	192.99	01/13/15	1821	Y (A)	REAR-END	W	PASS CAR/VAN	ALCOHOL	UNKNOWN	025	GOING STRAIGHT
4623	025A	192.99	02/03/15	1253	Y (A)	REAR-END	E	SUV	ALCOHOL	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4624	025A	192.99	08/12/15	0735	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4625	025A	192.99	09/03/15	1149	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	007	SLOWING
4626	025A	192.99	11/02/15	1533	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4627	025A	192.99	11/08/15	1851	Y (A)	REAR-END	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
4628	025A	192.99	12/29/15	1237	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
4629	025A	192.99	01/02/11	1614	Y (A)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4630	025A	192.99	06/09/11	1815	Y (A)	SIDESWIPE SAME DIRECTION	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
4631	025A	192.99	10/25/11	1747	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	CHANGING LANES
4632	025A	192.99	12/21/11	1641	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	CHANGING LANES
4633	025A	192.99	12/22/11	1812	Y (A)	SIDESWIPE SAME DIRECTION	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	020	CHANGING LANES
4634	025A	192.99	03/23/12	0729	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	AVOIDING OBJECT/VEHICLE IN ROAD
4635	025A	192.99	11/23/12	1710	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	CHANGING LANES
4636	025A	192.99	01/18/13	1005	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
4637	025A	192.99	01/28/13	1622	Y (A)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	040	CHANGING LANES
4638	025A	192.99	03/29/13	1125	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	CHANGING LANES
4639	025A	192.99	04/17/13	0715	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
4640	025A	192.99	06/25/13	1721	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	CHANGING LANES
4641	025A	192.99	09/28/13	1910	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	CHANGING LANES
4642	025A	192.99	10/11/13	1905	Y (A)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
4643	025A	192.99	02/07/14	1056	Y (A)	SIDESWIPE SAME DIRECTION	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	040	CHANGING LANES
4644	025A	192.99	02/12/14	0748	Y (A)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4645	025A	192.99	03/17/14	1510	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	CHANGING LANES
4646	025A	192.99	08/07/14	2150	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	CHANGING LANES
4647	025A	192.99	08/30/14	2037	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4648	025A	192.99	01/22/15	1727	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	CHANGING LANES
4649	025A	192.99	11/23/15	0820	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	CHANGING LANES
4650	025A	192.99	12/03/11	1600	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4651	025A	192.99	09/20/14	2210	Y (A)	PARKED MOTOR VEHICLE	E	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	GOING STRAIGHT
4652	025A	192.99	04/05/15	1847	Y (A)	LIGHT/UTILITY POLE	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4653	025A	192.99	01/29/13	1904	Y (A)	TRAFFIC SIGNAL POLE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	OTHER
4654	025A	192.99	07/22/12	2121	Y (A)	INVOLVING OTHER OBJECT	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	CHANGING LANES
4655	025A	192.99	07/14/11	1422	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4656	025A	192.99	08/18/12	1959	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4657	025A	192.99	10/22/12	0744	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4658	025A	192.99	01/03/13	1712	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4659	025A	192.99	02/04/13	0845	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4660	025A	192.99	02/19/13	0704	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4661	025A	192.99	03/10/13	1817	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4662	025A	192.99	03/03/14	1714	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4663	025A	192.99	04/14/14	1744	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
4664	025A	192.99	07/20/14	1550	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4665	025A	192.99	08/28/14	1720	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
4666	025A	192.99	04/29/15	1652	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
4667	025A	192.99	04/29/15	1836	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4668	025A	192.99	01/06/14	0830	Y (I)	REAR-END	W	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4669	025A	192.99	10/23/12	2155	Y (N)	OVERTURNING	E	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
4670	025A	192.99	11/19/15	1108	Y (N)	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	MAKING LEFT TURN
4671	025A	192.99	01/09/11	1731	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4672	025A	192.99	08/02/13	0900	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
4673	025A	192.99	08/09/14	1550	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4674	025A	192.99	04/18/11	1331	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4675	025A	192.99	05/18/11	1720	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	MAKING RIGHT TURN
4676	025A	192.99	05/26/11	1301	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	008	GOING STRAIGHT
4677	025A	192.99	06/09/11	0735	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4678	025A	192.99	06/21/11	1431	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
4679	025A	192.99	06/21/11	1453	Y (N)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4680	025A	192.99	07/25/11	0815	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4681	025A	192.99	08/22/11	1443	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4682	025A	192.99	08/30/11	1243	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4683	025A	192.99	10/31/11	0911	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4684	025A	192.99	06/06/12	0830	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4685	025A	192.99	07/09/12	1715	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4686	025A	192.99	11/14/12	1729	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4687	025A	192.99	12/24/12	1525	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4688	025A	192.99	04/08/13	1724	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4689	025A	192.99	08/16/13	0655	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4690	025A	192.99	05/25/14	1409	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4691	025A	192.99	07/05/14	1442	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4692	025A	192.99	11/17/14	0817	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4693	025A	192.99	01/30/15	1400	PDO	ON	INTERSECTION RELATED	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4694	025A	192.99	02/12/15	1604	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4695	025A	192.99	05/28/15	0807	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4696	025A	192.99	06/08/15	0905	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4697	025A	192.99	07/03/15	1448	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4698	025A	192.99	08/20/15	1605	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4699	025A	192.99	09/04/15	1512	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4700	025A	192.99	09/24/15	2301	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4701	025A	192.99	10/06/15	1349	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4702	025A	192.99	11/02/15	0810	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4703	025A	192.99	03/24/11	0756	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4704	025A	192.99	07/21/11	1011	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4705	025A	192.99	07/28/11	1212	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4706	025A	192.99	09/14/11	2330	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
4707	025A	192.99	11/19/13	1536	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4708	025A	192.99	01/08/14	1757	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4709	025A	192.99	09/10/14	1615	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4710	025A	192.99	11/14/14	0649	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	UNKNOWN	DAYLIGHT	NONE
4711	025A	192.99	12/23/14	2218	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
4712	025A	192.99	05/12/15	0823	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4713	025A	192.99	07/03/15	1310	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4714	025A	192.99	03/20/12	1837	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4715	025A	192.99	10/14/14	2010	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4716	025A	192.99	12/18/11	0935	PDO	ON	AT INTERSECTION	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4717	025A	192.99	04/18/14	0755	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4718	025A	192.99	06/02/14	1542	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4719	025A	192.99	08/13/14	1428	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4720	025A	192.99	07/08/15	1529	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4721	025A	192.99	03/28/13	1253	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4722	025A	192.99	07/17/12	0742	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4723	025A	192.99	03/31/12	1301	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4724	025A	192.99	04/16/15	1922	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4725	025A	192.99	01/04/11	1626	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4726	025A	192.99	01/19/11	1705	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4727	025A	192.99	01/19/11	1727	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4728	025A	192.99	07/12/11	1748	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4729	025A	192.99	07/13/11	1040	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4730	025A	192.99	07/21/11	0717	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4731	025A	192.99	08/04/11	1700	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4732	025A	192.99	08/16/11	1413	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4733	025A	192.99	09/11/11	1925	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4734	025A	192.99	11/16/11	1420	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4735	025A	192.99	01/17/12	0738	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	NONE
4736	025A	192.99	02/26/12	1445	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4737	025A	192.99	03/21/12	0945	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	SNOW/SLEET/HAIL
4738	025A	192.99	04/10/12	1637	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4739	025A	192.99	06/01/12	1701	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4740	025A	192.99	06/13/12	1312	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4681	025A	192.99	08/22/11	1443	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4682	025A	192.99	08/30/11	1243	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4683	025A	192.99	10/31/11	0911	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	SLOWING
4684	025A	192.99	06/06/12	0830	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4685	025A	192.99	07/09/12	1715	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4686	025A	192.99	11/14/12	1729	Y (N)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4687	025A	192.99	12/24/12	1525	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	008	SLOWING
4688	025A	192.99	04/08/13	1724	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
4689	025A	192.99	08/16/13	0655	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4690	025A	192.99	05/25/14	1409	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	SLOWING
4691	025A	192.99	07/05/14	1442	Y (N)	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	030	GOING STRAIGHT
4692	025A	192.99	11/17/14	0817	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4693	025A	192.99	01/30/15	1400	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4694	025A	192.99	02/12/15	1604	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4695	025A	192.99	05/28/15	0807	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
4696	025A	192.99	06/08/15	0905	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
4697	025A	192.99	07/03/15	1448	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4698	025A	192.99	08/20/15	1605	Y (N)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	SLOWING
4699	025A	192.99	09/04/15	1512	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	PHYSICAL DISABILITY	020	GOING STRAIGHT
4700	025A	192.99	09/24/15	2301	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
4701	025A	192.99	10/06/15	1349	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	STOPPED IN TRAFFIC
4702	025A	192.99	11/02/15	0810	Y (N)	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	005	MAKING LEFT TURN
4703	025A	192.99	03/24/11	0756	Y (N)	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
4704	025A	192.99	07/21/11	1011	Y (N)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4705	025A	192.99	07/28/11	1212	Y (N)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4706	025A	192.99	09/14/11	2330	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	025	MAKING LEFT TURN
4707	025A	192.99	11/19/13	1536	Y (N)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4708	025A	192.99	01/08/14	1757	Y (N)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	MAKING RIGHT TURN
4709	025A	192.99	09/10/14	1615	Y (N)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	MAKING LEFT TURN
4710	025A	192.99	11/14/14	0649	Y (N)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	MAKING LEFT TURN
4711	025A	192.99	12/23/14	2218	Y (N)	SIDESWIPE SAME DIRECTION	SE	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
4712	025A	192.99	05/12/15	0823	Y (N)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
4713	025A	192.99	07/03/15	1310	Y (N)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	CHANGING LANES
4714	025A	192.99	03/20/12	1837	Y (N)	BICYCLE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING RIGHT TURN
4715	025A	192.99	10/14/14	2010	Y (N)	CONCRETE BARRIER	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	MAKING LEFT TURN
4716	025A	192.99	12/18/11	0935	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	SLOWING
4717	025A	192.99	04/18/14	0755	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
4718	025A	192.99	06/02/14	1542	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	045	STOPPED IN TRAFFIC
4719	025A	192.99	08/13/14	1428	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
4720	025A	192.99	07/08/15	1529	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4721	025A	192.99	03/28/13	1253	Y (O)	OVERTURNING	W	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
4722	025A	192.99	07/17/12	0742	Y (O)	SIDESWIPE SAME DIRECTION	W	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4723	025A	192.99	03/31/12	1301	Y (O)	BROADSIDE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
4724	025A	192.99	04/16/15	1922	Y (O)	BROADSIDE	E	SUV	NO IMPAIRMENT	UNKNOWN	025	SLOWING
4725	025A	192.99	01/04/11	1626	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
4726	025A	192.99	01/19/11	1705	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
4727	025A	192.99	01/19/11	1727	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	SLOWING
4728	025A	192.99	07/12/11	1748	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4729	025A	192.99	07/13/11	1040	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4730	025A	192.99	07/21/11	0717	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
4731	025A	192.99	08/04/11	1700	Y (O)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4732	025A	192.99	08/16/11	1413	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4733	025A	192.99	09/11/11	1925	Y (O)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	SLOWING
4734	025A	192.99	11/16/11	1420	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4735	025A	192.99	01/17/12	0738	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
4736	025A	192.99	02/26/12	1445	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4737	025A	192.99	03/21/12	0945	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4738	025A	192.99	04/10/12	1637	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4739	025A	192.99	06/01/12	1701	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
4740	025A	192.99	06/13/12	1312	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4741	025A	192.99	02/10/13	1217	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4742	025A	192.99	02/14/13	1800	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4743	025A	192.99	02/19/13	0704	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4744	025A	192.99	03/14/13	0834	PDO	ON	INTERSECTION RELATED	3	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4745	025A	192.99	04/08/14	1448	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4746	025A	192.99	06/20/14	0659	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4747	025A	192.99	10/14/14	0919	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4748	025A	192.99	11/15/14	1909	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4749	025A	192.99	02/25/15	1718	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4750	025A	192.99	07/01/15	1802	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4751	025A	192.99	11/21/15	1009	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4752	025A	192.99	02/17/11	1710	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4753	025A	192.99	07/19/12	1415	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4754	025A	192.99	09/25/12	2051	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4755	025A	192.99	11/19/14	1550	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4756	025A	192.99	05/22/15	1632	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4757	025A	192.99	11/23/15	0744	INJ	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4758	025A	192.99	04/10/14	0631	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4759	025A	192.99	10/27/15	0637	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4760	025A	192.99	12/29/14	1509	PDO	OFF RIGHT	AT INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4761	025A	192.99	07/05/14	1123	INJ	OFF RIGHT	INTERSECTION RELATED	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4762	025A	192.99	04/01/11	2203	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4763	025A	192.99	10/23/13	0804	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4764	025A	192.99	03/03/14	1640	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4765	025A	192.99	06/04/14	1508	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4766	025A	192.99	08/15/14	1133	INJ	ON	INTERSECTION RELATED	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4767	025A	192.99	11/25/14	1649	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4768	025A	193.00	08/02/12	1034	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4769	025A	193.00	06/22/12	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4770	025A	193.00	06/22/12	0742	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4771	025A	193.00	08/30/13	1554	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4772	025A	193.00	09/27/13	1544	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4773	025A	193.00	10/23/13	1625	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4774	025A	193.00	12/14/13	1715	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4775	025A	193.00	05/20/14	0733	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4776	025A	193.00	03/03/15	1728	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	SNOW/SLEET/HAIL
4777	025A	193.00	11/13/15	1330	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4778	025A	193.00	11/10/11	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4779	025A	193.00	05/19/12	0948	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4780	025A	193.00	08/27/13	1137	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4781	025A	193.00	12/06/13	2001	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4782	025A	193.00	05/06/15	1438	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4783	025A	193.00	12/23/15	1100	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4784	025A	193.00	02/21/13	0703	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4785	025A	193.00	08/06/13	0318	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
4786	025A	193.00	03/07/14	1818	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-LIGHTED	SNOW/SLEET/HAIL
4787	025A	193.00	12/25/15	2200	PDO	OFF RIGHT	NON-INTERSECTION	1	HILLCREST	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4788	025A	193.00	06/20/13	1638	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4789	025A	193.00	12/05/13	1825	PDO	OFF LEFT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
4790	025A	193.00	07/18/14	0753	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4791	025A	193.00	08/08/15	1000	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4792	025A	193.00	10/13/15	0850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4793	025A	193.00	06/06/11	1044	PDO	ON	RAMP	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4794	025A	193.00	01/20/12	0759	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4795	025A	193.00	01/26/13	2215	PDO	ON	RAMP	2	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
4796	025A	193.00	12/23/12	1340	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4797	025A	193.00	12/05/15	0551	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4798	025A	193.00	06/04/11	0843	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4799	025A	193.01	12/26/14	0717	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4800	025A	193.01	04/18/12	0800	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4741	025A	192.99	02/10/13	1217	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	002	STOPPED IN TRAFFIC
4742	025A	192.99	02/14/13	1800	Y (O)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	005	MAKING RIGHT TURN
4743	025A	192.99	02/19/13	0704	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4744	025A	192.99	03/14/13	0834	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	015	CHANGING LANES
4745	025A	192.99	04/08/14	1448	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4746	025A	192.99	06/20/14	0659	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4747	025A	192.99	10/14/14	0919	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4748	025A	192.99	11/15/14	1909	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4749	025A	192.99	02/25/15	1718	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4750	025A	192.99	07/01/15	1802	Y (O)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4751	025A	192.99	11/21/15	1009	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4752	025A	192.99	02/17/11	1710	Y (O)	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	MAKING RIGHT TURN
4753	025A	192.99	07/19/12	1415	Y (O)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4754	025A	192.99	09/25/12	2051	Y (O)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	CHANGING LANES
4755	025A	192.99	11/19/14	1550	Y (O)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	MAKING LEFT TURN
4756	025A	192.99	05/22/15	1632	Y (O)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
4757	025A	192.99	11/23/15	0744	Y (O)	SIDESWIPE SAME DIRECTION	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	MAKING RIGHT TURN
4758	025A	192.99	04/10/14	0631	Y (O)	APPROACH TURN	W	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING U-TURN
4759	025A	192.99	10/27/15	0637	Y (O)	OVERTAKING TURN	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	GOING STRAIGHT
4760	025A	192.99	12/29/14	1509	Y (O)	SIGN	N	SUV	NO IMPAIRMENT	UNKNOWN	040	MAKING LEFT TURN
4761	025A	192.99	07/05/14	1123	Y (O)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
4762	025A	192.99	04/01/11	2203	Y (O)	REAR-END	E	SUV	ALCOHOL	UNKNOWN	040	GOING STRAIGHT
4763	025A	192.99	10/23/13	0804	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
4764	025A	192.99	03/03/14	1640	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
4765	025A	192.99	06/04/14	1508	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
4766	025A	192.99	08/15/14	1133	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	GOING STRAIGHT
4767	025A	192.99	11/25/14	1649	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4768	025A	193.00	08/02/12	1034	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4769	025A	193.00	06/22/12	0724	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4770	025A	193.00	06/22/12	0742	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	SLOWING
4771	025A	193.00	08/30/13	1554	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4772	025A	193.00	09/27/13	1544	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
4773	025A	193.00	10/23/13	1625	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4774	025A	193.00	12/14/13	1715	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4775	025A	193.00	05/20/14	0733	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4776	025A	193.00	03/03/15	1728	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
4777	025A	193.00	11/13/15	1330	N	REAR-END	S	SUV	NO IMPAIRMENT	ILLNESS	040	GOING STRAIGHT
4778	025A	193.00	11/10/11	1900	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	070	GOING STRAIGHT
4779	025A	193.00	05/19/12	0948	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
4780	025A	193.00	08/27/13	1137	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4781	025A	193.00	12/06/13	2001	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	WEAVING
4782	025A	193.00	05/06/15	1438	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4783	025A	193.00	12/23/15	1100	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4784	025A	193.00	02/21/13	0703	N	LIGHT/UTILITY POLE	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	OTHER
4785	025A	193.00	08/06/13	0318	N	LIGHT/UTILITY POLE	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4786	025A	193.00	03/07/14	1818	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
4787	025A	193.00	12/25/15	2200	N	SIGN	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	UK	OTHER
4788	025A	193.00	06/20/13	1638	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	UNKNOWN	065	OTHER
4789	025A	193.00	12/05/13	1825	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
4790	025A	193.00	07/18/14	0753	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
4791	025A	193.00	08/08/15	1000	N	REAR-END	N	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	015	GOING STRAIGHT
4792	025A	193.00	10/13/15	0850	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4793	025A	193.00	06/06/11	1044	Y (C)	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4794	025A	193.00	01/20/12	0759	Y (D)	REAR-END	SW	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
4795	025A	193.00	01/26/13	2215	Y (D)	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	010	STOPPED IN TRAFFIC
4796	025A	193.00	12/23/12	1340	Y (D)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	CHANGING LANES
4797	025A	193.00	12/05/15	0551	Y (D)	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4798	025A	193.00	06/04/11	0843	Y (D)	OVERTURNING	SW	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING RIGHT TURN
4799	025A	193.01	12/26/14	0717	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4800	025A	193.01	04/18/12	0800	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	003	STOPPED IN TRAFFIC

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4801	025A	193.01	06/12/14	0958	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4802	025A	193.01	12/29/12	1900	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4803	025A	193.01	10/31/15	1131	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4804	025A	193.01	07/25/14	0536	PDO	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4805	025A	193.01	05/15/12	1136	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4806	025A	193.01	07/27/14	2019	PDO	ON	RAMP	1	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
4807	025A	193.01	04/11/14	0831	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4808	025A	193.01	12/03/14	1445	INJ	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4809	025A	193.01	03/21/13	1255	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4810	025A	193.01	09/27/15	1643	INJ	ON	RAMP	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4811	025A	193.01	12/13/12	1838	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4812	025A	193.01	05/12/13	1558	PDO	ON	RAMP	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4813	025A	193.01	04/03/15	0935	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4814	025A	193.02	12/18/13	1538	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4815	025A	193.02	11/27/13	0143	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4816	025A	193.02	03/17/15	1004	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4817	025A	193.02	06/08/14	1536	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4818	025A	193.03	03/09/11	0913	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4819	025A	193.03	10/23/12	0806	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4820	025A	193.03	08/28/15	1054	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4821	025A	193.03	05/11/14	2300	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4822	025A	193.03	09/16/15	0827	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4823	025A	193.03	12/04/13	1134	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
4824	025A	193.03	10/23/11	1216	INJ	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
4825	025A	193.03	04/03/14	0904	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4826	025A	193.03	11/15/15	0737	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4827	025A	193.04	05/22/12	1206	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4828	025A	193.04	05/31/13	1223	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4829	025A	193.04	05/31/13	1309	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4830	025A	193.04	01/05/14	0702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	NONE
4831	025A	193.04	03/14/13	1324	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4832	025A	193.05	07/15/15	1452	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4833	025A	193.05	08/03/15	1534	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4834	025A	193.05	12/11/12	0200	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4835	025A	193.05	11/03/13	1821	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4836	025A	193.05	04/23/14	0847	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4837	025A	193.05	05/03/15	1138	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4838	025A	193.05	07/02/15	0954	PDO	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4839	025A	193.05	10/06/13	1303	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4840	025A	193.05	09/03/15	1756	PDO	OFF RIGHT	RAMP	1	HILLCREST	WET	DAYLIGHT	RAIN
4841	025A	193.05	05/31/14	1138	INJ	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4842	025A	193.05	09/09/15	1559	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4843	025A	193.06	01/19/13	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4844	025A	193.06	08/06/14	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4845	025A	193.07	04/16/11	1702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4846	025A	193.07	10/10/12	0750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4847	025A	193.07	07/19/13	1448	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4848	025A	193.08	11/25/11	1048	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4849	025A	193.08	12/09/14	0713	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4850	025A	193.08	12/09/14	0713	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4851	025A	193.08	02/08/11	2316	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	UNKNOWN	DARK-LIGHTED	NONE
4852	025A	193.08	03/10/12	0903	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4853	025A	193.09	02/17/13	0004	FAT	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4854	025A	193.09	11/30/15	0424	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4855	025A	193.09	04/04/14	1716	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4856	025A	193.10	11/14/12	0920	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4857	025A	193.10	11/15/12	1736	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4858	025A	193.10	12/03/11	1326	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	SNOW/SLEET/HAIL
4859	025A	193.10	08/28/15	1616	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
4860	025A	193.10	10/05/11	0844	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4801	025A	193.01	06/12/14	0958	Y (C)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4802	025A	193.01	12/29/12	1900	Y (C)	SIDESWIPE SAME DIRECTION	NW	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	MAKING RIGHT TURN
4803	025A	193.01	10/31/15	1131	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
4804	025A	193.01	07/25/14	0536	Y (C)	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4805	025A	193.01	05/15/12	1136	Y (C)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
4806	025A	193.01	07/27/14	2019	Y (C)	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	GOING STRAIGHT
4807	025A	193.01	04/11/14	0831	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4808	025A	193.01	12/03/14	1445	Y (C)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
4809	025A	193.01	03/21/13	1255	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
4810	025A	193.01	09/27/15	1643	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
4811	025A	193.01	12/13/12	1838	Y (D)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	CHANGING LANES
4812	025A	193.01	05/12/13	1558	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4813	025A	193.01	04/03/15	0935	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	STOPPED IN TRAFFIC
4814	025A	193.02	12/18/13	1538	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4815	025A	193.02	11/27/13	0143	N	LIGHT/UTILITY POLE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
4816	025A	193.02	03/17/15	1004	Y (D)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	005	SLOWING
4817	025A	193.02	06/08/14	1536	Y (D)	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	002	BACKING
4818	025A	193.03	03/09/11	0913	N	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4819	025A	193.03	10/23/12	0806	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4820	025A	193.03	08/28/15	1054	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
4821	025A	193.03	05/11/14	2300	N	BRIDGE RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	OTHER
4822	025A	193.03	09/16/15	0827	Y (C)	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
4823	025A	193.03	12/04/13	1134	Y (C)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
4824	025A	193.03	10/23/11	1216	Y (D)	OVERTURNING	S	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	GOING STRAIGHT
4825	025A	193.03	04/03/14	0904	Y (G)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4826	025A	193.03	11/15/15	0737	Y (I)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	CHANGING LANES
4827	025A	193.04	05/22/12	1206	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4828	025A	193.04	05/31/13	1223	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4829	025A	193.04	05/31/13	1309	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	002	SLOWING
4830	025A	193.04	01/05/14	0702	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4831	025A	193.04	03/14/13	1324	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4832	025A	193.05	07/15/15	1452	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	GOING STRAIGHT
4833	025A	193.05	08/03/15	1534	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
4834	025A	193.05	12/11/12	0200	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	OTHER
4835	025A	193.05	11/03/13	1821	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	045	CHANGING LANES
4836	025A	193.05	04/23/14	0847	Y (C)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4837	025A	193.05	05/03/15	1138	Y (C)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	CHANGING LANES
4838	025A	193.05	07/02/15	0954	Y (C)	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4839	025A	193.05	10/06/13	1303	Y (C)	OTHER FIXED OBJECT	NW	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4840	025A	193.05	09/03/15	1756	Y (D)	SIGN	SE	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	MAKING LEFT TURN
4841	025A	193.05	05/31/14	1138	Y (G)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4842	025A	193.05	09/09/15	1559	Y (I)	VEHICLE CARGO/DEBRIS	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	MAKING RIGHT TURN
4843	025A	193.06	01/19/13	1250	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4844	025A	193.06	08/06/14	0735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4845	025A	193.07	04/16/11	1702	N	VEHICLE CARGO/DEBRIS	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4846	025A	193.07	10/10/12	0750	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
4847	025A	193.07	07/19/13	1448	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4848	025A	193.08	11/25/11	1048	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
4849	025A	193.08	12/09/14	0713	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4850	025A	193.08	12/09/14	0713	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
4851	025A	193.08	02/08/11	2316	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4852	025A	193.08	03/10/12	0903	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4853	025A	193.09	02/17/13	0004	N	PEDESTRIAN	W	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	005	OTHER
4854	025A	193.09	11/30/15	0424	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	OTHER
4855	025A	193.09	04/04/14	1716	N	INVOLVING OTHER OBJECT	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	067	GOING STRAIGHT
4856	025A	193.10	11/14/12	0920	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4857	025A	193.10	11/15/12	1736	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
4858	025A	193.10	12/03/11	1326	N	LIGHT/UTILITY POLE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
4859	025A	193.10	08/28/15	1616	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4860	025A	193.10	10/05/11	0844	Y (D)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4861	025A	193.14	05/07/14	1244	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4862	025A	193.15	03/26/13	1120	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4863	025A	193.16	10/10/12	0649	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4864	025A	193.17	05/07/14	1243	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4865	025A	193.17	09/24/15	0638	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4866	025A	193.18	10/22/12	1541	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4867	025A	193.18	10/06/14	2022	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4868	025A	193.18	10/03/14	0952	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4869	025A	193.18	10/22/12	1541	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4870	025A	193.19	03/14/15	1154	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4871	025A	193.19	10/18/15	1050	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4872	025A	193.20	10/13/11	0946	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4873	025A	193.20	05/19/11	0744	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4874	025A	193.20	01/09/11	0901	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
4875	025A	193.21	05/07/14	0856	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4876	025A	193.23	08/12/11	1836	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4877	025A	193.24	07/31/11	2026	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4878	025A	193.24	05/02/15	2140	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4879	025A	193.24	02/10/11	0657	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4880	025A	193.24	04/04/12	0759	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4881	025A	193.24	04/14/13	1730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4882	025A	193.24	04/10/14	1858	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4883	025A	193.24	08/01/14	0631	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4884	025A	193.24	08/13/15	1649	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4885	025A	193.24	09/02/15	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4886	025A	193.24	11/03/15	1512	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4887	025A	193.24	11/25/15	1207	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4888	025A	193.24	04/02/14	1553	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4889	025A	193.24	08/12/14	1958	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4890	025A	193.24	03/25/15	0716	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4891	025A	193.24	11/27/11	0840	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
4892	025A	193.24	03/07/14	1650	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
4893	025A	193.24	01/01/14	1135	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
4894	025A	193.24	07/14/12	1930	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4895	025A	193.24	08/12/15	0702	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4896	025A	193.25	06/09/11	1616	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4897	025A	193.25	08/08/13	1250	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4898	025A	193.25	03/20/15	0834	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4899	025A	193.25	05/22/15	1224	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4900	025A	193.25	04/19/12	1559	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4901	025A	193.25	07/30/12	1625	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4902	025A	193.25	08/21/12	1522	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4903	025A	193.26	07/31/14	1335	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4904	025A	193.26	04/26/15	1818	PDO	ON	RAMP	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
4905	025A	193.27	11/19/13	0640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4906	025A	193.30	04/09/14	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4907	025A	193.30	04/09/14	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4908	025A	193.30	05/22/11	0952	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4909	025A	193.31	06/13/14	1657	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
4910	025A	193.31	09/10/14	0614	PDO	ON	RAMP	2	CURVE ON-LEVEL	WET	DAWN OR DUSK	RAIN
4911	025A	193.33	04/18/11	2310	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4912	025A	193.33	07/17/13	1722	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4913	025A	193.33	10/08/11	0126	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
4914	025A	193.33	10/11/15	0609	INJ	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4915	025A	193.34	06/22/15	0637	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4916	025A	193.35	08/20/15	2111	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4917	025A	193.40	03/14/15	0244	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4918	025A	193.44	09/18/15	0102	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4919	025A	193.45	07/03/14	1437	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4920	025A	193.46	11/25/14	1714	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4861	025A	193.14	05/07/14	1244	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
4862	025A	193.15	03/26/13	1120	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4863	025A	193.16	10/10/12	0649	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4864	025A	193.17	05/07/14	1243	N	INVOLVING OTHER OBJECT	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4865	025A	193.17	09/24/15	0638	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
4866	025A	193.18	10/22/12	1541	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4867	025A	193.18	10/06/14	2022	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4868	025A	193.18	10/03/14	0952	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	AVOIDING OBJECT/VEHICLE IN ROAD
4869	025A	193.18	10/22/12	1541	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4870	025A	193.19	03/14/15	1154	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4871	025A	193.19	10/18/15	1050	Y (C)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
4872	025A	193.20	10/13/11	0946	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	GOING STRAIGHT
4873	025A	193.20	05/19/11	0744	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	CHANGING LANES
4874	025A	193.20	01/09/11	0901	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	OTHER
4875	025A	193.21	05/07/14	0856	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4876	025A	193.23	08/12/11	1836	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
4877	025A	193.24	07/31/11	2026	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4878	025A	193.24	05/02/15	2140	N	VEHICLE CARGO/DEBRIS	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
4879	025A	193.24	02/10/11	0657	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4880	025A	193.24	04/04/12	0759	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4881	025A	193.24	04/14/13	1730	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4882	025A	193.24	04/10/14	1858	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4883	025A	193.24	08/01/14	0631	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	RX/MEDICATION/DR	UNKNOWN	070	GOING STRAIGHT
4884	025A	193.24	08/13/15	1649	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
4885	025A	193.24	09/02/15	0630	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4886	025A	193.24	11/03/15	1512	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
4887	025A	193.24	11/25/15	1207	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
4888	025A	193.24	04/02/14	1553	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	CHANGING LANES
4889	025A	193.24	08/12/14	1958	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4890	025A	193.24	03/25/15	0716	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4891	025A	193.24	11/27/11	0840	N	SIGN	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
4892	025A	193.24	03/07/14	1650	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
4893	025A	193.24	01/01/14	1135	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
4894	025A	193.24	07/14/12	1930	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
4895	025A	193.24	08/12/15	0702	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
4896	025A	193.25	06/09/11	1616	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	GOING STRAIGHT
4897	025A	193.25	08/08/13	1250	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
4898	025A	193.25	03/20/15	0834	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4899	025A	193.25	05/22/15	1224	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
4900	025A	193.25	04/19/12	1559	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4901	025A	193.25	07/30/12	1625	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	CHANGING LANES
4902	025A	193.25	08/21/12	1522	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	075	GOING STRAIGHT
4903	025A	193.26	07/31/14	1335	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
4904	025A	193.26	04/26/15	1818	Y (C)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4905	025A	193.27	11/19/13	0640	N	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
4906	025A	193.30	04/09/14	0724	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
4907	025A	193.30	04/09/14	0724	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4908	025A	193.30	05/22/11	0952	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	CHANGING LANES
4909	025A	193.31	06/13/14	1657	Y (D)	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4910	025A	193.31	09/10/14	0614	Y (D)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
4911	025A	193.33	04/18/11	2310	N	VEHICLE CARGO/DEBRIS	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4912	025A	193.33	07/17/13	1722	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
4913	025A	193.33	10/08/11	0126	N	TREE/SHRUBBERY	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	099	OTHER
4914	025A	193.33	10/11/15	0609	Y (D)	CRASH CUSHION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
4915	025A	193.34	06/22/15	0637	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
4916	025A	193.35	08/20/15	2111	Y (C)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	GOING STRAIGHT
4917	025A	193.40	03/14/15	0244	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	069	CHANGING LANES
4918	025A	193.44	09/18/15	0102	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
4919	025A	193.45	07/03/14	1437	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	050	GOING STRAIGHT
4920	025A	193.46	11/25/14	1714	Y (C)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4921	025A	193.48	09/18/12	1852	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4922	025A	193.48	08/31/12	1318	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4923	025A	193.49	09/24/12	1505	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4924	025A	193.49	06/21/11	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4925	025A	193.49	08/07/12	0700	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4926	025A	193.49	08/29/12	1755	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4927	025A	193.49	12/11/12	0044	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4928	025A	193.49	12/05/13	1935	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4929	025A	193.49	09/11/14	0842	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4930	025A	193.49	09/28/15	0851	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4931	025A	193.49	10/07/12	1337	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4932	025A	193.49	11/10/12	1736	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4933	025A	193.49	03/16/13	1345	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4934	025A	193.49	04/24/14	1457	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4935	025A	193.49	05/21/14	0820	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4936	025A	193.49	03/09/13	0654	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
4937	025A	193.49	09/14/11	2144	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
4938	025A	193.49	11/18/12	0837	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4939	025A	193.49	12/09/12	0044	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4940	025A	193.49	12/08/13	1653	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLUSHY	DARK-LIGHTED	NONE
4941	025A	193.49	05/15/14	0325	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4942	025A	193.49	08/04/12	1753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4943	025A	193.49	06/23/14	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4944	025A	193.49	10/08/15	0646	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4945	025A	193.50	12/29/13	1253	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4946	025A	193.50	07/06/15	0541	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4947	025A	193.50	06/18/12	1426	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4948	025A	193.50	08/09/12	1712	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4949	025A	193.50	10/24/12	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
4950	025A	193.50	01/24/13	1840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
4951	025A	193.50	01/24/13	1840	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4952	025A	193.50	02/19/13	1557	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4953	025A	193.50	06/20/13	1909	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4954	025A	193.50	08/03/13	2026	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
4955	025A	193.50	12/19/14	0753	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4956	025A	193.50	05/20/15	1539	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4957	025A	193.50	06/09/15	1602	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4958	025A	193.50	10/21/15	0743	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4959	025A	193.50	10/29/15	0458	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4960	025A	193.50	02/04/11	0540	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
4961	025A	193.50	08/08/12	0957	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4962	025A	193.50	02/21/13	2139	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
4963	025A	193.50	02/03/14	1616	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
4964	025A	193.50	10/14/14	1539	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4965	025A	193.50	04/26/15	0913	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4966	025A	193.50	10/14/15	1633	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4967	025A	193.50	01/29/13	0454	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
4968	025A	193.50	08/04/12	1447	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4969	025A	193.50	11/17/15	0240	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
4970	025A	193.50	11/26/14	1416	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4971	025A	193.50	05/21/15	1648	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4972	025A	193.55	04/26/11	1338	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4973	025A	193.56	04/06/15	0630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4974	025A	193.62	02/03/12	1626	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
4975	025A	193.69	01/08/11	1042	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4976	025A	193.73	09/25/13	2152	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4977	025A	193.74	11/12/15	1915	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4978	025A	193.74	12/04/15	1817	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
4979	025A	193.74	03/14/11	1014	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4980	025A	193.74	06/11/14	0813	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4921	025A	193.48	09/18/12	1852	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4922	025A	193.48	08/31/12	1318	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
4923	025A	193.49	09/24/12	1505	N	VEHICLE CARGO/DEBRIS	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
4924	025A	193.49	06/21/11	0735	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
4925	025A	193.49	08/07/12	0700	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	SLOWING
4926	025A	193.49	08/29/12	1755	N	REAR-END	S	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	005	STOPPED IN TRAFFIC
4927	025A	193.49	12/11/12	0044	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4928	025A	193.49	12/05/13	1935	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	OTHER
4929	025A	193.49	09/11/14	0842	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
4930	025A	193.49	09/28/15	0851	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
4931	025A	193.49	10/07/12	1337	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
4932	025A	193.49	11/10/12	1736	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	CHANGING LANES
4933	025A	193.49	03/16/13	1345	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	075	CHANGING LANES
4934	025A	193.49	04/24/14	1457	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4935	025A	193.49	05/21/14	0820	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	WEAVING
4936	025A	193.49	03/09/13	0654	N	SIGN	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
4937	025A	193.49	09/14/11	2144	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	SLOWING
4938	025A	193.49	11/18/12	0837	N	OVERTURNING	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	010	GOING STRAIGHT
4939	025A	193.49	12/09/12	0044	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4940	025A	193.49	12/08/13	1653	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4941	025A	193.49	05/15/14	0325	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	065	GOING STRAIGHT
4942	025A	193.49	08/04/12	1753	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4943	025A	193.49	06/23/14	1750	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4944	025A	193.49	10/08/15	0646	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
4945	025A	193.50	12/29/13	1253	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
4946	025A	193.50	07/06/15	0541	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
4947	025A	193.50	06/18/12	1426	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
4948	025A	193.50	08/09/12	1712	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4949	025A	193.50	10/24/12	1620	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
4950	025A	193.50	01/24/13	1840	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	SLOWING
4951	025A	193.50	01/24/13	1840	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
4952	025A	193.50	02/19/13	1557	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
4953	025A	193.50	06/20/13	1909	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	CHANGING LANES
4954	025A	193.50	08/03/13	2026	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
4955	025A	193.50	12/19/14	0753	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
4956	025A	193.50	05/20/15	1539	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4957	025A	193.50	06/09/15	1602	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4958	025A	193.50	10/21/15	0743	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
4959	025A	193.50	10/29/15	0458	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
4960	025A	193.50	02/04/11	0540	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
4961	025A	193.50	08/08/12	0957	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
4962	025A	193.50	02/21/13	2139	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	OTHER
4963	025A	193.50	02/03/14	1616	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	CHANGING LANES
4964	025A	193.50	10/14/14	1539	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	055	GOING STRAIGHT
4965	025A	193.50	04/26/15	0913	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
4966	025A	193.50	10/14/15	1633	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
4967	025A	193.50	01/29/13	0454	N	GUARD RAIL	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
4968	025A	193.50	08/04/12	1447	N	CONCRETE BARRIER	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	065	GOING STRAIGHT
4969	025A	193.50	11/17/15	0240	N	FENCE	N	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
4970	025A	193.50	11/26/14	1416	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
4971	025A	193.50	05/21/15	1648	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
4972	025A	193.55	04/26/11	1338	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4973	025A	193.56	04/06/15	0630	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
4974	025A	193.62	02/03/12	1626	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	CHANGING LANES
4975	025A	193.69	01/08/11	1042	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
4976	025A	193.73	09/25/13	2152	N	TREE/SHRUBBERY	N	PASS CAR/VAN	NO IMPAIRMENT	ILLNESS	065	GOING STRAIGHT
4977	025A	193.74	11/12/15	1915	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
4978	025A	193.74	12/04/15	1817	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4979	025A	193.74	03/14/11	1014	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
4980	025A	193.74	06/11/14	0813	Y (D)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
4981	025A	193.75	10/18/11	1457	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4982	025A	193.75	06/21/12	2118	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4983	025A	193.75	06/22/12	2330	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
4984	025A	193.75	12/12/12	1849	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4985	025A	193.79	03/21/12	0833	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4986	025A	193.80	02/25/13	1624	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
4987	025A	193.80	11/18/15	1149	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4988	025A	193.80	10/20/13	1134	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
4989	025A	193.80	11/20/15	0505	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
4990	025A	193.80	05/23/15	0907	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
4991	025A	193.80	10/24/14	1847	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
4992	025A	193.81	05/10/11	1644	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4993	025A	193.81	11/19/12	1437	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4994	025A	193.81	08/14/14	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4995	025A	193.81	04/20/13	1537	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4996	025A	193.81	05/09/13	1649	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
4997	025A	193.81	11/03/13	1758	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
4998	025A	193.81	10/21/15	1025	INJ	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
4999	025A	193.81	11/19/15	0825	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5000	025A	193.81	11/29/15	2141	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE
5001	025A	193.82	10/11/12	2237	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5002	025A	193.84	09/10/14	0958	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5003	025A	193.85	10/16/15	0546	INJ	ON	RAMP	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
5004	025A	193.88	04/09/15	0107	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	NONE
5005	025A	193.90	04/06/15	1315	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5006	025A	193.90	10/30/12	1021	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5007	025A	193.90	01/16/15	1240	PDO	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5008	025A	193.91	08/07/14	1749	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5009	025A	193.92	11/17/11	1722	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5010	025A	193.92	07/21/15	1558	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5011	025A	193.92	11/09/15	1612	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5012	025A	193.92	02/16/15	1715	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
5013	025A	193.92	10/26/12	1129	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5014	025A	193.92	03/03/12	1320	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	FOREIGN MATERIAL	DAYLIGHT	NONE
5015	025A	193.92	12/09/14	2253	PDO	ON	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5016	025A	193.92	04/19/15	2000	INJ	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5017	025A	193.94	05/30/14	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5018	025A	193.94	11/21/15	0108	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	NONE
5019	025A	193.94	12/27/15	0456	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5020	025A	193.96	07/16/12	1821	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5021	025A	193.96	06/18/13	1237	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5022	025A	193.97	01/20/15	0420	INJ	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
5023	025A	193.98	04/18/12	0706	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5024	025A	194.00	06/03/11	1516	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5025	025A	194.00	11/09/11	1822	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5026	025A	194.00	12/11/12	1711	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5027	025A	194.00	12/12/13	1849	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5028	025A	194.00	10/16/14	1108	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5029	025A	194.00	12/10/14	1812	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5030	025A	194.00	09/18/15	1512	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5031	025A	194.00	05/21/11	1722	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5032	025A	194.00	11/14/11	0800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5033	025A	194.00	04/17/14	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5034	025A	194.00	07/19/14	1439	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5035	025A	194.00	03/09/13	1023	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	UNKNOWN	DAYLIGHT	SNOW/SLEET/HAIL
5036	025A	194.00	09/13/13	1817	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5037	025A	194.00	07/30/14	0936	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5038	025A	194.00	05/09/15	0008	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
5039	025A	194.00	03/31/13	0051	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5040	025A	194.00	01/08/15	1853	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
4981	025A	193.75	10/18/11	1457	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	DRIVER PREOCCUPIED	060	OTHER
4982	025A	193.75	06/21/12	2118	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	008	GOING STRAIGHT
4983	025A	193.75	06/22/12	2330	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4984	025A	193.75	12/12/12	1849	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4985	025A	193.79	03/21/12	0833	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	GOING STRAIGHT
4986	025A	193.80	02/25/13	1624	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
4987	025A	193.80	11/18/15	1149	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	060	CHANGING LANES
4988	025A	193.80	10/20/13	1134	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	065	WEAVING
4989	025A	193.80	11/20/15	0505	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	OTHER
4990	025A	193.80	05/23/15	0907	N	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
4991	025A	193.80	10/24/14	1847	Y (E)	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	UNKNOWN	015	SLOWING
4992	025A	193.81	05/10/11	1644	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	075	GOING STRAIGHT
4993	025A	193.81	11/19/12	1437	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
4994	025A	193.81	08/14/14	1620	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
4995	025A	193.81	04/20/13	1537	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
4996	025A	193.81	05/09/13	1649	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER FATIGUE	015	CHANGING LANES
4997	025A	193.81	11/03/13	1758	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	SLOWING
4998	025A	193.81	10/21/15	1025	N	CONCRETE BARRIER	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
4999	025A	193.81	11/19/15	0825	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5000	025A	193.81	11/29/15	2141	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
5001	025A	193.82	10/11/12	2237	N	PARKED MOTOR VEHICLE	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	065	GOING STRAIGHT
5002	025A	193.84	09/10/14	0958	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5003	025A	193.85	10/16/15	0546	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
5004	025A	193.88	04/09/15	0107	N	OTHER NON-COLLISION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
5005	025A	193.90	04/06/15	1315	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	CHANGING LANES
5006	025A	193.90	10/30/12	1021	N	WILD ANIMAL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5007	025A	193.90	01/16/15	1240	Y (B)	OTHER NON-COLLISION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
5008	025A	193.91	08/07/14	1749	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5009	025A	193.92	11/17/11	1722	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
5010	025A	193.92	07/21/15	1558	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	CHANGING LANES
5011	025A	193.92	11/09/15	1612	N	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5012	025A	193.92	02/16/15	1715	N	OTHER FIXED OBJECT	N	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
5013	025A	193.92	10/26/12	1129	Y (B)	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5014	025A	193.92	03/03/12	1320	Y (B)	OVERTURNING	NE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	055	CHANGING LANES
5015	025A	193.92	12/09/14	2253	Y (B)	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	CHANGING LANES
5016	025A	193.92	04/19/15	2000	Y (B)	CRASH CUSHION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	080	GOING STRAIGHT
5017	025A	193.94	05/30/14	1720	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
5018	025A	193.94	11/21/15	0108	N	SIDESWIPE SAME DIRECTION	S	SUV	ALCOHOL	UNKNOWN	060	OTHER
5019	025A	193.94	12/27/15	0456	Y (B)	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	078	CHANGING LANES
5020	025A	193.96	07/16/12	1821	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	025	SLOWING
5021	025A	193.96	06/18/13	1237	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
5022	025A	193.97	01/20/15	0420	N	BARRICADE/TRAFFIC BARRIER	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER FATIGUE	055	MAKING RIGHT TURN
5023	025A	193.98	04/18/12	0706	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
5024	025A	194.00	06/03/11	1516	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	GOING STRAIGHT
5025	025A	194.00	11/09/11	1822	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
5026	025A	194.00	12/11/12	1711	N	REAR-END	S	SUV	NO IMPAIRMENT	ILLNESS	005	SLOWING
5027	025A	194.00	12/12/13	1849	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	STOPPED IN TRAFFIC
5028	025A	194.00	10/16/14	1108	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5029	025A	194.00	12/10/14	1812	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
5030	025A	194.00	09/18/15	1512	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5031	025A	194.00	05/21/11	1722	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
5032	025A	194.00	11/14/11	0800	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5033	025A	194.00	04/17/14	1800	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
5034	025A	194.00	07/19/14	1439	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
5035	025A	194.00	03/09/13	1023	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
5036	025A	194.00	09/13/13	1817	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
5037	025A	194.00	07/30/14	0936	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	060	WRONG WAY
5038	025A	194.00	05/09/15	0008	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
5039	025A	194.00	03/31/13	0051	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
5040	025A	194.00	01/08/15	1853	N	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5041	025A	194.00	05/15/15	1612	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5042	025A	194.00	08/27/15	0630	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5043	025A	194.00	12/16/15	0612	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-UNLIGHTED	NONE
5044	025A	194.00	05/13/11	1503	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5045	025A	194.00	03/28/15	1436	PDO	ON	RAMP	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5046	025A	194.00	04/09/13	0457	INJ	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5047	025A	194.02	09/29/11	1525	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5048	025A	194.04	08/12/12	2104	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5049	025A	194.04	11/10/14	0724	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5050	025A	194.04	05/24/12	1224	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5051	025A	194.05	05/10/15	1744	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5052	025A	194.06	11/17/11	1739	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5053	025A	194.06	11/17/11	1813	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5054	025A	194.06	02/20/14	1851	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5055	025A	194.06	08/20/14	2127	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5056	025A	194.06	04/10/14	1837	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5057	025A	194.06	09/16/15	0745	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5058	025A	194.06	02/23/12	0940	PDO	OFF LEFT	RAMP	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5059	025A	194.08	04/20/11	2236	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5060	025A	194.09	08/07/14	1556	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5061	025A	194.09	08/12/15	1733	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5062	025A	194.09	11/13/15	0944	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5063	025A	194.10	12/28/13	2131	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
5064	025A	194.10	07/30/11	1459	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5065	025A	194.10	11/14/13	1700	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5066	025A	194.12	12/14/11	1155	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5067	025A	194.13	09/28/11	1322	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5068	025A	194.19	05/07/14	0804	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5069	025A	194.19	02/04/11	0743	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
5070	025A	194.19	02/25/12	1407	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5071	025A	194.20	01/18/13	1808	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5072	025A	194.20	06/22/12	1810	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5073	025A	194.20	08/07/13	1650	INJ	OFF LEFT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5074	025A	194.20	03/04/15	1527	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5075	025A	194.21	12/06/14	1030	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5076	025A	194.21	06/18/15	0953	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5077	025A	194.21	02/04/11	0524	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	UNKNOWN	DARK-LIGHTED	NONE
5078	025A	194.22	03/21/11	1604	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5079	025A	194.22	03/12/14	2150	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	NONE
5080	025A	194.22	05/08/14	0250	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	RAIN
5081	025A	194.22	05/09/13	1530	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5082	025A	194.25	08/06/13	0930	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5083	025A	194.25	12/16/11	1803	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5084	025A	194.25	04/20/12	1623	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5085	025A	194.25	08/01/13	1721	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5086	025A	194.25	03/24/15	1312	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5087	025A	194.25	07/21/15	0632	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5088	025A	194.25	03/27/14	0945	INJ	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5089	025A	194.25	11/29/11	0709	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5090	025A	194.25	01/29/11	0345	INJ	OFF LEFT	RAMP	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5091	025A	194.25	05/29/14	1610	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5092	025A	194.25	01/03/14	1625	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5093	025A	194.26	11/02/15	1850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5094	025A	194.26	11/14/15	1204	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5095	025A	194.26	03/20/14	0740	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5096	025A	194.26	10/27/12	0226	PDO	OFF RIGHT	RAMP	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5097	025A	194.27	10/17/13	1742	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5098	025A	194.27	06/18/15	1613	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5099	025A	194.27	08/12/15	1506	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5100	025A	194.27	11/10/14	1423	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	WIND

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5041	025A	194.00	05/15/15	1612	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	020	CHANGING LANES
5042	025A	194.00	08/27/15	0630	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	GOING STRAIGHT
5043	025A	194.00	12/16/15	0612	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
5044	025A	194.00	05/13/11	1503	Y (B)	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5045	025A	194.00	03/28/15	1436	Y (E)	OTHER NON-COLLISION	S	SUV	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
5046	025A	194.00	04/09/13	0457	Y (E)	BARRICADE/TRAFFIC BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
5047	025A	194.02	09/29/11	1525	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	PASSING
5048	025A	194.04	08/12/12	2104	Y (B)	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	OTHER
5049	025A	194.04	11/10/14	0724	Y (H)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
5050	025A	194.04	05/24/12	1224	Y (H)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5051	025A	194.05	05/10/15	1744	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
5052	025A	194.06	11/17/11	1739	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
5053	025A	194.06	11/17/11	1813	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
5054	025A	194.06	02/20/14	1851	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	035	AVOIDING OBJECT/VEHICLE IN ROAD
5055	025A	194.06	08/20/14	2127	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	SLOWING
5056	025A	194.06	04/10/14	1837	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5057	025A	194.06	09/16/15	0745	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
5058	025A	194.06	02/23/12	0940	Y (E)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
5059	025A	194.08	04/20/11	2236	N	DELINEATOR POST	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
5060	025A	194.09	08/07/14	1556	Y (E)	SIDESWIPE SAME DIRECTION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	010	CHANGING LANES
5061	025A	194.09	08/12/15	1733	Y (E)	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5062	025A	194.09	11/13/15	0944	Y (E)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	035	STOPPED IN TRAFFIC
5063	025A	194.10	12/28/13	2131	Y (B)	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	OTHER
5064	025A	194.10	07/30/11	1459	Y (E)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	060	CHANGING LANES
5065	025A	194.10	11/14/13	1700	Y (H)	REAR-END	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	020	SLOWING
5066	025A	194.12	12/14/11	1155	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
5067	025A	194.13	09/28/11	1322	N	CONCRETE BARRIER	N	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	UNKNOWN	060	SLOWING
5068	025A	194.19	05/07/14	0804	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5069	025A	194.19	02/04/11	0743	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	060	OTHER
5070	025A	194.19	02/25/12	1407	Y (E)	OTHER NON-COLLISION	S	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5071	025A	194.20	01/18/13	1808	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5072	025A	194.20	06/22/12	1810	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	OTHER
5073	025A	194.20	08/07/13	1650	Y (B)	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
5074	025A	194.20	03/04/15	1527	Y (E)	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	UNKNOWN	010	SLOWING
5075	025A	194.21	12/06/14	1030	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	035	SLOWING
5076	025A	194.21	06/18/15	0953	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
5077	025A	194.21	02/04/11	0524	N	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	060	OTHER
5078	025A	194.22	03/21/11	1604	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
5079	025A	194.22	03/12/14	2150	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5080	025A	194.22	05/08/14	0250	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	OTHER
5081	025A	194.22	05/09/13	1530	Y (B)	SIGN	NE	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
5082	025A	194.25	08/06/13	0930	N	VEHICLE CARGO/DEBRIS	N	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
5083	025A	194.25	12/16/11	1803	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5084	025A	194.25	04/20/12	1623	N	REAR-END	SW	SUV	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
5085	025A	194.25	08/01/13	1721	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	020	SLOWING
5086	025A	194.25	03/24/15	1312	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
5087	025A	194.25	07/21/15	0632	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
5088	025A	194.25	03/27/14	0945	Y (E)	REAR-END	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
5089	025A	194.25	11/29/11	0709	Y (E)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	AVOIDING OBJECT/VEHICLE IN ROAD
5090	025A	194.25	01/29/11	0345	Y (F)	OVERTURNING	E	SUV	ALCOHOL	UNKNOWN	070	WEAVING
5091	025A	194.25	05/29/14	1610	Y (J)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
5092	025A	194.25	01/03/14	1625	Y (J)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	UK	CHANGING LANES
5093	025A	194.26	11/02/15	1850	N	CRASH CUSHION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
5094	025A	194.26	11/14/15	1204	N	CRASH CUSHION	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
5095	025A	194.26	03/20/14	0740	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
5096	025A	194.26	10/27/12	0226	Y (E)	CONCRETE BARRIER	SE	SUV	NO IMPAIRMENT	UNKNOWN	050	MAKING RIGHT TURN
5097	025A	194.27	10/17/13	1742	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5098	025A	194.27	06/18/15	1613	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5099	025A	194.27	08/12/15	1506	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	SLOWING
5100	025A	194.27	11/10/14	1423	N	CONCRETE BARRIER	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5101	025A	194.27	10/04/12	0013	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5102	025A	194.27	06/14/12	2237	PDO	ON	RAMP	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5103	025A	194.27	12/26/15	0725	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5104	025A	194.28	11/16/12	1513	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5105	025A	194.28	02/05/14	0608	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
5106	025A	194.28	05/19/14	0936	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5107	025A	194.28	10/27/11	1742	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5108	025A	194.28	02/04/12	1003	PDO	ON	RAMP	2	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
5109	025A	194.28	10/21/11	0650	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
5110	025A	194.28	02/04/12	0957	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SLUSHY	DAYLIGHT	NONE
5111	025A	194.28	04/10/14	1215	INJ	ON	RAMP	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5112	025A	194.29	10/29/12	1545	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5113	025A	194.29	06/01/13	1206	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5114	025A	194.29	05/19/15	0630	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
5115	025A	194.29	06/14/12	2236	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5116	025A	194.29	07/02/14	1550	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5117	025A	194.29	08/06/12	1651	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5118	025A	194.29	05/08/15	0802	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
5119	025A	194.29	05/04/12	1649	INJ	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5120	025A	194.29	02/23/12	0430	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
5121	025A	194.30	01/23/15	0542	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5122	025A	194.30	03/23/13	1757	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5123	025A	194.30	12/29/13	0933	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5124	025A	194.30	12/29/13	0950	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
5125	025A	194.30	04/13/14	1607	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5126	025A	194.30	12/26/15	0907	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
5127	025A	194.30	12/25/15	1446	PDO	OFF LEFT	RAMP	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5128	025A	194.30	05/02/13	1917	PDO	OFF LEFT	RAMP	3	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5129	025A	194.30	05/30/11	2023	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
5130	025A	194.31	10/13/12	2224	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5131	025A	194.31	05/23/13	0800	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5132	025A	194.31	05/24/15	1542	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5133	025A	194.31	06/02/13	1609	PDO	ON	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5134	025A	194.31	08/25/11	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5135	025A	194.31	04/24/13	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5136	025A	194.31	05/07/14	1314	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5137	025A	194.31	08/11/11	1101	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5138	025A	194.31	11/03/11	0716	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	SLUSHY	DAYLIGHT	NONE
5139	025A	194.31	01/27/12	1607	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5140	025A	194.31	02/15/12	1646	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5141	025A	194.31	03/02/12	0730	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5142	025A	194.31	05/12/12	1627	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5143	025A	194.31	07/27/12	1655	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5144	025A	194.31	11/06/12	1711	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5145	025A	194.31	03/19/13	1817	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5146	025A	194.31	06/21/13	1539	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5147	025A	194.31	06/29/13	0928	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	NONE
5148	025A	194.31	07/02/13	1543	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5149	025A	194.31	09/09/13	1230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5150	025A	194.31	11/22/13	1726	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5151	025A	194.31	12/20/13	1748	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5152	025A	194.31	12/20/13	1748	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5153	025A	194.31	01/15/14	1828	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5154	025A	194.31	01/17/14	1739	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5155	025A	194.31	03/04/14	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5156	025A	194.31	05/30/14	1609	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5157	025A	194.31	07/17/14	0259	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5158	025A	194.31	08/21/14	1541	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5159	025A	194.31	08/22/14	1230	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5160	025A	194.31	10/05/14	1907	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5101	025A	194.27	10/04/12	0013	Y (E)	OVERTURNING	E	PASS CAR/VAN	ALCOHOL	UNKNOWN	060	MAKING RIGHT TURN
5102	025A	194.27	06/14/12	2237	Y (E)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
5103	025A	194.27	12/26/15	0725	Y (G)	CONCRETE BARRIER	E	SUV	NO IMPAIRMENT	UNKNOWN	050	OTHER
5104	025A	194.28	11/16/12	1513	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	065	CHANGING LANES
5105	025A	194.28	02/05/14	0608	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	050	SLOWING
5106	025A	194.28	05/19/14	0936	N	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5107	025A	194.28	10/27/11	1742	Y (E)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5108	025A	194.28	02/04/12	1003	Y (E)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	UNKNOWN	045	OTHER
5109	025A	194.28	10/21/11	0650	Y (G)	SIDESWIPE SAME DIRECTION	NE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	WEAVING
5110	025A	194.28	02/04/12	0957	Y (G)	SIGN	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
5111	025A	194.28	04/10/14	1215	Y (H)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
5112	025A	194.29	10/29/12	1545	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
5113	025A	194.29	06/01/13	1206	N	OVERTURNING	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	070	CHANGING LANES
5114	025A	194.29	05/19/15	0630	N	CONCRETE BARRIER	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
5115	025A	194.29	06/14/12	2236	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	GOING STRAIGHT
5116	025A	194.29	07/02/14	1550	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
5117	025A	194.29	08/06/12	1651	Y (G)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
5118	025A	194.29	05/08/15	0802	Y (G)	LIGHT/UTILITY POLE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
5119	025A	194.29	05/04/12	1649	Y (G)	OVERTURNING	E	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	GOING STRAIGHT
5120	025A	194.29	02/23/12	0430	Y (I)	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	OTHER
5121	025A	194.30	01/23/15	0542	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	GOING STRAIGHT
5122	025A	194.30	03/23/13	1757	Y (G)	SIGN	NE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	GOING STRAIGHT
5123	025A	194.30	12/29/13	0933	Y (G)	CONCRETE BARRIER	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
5124	025A	194.30	12/29/13	0950	Y (G)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	AVOIDING OBJECT/VEHICLE IN ROAD
5125	025A	194.30	04/13/14	1607	Y (G)	CONCRETE BARRIER	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	035	GOING STRAIGHT
5126	025A	194.30	12/26/15	0907	Y (G)	GUARD RAIL	NE	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5127	025A	194.30	12/25/15	1446	Y (G)	CRASH CUSHION	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	PASSING
5128	025A	194.30	05/02/13	1917	Y (H)	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5129	025A	194.30	05/30/11	2023	Y (I)	GUARD RAIL	SW	PASS CAR/VAN	ALCOHOL	UNKNOWN	010	OTHER
5130	025A	194.31	10/13/12	2224	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	OTHER
5131	025A	194.31	05/23/13	0800	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	CHANGING LANES
5132	025A	194.31	05/24/15	1542	N	OVERTURNING	N	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	055	OTHER
5133	025A	194.31	06/02/13	1609	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5134	025A	194.31	08/25/11	1115	N	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5135	025A	194.31	04/24/13	1300	N	VEHICLE CARGO/DEBRIS	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	065	OTHER
5136	025A	194.31	05/07/14	1314	N	VEHICLE CARGO/DEBRIS	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5137	025A	194.31	08/11/11	1101	N	REAR-END	W	MOTOR HOME	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
5138	025A	194.31	11/03/11	0716	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	007	GOING STRAIGHT
5139	025A	194.31	01/27/12	1607	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	070	GOING STRAIGHT
5140	025A	194.31	02/15/12	1646	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
5141	025A	194.31	03/02/12	0730	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
5142	025A	194.31	05/12/12	1627	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5143	025A	194.31	07/27/12	1655	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	SLOWING
5144	025A	194.31	11/06/12	1711	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
5145	025A	194.31	03/19/13	1817	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	SLOWING
5146	025A	194.31	06/21/13	1539	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5147	025A	194.31	06/29/13	0928	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
5148	025A	194.31	07/02/13	1543	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5149	025A	194.31	09/09/13	1230	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
5150	025A	194.31	11/22/13	1726	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5151	025A	194.31	12/20/13	1748	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5152	025A	194.31	12/20/13	1748	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	018	GOING STRAIGHT
5153	025A	194.31	01/15/14	1828	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
5154	025A	194.31	01/17/14	1739	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	UK	GOING STRAIGHT
5155	025A	194.31	03/04/14	1720	N	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5156	025A	194.31	05/30/14	1609	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
5157	025A	194.31	07/17/14	0259	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	090	GOING STRAIGHT
5158	025A	194.31	08/21/14	1541	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	SLOWING
5159	025A	194.31	08/22/14	1230	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	050	GOING STRAIGHT
5160	025A	194.31	10/05/14	1907	N	REAR-END	S	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5161	025A	194.31	11/21/14	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5162	025A	194.31	12/13/14	1756	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5163	025A	194.31	02/04/15	2153	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5164	025A	194.31	06/02/15	1735	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5165	025A	194.31	06/21/15	1725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5166	025A	194.31	06/25/15	1710	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5167	025A	194.31	07/15/15	1641	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5168	025A	194.31	08/10/15	1814	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5169	025A	194.31	08/18/15	1426	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5170	025A	194.31	09/10/15	0635	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5171	025A	194.31	09/24/15	1622	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5172	025A	194.31	10/02/15	1557	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5173	025A	194.31	11/06/15	1903	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5174	025A	194.31	02/04/11	0725	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5175	025A	194.31	08/02/11	1434	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5176	025A	194.31	09/07/11	2042	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAWN OR DUSK	RAIN
5177	025A	194.31	06/06/12	2323	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5178	025A	194.31	02/15/13	1856	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5179	025A	194.31	04/18/13	0659	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	NONE
5180	025A	194.31	04/19/13	0539	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	NONE
5181	025A	194.31	11/07/13	1914	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5182	025A	194.31	03/04/14	1720	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5183	025A	194.31	09/14/14	2009	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5184	025A	194.31	09/26/14	1757	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5185	025A	194.31	10/07/14	1614	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	SNOW/SLEET/HAIL
5186	025A	194.31	10/10/14	0454	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
5187	025A	194.31	10/28/14	1903	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5188	025A	194.31	04/21/15	1714	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5189	025A	194.31	07/06/15	1800	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5190	025A	194.31	08/19/15	1600	PDO	ON	NON-INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
5191	025A	194.31	08/24/15	1633	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5192	025A	194.31	11/23/15	1859	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5193	025A	194.31	12/05/13	1952	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
5194	025A	194.31	05/13/11	0000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	WET	DARK-UNLIGHTED	RAIN
5195	025A	194.31	03/28/12	0618	INJ	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5196	025A	194.31	12/26/14	0611	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	UNKNOWN	DARK-LIGHTED	SNOW/SLEET/HAIL
5197	025A	194.31	01/09/11	1310	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5198	025A	194.31	02/04/11	0628	PDO	OFF LEFT	NON-INTERSECTION	6	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5199	025A	194.31	03/01/11	1812	INJ	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5200	025A	194.31	03/24/11	0926	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5201	025A	194.31	02/01/14	2132	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5202	025A	194.31	07/29/14	2215	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	RAIN
5203	025A	194.31	01/04/15	1742	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLUSHY	DARK-LIGHTED	NONE
5204	025A	194.31	02/16/15	0540	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5205	025A	194.31	06/05/15	1756	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5206	025A	194.31	11/17/15	2325	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	ICY W/VIS ICY ROAD TREATMENT	DARK-LIGHTED	NONE
5207	025A	194.31	07/26/11	0927	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5208	025A	194.31	02/02/13	1130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5209	025A	194.31	11/15/14	2235	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5210	025A	194.31	10/11/15	1715	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5211	025A	194.31	04/01/11	0752	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5212	025A	194.31	09/13/11	0744	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5213	025A	194.31	12/19/11	2228	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	RAIN
5214	025A	194.31	12/20/11	1713	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5215	025A	194.31	06/22/12	0717	PDO	ON	NON-INTERSECTION	4	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5216	025A	194.31	01/19/13	1726	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5217	025A	194.31	02/01/14	2158	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5218	025A	194.31	06/12/14	1610	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5219	025A	194.31	08/04/14	1747	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5220	025A	194.31	12/02/14	1752	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5161	025A	194.31	11/21/14	1700	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	SLOWING
5162	025A	194.31	12/13/14	1756	N	REAR-END	N	SUV	NO IMPAIRMENT	ASLEEP AT WHEEL	005	GOING STRAIGHT
5163	025A	194.31	02/04/15	2153	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	SLOWING
5164	025A	194.31	06/02/15	1735	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	SLOWING
5165	025A	194.31	06/21/15	1725	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
5166	025A	194.31	06/25/15	1710	N	REAR-END	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	010	GOING STRAIGHT
5167	025A	194.31	07/15/15	1641	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5168	025A	194.31	08/10/15	1814	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	CHANGING LANES
5169	025A	194.31	08/18/15	1426	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5170	025A	194.31	09/10/15	0635	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	CHANGING LANES
5171	025A	194.31	09/24/15	1622	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5172	025A	194.31	10/02/15	1557	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	ASLEEP AT WHEEL	040	GOING STRAIGHT
5173	025A	194.31	11/06/15	1903	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
5174	025A	194.31	02/04/11	0725	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	045	CHANGING LANES
5175	025A	194.31	08/02/11	1434	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
5176	025A	194.31	09/07/11	2042	N	SIDESWIPE SAME DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	065	CHANGING LANES
5177	025A	194.31	06/06/12	2323	N	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	ENTERING/LEAVING PARKED POSITION
5178	025A	194.31	02/15/13	1856	N	SIDESWIPE SAME DIRECTION	S	SUV	ALCOHOL	UNKNOWN	045	CHANGING LANES
5179	025A	194.31	04/18/13	0659	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	040	OTHER
5180	025A	194.31	04/19/13	0539	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
5181	025A	194.31	11/07/13	1914	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	040	WEAVING
5182	025A	194.31	03/04/14	1720	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	CHANGING LANES
5183	025A	194.31	09/14/14	2009	N	SIDESWIPE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	055	CHANGING LANES
5184	025A	194.31	09/26/14	1757	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	OTHER
5185	025A	194.31	10/07/14	1614	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5186	025A	194.31	10/10/14	0454	N	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
5187	025A	194.31	10/28/14	1903	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	UNKNOWN	045	OTHER
5188	025A	194.31	04/21/15	1714	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
5189	025A	194.31	07/06/15	1800	N	SIDESWIPE SAME DIRECTION	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
5190	025A	194.31	08/19/15	1600	N	SIDESWIPE SAME DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5191	025A	194.31	08/24/15	1633	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	UK	CHANGING LANES
5192	025A	194.31	11/23/15	1859	N	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
5193	025A	194.31	12/05/13	1952	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	GOING STRAIGHT
5194	025A	194.31	05/13/11	0000	N	SIGN	S	SUV	NO IMPAIRMENT	UNKNOWN	060	WEAVING
5195	025A	194.31	03/28/12	0618	N	BRIDGE RAIL	S	SUV	NO IMPAIRMENT	ILLNESS	070	GOING STRAIGHT
5196	025A	194.31	12/26/14	0611	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
5197	025A	194.31	01/09/11	1310	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	050	OTHER
5198	025A	194.31	02/04/11	0628	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	OTHER
5199	025A	194.31	03/01/11	1812	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5200	025A	194.31	03/24/11	0926	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
5201	025A	194.31	02/01/14	2132	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5202	025A	194.31	07/29/14	2215	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	OTHER
5203	025A	194.31	01/04/15	1742	N	CONCRETE BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	050	OTHER
5204	025A	194.31	02/16/15	0540	N	CONCRETE BARRIER	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	055	OTHER
5205	025A	194.31	06/05/15	1756	N	CONCRETE BARRIER	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	065	CHANGING LANES
5206	025A	194.31	11/17/15	2325	N	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
5207	025A	194.31	07/26/11	0927	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	060	CHANGING LANES
5208	025A	194.31	02/02/13	1130	N	EMBANKMENT CUT/FILL SLOPE	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	001	BACKING
5209	025A	194.31	11/15/14	2235	N	BARRICADE/TRAFFIC BARRIER	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	AVOIDING OBJECT/VEHICLE IN ROAD
5210	025A	194.31	10/11/15	1715	N	INVOLVING OTHER OBJECT	N	SUV	NO IMPAIRMENT	NONE APPARENT	055	GOING STRAIGHT
5211	025A	194.31	04/01/11	0752	N	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	045	GOING STRAIGHT
5212	025A	194.31	09/13/11	0744	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5213	025A	194.31	12/19/11	2228	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	045	OTHER
5214	025A	194.31	12/20/11	1713	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	OTHER
5215	025A	194.31	06/22/12	0717	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5216	025A	194.31	01/19/13	1726	N	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
5217	025A	194.31	02/01/14	2158	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	SLOWING
5218	025A	194.31	06/12/14	1610	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5219	025A	194.31	08/04/14	1747	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	CHANGING LANES
5220	025A	194.31	12/02/14	1752	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5221	025A	194.31	02/09/15	0629	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5222	025A	194.31	03/16/15	1625	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5223	025A	194.31	05/06/15	0737	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5224	025A	194.31	06/08/15	1941	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5225	025A	194.31	10/12/15	1528	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5226	025A	194.31	10/30/15	1556	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5227	025A	194.31	11/10/15	1546	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5228	025A	194.31	11/12/15	1923	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5229	025A	194.31	12/23/15	1657	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5230	025A	194.31	12/29/15	1725	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
5231	025A	194.31	09/24/13	0323	INJ	OFF AT TEE	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5232	025A	194.31	07/22/11	0850	INJ	ON	RAMP	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5233	025A	194.31	08/01/11	1429	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5234	025A	194.31	04/16/15	0834	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5235	025A	194.31	02/26/13	1728	PDO	ON	RAMP	2	STRAIGHT ON-LEVEL	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
5236	025A	194.31	07/09/11	1326	PDO	ON	RAMP	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5237	025A	194.31	04/12/15	0608	PDO	OFF LEFT	RAMP	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE
5238	025A	194.31	12/22/11	1327	PDO	OFF LEFT	RAMP	2	CURVE ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5239	025A	194.31	05/30/12	2313	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	DRY	DARK-LIGHTED	NONE
5240	025A	194.31	01/06/14	1000	PDO	OFF RIGHT	RAMP	1	CURVE ON-LEVEL	ICY	DAYLIGHT	NONE
5241	025A	194.31	12/13/15	0837	INJ	OFF RIGHT	RAMP	2	CURVE ON-GRADE	ICY	DAYLIGHT	NONE
5242	025A	194.31	03/09/13	1451	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5243	025A	194.31	05/08/12	1757	INJ	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5244	025A	194.31	08/27/11	1500	PDO	ON	RAMP	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5245	025A	194.31	04/12/15	1615	PDO	ON	RAMP	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5246	025A	194.31	01/09/11	0000	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5247	025A	194.31	10/26/11	1816	PDO	OFF RIGHT	RAMP	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	NONE
5248	025A	194.31	04/03/14	1137	PDO	OFF RIGHT	RAMP	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5249	085B	184.67	03/23/11	1557	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5250	085B	184.67	04/08/11	1240	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5251	085B	184.67	06/20/11	0934	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5252	085B	184.67	07/09/11	1105	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5253	085B	184.67	07/15/11	1648	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5254	085B	184.67	08/12/11	2015	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5255	085B	184.67	12/20/11	1317	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
5256	085B	184.67	02/09/12	1613	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5257	085B	184.67	04/20/12	0749	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5258	085B	184.67	04/20/12	1558	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5259	085B	184.67	07/30/12	2049	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5260	085B	184.67	07/17/13	1702	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5261	085B	184.67	07/18/13	1720	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5262	085B	184.67	07/22/13	0825	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5263	085B	184.67	12/05/13	1715	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5264	085B	184.67	02/08/14	1620	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5265	085B	184.67	07/21/14	1154	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5266	085B	184.67	08/09/14	1210	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5267	085B	184.67	08/14/14	0803	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5268	085B	184.67	09/10/14	1841	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5269	085B	184.67	09/16/14	1350	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5270	085B	184.67	12/11/14	1839	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5271	085B	184.67	03/07/15	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5272	085B	184.67	03/12/15	1247	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5273	085B	184.67	05/28/15	0940	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5274	085B	184.67	05/29/15	0000	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5275	085B	184.67	06/19/15	1211	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5276	085B	184.67	11/16/15	1712	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAWN OR DUSK	SNOW/SLEET/HAIL
5277	085B	184.67	01/29/11	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5278	085B	184.67	05/18/12	1356	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5279	085B	184.67	01/19/13	1541	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5280	085B	184.67	07/13/13	1737	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5221	025A	194.31	02/09/15	0629	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER EMOTIONALLY UPSET	035	GOING STRAIGHT
5222	025A	194.31	03/16/15	1625	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	GOING STRAIGHT
5223	025A	194.31	05/06/15	0737	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
5224	025A	194.31	06/08/15	1941	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	065	GOING STRAIGHT
5225	025A	194.31	10/12/15	1528	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	055	GOING STRAIGHT
5226	025A	194.31	10/30/15	1556	N	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
5227	025A	194.31	11/10/15	1546	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
5228	025A	194.31	11/12/15	1923	N	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	SLOWING
5229	025A	194.31	12/23/15	1657	N	REAR-END	S	HIT & RUN - UNKNOWN	NO IMPAIRMENT	UNKNOWN	050	CHANGING LANES
5230	025A	194.31	12/29/15	1725	N	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	055	GOING STRAIGHT
5231	025A	194.31	09/24/13	0323	Y (F)	OVERTURNING	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	010	OTHER
5232	025A	194.31	07/22/11	0850	Y (G)	VEHICLE CARGO/DEBRIS	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5233	025A	194.31	08/01/11	1429	Y (G)	VEHICLE CARGO/DEBRIS	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	065	GOING STRAIGHT
5234	025A	194.31	04/16/15	0834	Y (G)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5235	025A	194.31	02/26/13	1728	Y (G)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	040	OTHER
5236	025A	194.31	07/09/11	1326	Y (G)	PARKED MOTOR VEHICLE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	065	GOING STRAIGHT
5237	025A	194.31	04/12/15	0608	Y (G)	SIGN	E	HIT & RUN - UNKNOWN	ALCOHOL	NONE APPARENT	065	GOING STRAIGHT
5238	025A	194.31	12/22/11	1327	Y (G)	CONCRETE BARRIER	NE	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	040	GOING STRAIGHT
5239	025A	194.31	05/30/12	2313	Y (G)	CONCRETE BARRIER	NE	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	096	OTHER
5240	025A	194.31	01/06/14	1000	Y (G)	CONCRETE BARRIER	NE	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5241	025A	194.31	12/13/15	0837	Y (G)	CONCRETE BARRIER	NE	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
5242	025A	194.31	03/09/13	1451	Y (G)	CABLE RAIL	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	045	OTHER
5243	025A	194.31	05/08/12	1757	Y (H)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5244	025A	194.31	08/27/11	1500	Y (H)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	065	CHANGING LANES
5245	025A	194.31	04/12/15	1615	Y (H)	SIDESWIPE SAME DIRECTION	N	HIT & RUN - UNKNOWN	NO IMPAIRMENT	EVADING LAW ENFORCEMENT OFFICER	090	GOING STRAIGHT
5246	025A	194.31	01/09/11	0000	Y (H)	GUARD RAIL	N	SUV	NO IMPAIRMENT	NONE APPARENT	035	OTHER
5247	025A	194.31	10/26/11	1816	Y (H)	CONCRETE BARRIER	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	045	OTHER
5248	025A	194.31	04/03/14	1137	Y (H)	CONCRETE BARRIER	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	055	OTHER
5249	085B	184.67	03/23/11	1557	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5250	085B	184.67	04/08/11	1240	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
5251	085B	184.67	06/20/11	0934	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5252	085B	184.67	07/09/11	1105	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5253	085B	184.67	07/15/11	1648	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5254	085B	184.67	08/12/11	2015	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	000	STOPPED IN TRAFFIC
5255	085B	184.67	12/20/11	1317	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5256	085B	184.67	02/09/12	1613	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
5257	085B	184.67	04/20/12	0749	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5258	085B	184.67	04/20/12	1558	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	SLOWING
5259	085B	184.67	07/30/12	2049	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
5260	085B	184.67	07/17/13	1702	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5261	085B	184.67	07/18/13	1720	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5262	085B	184.67	07/22/13	0825	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5263	085B	184.67	12/05/13	1715	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
5264	085B	184.67	02/08/14	1620	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5265	085B	184.67	07/21/14	1154	Y (A)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
5266	085B	184.67	08/09/14	1210	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5267	085B	184.67	08/14/14	0803	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	015	SLOWING
5268	085B	184.67	09/10/14	1841	Y (A)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5269	085B	184.67	09/16/14	1350	Y (A)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5270	085B	184.67	12/11/14	1839	Y (A)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER FATIGUE	025	STOPPED IN TRAFFIC
5271	085B	184.67	03/07/15	1330	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5272	085B	184.67	03/12/15	1247	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	GOING STRAIGHT
5273	085B	184.67	05/28/15	0940	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5274	085B	184.67	05/29/15	0000	Y (A)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5275	085B	184.67	06/19/15	1211	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5276	085B	184.67	11/16/15	1712	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	GOING STRAIGHT
5277	085B	184.67	01/29/11	1415	Y (A)	SIDESWIPE SAME DIRECTION	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
5278	085B	184.67	05/18/12	1356	Y (A)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	CHANGING LANES
5279	085B	184.67	01/19/13	1541	Y (A)	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	CHANGING LANES
5280	085B	184.67	07/13/13	1737	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5281	085B	184.67	12/05/13	1608	PDO	OFF RIGHT	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5282	085B	184.67	02/27/14	0759	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5283	085B	184.67	06/28/13	0621	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5284	085B	184.67	09/19/11	1555	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5285	085B	184.67	04/19/12	1055	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5286	085B	184.67	05/23/12	1503	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5287	085B	184.67	04/21/14	1248	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5288	085B	184.67	09/19/14	1525	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5289	085B	184.67	05/11/15	1515	INJ	ON	NON-INTERSECTION	4	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5290	085B	184.67	09/19/15	0930	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5291	085B	184.67	10/09/15	1841	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
5292	085B	184.68	09/06/14	1044	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5293	085B	184.68	09/16/15	2142	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	NONE
5294	085B	184.68	09/17/15	1158	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5295	085B	184.69	11/11/15	1101	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5296	085B	184.71	07/10/13	1330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
5297	085B	184.71	06/28/15	1358	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5298	085B	184.71	07/02/15	1409	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5299	085B	184.73	05/16/14	0715	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5300	085B	184.73	01/11/15	1729	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
5301	085B	184.75	11/24/15	1850	INJ	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5302	085B	184.76	06/13/11	1245	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5303	085B	184.77	04/23/12	1530	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5304	085B	184.77	05/09/15	1121	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5305	085B	184.77	01/01/15	1720	PDO	OFF IN MEDIATE	NON-INTERSECTION	2	CURVE ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAIL
5306	085B	184.77	12/06/12	1805	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5307	085B	184.78	05/14/13	0735	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5308	085B	184.79	02/08/11	1228	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5309	085B	184.79	03/18/11	1615	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5310	085B	184.79	04/09/11	1649	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5311	085B	184.79	08/17/12	1502	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5312	085B	184.79	08/21/12	1442	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5313	085B	184.79	11/24/12	1821	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5314	085B	184.79	02/14/13	1700	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	NONE
5315	085B	184.79	02/18/13	1306	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5316	085B	184.79	07/08/13	1310	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5317	085B	184.79	07/25/13	1350	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5318	085B	184.79	09/13/13	0945	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5319	085B	184.79	10/16/13	2026	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5320	085B	184.79	11/19/13	1130	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5321	085B	184.79	12/04/13	1204	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5322	085B	184.79	05/02/14	2057	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5323	085B	184.79	05/07/14	1741	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5324	085B	184.79	12/21/14	1931	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DARK-UNLIGHTED	RAIN
5325	085B	184.79	01/31/15	2329	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5326	085B	184.79	04/16/15	1134	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5327	085B	184.79	05/11/15	2146	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5328	085B	184.79	11/05/15	0206	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5329	085B	184.79	02/03/11	1539	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5330	085B	184.79	02/12/11	1503	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5331	085B	184.79	03/07/11	2143	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5332	085B	184.79	06/09/11	1549	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5333	085B	184.79	08/03/11	1845	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5334	085B	184.79	08/08/11	2004	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5335	085B	184.79	03/16/12	1413	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5336	085B	184.79	03/24/12	1746	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5337	085B	184.79	05/22/12	1505	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5338	085B	184.79	07/13/12	1318	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5339	085B	184.79	07/21/12	0958	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5340	085B	184.79	08/03/12	0917	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5281	085B	184.67	12/05/13	1608	Y (A)	GUARD RAIL	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
5282	085B	184.67	02/27/14	0759	Y (A)	SIGN	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	CHANGING LANES
5283	085B	184.67	06/28/13	0621	Y (A)	BARRICADE/TRAFFIC BARRIER	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5284	085B	184.67	09/19/11	1555	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5285	085B	184.67	04/19/12	1055	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	040	SLOWING
5286	085B	184.67	05/23/12	1503	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5287	085B	184.67	04/21/14	1248	Y (A)	REAR-END	NE	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5288	085B	184.67	09/19/14	1525	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	020	CHANGING LANES
5289	085B	184.67	05/11/15	1515	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
5290	085B	184.67	09/19/15	0930	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5291	085B	184.67	10/09/15	1841	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	CHANGING LANES
5292	085B	184.68	09/06/14	1044	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5293	085B	184.68	09/16/15	2142	Y (A)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	003	GOING STRAIGHT
5294	085B	184.68	09/17/15	1158	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	PHYSICAL DISABILITY	010	BACKING
5295	085B	184.69	11/11/15	1101	Y (A)	SIDESWIPE SAME DIRECTION	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	CHANGING LANES
5296	085B	184.71	07/10/13	1330	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5297	085B	184.71	06/28/15	1358	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	OTHER
5298	085B	184.71	07/02/15	1409	Y (A)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
5299	085B	184.73	05/16/14	0715	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
5300	085B	184.73	01/11/15	1729	Y (A)	OVERTAKING TURN	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
5301	085B	184.75	11/24/15	1850	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
5302	085B	184.76	06/13/11	1245	Y (A)	OVERTURNING	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5303	085B	184.77	04/23/12	1530	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
5304	085B	184.77	05/09/15	1121	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
5305	085B	184.77	01/01/15	1720	Y (A)	SIDESWIPE OPPOSITE DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	045	OTHER
5306	085B	184.77	12/06/12	1805	Y (A)	REAR-END	E	SUV	RX/MEDICATION/DR	NONE APPARENT	035	GOING STRAIGHT
5307	085B	184.78	05/14/13	0735	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5308	085B	184.79	02/08/11	1228	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
5309	085B	184.79	03/18/11	1615	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	MAKING RIGHT TURN
5310	085B	184.79	04/09/11	1649	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
5311	085B	184.79	08/17/12	1502	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	MAKING LEFT TURN
5312	085B	184.79	08/21/12	1442	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	MAKING LEFT TURN
5313	085B	184.79	11/24/12	1821	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	GOING STRAIGHT
5314	085B	184.79	02/14/13	1700	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
5315	085B	184.79	02/18/13	1306	Y (N)	BROADSIDE	E	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
5316	085B	184.79	07/08/13	1310	Y (N)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
5317	085B	184.79	07/25/13	1350	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	035	GOING STRAIGHT
5318	085B	184.79	09/13/13	0945	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
5319	085B	184.79	10/16/13	2026	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	005	MAKING LEFT TURN
5320	085B	184.79	11/19/13	1130	Y (N)	BROADSIDE	E	SUV	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
5321	085B	184.79	12/04/13	1204	Y (N)	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	MAKING RIGHT TURN
5322	085B	184.79	05/02/14	2057	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	045	GOING STRAIGHT
5323	085B	184.79	05/07/14	1741	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5324	085B	184.79	12/21/14	1931	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
5325	085B	184.79	01/31/15	2329	Y (N)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5326	085B	184.79	04/16/15	1134	Y (N)	BROADSIDE	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	038	GOING STRAIGHT
5327	085B	184.79	05/11/15	2146	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5328	085B	184.79	11/05/15	0206	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	MAKING LEFT TURN
5329	085B	184.79	02/03/11	1539	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	006	SLOWING
5330	085B	184.79	02/12/11	1503	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5331	085B	184.79	03/07/11	2143	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	SLOWING
5332	085B	184.79	06/09/11	1549	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	002	GOING STRAIGHT
5333	085B	184.79	08/03/11	1845	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5334	085B	184.79	08/08/11	2004	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5335	085B	184.79	03/16/12	1413	Y (D)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
5336	085B	184.79	03/24/12	1746	Y (N)	REAR-END	SE	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	MAKING LEFT TURN
5337	085B	184.79	05/22/12	1505	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5338	085B	184.79	07/13/12	1318	Y (N)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	010	BACKING
5339	085B	184.79	07/21/12	0958	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	MAKING RIGHT TURN
5340	085B	184.79	08/03/12	0917	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5341	085B	184.79	08/16/12	1526	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5342	085B	184.79	10/26/12	1557	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5343	085B	184.79	08/05/13	0805	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5344	085B	184.79	08/18/13	1545	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5345	085B	184.79	10/29/13	1548	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5346	085B	184.79	02/16/14	1637	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5347	085B	184.79	04/30/14	0927	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5348	085B	184.79	06/08/14	1104	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5349	085B	184.79	07/06/14	1205	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5350	085B	184.79	08/07/14	1721	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5351	085B	184.79	08/13/14	0801	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5352	085B	184.79	08/17/14	1308	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5353	085B	184.79	11/18/14	0752	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5354	085B	184.79	03/13/15	1217	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5355	085B	184.79	06/12/15	1438	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5356	085B	184.79	08/02/15	1825	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5357	085B	184.79	09/03/15	1500	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5358	085B	184.79	09/17/15	0630	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5359	085B	184.79	09/21/15	1839	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5360	085B	184.79	12/20/15	1300	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5361	085B	184.79	01/02/12	1530	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5362	085B	184.79	03/10/12	1411	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5363	085B	184.79	03/31/12	1825	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5364	085B	184.79	07/30/12	1155	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5365	085B	184.79	09/04/12	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5366	085B	184.79	10/24/13	1851	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
5367	085B	184.79	04/30/14	1226	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5368	085B	184.79	05/01/14	1212	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5369	085B	184.79	08/10/14	1420	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5370	085B	184.79	03/27/15	0928	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5371	085B	184.79	02/21/15	1649	PDO	OFF LEFT	AT INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	UNKNOWN	NONE
5372	085B	184.79	10/19/14	2251	PDO	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DARK-LIGHTED	NONE
5373	085B	184.79	10/25/12	2050	INJ	OFF LEFT	INTERSECTION RELATED	1	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
5374	085B	184.79	03/01/11	1223	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5375	085B	184.79	05/14/11	1659	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5376	085B	184.79	01/14/14	0825	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5377	085B	184.79	07/18/14	1521	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5378	085B	184.79	11/15/14	1840	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5379	085B	184.79	03/10/15	0750	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5380	085B	184.79	10/26/15	1504	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5381	085B	184.79	07/31/14	1110	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5382	085B	184.80	05/21/11	1255	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5383	085B	184.80	02/18/13	1528	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5384	085B	184.80	07/27/13	1055	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5385	085B	184.80	10/05/13	1245	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5386	085B	184.80	04/22/12	1658	INJ	ON	AT INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	NONE
5387	086B	100.00	12/18/13	2045	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5388	086B	100.00	02/21/14	1425	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5389	086B	100.00	10/09/14	1457	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5390	086B	100.00	10/25/14	1650	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5391	086B	100.00	03/27/15	1342	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5392	086B	100.00	06/19/15	1630	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5393	086B	100.00	06/22/15	1100	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5394	086B	100.00	06/02/15	1615	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5395	086B	100.02	11/15/13	1746	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5396	086B	100.02	04/08/15	0000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5397	086B	100.02	12/24/14	2318	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5398	086B	100.02	01/28/13	0650	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5399	086B	100.03	11/10/14	1607	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/HAIL
5400	086B	100.04	12/11/14	0724	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5341	085B	184.79	08/16/12	1526	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
5342	085B	184.79	10/26/12	1557	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
5343	085B	184.79	08/05/13	0805	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
5344	085B	184.79	08/18/13	1545	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5345	085B	184.79	10/29/13	1548	Y (N)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5346	085B	184.79	02/16/14	1637	Y (N)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	001	STOPPED IN TRAFFIC
5347	085B	184.79	04/30/14	0927	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5348	085B	184.79	06/08/14	1104	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	SLOWING
5349	085B	184.79	07/06/14	1205	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	003	GOING STRAIGHT
5350	085B	184.79	08/07/14	1721	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5351	085B	184.79	08/13/14	0801	Y (N)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
5352	085B	184.79	08/17/14	1308	Y (N)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
5353	085B	184.79	11/18/14	0752	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5354	085B	184.79	03/13/15	1217	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
5355	085B	184.79	06/12/15	1438	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5356	085B	184.79	08/02/15	1825	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
5357	085B	184.79	09/03/15	1500	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
5358	085B	184.79	09/17/15	0630	Y (N)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5359	085B	184.79	09/21/15	1839	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5360	085B	184.79	12/20/15	1300	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5361	085B	184.79	01/02/12	1530	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	GOING STRAIGHT
5362	085B	184.79	03/10/12	1411	Y (N)	BROADSIDE	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
5363	085B	184.79	03/31/12	1825	Y (N)	SIDESWIPE SAME DIRECTION	SE	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
5364	085B	184.79	07/30/12	1155	Y (N)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
5365	085B	184.79	09/04/12	1610	Y (A)	SIDESWIPE SAME DIRECTION	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	035	CHANGING LANES
5366	085B	184.79	10/24/13	1851	Y (N)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	030	MAKING LEFT TURN
5367	085B	184.79	04/30/14	1226	Y (N)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5368	085B	184.79	05/01/14	1212	Y (N)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
5369	085B	184.79	08/10/14	1420	Y (N)	SIDESWIPE SAME DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	MAKING LEFT TURN
5370	085B	184.79	03/27/15	0928	Y (N)	BROADSIDE	S	SUV	NO IMPAIRMENT	UNKNOWN	030	MAKING RIGHT TURN
5371	085B	184.79	02/21/15	1649	Y (N)	LIGHT/UTILITY POLE	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	002	OTHER
5372	085B	184.79	10/19/14	2251	Y (N)	CONCRETE BARRIER	S	SUV	NO IMPAIRMENT	NONE APPARENT	050	MAKING LEFT TURN
5373	085B	184.79	10/25/12	2050	Y (N)	CRASH CUSHION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	CHANGING LANES
5374	085B	184.79	03/01/11	1223	Y (N)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5375	085B	184.79	05/14/11	1659	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5376	085B	184.79	01/14/14	0825	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5377	085B	184.79	07/18/14	1521	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5378	085B	184.79	11/15/14	1840	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
5379	085B	184.79	03/10/15	0750	Y (N)	REAR-END	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	GOING STRAIGHT
5380	085B	184.79	10/26/15	1504	Y (N)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	030	GOING STRAIGHT
5381	085B	184.79	07/31/14	1110	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5382	085B	184.80	05/21/11	1255	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	005	SLOWING
5383	085B	184.80	02/18/13	1528	Y (N)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	MAKING RIGHT TURN
5384	085B	184.80	07/27/13	1055	Y (N)	REAR-END	E	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
5385	085B	184.80	10/05/13	1245	Y (N)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5386	085B	184.80	04/22/12	1658	Y (N)	OVERTURNING	SW	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	MAKING RIGHT TURN
5387	086B	100.00	12/18/13	2045	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5388	086B	100.00	02/21/14	1425	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	002	OTHER
5389	086B	100.00	10/09/14	1457	Y (A)	REAR-END	W	PASS CAR/VAN W/TRAILER	NO IMPAIRMENT	DRIVER INEXPERIENCE	025	SLOWING
5390	086B	100.00	10/25/14	1650	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	008	SLOWING
5391	086B	100.00	03/27/15	1342	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	BACKING
5392	086B	100.00	06/19/15	1630	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5393	086B	100.00	06/22/15	1100	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5394	086B	100.00	06/02/15	1615	Y (A)	SIDESWIPE SAME DIRECTION	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	CHANGING LANES
5395	086B	100.02	11/15/13	1746	Y (A)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5396	086B	100.02	04/08/15	0000	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	CHANGING LANES
5397	086B	100.02	12/24/14	2318	Y (A)	GUARD RAIL	W	PASS CAR/VAN	ALCOHOL	DRIVER UNFAMILIAR W/AREA	045	WEAVING
5398	086B	100.02	01/28/13	0650	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	GOING STRAIGHT
5399	086B	100.03	11/10/14	1607	Y (N)	REAR-END	W	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	OTHER
5400	086B	100.04	12/11/14	0724	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5401	086B	100.04	02/04/15	0635	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	WET	DARK-LIGHTED	SNOW/SLEET/HAIL
5402	086B	100.04	09/04/15	0935	PDO	ON	RAMP	3	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5403	086B	100.05	08/14/13	1335	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5404	086B	100.09	02/14/15	1102	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5405	086B	100.10	05/12/14	1107	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5406	086B	100.10	05/31/15	1048	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5407	086B	100.10	10/13/15	1155	INJ	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE
5408	086B	100.10	07/22/11	1716	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5409	086B	100.10	11/09/14	2009	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5410	086B	100.10	01/01/15	1401	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	SNOW/SLEET/HAIL
5411	086B	100.10	01/02/15	0000	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5412	086B	100.10	06/11/15	1351	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5413	086B	100.10	06/12/15	1110	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	WET	DAYLIGHT	RAIN
5414	086B	100.10	04/23/14	1635	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5415	086B	100.10	06/24/14	1224	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5416	086B	100.10	02/13/15	0722	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5417	086B	100.10	04/23/15	1416	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5418	086B	100.10	08/26/15	0730	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5419	086B	100.10	05/20/14	1456	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5420	086B	100.10	04/14/14	0657	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE
5421	086B	100.11	08/01/11	0802	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5422	086B	100.11	06/24/11	1210	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5423	086B	100.11	06/24/11	1210	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5424	086B	100.11	12/23/11	1204	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
5425	086B	100.11	07/12/12	1346	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5426	086B	100.11	07/28/12	2004	PDO	ON	AT INTERSECTION	2	CURVE ON-GRADE	WET	DARK-LIGHTED	RAIN
5427	086B	100.11	08/02/12	1753	PDO	ON	AT INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	NONE
5428	086B	100.11	10/21/12	1933	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5429	086B	100.11	10/30/13	1639	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5430	086B	100.11	06/11/14	0825	INJ	ON	AT INTERSECTION	2	CURVE ON-GRADE	DRY	DAWN OR DUSK	NONE
5431	086B	100.11	03/19/15	0840	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	SNOW/SLEET/HAIL
5432	086B	100.11	02/09/11	0838	PDO	OFF RIGHT	INTERSECTION RELATED	1	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE
5433	086B	100.11	09/03/11	1805	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5434	086B	100.11	02/08/12	1120	INJ	ON	INTERSECTION RELATED	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5435	086B	100.12	06/10/12	1134	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5436	086B	100.12	06/20/13	1110	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5437	086B	100.12	09/28/13	1835	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5438	086B	100.12	05/28/14	0715	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5439	086B	100.12	10/15/12	1830	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5440	086B	100.12	11/25/12	1213	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5441	086B	100.12	05/25/13	1400	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5442	086B	100.12	11/16/12	1234	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5443	086B	100.12	11/20/12	1700	PDO	ON	AT INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-LIGHTED	NONE
5444	086B	100.12	12/28/12	1513	PDO	ON	AT INTERSECTION	3	CURVE ON-GRADE	WET	DAYLIGHT	NONE
5445	105A	4.89	05/18/11	1548	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5446	105A	4.90	07/02/15	1550	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5447	105A	4.90	06/05/11	0930	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5448	105A	4.90	11/06/11	1740	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5449	105A	4.90	10/10/12	1345	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5450	105A	4.90	04/27/13	1605	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5451	105A	4.90	05/09/13	1220	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	RAIN
5452	105A	4.90	07/01/14	1705	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5453	105A	4.90	07/08/14	1856	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5454	105A	4.90	08/14/14	1806	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5455	105A	4.90	09/22/14	1722	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5456	105A	4.90	11/19/14	1740	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5457	105A	4.90	12/06/14	1621	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAWN OR DUSK	NONE
5458	105A	4.90	03/12/15	1350	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5459	105A	4.90	07/13/15	0850	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5460	105A	4.90	10/30/15	1402	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5401	086B	100.04	02/04/15	0635	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5402	086B	100.04	09/04/15	0935	Y (G)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
5403	086B	100.05	08/14/13	1335	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	SLOWING
5404	086B	100.09	02/14/15	1102	Y (A)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	015	CHANGING LANES
5405	086B	100.10	05/12/14	1107	Y (O)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	GOING STRAIGHT
5406	086B	100.10	05/31/15	1048	Y (O)	BROADSIDE	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
5407	086B	100.10	10/13/15	1155	Y (O)	BROADSIDE	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	GOING STRAIGHT
5408	086B	100.10	07/22/11	1716	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5409	086B	100.10	11/09/14	2009	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	SLOWING
5410	086B	100.10	01/01/15	1401	Y (O)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	SLOWING
5411	086B	100.10	01/02/15	0000	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5412	086B	100.10	06/11/15	1351	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5413	086B	100.10	06/12/15	1110	Y (O)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	MAKING RIGHT TURN
5414	086B	100.10	04/23/14	1635	Y (O)	BROADSIDE	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	025	GOING STRAIGHT
5415	086B	100.10	06/24/14	1224	Y (O)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
5416	086B	100.10	02/13/15	0722	Y (O)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5417	086B	100.10	04/23/15	1416	Y (O)	SIDESWIPE SAME DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	030	CHANGING LANES
5418	086B	100.10	08/26/15	0730	Y (O)	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	002	MAKING RIGHT TURN
5419	086B	100.10	05/20/14	1456	Y (O)	BROADSIDE	N	SUV	NO IMPAIRMENT	NONE APPARENT	015	MAKING LEFT TURN
5420	086B	100.10	04/14/14	0657	Y (O)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5421	086B	100.11	08/01/11	0802	Y (O)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	GOING STRAIGHT
5422	086B	100.11	06/24/11	1210	Y (O)	REAR-END	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
5423	086B	100.11	06/24/11	1210	Y (O)	REAR-END	E	HIT & RUN - UNKNOWN	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT
5424	086B	100.11	12/23/11	1204	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	005	SLOWING
5425	086B	100.11	07/12/12	1346	Y (O)	REAR-END	NW	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	GOING STRAIGHT
5426	086B	100.11	07/28/12	2004	Y (O)	REAR-END	NE	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	019	MAKING RIGHT TURN
5427	086B	100.11	08/02/12	1753	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	SLOWING
5428	086B	100.11	10/21/12	1933	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
5429	086B	100.11	10/30/13	1639	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	SLOWING
5430	086B	100.11	06/11/14	0825	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	030	GOING STRAIGHT
5431	086B	100.11	03/19/15	0840	Y (O)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5432	086B	100.11	02/09/11	0838	Y (O)	CRASH CUSHION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	020	AVOIDING OBJECT/VEHICLE IN ROAD
5433	086B	100.11	09/03/11	1805	Y (A)	CRASH CUSHION	W	PASS CAR/VAN	ALCOHOL	UNKNOWN	025	CHANGING LANES
5434	086B	100.11	02/08/12	1120	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5435	086B	100.12	06/10/12	1134	Y (O)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
5436	086B	100.12	06/20/13	1110	Y (O)	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5437	086B	100.12	09/28/13	1835	Y (O)	BROADSIDE	W	SUV	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT
5438	086B	100.12	05/28/14	0715	Y (O)	BROADSIDE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5439	086B	100.12	10/15/12	1830	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	002	OTHER
5440	086B	100.12	11/25/12	1213	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
5441	086B	100.12	05/25/13	1400	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	025	GOING STRAIGHT
5442	086B	100.12	11/16/12	1234	Y (O)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	CHANGING LANES
5443	086B	100.12	11/20/12	1700	Y (O)	SIDESWIPE SAME DIRECTION	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	MAKING LEFT TURN
5444	086B	100.12	12/28/12	1513	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	002	SLOWING
5445	105A	4.89	05/18/11	1548	Y (O)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5446	105A	4.90	07/02/15	1550	Y (O)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5447	105A	4.90	06/05/11	0930	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5448	105A	4.90	11/06/11	1740	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	040	GOING STRAIGHT
5449	105A	4.90	10/10/12	1345	Y (O)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	035	GOING STRAIGHT
5450	105A	4.90	04/27/13	1605	Y (O)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5451	105A	4.90	05/09/13	1220	Y (O)	REAR-END	N	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	005	MAKING LEFT TURN
5452	105A	4.90	07/01/14	1705	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	MAKING RIGHT TURN
5453	105A	4.90	07/08/14	1856	Y (O)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	025	GOING STRAIGHT
5454	105A	4.90	08/14/14	1806	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5455	105A	4.90	09/22/14	1722	Y (O)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	BACKING
5456	105A	4.90	11/19/14	1740	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5457	105A	4.90	12/06/14	1621	Y (O)	REAR-END	N	SUV	NO IMPAIRMENT	UNKNOWN	010	GOING STRAIGHT
5458	105A	4.90	03/12/15	1350	Y (O)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	003	GOING STRAIGHT
5459	105A	4.90	07/13/15	0850	Y (O)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5460	105A	4.90	10/30/15	1402	Y (O)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5461	105A	4.98	10/28/11	1250	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5462	105A	4.99	05/06/13	0705	INJ	ON	NON-INTERSECTION	3	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5463	105A	5.00	08/21/11	1115	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5464	105A	5.00	08/13/13	1341	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5465	105A	5.00	04/26/15	1730	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5466	105A	5.00	06/29/11	1700	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5467	105A	5.00	09/03/14	0845	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5468	105A	5.00	01/12/13	2050	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5469	105A	5.08	07/17/15	1640	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5470	105A	5.10	05/24/11	1510	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5471	105A	5.10	08/28/13	1750	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5472	105A	5.10	07/02/14	1505	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5473	105A	5.12	11/29/11	0925	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5474	105A	5.12	02/10/14	0730	PDO	OFF RIGHT	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
5475	105A	5.13	10/30/14	0800	INJ	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5476	105A	5.14	03/20/13	1340	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5477	105A	5.14	01/13/12	1200	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5478	105A	5.14	08/29/12	1200	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	DRY	DAYLIGHT	NONE
5479	105A	5.14	10/24/12	0730	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5480	105A	5.14	10/30/12	1615	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5481	105A	5.14	01/03/13	1745	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5482	105A	5.14	03/01/13	1010	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5483	105A	5.14	05/28/13	1134	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5484	105A	5.14	10/27/13	1039	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5485	105A	5.14	03/07/14	1845	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	SNOWY	DARK-LIGHTED	SNOW/SLEET/HAIL
5486	105A	5.14	04/17/14	1020	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5487	105A	5.14	04/23/14	0820	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5488	105A	5.14	08/21/14	0720	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5489	105A	5.14	02/25/15	1220	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5490	105A	5.14	12/27/15	1000	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5491	105A	5.14	06/08/14	1630	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5492	105A	5.14	04/26/15	1750	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5493	105A	5.14	04/03/14	0650	INJ	ON	AT INTERSECTION	4	STRAIGHT ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAIL
5494	105A	5.15	03/22/12	0740	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5495	105A	5.15	04/27/12	1530	INJ	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5496	105A	5.15	07/31/13	1640	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5497	105A	5.15	07/30/14	1325	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	WET	DAYLIGHT	RAIN
5498	105A	5.15	09/09/15	0655	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	UNKNOWN
5499	105A	5.15	09/10/14	1540	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5500	105A	5.16	01/16/14	1644	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5501	105A	5.20	04/04/14	1610	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5502	105A	5.20	05/03/14	1415	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5503	105A	5.20	09/09/12	1605	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5504	105A	5.21	09/14/13	1120	INJ	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5505	105A	5.21	02/12/11	1115	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5506	105A	5.21	12/20/12	1310	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
5507	105A	5.21	02/14/13	0910	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5508	105A	5.21	01/30/14	1300	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5509	105A	5.21	05/22/14	1640	PDO	ON	NON-INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5510	105A	5.28	11/13/11	1730	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	WIND
5511	105A	5.28	02/04/12	1620	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	NONE
5512	105A	5.28	01/14/13	1410	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE
5513	105A	5.28	09/10/14	0850	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5514	105A	5.28	04/18/13	1845	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5515	105A	5.29	10/29/11	1040	PDO	ON	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5516	105A	5.29	08/31/12	2130	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5517	105A	5.29	01/14/11	1735	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5518	105A	5.29	04/19/11	1200	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5519	105A	5.29	09/02/11	1200	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5520	105A	5.29	01/29/12	1703	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5461	105A	4.98	10/28/11	1250	Y (A)	REAR-END	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5462	105A	4.99	05/06/13	0705	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5463	105A	5.00	08/21/11	1115	Y (A)	OVERTURNING	W	MOTORCYCLE	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	GOING STRAIGHT
5464	105A	5.00	08/13/13	1341	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5465	105A	5.00	04/26/15	1730	Y (O)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	030	GOING STRAIGHT
5466	105A	5.00	06/29/11	1700	Y (A)	SIDESWIPE SAME DIRECTION	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	025	CHANGING LANES
5467	105A	5.00	09/03/14	0845	Y (A)	SIDESWIPE SAME DIRECTION	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	CHANGING LANES
5468	105A	5.00	01/12/13	2050	Y (A)	SIGN	W	PASS CAR/VAN	ALCOHOL	UNKNOWN	UK	OTHER
5469	105A	5.08	07/17/15	1640	Y (P)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5470	105A	5.10	05/24/11	1510	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	GOING STRAIGHT
5471	105A	5.10	08/28/13	1750	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
5472	105A	5.10	07/02/14	1505	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
5473	105A	5.12	11/29/11	0925	Y (P)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5474	105A	5.12	02/10/14	0730	Y (P)	GUARD RAIL	E	SUV	NO IMPAIRMENT	NONE APPARENT	030	OTHER
5475	105A	5.13	10/30/14	0800	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5476	105A	5.14	03/20/13	1340	Y (P)	BROADSIDE	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	050	GOING STRAIGHT
5477	105A	5.14	01/13/12	1200	Y (P)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	GOING STRAIGHT
5478	105A	5.14	08/29/12	1200	Y (P)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5479	105A	5.14	10/24/12	0730	Y (P)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5480	105A	5.14	10/30/12	1615	Y (P)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
5481	105A	5.14	01/03/13	1745	Y (P)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5482	105A	5.14	03/01/13	1010	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	003	SLOWING
5483	105A	5.14	05/28/13	1134	Y (P)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5484	105A	5.14	10/27/13	1039	Y (P)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	MAKING RIGHT TURN
5485	105A	5.14	03/07/14	1845	Y (P)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	010	GOING STRAIGHT
5486	105A	5.14	04/17/14	1020	Y (P)	REAR-END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5487	105A	5.14	04/23/14	0820	Y (P)	REAR-END	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	020	GOING STRAIGHT
5488	105A	5.14	08/21/14	0720	Y (P)	REAR-END	E	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
5489	105A	5.14	02/25/15	1220	Y (P)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5490	105A	5.14	12/27/15	1000	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	UNKNOWN	005	GOING STRAIGHT
5491	105A	5.14	06/08/14	1630	Y (P)	APPROACH TURN	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	GOING STRAIGHT
5492	105A	5.14	04/26/15	1750	Y (P)	APPROACH TURN	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	030	GOING STRAIGHT
5493	105A	5.14	04/03/14	0650	Y (P)	REAR-END	W	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	015	SLOWING
5494	105A	5.15	03/22/12	0740	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5495	105A	5.15	04/27/12	1530	Y (A)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	015	GOING STRAIGHT
5496	105A	5.15	07/31/13	1640	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	010	SLOWING
5497	105A	5.15	07/30/14	1325	Y (P)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	035	SLOWING
5498	105A	5.15	09/09/15	0655	Y (P)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	005	GOING STRAIGHT
5499	105A	5.15	09/10/14	1540	Y (P)	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	045	GOING STRAIGHT
5500	105A	5.16	01/16/14	1644	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	GOING STRAIGHT
5501	105A	5.20	04/04/14	1610	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DISTRACTED BY PASSENGER	015	GOING STRAIGHT
5502	105A	5.20	05/03/14	1415	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	GOING STRAIGHT
5503	105A	5.20	09/09/12	1605	Y (N)	REAR-END	W	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	035	GOING STRAIGHT
5504	105A	5.21	09/14/13	1120	Y (A)	OVERTURNING	W	MOTORCYCLE	NO IMPAIRMENT	NONE APPARENT	025	SLOWING
5505	105A	5.21	02/12/11	1115	Y (A)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5506	105A	5.21	12/20/12	1310	Y (A)	REAR-END	W	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	005	GOING STRAIGHT
5507	105A	5.21	02/14/13	0910	Y (A)	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5508	105A	5.21	01/30/14	1300	Y (A)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	005	SLOWING
5509	105A	5.21	05/22/14	1640	Y (A)	REAR-END	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	035	SLOWING
5510	105A	5.28	11/13/11	1730	Y (N)	REAR-END	W	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5511	105A	5.28	02/04/12	1620	Y (N)	REAR-END	E	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	BACKING
5512	105A	5.28	01/14/13	1410	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5513	105A	5.28	09/10/14	0850	Y (N)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	025	GOING STRAIGHT
5514	105A	5.28	04/18/13	1845	Y (N)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	020	CHANGING LANES
5515	105A	5.29	10/29/11	1040	Y (N)	OVERTURNING	S	TRUCK GVW > 10K/BUSSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	010	MAKING LEFT TURN
5516	105A	5.29	08/31/12	2130	Y (N)	BROADSIDE	W	SUV	NO IMPAIRMENT	DISTRACTED BY PASSENGER	040	GOING STRAIGHT
5517	105A	5.29	01/14/11	1735	Y (N)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5518	105A	5.29	04/19/11	1200	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	MAKING RIGHT TURN
5519	105A	5.29	09/02/11	1200	Y (N)	REAR-END	W	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5520	105A	5.29	01/29/12	1703	Y (N)	REAR-END	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	015	GOING STRAIGHT

#	Hwy	MP	Date	Time	Sev	Location	Road Description	# of Veh	Contour	Road Condition	Lighting	Weather
5521	105A	5.29	07/26/12	1335	INJ	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5522	105A	5.29	07/20/13	1840	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5523	105A	5.29	05/15/14	1848	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE
5524	105A	5.29	06/07/14	1145	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5525	105A	5.29	03/20/15	1200	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5526	105A	5.29	12/03/15	1850	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE
5527	105A	5.29	11/29/11	1810	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE
5528	105A	5.29	08/27/15	1540	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	WET	DAYLIGHT	RAIN
5529	105A	5.29	09/05/15	1805	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	UNKNOWN
5530	105A	5.29	01/17/11	1520	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5531	105A	5.29	07/14/13	1030	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5532	105A	5.29	11/03/14	1353	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5533	105A	5.29	12/04/13	0700	INJ	OFF RIGHT	AT INTERSECTION	1	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAIL
5534	105A	5.29	04/14/14	2128	PDO	OFF LEFT	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	DRY	DARK-LIGHTED	NONE
5535	105A	5.29	06/19/13	1702	PDO	ON	AT INTERSECTION	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5536	105A	5.29	08/30/13	1100	PDO	ON	INTERSECTION RELATED	3	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE
5537	105A	5.29	07/06/14	1716	INJ	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE

#	Hwy	MP	Date	Time	Ramp	Accident Type	Dir	Vehicle Type	Driver Factor	Human Factor	Speed	Vehicle Movement
5521	105A	5.29	07/26/12	1335	Y (N)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	015	SLOWING
5522	105A	5.29	07/20/13	1840	Y (N)	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	005	GOING STRAIGHT
5523	105A	5.29	05/15/14	1848	Y (N)	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	015	GOING STRAIGHT
5524	105A	5.29	06/07/14	1145	Y (N)	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	010	ENTERING/LEAVING PARKED POSITION
5525	105A	5.29	03/20/15	1200	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	NONE APPARENT	005	GOING STRAIGHT
5526	105A	5.29	12/03/15	1850	Y (N)	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	040	GOING STRAIGHT
5527	105A	5.29	11/29/11	1810	Y (N)	SIDESWIPE SAME DIRECTION	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	020	AVOIDING OBJECT/VEHICLE IN ROAD
5528	105A	5.29	08/27/15	1540	Y (N)	SIDESWIPE SAME DIRECTION	S	OTHER - SEE REPORT	NO IMPAIRMENT	UNKNOWN	020	MAKING LEFT TURN
5529	105A	5.29	09/05/15	1805	Y (N)	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	005	MAKING RIGHT TURN
5530	105A	5.29	01/17/11	1520	Y (N)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	015	MAKING LEFT TURN
5531	105A	5.29	07/14/13	1030	Y (N)	APPROACH TURN	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	010	MAKING LEFT TURN
5532	105A	5.29	11/03/14	1353	Y (N)	APPROACH TURN	E	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	035	MAKING LEFT TURN
5533	105A	5.29	12/04/13	0700	Y (N)	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	030	GOING STRAIGHT
5534	105A	5.29	04/14/14	2128	Y (N)	INVOLVING OTHER OBJECT	SW	SUV	ALCOHOL	UNKNOWN	005	MAKING LEFT TURN
5535	105A	5.29	06/19/13	1702	Y (N)	REAR-END	S	SUV	NO IMPAIRMENT	UNKNOWN	020	GOING STRAIGHT
5536	105A	5.29	08/30/13	1100	Y (N)	BROADSIDE	E	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	010	GOING STRAIGHT
5537	105A	5.29	07/06/14	1716	Y (N)	APPROACH TURN	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	015	MAKING LEFT TURN

Technical Memorandum - I-25 South Environmental Assessment Travel Demand Forecasting

This memorandum documents the travel demand forecasting work performed by Steer Davies Gleave (SDG) for the I-25 South Environmental Assessment (EA). The work discussed in this memo was performed between April 2017 and February 2018.

This memo contains the following sections:

1. Background and Prior Work
2. General Modeling Assumptions
3. Project Alternatives
4. Forecasts
5. Conclusions

1. Background and Prior Work

Between April and October 2017, SDG developed the travel modeling tool which produced the forecasts reported in this memo. This required the following tasks:

- An extensive data collection effort, including
 - Current traffic counts on both weekdays and weekends along I-25 (between C-470 and Monument) and on parallel routes (SH-105 and SH-83);
 - Travel times from INRIX (calendar year 2016, as well as on days traffic counts were collected in 2017);
 - Travel patterns from Streetlight (calendar year 2016) to assist in creation of an inter-MPO trip matrix; and
 - Toll transaction data on US-36 to estimate local travelers' values of time on an existing express lane facility.
- Assembly of the model network and trip matrices from a combination of the two local MPOs' (DRCOG and PPACG) travel demand models.
- Calibration of the model and validation to observed traffic conditions.

SDG reported this work in detail in the document titled *I-25 PEL/NEPA Technical Memorandum – Model Development and Calibration*, dated January 30, 2018, included as an appendix to this memo. This section provides a more concise summary of these efforts. Throughout the model development, calibration, and reporting, SDG worked with a small working group of travel demand modeling experts from the consultant team, CDOT, and the MPOs to validate and QC results.

A. Model Development

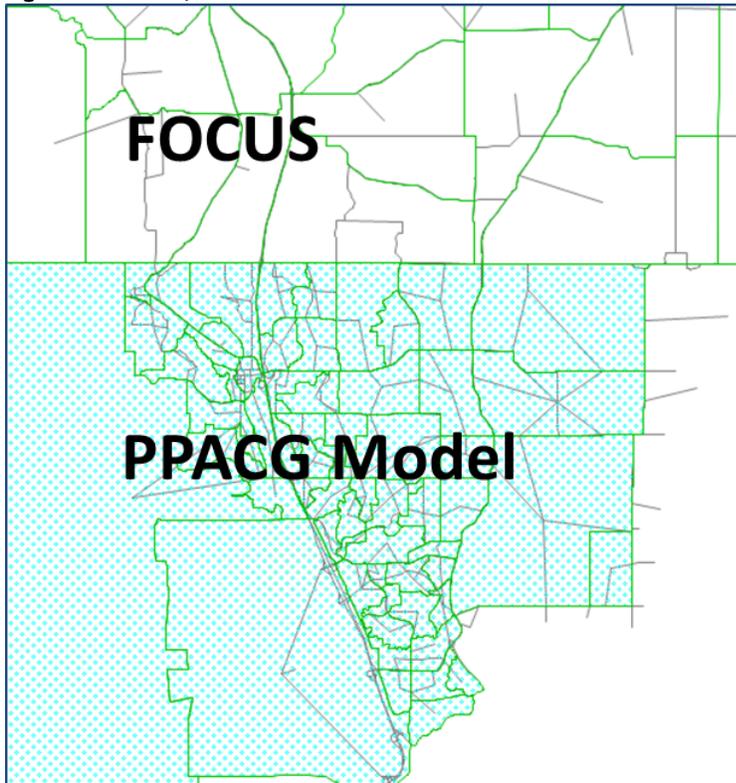
SDG created a travel demand model for this project ("I-25 Model") by extending DRCOG's FOCUS 2.0 model into northern El Paso County. This involved combining both the networks and the trip matrices of the models from the two MPO regions.

1. Network

The southern edge of the FOCUS network is at the Douglas / El Paso County Line; SDG extended this south to the Briargate area of Colorado Springs, with the major external stations including I-25 just south of the Academy Boulevard interchange and SH-21 just south of SH-83. This extension was necessary to ensure that the study corridor of the I-25 South project (between C-470 and SH-105 at Monument) was reflective of travel between the model areas, included in the model in its entirety, and minimized the number of major roads at the edge of the model.

SDG joined the applicable portion of the PPACG model network with FOCUS to create the I-25 model network, and translated PPACG model network attributes into their FOCUS counterparts. Figure 1 shows the southern edge of the I-25 model network and illustrates the border between the original MPO model networks.

Figure 1 – FOCUS/PPACG Model Network Border



Source: Steer Davies Gleave

2. Trip Matrix

Initially, SDG combined the trip matrices of the two MPO models, eliminating the rows and columns corresponding to the entry and exit links at the Douglas / El Paso County border in the original MPO models, and adding rows and columns for the new external stations at the southern edge of the network in the Briargate region.

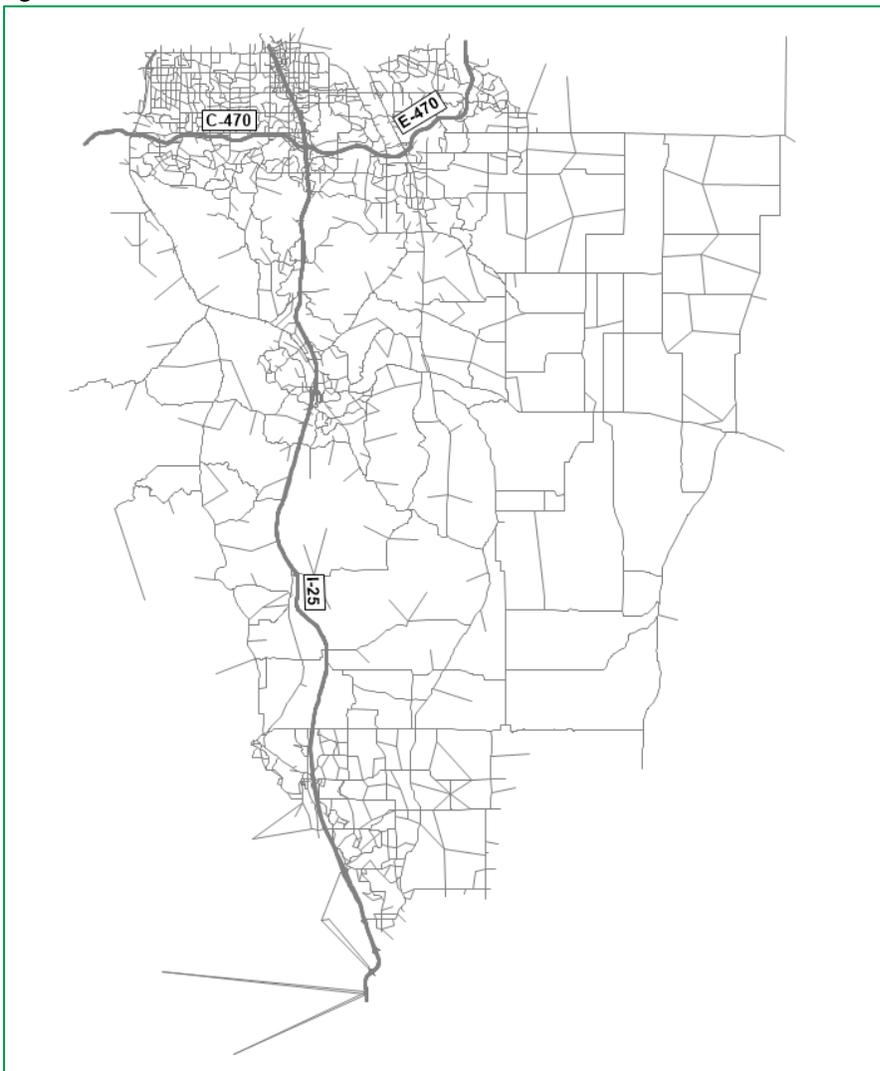
Subsequently, SDG distributed the trips previously using external stations at the Douglas / El Paso County border into 'inter-MPO' trips, some remaining internal, having their origin or destination in the northern PPACG model portion of the network, and some becoming external trips, ultimately leaving the network via one of the new external stations in the Briargate region. The travel pattern data collected from Streetlight was the key to this procedure. Trips remaining in a single MPO were unchanged from the original MPO model trip matrices as received.

Additionally, to model weekend (Friday afternoon and Sunday mid-day) time periods, SDG modified the trip matrices from the weekday periods, using evidence from traffic count data that indicated that weekend travel patterns in the study corridor differ from weekday primarily in the existence of additional end-to-end trips.

3. Subarea Extraction

SDG initially performed the calibration and validation exercises with a model that contained the FOCUS region in its entirety plus a small portion of the PPACG region as described above. Ultimately, however, we extracted a subarea of the FOCUS region to reduce model running time while forecasting future conditions. The subarea extended about 5 miles north of C-470, which we believe is beyond the area of significant project influence. The validation results were checked and compared, and they were not significantly different after the subarea extraction compared to the validation results when the entire FOCUS region was included. The validation results featured in the model development and calibration memo are from the subarea model. Figure 2 depicts the subarea model network used in the forecasting.

Figure 2 – I-25 Subarea Model Network



Source: Steer Davies Gleave

B. Model Calibration and Validation

The main challenges of model calibration were:

- Addressing modeled traffic volumes at the Douglas / El Paso County Line (combined I-25 / SH-83) which were significantly lower than the traffic counts collected in 2017 (66,300 vs 86,300 ADT) without significantly disrupting traffic volumes elsewhere in the corridor which were much closer to traffic counts
- Slow peak period travel times between C-470 and Castle Rock, particularly on the parallel routes
- Inability of the model to completely capture diversion to frontage roads in the southbound direction in the PM time periods, and the backups occurring from traffic merging back onto I-25 from the Sky View Lane on ramp

SDG addressed the above challenges through a combination of network and matrix adjustments, documented in the Model Development and Calibration Technical Memorandum, included as an appendix to this memo. These modifications carried through in the development of future year networks and matrices.

In the case of the final bullet above, SDG developed a post-processing methodology, which shifted southbound vehicles onto the frontage roads between Plum Creek Parkway and Sky View Lane, and reduced capacities on I-25 upstream of the Sky View Lane on ramp, with larger volumes of merging traffic resulting in greater capacity reductions. We applied this southbound PM post-processing methodology in the forecasts for any weekday and/or Friday PM conditions equally or more congested than today (i.e. 2021 No Action, and all 2040 Forecasts).

Tables 1 and 2 below compare the modeled volumes and traffic counts at several key points on I-25, and Tables 3 and 4 compare the travel times. These results were validated by the travel demand modeling working group and approved for use in the EA and concurrent PEL (Planning and Environmental Linkages) study.

Table 1. Weekday Peak Period Mainline Calibration Volumes

I-25 Location		Count		Model Volume		% Difference	
		6:30 –9 AM	3-7 PM	6:30 –9 AM	3-7 PM	6:30 – 9 AM	3-7 PM
N/O Baptist Rd	NB	7,700	12,900	8,400	14,000	9%	9%
	SB	7,200	13,800	7,400	14,800	3%	7%
N/O County Line Rd	NB	6,800	9,700	6,600	9,800	-2%	1%
	SB	4,700	11,600	4,800	11,600	2%	0%
S/O Plum Creek Pkwy	NB	7,200	9,900	7,500	10,600	4%	7%
	SB	4,800	11,100	5,200	11,900	8%	7%
N/O Founders Pkwy	NB	14,500	15,700	15,300	17,400	5%	11%
	SB	7,700	21,600	8,700	22,900	13%	6%

Source: Steer Davies Gleave

Table 2. Weekend Peak Period Mainline Calibration Volumes

I-25 Location		Count		Model Volume		% Difference	
		Friday 3-7 PM	Sunday 11:30 AM – 3 PM	Friday 3-7 PM	Sunday 11:30 AM – 3 PM	Friday 3-7 PM	Sunday 11:30 AM – 3 PM
N/O Baptist Rd	NB	14,700	12,500	15,700	13,500	7%	8%
	SB	14,700	11,800	16,100	12,500	9%	6%
N/O County Line Rd	NB	11,800	11,100	11,400	10,900	-3%	-2%
	SB	12,100	10,200	12,700	10,000	5%	-2%
S/O Plum Creek Pkwy	NB	11,800	10,800	12,200	11,400	3%	6%
	SB	9,300	10,000	10,200	10,600	10%	6%
N/O Founders Pkwy	NB	17,100	16,200	18,900	16,100	10%	-1%
	SB	20,900	16,100	24,100	15,400	15%	-4%

Source: Steer Davies Gleave

Table 3. Weekday Peak Period Mainline Travel Time Calibration

I-25 Location	Observed Travel Time (min)		Modeled Travel	
	6:30 –9 AM	3-7 PM	6:30 –9 AM	3-7 PM
NB Academy to County Line	12.9	11.7	12.2	12.2
NB County Line to Upper Lake Gulch	8.3	7.2	8.9	8.5
NB Upper Lake Gulch to Plum Creek	8.3	7.8	9.9	9.1
NB Plum Creek to Castle Pines	8.4	6.6	7.9	6.9
NB Castle Pines to CO-470	8.2	5.0	6.6	5.2
NB Cumulative	46.1	38.2	45.4	41.8
SB CO-470 to Castle Pines	5.7	5.6	5.3	6.3
SB Castle Pine to Plum Creek	6.8	6.6	6.7	8.2
SB Plum Creek to Upper Lake Gulch	8.4	9.4	8.4	9.5
SB Upper Lake Gulch to County Line	8.3	9.3	7.6	9.4
SB County Line to Academy	12.5	10.7	11.6	12.3
SB CUMULATIVE	41.7	41.5	39.5	45.7

Source: Steer Davies Gleave

Table 4. Weekend Peak Period Mainline Travel Time Calibration

I-25 Location	Observed Travel Time (min)		Modeled Travel	
	Friday 3-7 PM	Sunday 11:30 AM – 3 PM	Friday 3-7 PM	Sunday 11:30 AM – 3 PM
NB Academy to County Line	12.6	12.4	12.5	12.2
NB County Line to Upper Lake Gulch	7.8	8.6	9.5	9.9
NB Upper Lake Gulch to Plum Creek	8.9	11.4	10.1	10.4
NB Plum Creek to Castle Pines	6.5	6.5	7.0	6.8
NB Castle Pines to CO-470	5.1	4.8	5.2	5.1
NB Cumulative	40.9	43.7	44.3	44.5
SB CO-470 to Castle Pines	6.4	5.1	6.4	5.4
SB Castle Pine to Plum Creek	7.2	6.4	8.3	6.9
SB Plum Creek to Upper Lake Gulch	25.9	8.6	20.6	9.8
SB Upper Lake Gulch to County Line	8.7	8.3	10.4	8.9
SB County Line to Academy	10.8	10.6	12.7	11.9
SB CUMULATIVE	59.0	39.0	58.3	42.8

Source: Steer Davies Gleave

2. General Modeling Assumptions

A. Time Periods Modeled

The weekday time periods modeled in these forecasts were consistent with FOCUS's 10-period structure, as follows:

- OP1: 11 PM – 6:30 AM
- AM1: 6:30 – 7 AM
- AM2: 7-8 AM
- AM3: 8-9 AM
- OP2: 9-11:30 AM
- OP3: 11:30 AM – 3 PM
- PM1: 3-5 PM
- PM2: 5-6 PM
- PM3: 6-7 PM
- OP4: 7-11 PM

Additionally, due to the traffic patterns of the study corridor, SDG was asked to model some weekend periods currently experiencing the most delay. To facilitate compatibility with FOCUS, the following time periods were selected, which aligned closely with the actual times of greatest delay on I-25:

- Friday 3-5 PM (PM1)
- Friday 5-6 PM (PM2)
- Friday 6-7 PM (PM3)
- Sunday 11:30 AM – 3 PM (OP3)

B. Horizon Year (2040) Network

The 2040 network, with the same subarea boundaries depicted in Figure 2, incorporated both the network modifications from 2017 calibration and the network differences between the MPOs' 2015 and 2040 networks. The major future network projects in the study area include:

- Managed lanes on C-470 between I-25 and Kipling Pkwy
- New diamond interchange at I-25 and Crystal Valley Pkwy
- N/S connector between Lincoln Ave and Ridgeway Pkwy to the east of I-25 (and to the west of Peoria St)
- E/W connector from Sky Ridge Medical Center to Peoria St, between Lincoln Ave and Ridgeway Pkwy
- Two N/S connectors from Hess Ave to Crowfoot Valley Rd (one near I-25, one near SH 83)
- Woodlands Blvd N/S connector between Black Feather Tr and Scott Blvd (in Castle Rock)
- Valley Dr N/S connector between South St and Plum Creek Parkway (in Castle Rock)
- SH 21 northward / westward extension to new diamond interchange with I-25 between Interquest Pkwy and N Gate Rd

C. Horizon Year (2040) Trip Matrix

SDG derived 2040 trip matrices by applying a pivot method to the calibrated 2017 trip matrices, as follows:

- For all zones other than the new external stations, the “target” total origins and destinations for each time period were computed by adding the difference between the 2040 and 2015 totals in the MPO model matrices to the 2017 calibrated matrix totals.
- For the new external stations, the targets were computed by adding the origins from (or destinations to) all zones in the PPACG model other than those which were included in the I-25 and disaggregated to the relative traffic counts from (or to) each of the stations in the StreetLight Data OD data.
- An iterative “fratar”¹ process was applied to the 2017 matrices, using the 2040 row and column targets computed in the above steps.

To obtain 2040 weekend matrices, the same adjustment factors used in the development of 2017 weekend matrices were applied to the appropriate weekday time periods.

¹ A mathematical process that iteratively adjusts entire rows and columns of a matrix to bring each row and column total as close to a set of target values as possible.

D. Post-Processing of Southbound Weekday and Friday PM Time Periods

On I-25 between Plum Creek Parkway and Sky View Lane in the Weekday and Friday PM periods, SDG applied the two southbound post-processing steps from calibration discussed earlier in this memo in the following manner:

- Shifting the same volume of traffic away from I-25 onto the Exit 181 off ramp, frontage roads and Exit 174 (Sky View Lane) on ramp that was shifted in base year calibration
- Reducing capacity by amounts which vary, depending on the volumes on the Exit 174 on ramp. These reductions were equal to or greater than in the base year, due to the higher overall level of traffic. We ensured consistent capacities among the various alternatives we ran.

Note that the above steps were applied for all the 2040 forecasts, but in 2021, only the No Action forecasts required these adjustments. Other 2021 alternatives (additional general purpose and/or express lanes) either removed or nearly removed delay, and thus were determined not to result in significant diversion to the frontage roads beyond the amount modeled.

E. Assumptions Supporting Express Lanes

This section describes modeling assumptions specifically related to the forecasts of alternatives with express lanes. Separately, the design of the express lanes is covered in Section 4 of this memo.

1. Values of Time

The US 36 Express Lanes transaction data provided continuous traffic count data and, in conjunction with other data, allowed SDG to estimate the portion of total US 36 traffic using managed lanes and their travel time savings. In combination with toll rate data, managed lane traffic share, and travel time savings, SDG was able to estimate value of time for the upper-percentile travelers, i.e. those with the highest values of time, who were willing to pay the toll to save travel time.

However, these data only provide values of time for those travelers who are willing to pay the toll charged for the time savings realized at the corresponding time during the day. Overall, this is roughly 24% of total US 36 traffic. The values of time for the remaining travelers were estimated based upon Denver MSA census income, setting a median value of time of \$18 per hour, consistent with the original FOCUS model input and the upper-percentile results calculated from the US 36 toll transactions. Table 5 presents the results of the value of time analysis. The decision to use eight classes specifically was based on a trade-off between representing variation of traveler preferences and model run time. The values of time for each class represent the midpoints of eight equally sized groups, each with 12.5% of trips.

Table 5. Value of Time Classes

Note: Each class represents 1/8 of the trips

Class	Percentile	Value of Time (\$/hour)
1	6.25%	3.00
2	18.75%	7.20
3	31.25%	12.00
4	43.75%	16.20
5	56.25%	21.00
6	68.75%	27.60
7	81.25%	36.00
8	93.75%	60.00

Source: Steer Davies Gleave

It is now well accepted that managed lanes users are not only paying for travel time savings, but are also paying for increased reliability and other perceived benefits such as safety improvements and comfort. As a result, the FOCUS model includes a “toll bonus” of 8 cents per mile on tolled facilities. This assumption was retained for the I-25 managed lanes, which effectively reduces the cost of the managed lanes by 8 cents per mile during the highway assignment step, where travelers are assigned to routes based on minimizing their generalized trip costs.²

2. Tolling Structure

Based on discussions with the design and traffic engineering teams, the forecasts in this memo are based on drivers using the express lanes through the gap paying a single, uniform toll, regardless of the entry and exit locations on the express lane. In most cases, trips on the express lanes through the gap would use the entirety of the express lanes, as opposed to the portion either north or south of the intermediate access and egress locations. It is likely that designs incorporating longer express lane corridors, which are continuing to be evaluated in the PEL study, would deviate from this assumption, to attract a broader scope of drivers such as those desiring to travel between Castle Rock and Denver County.

Thus, the toll charges modeled were expressed as toll amounts, in dollar increments, as opposed to per-mile charges. Tolls varied by model time period and direction, to simulate the operation of variable tolling in the express lanes (i.e., toll rates were higher in the peak periods in accordance with the tolling objectives described below).

3. Tolling Objective

At the direction of CDOT, SDG modeled the express lanes with a *throughput maximization* objective. The technical assumptions supporting this objective are to charge as low a toll as possible, to allow as many vehicles as possible to use the express lanes, with the following two constraints:

- The express lanes had to operate at congested speeds of 45 mph or higher
- The minimum toll charged in any time period and direction was \$3.00. The minimum toll amount was set based on experience with managed lanes in Colorado and elsewhere in the U.S. The \$3.00 minimum, for the 15-mile express lane facility being modeled, translates to 20 cents / mile, which is a typical and reasonable amount to charge outside of peak times of travel, and is similar to tolls currently charged on I-70 and US-36.³

4. Other Assumptions

Other key modeling assumptions related to the express lanes include the following:

- Vehicles with occupancy 1 or 2 must pay a toll
- Vehicles with occupancy 3 or higher, including transit, are allowed to use the express lanes free of charge

² “Generalized trip costs” combine monetary cost and travel time, with the latter converted into monetary units with values of time.

³ The toll pricing and minimum toll assumptions in this work represent modeling practices for initial studies. A level 2 traffic and revenue study, conducted by the Colorado High Performance Transportation Enterprise (HPTE), is currently underway and will contain more refined assumptions about toll pricing. The performance of the express lanes in the off-peak periods where the minimum toll is in effect is not likely to vary significantly with the minimum toll assumption, since the general purpose lanes are performing well in those periods.

- Trucks would not use the express lanes⁴

F. Treatment of Opening Year (2021)

The assumptions supporting 2021 forecasts were:

- Trip matrices were interpolated linearly between 2017 and 2021.
- As an exception to the above, trips to or from the model zones containing planned development projects in several TAZs in southern Castle Rock near Crystal Valley Parkway and Plum Creek Parkway were not included. Essentially, it was assumed those development projects would not happen before 2021.
- The highway network from 2017 was retained as is for the No Action forecasts, adding only the proposed I-25 improvements for the Proposed Action forecasts.

3. Project Alternatives

I-25 currently has 2 general purpose (GP) lanes in each direction through “the Gap,” from 2 miles south of Plum Creek Parkway (exit 181) approximately to County Line Road (exit 163)⁵. The forecasts in this memo include two alternatives for each forecast year (2021 and 2040): one with an additional GP lane, and one with an express lane.

A. 3 General Purpose Lanes

The first alternative would have 3 GP lanes through the gap, widening the sections currently with two GP lanes to three. Tolls are not part of this alternative.

B. Express Lane + 2 General Purpose Lanes (“2+1”)

The second alternative, often referred to as “2+1”, would add a variably-tolled express lane in each direction. The express lanes would span about 15 miles, extending from about one mile south of Plum Creek Parkway at the northern terminus to just north of County Line Road at the southern terminus. Some notable details of this design are:

- Vehicles entering I-25 southbound from the future Crystal Valley Parkway interchange (expected to be built in the 2040 forecasts only) would not be able to access the express lanes. Similarly, vehicles traveling northbound in the express lanes would not be able to exit at the Crystal Valley Parkway interchange.
- Intermediate access / egress points would include:
 - Northbound egress before Upper Lake Gulch Road (exit 172)
 - Northbound access after Spruce Mountain Road (exit 173)
 - Southbound egress before Spruce Mountain Road (exit 173)
 - Southbound access after Upper Lake Gulch Road (exit 172)

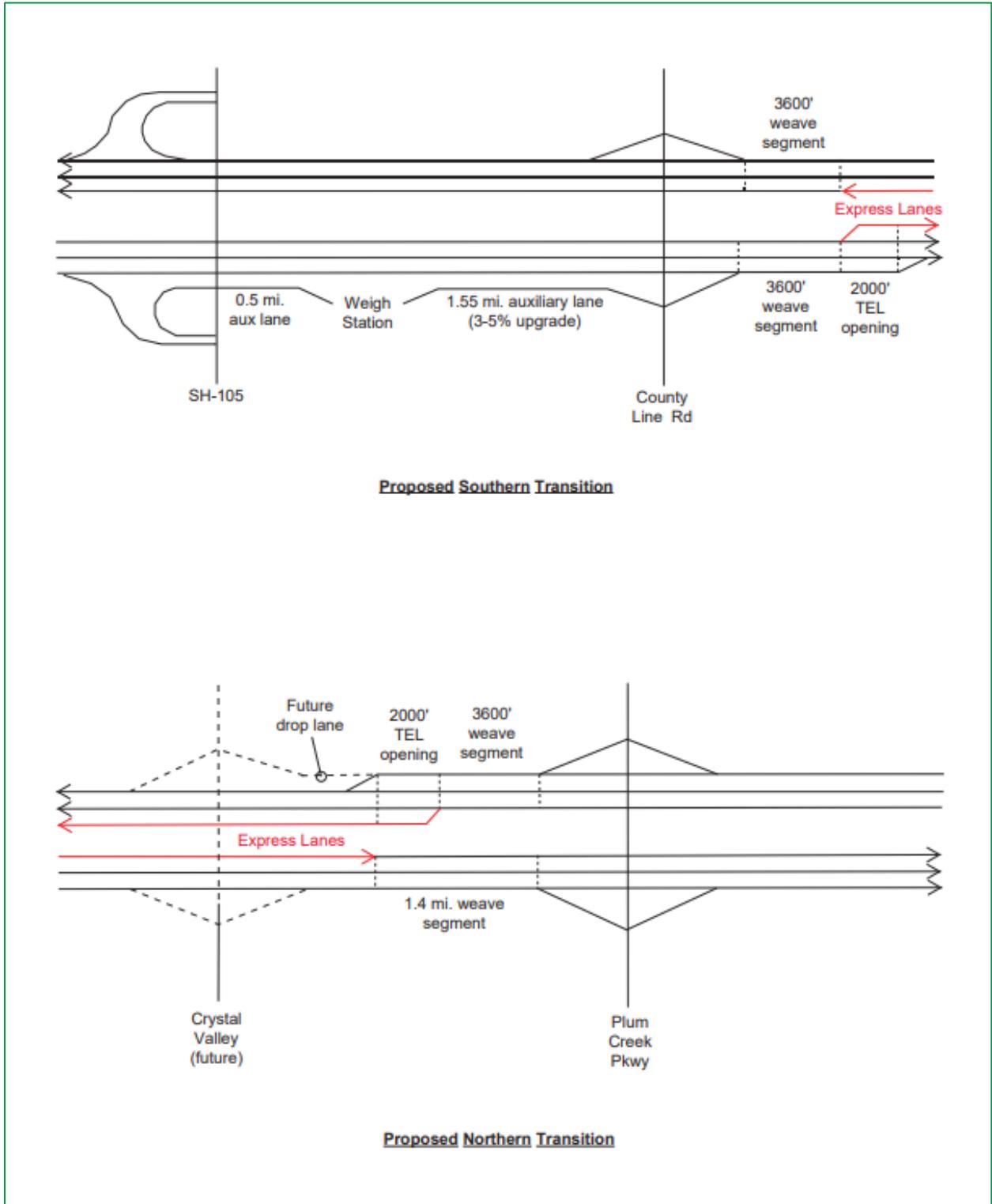
⁴ Although this is a policy decision that has not yet been made, not allowing trucks is equivalent to charging an extremely high toll for trucks from a modeling standpoint. Thus, even if trucks are ultimately allowed, as long as they are charged very high tolls, the exclusion of trucks from the express lanes in these forecasts does not significantly affect the forecasts’ accuracy.

⁵ In the southbound direction, I-25 widens to three lanes again about a mile south of County Line Road, whereas in the northbound direction, I-25 narrows to 2 lanes at the County Line Road overpass. Due to the various lane transitions occurring between mileposts 161 and 163, the southern limit of the Gap is described at milepost 161.

It is important to note that the design and location of the express lanes are still being evaluated, and it is likely that they will be further refined in final design and informed by the results of the Traffic and Revenue Study. However, the modeling results reported here are based on these assumptions.

Figure 3 below shows design details of the northern and southern transitions between the “2+1” express lane sections of I-25 and the 3 GP lane sections immediately to the north and south.

Figure 3 – Project Design, “2+1” Alternative



Source: CH2M

4. Forecasts

A. Traffic Volumes

Table 6 presents forecasts of average daily volumes for 2021 and 2040 across each scenario. Volumes for scenarios including express lanes are broken down by general purpose (GP) versus Express Lane (EL) usage for the count location on I-25 north of Greenland Road.

Table 6. Average Weekday – Daily Bi-Directional Traffic Volumes

I-25 Location	2017	2021			2040		
		No Action	3 GP Lanes	2 GP + Express Lane	No Action	3 GP Lanes	2 GP + Express Lane
I-25 N/O Baptist Rd	95,540	99,830	100,540	100,170	138,540	140,980	140,380
I-25 N/O Greenland Rd	78,140	83,740	85,270	GP: 75,720 EL: 9,330 85,050	110,670	119,650	GP: 92,710 EL: 25,940 118,650
I-25 S/O Plum Creek Pkwy	79,000	84,730	87,430	86,950	146,170	153,030	149,980
I-25 N/O Founders Pkwy	132,750	139,760	140,550	140,200	189,940	191,530	190,860
C-105 S/O Tomah Rd	3,130	4,320	3,290	3,430	13,110	8,080	9,510
C-83 S/O Gillian Ave	5,790	7,750	6,770	7,200	16,130	12,220	13,100

Source: Steer Davies Gleave

Note that volumes on I-25 for a given forecast year are highest for the 3 GP scenario, followed by the 2 GP + 1 express lane scenario, and finally the No Action scenario. Volumes on the parallel routes exhibit the opposite trend, i.e. they are higher when there is less capacity on I-25.

B. Travel Times

Travel time forecasts (in minutes) by time period for the 20.7-mile Gap section of I-25 (exits 160-181) are shown in Table 7 for 2021 and Table 8 for 2040. Note that a 17.7-minute travel time corresponds to the modeled free flow speed of 70 MPH; actual speeds may be higher during uncongested time periods.

Table 7. 2021 Forecast I-25 Gap Travel Times by Time of Day (minutes)

Scenario / Direction		AM1	AM2	AM3	OP2	OP3	PM1	PM2	PM3	OP4	OP1	FR1	FR2	FR3	SU3
		6:30-7A	7-8A	8-9A	9-11:30A	11:30A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30A-3P
Existing Conditions	NB	18.7	20.6	20.3	17.9	18.1	17.7	17.7	17.2	18.1	18.2	21.7	18.4	18.2	23.6
2021 No Action	NB	20.0	24.6	21.4	19.3	19.4	19.0	18.7	17.7	18.1	18.2	22.8	19.7	19.0	25.8
2021 3 GP lanes	NB	17.7	17.9	17.7	17.7	17.7	17.7	17.7	17.7	18.1	18.2	17.7	17.7	17.7	19.3
2021 2GP + 1 Express - using GP	NB	17.7	20.2	19.3	17.7	18.5	17.7	17.7	17.7	18.1	18.2	18.7	17.7	18.6	21.8
2021 2GP + 1 Express - using Express	NB	17.7	17.7	17.7	17.7	17.7	17.7	17.7	17.7	18.1	18.2	17.7	17.7	17.7	18.7
Existing Conditions	SB	19.1	20.4	18.9	18.5	19.1	22.4	21.9	18.0	18.9	19.1	36.4	40.1	35.3	19.3
2021 No Action	SB	19.6	21.1	19.4	18.9	20.0	25.7	26.2	20.0	18.9	19.1	49.3	52.0	47.7	20.5
2021 3 GP lanes	SB	18.5	19.4	18.2	17.7	17.7	19.3	17.9	17.7	18.8	19.1	23.3	22.0	21.8	17.7
2021 2GP + 1 Express - using GP	SB	19.5	20.8	19.1	18.6	19.3	21.7	20.5	18.4	18.9	19.1	26.0	25.5	24.0	18.9
2021 2GP + 1 Express - using Express	SB	18.5	19.4	18.2	17.7	17.7	19.3	17.9	17.7	18.7	19.1	23.2	21.9	22.2	17.7

Source: Steer Davies Gleave

Table 8. 2040 Forecast I-25 Travel Times by Time of Day for the 20.7-miles of the Gap (minutes)

Scenario / Direction		AM1	AM2	AM3	OP2	OP3	PM1	PM2	PM3	OP4	OP1	FR1	FR2	FR3	SU3
		6:30-7A	7-8A	8-9A	9-11:30A	11:30A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30A-3P
Existing Conditions	NB	18.7	20.6	20.3	17.9	18.1	17.7	17.7	17.2	18.1	18.2	21.7	18.4	18.2	23.6
2040 No Action	NB	28.7	63.2	30.5	29.5	30.4	29.8	28.6	23.8	18.4	18.2	36.0	30.8	27.8	40.2
2040 3 GP lanes	NB	18.1	35.8	20.8	18.5	19.5	18.4	18.8	18.0	18.1	18.2	21.5	18.7	19.5	25.5
2040 2GP + 1 Express - using GP	NB	20.8	43.3	23.1	22.8	23.3	20.9	21.4	20.2	18.4	18.2	24.0	21.2	21.8	28.6
2040 2GP + 1 Express - using Express	NB	17.7	28.7	19.3	17.7	17.7	17.7	17.7	17.7	18.1	18.2	20.1	17.4	18.3	23.4
Existing Conditions	SB	19.1	20.4	18.9	18.5	19.1	22.4	21.9	18.0	18.9	19.1	36.4	40.1	35.3	19.3
2040 No Action	SB	26.7	31.7	26.4	25.2	28.6	82.2	120.1	31.2	20.0	19.1	120.2	128.0	108.4	30.7
2040 3 GP lanes	SB	19.7	21.9	19.5	18.8	19.4	30.4	68.5	18.8	18.9	19.1	53.6	64.5	26.5	19.2
2040 2GP + 1 Express - using GP	SB	21.5	23.9	21.2	21.4	22.3	41.7	68.4	19.0	19.7	19.1	70.0	97.0	40.1	21.6
2040 2GP + 1 Express - using Express	SB	18.6	20.0	18.3	17.7	17.7	28.9	30.2	17.7	18.7	19.1	34.0	34.0	24.4	16.6

Source: Steer Davies Gleave

C. Travel Speeds

Travel speeds (in mph) in 2021 and 2040 are derived from the travel time forecasts over the 20.7 miles of the Gap, shown in Tables 9 and 10.

Table 9. 2021 Forecast I-25 Gap Average Speeds by Time of Day (miles per hour)

Scenario / Direction		AM1	AM2	AM3	OP2	OP3	PM1	PM2	PM3	OP4	OP1	FR1	FR2	FR3	SU3
		6:30-7A	7-8A	8-9A	9-11:30A	11:30A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30A-3P
Existing Conditions	NB	66	60	61	69	69	70	70	72	69	68	57	68	68	53
2021 No Action	NB	62	51	58	64	64	65	66	70	69	68	55	63	65	48
2021 3 GP lanes	NB	70	69	70	70	70	70	70	70	69	68	70	70	70	64
2021 2GP + 1 Express - using GP	NB	70	62	64	70	67	70	70	70	69	68	66	70	67	57
2021 2GP + 1 Express - using Express	NB	70	70	70	70	70	70	70	70	69	68	70	70	70	66
Existing Conditions	SB	65	61	66	67	65	56	57	69	66	65	34	31	35	64
2021 No Action	SB	63	59	64	66	62	48	47	62	66	65	25	24	26	61
2021 3 GP lanes	SB	67	64	68	70	70	64	69	70	66	65	53	56	57	70
2021 2GP + 1 Express - using GP	SB	64	60	65	67	64	57	61	67	66	65	48	49	52	66
2021 2GP + 1 Express - using Express	SB	67	64	68	70	70	64	69	70	66	65	54	57	56	70

Source: Steer Davies Gleave

Table 10. 2040 Forecast I-25 Gap Average Speeds by Time of Day (miles per hour)

Scenario / Direction		AM1	AM2	AM3	OP2	OP3	PM1	PM2	PM3	OP4	OP1	FR1	FR2	FR3	SU3
		6:30-7A	7-8A	8-9A	9-11:30A	11:30A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30A-3P
Existing Conditions	NB	66	60	61	69	69	70	70	72	69	68	57	68	68	53
2021 No Action	NB	62	51	58	64	64	65	66	70	69	68	55	63	65	48
2021 3 GP lanes	NB	70	69	70	70	70	70	70	70	69	68	70	70	70	64
2021 2GP + 1 Express - using GP	NB	70	62	64	70	67	70	70	70	69	68	66	70	67	57
2021 2GP + 1 Express - using Express	NB	70	70	70	70	70	70	70	70	69	68	70	70	70	66
Existing Conditions	SB	65	61	66	67	65	56	57	69	66	65	34	31	35	64
2021 No Action	SB	63	59	64	66	62	48	47	62	66	65	25	24	26	61
2021 3 GP lanes	SB	67	64	68	70	70	64	69	70	66	65	53	56	57	70
2021 2GP + 1 Express - using GP	SB	64	60	65	67	64	57	61	67	66	65	48	49	52	66
2021 2GP + 1 Express - using Express	SB	67	64	68	70	70	64	69	70	66	65	54	57	56	70

Source: Steer Davies Gleave

The following four tables show average speed forecasts through the gap for each scenario and lane type based on existing (2017) conditions and the 2021 and 2040 modeling results, with all other years linearly interpolated, color-coded by speed. These tables also include express lane capture rates by Passenger Car Equivalents (PCE) based on the assumption that trucks correspond to three passenger vehicles.⁶ Table 11 shows average Weekday travel speeds in the Northbound direction during the AM period (6:30-9AM). Table 12 shows average Weekday travel speeds in the Southbound direction during the PM period (3-7PM). Tables 13 and 14 display Weekend travel speeds, defined as Friday PM period travel speeds in the Southbound direction and Sunday afternoons (11:30AM-3PM) in the Northbound direction.⁷

Table 11. Weekday AM Northbound Year by Year Average Speeds (MPH) from Monument to Plum Creek (Exits 160-181)

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	62				
2018	60				
2019	59				
2020	57				
2021	56	70	70	64	15%
2022	55	69	69	63	16%
2023	53	68	69	62	17%
2024	52	67	68	61	17%
2025	51	66	67	60	18%
2026	50	65	67	59	19%
2027	49	64	66	58	19%
2028	47	63	65	57	20%
2029	46	62	65	56	21%
2030	45	61	64	55	22%
2031	44	60	63	54	22%
2032	42	59	62	53	23%
2033	41	58	62	52	24%
2034	40	57	61	51	25%
2035	39	56	60	50	25%
2036	38	55	60	49	26%
2037	36	54	59	48	27%
2038	35	53	58	47	28%
2039	34	53	58	46	28%
2040	33	52	57	45	29%

Source: Steer Davies Gleave

⁶ Capture rates are equal to the percentage of the total (GP+EL) volume which is on the express lane. The GP volume includes both autos and trucks, with trucks counted as 3 autos for the purposes of this calculation.

⁷ Note that tables 11-14 represent a more aggregate set of time periods than tables 7-10, and contain the periods of highest congestion only. To obtain 6:30-9:00 AM speeds, travel times were weighted across the AM1, AM2 and AM3 periods and converted to speeds. A similar process was followed for PM. The speeds in tables 11-14 represent averages across the entire peak period, and if peak spreading occurs, may actually be a more accurate representations of typical conditions.

Table 12. Weekday PM Southbound Year by Year Average Speeds (MPH) from Plum Creek to Monument (Exits 181-160)

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	59				
2018	57				
2019	55				
2020	53				
2021	52	67	67	61	18%
2022	50	66	66	59	19%
2023	48	64	65	58	19%
2024	47	63	64	57	20%
2025	45	62	63	55	20%
2026	43	60	62	54	20%
2027	42	59	61	53	21%
2028	40	58	60	51	21%
2029	38	56	60	50	22%
2030	37	55	59	49	22%
2031	35	54	58	48	22%
2032	33	52	57	46	23%
2033	32	51	56	45	23%
2034	30	50	55	44	24%
2035	28	48	54	42	24%
2036	27	47	53	41	24%
2037	25	45	52	40	25%
2038	23	44	51	38	25%
2039	22	43	50	37	26%
2040	20	41	49	36	26%

Source: Steer Davies Gleave

Table 13. Weekend Southbound (Friday afternoon) Year by Year Average Speeds (MPH) from Plum Creek to Monument (Exits 181-160)

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	34				
2018	31				
2019	29				
2020	27				
2021	25	55	55	49	21%
2022	24	54	54	47	21%
2023	24	52	53	46	22%
2024	23	51	53	44	22%
2025	22	49	52	43	22%
2026	21	48	51	41	23%
2027	20	46	50	40	23%
2028	20	45	50	38	23%
2029	19	44	49	37	24%
2030	18	42	48	35	24%
2031	17	41	47	34	24%
2032	17	39	46	32	24%
2033	16	38	46	31	25%
2034	15	37	45	29	25%
2035	14	35	44	27	25%
2036	14	34	43	26	26%
2037	13	32	42	24	26%
2038	12	31	42	23	26%
2039	11	30	41	21	27%
2040	10	28	40	20	27%

Source: Steer Davies Gleave

Table 14. Weekend Northbound (Sunday mid-day) Year by Year Average Speeds (MPH) from Monument to Plum Creek (Exits 160-181)

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	53				
2018	51				
2019	50				
2020	49				
2021	48	64	66	57	15%
2022	47	64	66	56	16%
2023	46	63	65	55	17%
2024	45	62	64	55	17%
2025	44	61	64	54	18%
2026	44	60	63	53	19%
2027	43	59	62	53	19%
2028	42	59	61	52	20%
2029	41	58	61	51	21%
2030	40	57	60	50	22%
2031	39	56	59	50	22%
2032	38	55	59	49	23%
2033	37	54	58	48	24%
2034	36	54	57	48	25%
2035	35	53	57	47	25%
2036	35	52	56	46	26%
2037	34	51	55	45	27%
2038	33	50	54	45	27%
2039	32	50	54	44	28%
2040	31	49	53	43	29%

Source: Steer Davies Gleave

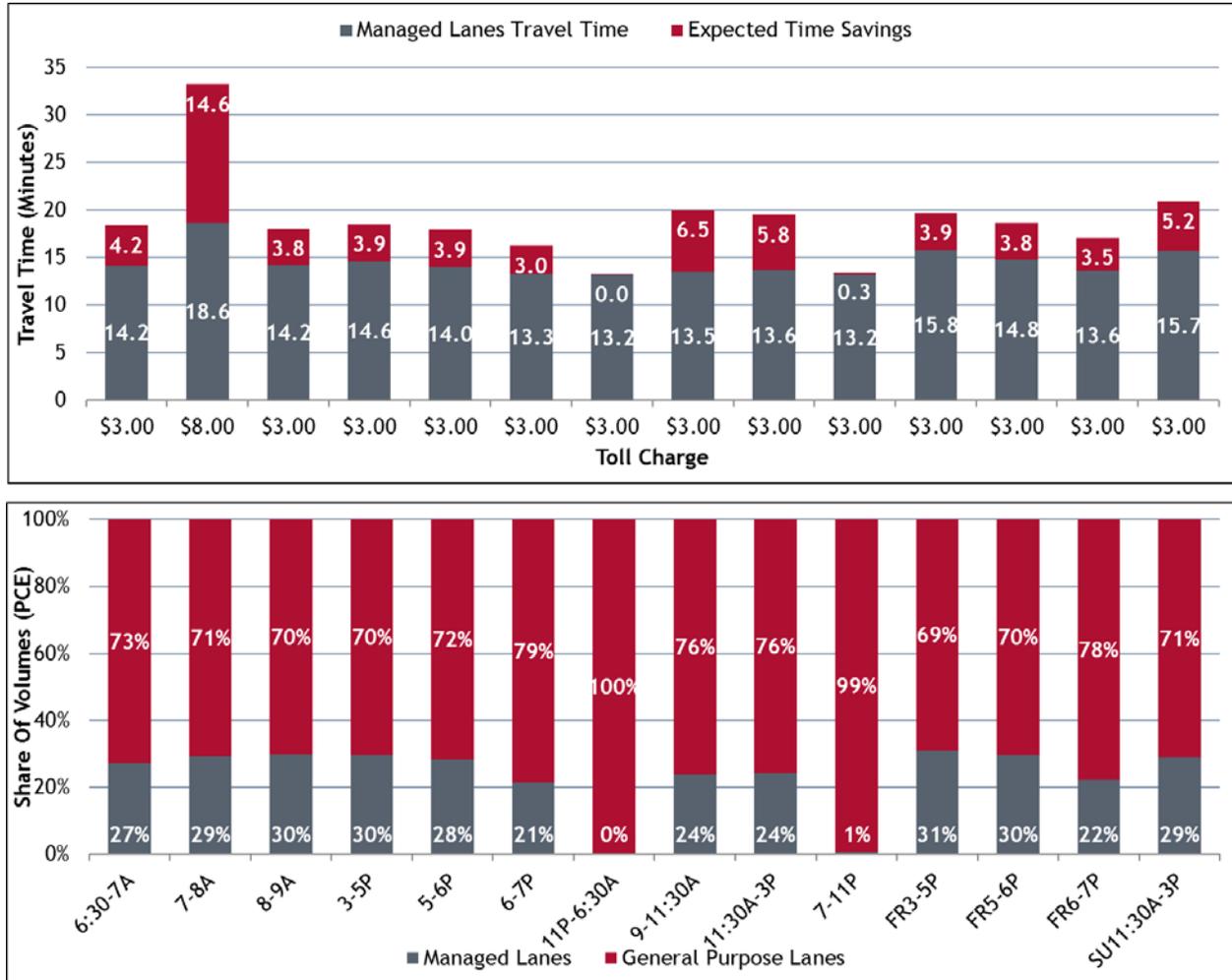
It should also be noted that the speeds shown in Tables 11-14 represent the average speed through the 20.7 mile gap, of which about 15 miles includes express lanes. As a result, for the remaining 5.7 miles of the “Express lane” (EL), travel would be in general purpose lanes. As a consequence, EL speeds eventually drop below 45 MPH in Table 13, because although the 15 miles can be managed to the minimum 45 mph speed, the remaining 5.7 miles of GP lane travel cannot be managed in this way so in congested periods, this brings down the average speeds in the EL. The full 20.7-mile Gap is reported so that results can be compared easily between the project design documented in this modeling effort and potential other project designs, where the express lanes start and/or end in slightly different locations within the gap.

D. Express Lane Usage

For the 2 General Purpose Lane +1 Express Lane (“2+1”) scenario in 2040, Figure 4 shows total expected travel time in the Northbound direction for the 15-mile section of the Gap that would include express lanes by time period. This total travel time is broken down by travel time in the express lanes and the expected time saved by taking the express lanes instead of the general purpose lanes. The bottom half of Figure 4 shows the breakdown of vehicles (based on PCE) taking the express lane versus general purpose lanes for the 2+1 scenario in the Northbound direction in 2040.

Note that for both Figures 4 and 5, the bars in the top half of the chart line up with the bottom half, so the time period labels in the bottom half each apply to the bars immediately above them in the top half too. The time periods correspond to those listed in section 2 of this memo; the first 10 are average weekday periods, the next three (labeled “FR”) are Friday and the last one (labeled “SU”) is Sunday.

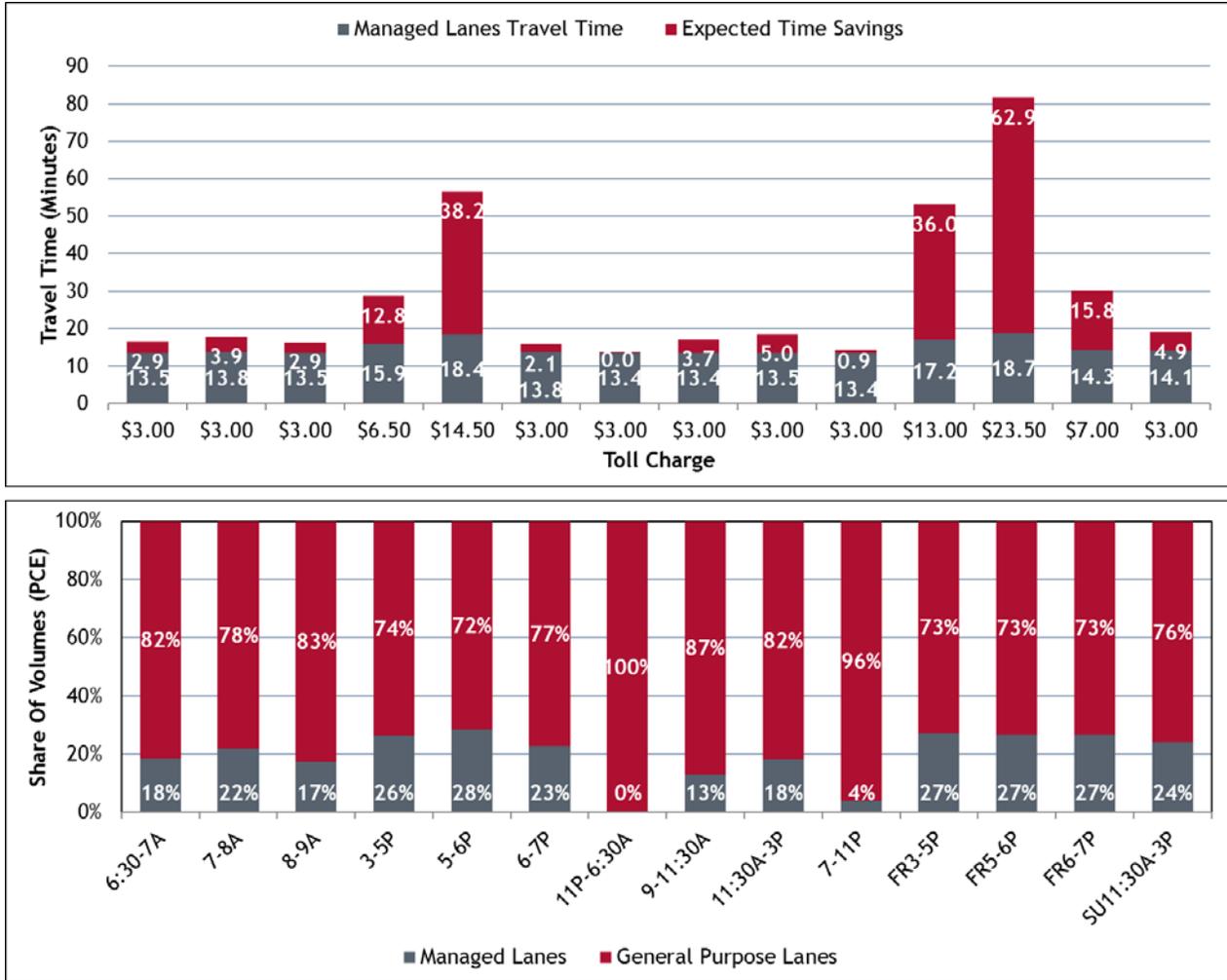
Figure 4. 2040 2+1 Scenario Northbound Express Lane Travel Time Savings and Volume Share



Source: Steer Davies Gleave

Southbound travel times and express lane travel time savings for the 2040 2+1 scenario are shown in Figure 5 with the spread of PCE volumes by lane type.

Figure 5. 2040 2+1 Scenario Southbound Express Lane Travel Time Savings and Volume Share



Source: Steer Davies Gleave

5. Conclusions

A. Benefits of the Project

The tables in Section 4 show that in the peak periods, both alternatives tested provide travel improvements through the gap compared to a No Action scenario. Conditions would be near free flow on average in opening year (2021), with the exception of Friday afternoon for general purpose lane users in the 2+1 alternative (which would still improve average speeds from 25 to 49 miles per hour). In general, the differences between No Action and *any* of the action alternatives are greater than the differences in between the various action alternatives. To summarize 2021 projections:

- Express lanes offer at most two minutes of time savings over general purpose lanes; and
- Travel times in the 3 GP lane alternative are no more than three minutes faster than the express lanes in the 2+1 alternative.

The above observations, however, do not hold in 2040, as follows:

- Express lanes are faster than general purpose lanes in the 2+1 alternative in the Friday PM period by about 20 MPH. They are also faster in the weekday AM and PM and Sunday mid-day peaks by 10 to 13 MPH; and
- Express lanes in the 2+1 alternative are 8 to 12 MPH faster than general purpose lanes in the 3 GP lane alternative in the weekday and Friday PM peaks. In the weekday AM and Sunday mid-day peaks they are faster by 4 to 5 MPH.
- Average travel speeds are likely to be more consistent in the Express Lanes.

Tables 15 and 16 summarize these trends for the four key time periods, expressed in travel times (Table 15) and average speeds (Table 16).

Table 15 – Summary of Gap Travel Time Forecasts in Peak Periods

Time Period / Direction	Existing 2017 Travel Time (Minutes)	Year	Travel Time (Minutes)			
			No Action	3 GP Lanes	2 GP + 1 Express Lane	
				Express Lane	GP Lane	
Weekday 6:30 – 9:00 AM Northbound	20	2021	22	18	18	19
		2040	43	26	23	31
Weekday 3:00 – 7:00 PM Southbound	21	2021	24	19	19	21
		2040	79	37	26	43
Friday 3:00 – 7:00 PM Southbound	37	2021	50	23	23	25
		2040	119	50	32	69
Sunday 11:30 AM – 3:00 PM Northbound	24	2021	26	19	19	22
		2040	40	26	23	29

Source: Steer Davies Gleave

Table 16 – Summary of Gap Average Speed Forecasts in Peak Periods

Time Period / Direction	Existing 2017 Average Speed (MPH)	Year	Average Speed (MPH)			
			No Action	3 GP Lanes	2 GP + 1 Express Lane	
				Express Lane	GP Lane	
Weekday 6:30 – 9:00 AM Northbound	62	2021	56	70	70	64
		2040	33	52	57	45
Weekday 3:00 – 7:00 PM Southbound	59	2021	52	67	67	61
		2040	20	41	49	36
Friday 3:00 – 7:00 PM Southbound	34	2021	25	55	55	49
		2040	10	28	40	20
Sunday 11:30 AM – 3:00 PM Northbound	53	2021	48	64	66	57
		2040	31	49	53	43

Source: Steer Davies Gleave

Attachment 1

I-25 PEL/NEPA Technical
Memorandum - Model Development
and Calibration

To Chris Bisio, CH2M / Jacobs

Cc Mandy Whorton, Peak Consulting Group
Shane Binder, Apex Engineering
Steve Cook, DRCOG
Ken Prather, PPACG
Nnaemeka Ezekwemba, FHWA
Sam Bohluli, C&M Associates

From Mark Feldman

Date January 30th, 2018

Project I-25 South PEL/NEPA

Project No. 23029302

I-25 PEL/NEPA Technical Memorandum - Model Development and Calibration

Introduction

This memorandum documents both the data collection and model development, calibration and validation portions of the travel demand modeling work performed by Steer Davies Gleave (SDG), corresponding to the I-25 South travel demand modeling efforts.

The work discussed in this memo was performed in April – October 2017, and will feed into two different forecasting efforts:

- Environmental Assessment (EA) forecasts for NEPA documents, late 2017 / early 2018, focusing on the Gap Section of I-25 (between exits 161 and 181)
- Development and calibration of the model for the Planning and Environmental Linkage (PEL) study area, spanning the DRCOG and PPACG regions along the I-25 corridor, from approximately C/E-470 to Academy Blvd.

This memo contains the following sections:

- Data Collection
- Model Development
- Model Calibration and Validation
- Development of Forecast Year Model

Documentation of the NEPA/EA forecasts will occur in a separate memo after they are finalized in early 2018. The PEL forecasting will likely take place in mid-late 2018.

Data Collection

MPO Travel Demand Models

It is customary to use regional travel demand models for traffic forecasting. In a corridor study such as this, these models can forecast changes in traffic volumes to apply to existing traffic counts used in more detailed simulation models and HCM-based traffic analyses.

The project's area of influence extends through Douglas County and into northern El Paso County. Therefore, it was necessary for the travel demand model to include areas within both the DRCOG and PPACG MPOs. In late 2016, SDG proposed a modeling tool that extends DRCOG's FOCUS model into northern El Paso County, with its southern edge in the Briargate region of Colorado Springs. This was originally documented in the Modeling Approach Memo, dated January 3rd, 2017 (included as Appendix C to this memo). To create this extended travel demand model, SDG obtained a complete version of FOCUS 2.0 from DRCOG and the most current roadway network and trip matrices from PPACG. We will refer to this travel demand model, which pieces together information from the two MPOs from here on as the "I-25 Model". We will often refer to the official DRCOG (FOCUS) and PPACG models as the "MPO Models".

Both of the MPO models have 2015 and 2040 analysis years. SDG accounted for differences between 2015 and current (2017) levels of trip-making by adjusting trip matrices to validate 2017 traffic count data at the I-25 Model's southern edge in the Briargate area. This is discussed further in the calibration section of this memo. We assumed differences between 2015 and 2017 networks to be negligible.

By working exclusively with trip matrices (origin-destinations by time period and vehicle occupancy / type) and networks, SDG did not rerun or alter any of the model components prior to highway assignment. Thus, our analysis to date has assumed negligible effects of the project alternatives on trip generation, trip distribution, mode choice, or any of the activity based model components of FOCUS.

It should be noted that we performed the calibration and validation exercises with a model that contained the FOCUS region in its entirety, plus a small portion of the PPACG region, but ultimately extracted a subarea of the FOCUS region for the purposes of doing forecasts to reduce model running time. The validation results were checked and compared, and they were not significantly different after the subarea extraction compared to the validation results when the entire FOCUS region was included. Note that the validation results featured in this memo are from the subarea extraction model ("I-25 Model").

Traffic Counts

Traffic counts serve as indicators that the base year models are reasonably reflecting existing levels of traffic and congestion. We contracted All Traffic Data (ATD) to collect traffic count data at the following facilities:

- I-25 along the study corridor (from C-470 to Academy Boulevard)
- Ramps on/off I-25 along the study corridor; and
- Select locations on parallel routes (SH-105, SH-83, and I-25 Frontage Roads) on each segment separated by major intersections

Counts along I-25 were taken with video cameras and included vehicle classifications and 15-minute time intervals. Other counts were taken with traditional tubes at 30-minute time intervals. The count dates included:

- Wednesday / Thursday May 16-17 (alternate segments on I-25 between C-470 and Academy Blvd, all ramps, parallel routes)
- Friday / Saturday / Sunday August 4-6 (most segments on I-25 between Happy Canyon and Baptist Rd, all ramps, parallel routes)

For model calibration purposes, we averaged traffic counts on the two weekdays to obtain a weekday average count.

Quality Control Procedures for Traffic Count Data

SDG undertook a methodical approach to processing the traffic count data. We conducted extensive quality control procedures to ensure that the data was of good quality. This involved general scanning for anomalies and checking for the following:

- Directionally balanced daily counts on each facility¹
- Proper count magnitude (e.g. highways should carry about 20,000-30,000 vehicles per day per lane, major arterials 10,000)
- Low volumes on rural area ramps (less than 5,000 vehicles per day, often much less)
- Time of day profiles consistent with well-recognized travel patterns (in the case of our study area, more northbound traffic towards Denver than southbound in the morning, and the reverse in the evening)
- Consistency between adjacent mainline locations and reconciliation with ramp counts in between (see example in Table 2 below)
- Consistency between Friday and weekday counts (ensuring their time profiles were generally similar with Friday having slightly more traffic overall)

For the most part, the data collected from ATD was found to be of good quality. In a few isolated cases, we found anomalous data, and those instances were all addressed to our satisfaction.

Table 1 illustrates one example of SDG's quality control uncovering anomalous count data. Checking the traffic volumes from the August Friday and Sunday data at several points along I-25, we noticed that the northbound daily volume between exits 188 and 192 looked low, given the volumes to the north and south. Both the Friday and Sunday data showed sharp decreases between adjacent locations, followed by a much sharper increase. It did not seem possible for I-25 to lose 6,000 – 9,000 vehicles and then gain 14,000 - 20,000 vehicles immediately afterwards, given the volumes of the ramps in between, although not collected in the Friday and Sunday data, were all 11,000 or less on weekdays.

We brought this to the attention of ATD, and an investigation determined that they had mistakenly not included one of the lanes in the Friday and Sunday totals. They provided corrected data, also shown below, which was more in line with the other locations and with the weekday data.

¹ The frontage roads parallel to I-25 between exits 174 and 181 are an exception to this. One expects higher southbound than northbound volumes on these facilities, due to the tendency of travelers to avoid I-25 southbound where it drops from 3 to 2 lanes.

Table 1: Northbound Daily I-25 Count Quality Control

Location	Friday	Sunday
Between Exits 185-187	77,034	60,089
Between Exits 188-192 - original	68,238	53,854
Between Exits 188-192 - corrected	84,995	65,214
South of Exit 193 Diagonal On Ramp	88,199	67,834

Due to budget constraints, counts along I-25 in some cases were taken at alternate segments², and we estimated the traffic counts at the locations in between by using the upstream and downstream traffic count locations on I-25 and the ramp counts in between. We calculated estimates both ways ('forwards' and 'backwards'), checked to ensure that both directions yielded similar estimates, and averaged the two. This approach yielded appropriate data and did not result in any data concerns.

Table 2 presents an example of this. In this instance, northbound counts on I-25 were collected between exits 151-153 and between exits 156-158, but not between exits 153-156.

Table 2: Data for Estimation of I-25 Segments with Adjacent Segment and Ramp Counts

Location	Daily Traffic Count
I-25 Between Exit 151 (Briargate) and Exit 153 (Interquest)	58,336
Exit 153 off	9,510
Exit 153 on	8,818
I-25 Between Exit 153 (Interquest) and Exit 156 (N Gate)	Not Collected
Exit 156 off	9,428
Exit 156 on	5,041
I-25 Between Exit 156 (N Gate) and Exit 158 (Baptist)	53,248

Source: Steer Davies Gleave

We estimated the I-25 Mainline location between Exit 153 (Interquest Pkwy) and Exit 156 (North Gate Blvd) in the forwards direction as the upstream location count – off-ramp count + on-ramp count, or:

$$58,336 - 9,510 + 8,818 = 57,644$$

Conversely, in the backwards direction, we use the downstream location, back out the on- ramp, and add back in the off-ramp, to get:

$$53,248 - 5,041 + 9,428 = 57,635$$

The 'forwards' and 'backwards' estimates are quite similar, thus passing our quality check. We used the average of the estimates, or 57,639. We performed similar estimates and checks in each time period to cover

² In most cases we collected counts at alternate segments. In the gap, we collected counts just north of Palmer Divide Rd and just south of Plum Creek Parkway, and estimated all segments in between in this manner, because the interchanges in between all have very low traffic volumes and thus I-25 volume differences between segments in the gap are minimal.

other segments on I-25 where counts were not collected.

The calibration tables in Appendix A contain a detailed breakdown of the I-25 and select parallel route and frontage road traffic count data by direction and time period. The complete set of traffic count data is available in spreadsheets upon request.

Travel Times

We collected INRIX historical travel time and speed data for I-25 and parallel routes (US-85 and SH-105 to the west, and SH-83 to the east). We collected these data at the individual observation level for all of calendar year 2016 and on the select days in 2017 when we conducted traffic counts. We then summarized them by the following categories:

- **Highway segment:** nine per direction on I-25, and a combined 14 per direction on the parallel routes;
- **Time of day:** ten weekday time periods that align with those used in the FOCUS model, plus three Friday afternoon / evening periods and one Sunday afternoon period;
- **Day of week:** weekdays (Mondays – Thursdays), Fridays, and Sundays; and
- **Month of year:** summer months (June – August) and non-summer months.

For each unique time-location combination, we calculated the mean travel times and speeds across all observations recorded. We aggregated mean observed travel times into longer highway segments (five per direction on I-25, and a combined six per direction on the parallel routes), which were in turn compared against modeled travel times during the model calibration process. In total, we performed three separate calibration processes:

- Weekday AM and PM peak periods on the specific days traffic counts were collected (Wednesday, May 17 and Thursday, May 18, 2017);
- Weekday midday and night time periods for the overall 2016 average; and
- The three Friday periods and one Sunday period on the specific days traffic counts were collected (Friday, August 4 and Sunday, August 6, 2017).

Travel speed data also aided us in determining free-flow speeds when setting up the model network.

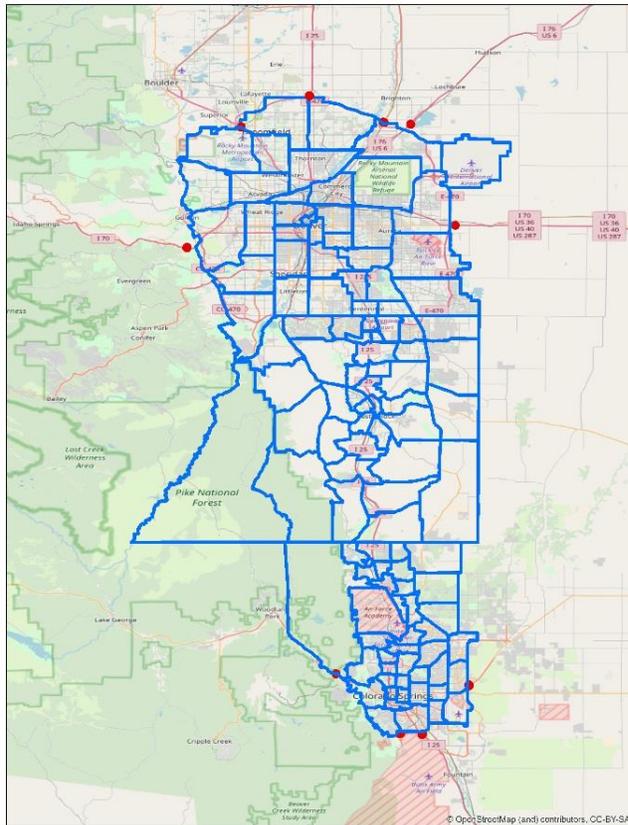
In addition, to inform our value of time analysis, we collected a further sample of similar travel time and speed data for the US-36 tolled express lanes. These data were collected for April 2017 (the month with the highest quality express lane transaction data) and averaged in 15-minute intervals. More details about this analysis are provided in a subsequent section.

Origin-Destination Data

StreetLight Data served as the primary data source for determining OD patterns. StreetLight Data offers current year information and is relatively low cost compared to other travel pattern sources.

We defined 114 zones to cover Denver, Douglas County, and Northern El Paso County down to Colorado Springs to ensure coverage of the study area. Additionally, ten pass-through zones were created to represent major external thoroughfares. These 124 zones are shown in Figure 1, below, with blue borders denoting the 114 zones and red indicating external pass-through locations.

Figure 1: StreetLight Data Zone System



Source: Steer Davies Gleave

The data enabled us to observe trip distributions between zones differentiated based on personal versus commercial vehicle type, summer versus off-season, time of day, and weekday, Friday, Saturday, and Sunday. For review, the 124 zones were aggregated into mega-zones representing Denver, Castle Rock, Larkspur, Monument, Colorado Springs, externals north of Denver, and externals south of Colorado Springs. The distributions at the mega-zone level were validated against the American Community Survey commuting flows between Colorado counties.

We applied these distributions, in Table 3 and Table 4, to join the weekday trip matrices for the separate FOCUS and PPACG MPO models. We did not modify any intra-MPO trips, but used the StreetLight Data along with traffic counts to expand the trip matrices for inter-MPO trips. The model development section of this memo contains further details on this process.

Table 3: Weekday Personal Trip Distribution

	FOCUS externals	Denver Metro	Castle Rock Area	Larkspur Area	Monument / Briargate Area	Colorado Springs Area	PPACG externals
FOCUS externals	0.04%	3.35%	0.05%	0.00%	0.01%	0.02%	0.01%
Denver Metro		77.88%	4.34%	0.06%	0.14%	0.27%	0.05%
Castle Rock Area			2.85%	0.09%	0.05%	0.08%	0.02%
Larkspur Area				0.04%	0.04%	0.04%	0.00%
Monument / Briargate Area					0.89%	1.32%	0.04%
Colorado Springs Area						7.89%	0.44%
PPACG externals							0.01%

Source: StreetLight Data

Table 4: Weekday Commercial Trip Distribution

	FOCUS externals	Denver Metro	Castle Rock Area	Larkspur Area	Monument / Briargate Area	Colorado Springs Area	PPACG externals
FOCUS externals	3.74%	18.24%	0.15%	0.04%	0.13%	0.38%	0.46%
Denver Metro		60.49%	1.95%	0.16%	0.40%	1.13%	1.48%
Castle Rock Area			1.14%	0.09%	0.08%	0.20%	0.13%
Larkspur Area				0.04%	0.06%	0.06%	0.07%
Monument / Briargate Area					0.54%	0.98%	0.30%
Colorado Springs Area						7.62%	1.68%
PPACG externals							0.27%

Source: StreetLight Data

For growing the inter-MPO portions of the trip matrices derived from StreetLight data to 2040, we used the 2017 calibrated trip matrix as a starting point for a fratar process, so the trip patterns derived from StreetLight were preserved, but also adjusted to account for projected growth at the zone level.

Transaction Data

For any study with planned tolled lanes, where modeling travelers’ choices between free and tolled routes is part of traffic forecasting, it is necessary to have an assumption of travelers’ values of time to model the tradeoff between time savings and increased trip cost. Stated preference (SP) surveys are sometimes conducted for this purpose, especially when there are no similar toll facilities to observe actual travel behavior. Given the planning-level of our study and the presence of similar toll facilities in the region, we relied on revealed behavior of travelers on a local facility with existing managed lanes to estimate values of time.³ The express lanes on US-36 were the ideal candidate for this purpose, due to the presence of permanent count stations on the express lane corridor and the availability of transaction data from CDOT.

To estimate revealed values of time, one must determine (a) the time savings on the tolled facility and (b) what percentage of travelers are willing to pay to save this amount of time, and how much they are paying.

CDOT provided transaction data to SDG, including number of transactions and toll collected, for several

³ An ongoing Traffic and Revenue study of the corridor by C&M Associates will include a stated preference survey on tolls specifically for this section of I-25.

months of 2016. We downloaded traffic count data by time of day from CDOT count stations #00004 and #00504 on US-36 from the CDOT website. We computed average tolls by taking the average toll paid in each transaction across the entire route for each hour.

We determined the share of traffic using managed lanes by dividing the CDOT transaction counts by the total traffic count. Because there were only two stations where transaction counts were recorded, only two segments had definite counts. Between the two count stations, we interpolated the value for managed lane share. For segments outside of the two stations, the share took the value of the closest segment with a count station on it.

The INRIX travel times for the section of US-36 with managed lanes include travel times for all vehicles on the corridor, and are unable to distinguish between vehicles using the managed lanes and the general-purpose lanes. Assuming that the INRIX data sampled vehicles randomly, we calculated the travel time and speed on the general-purpose lanes by using the average travel time from the INRIX data and the share of traffic using the managed lanes as calculated above. We assumed managed lane speeds of 70 mph for this purpose. In other words, we solved for GP travel time, using the equation

$$\text{GPtt} * (1 - \text{MLShare}) + \text{MLtt} * \text{MLShare} = \text{INRIXtt}$$

where:

- GPtt = general purpose lane travel time
- MLtt = managed lane travel time (assuming 70 mph speed)
- MLShare = managed lane share
- INRIXtt = INRIX travel time

All items in the equation above other than GPtt are known or easily calculated, as discussed above.

Finally, we calculated time savings by taking the general-purpose lane times and subtracting the managed lane travel times based on the 70mph speed assumption. Given the time savings, toll paid, and managed lane share, we estimated values of time using the procedure described in the values of time analysis section of this memo.

Model Development

Combining MPO Travel Demand Models

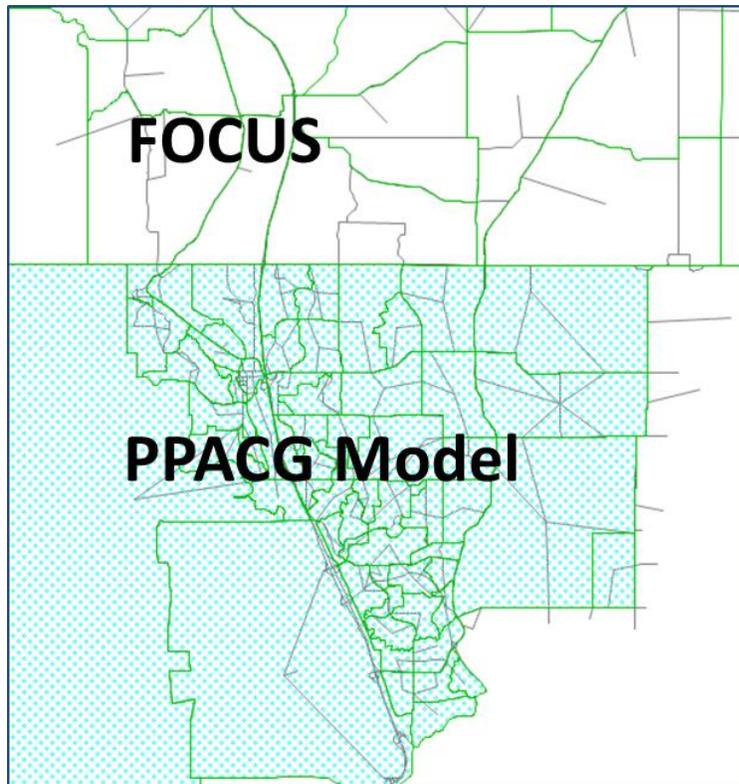
As discussed in the Data Collection section of this memo, SDG created the I-25 travel demand model by extending FOCUS into northern El Paso County. This involved combining both the networks and the trip matrices of the models from the two MPO regions, as discussed below.

Networks

Geographic Extension

Since the project study corridor extends beyond the FOCUS model project limits, SDG extended the highway network into the PPACG model region. Specifically, the FOCUS model network extends as far south as Palmer Divide Road (a.k.a. County Line Road) at the northern edge of Monument, whereas the study corridor extends a mile further south. Figure 2 shows the border between the two original MPO networks. We joined the two networks and zone systems at this border to provide a continuous network.

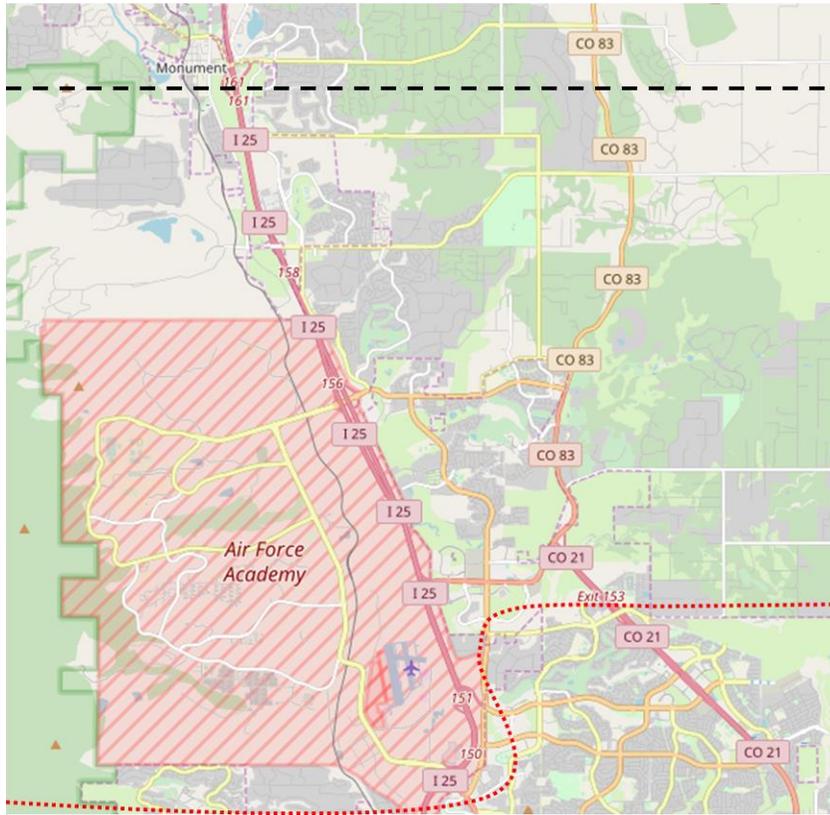
Figure 2: FOCUS/PPACG Model Network Border



Source: Steer Davies Gleave

To provide a minimal number of likely entrances and exits to the model network for ease of traffic count data collection, we picked a new southern edge of the model containing as few major roads as possible. Figure 3 depicts this new southern edge, as compared to the current FOCUS II model edge.

Figure 3: Comparison of Southern Edges of FOCUS Model to Extended Model



Approximate
Edge of Current
FOCUS Model



Proposed Edge of
I-25 Model

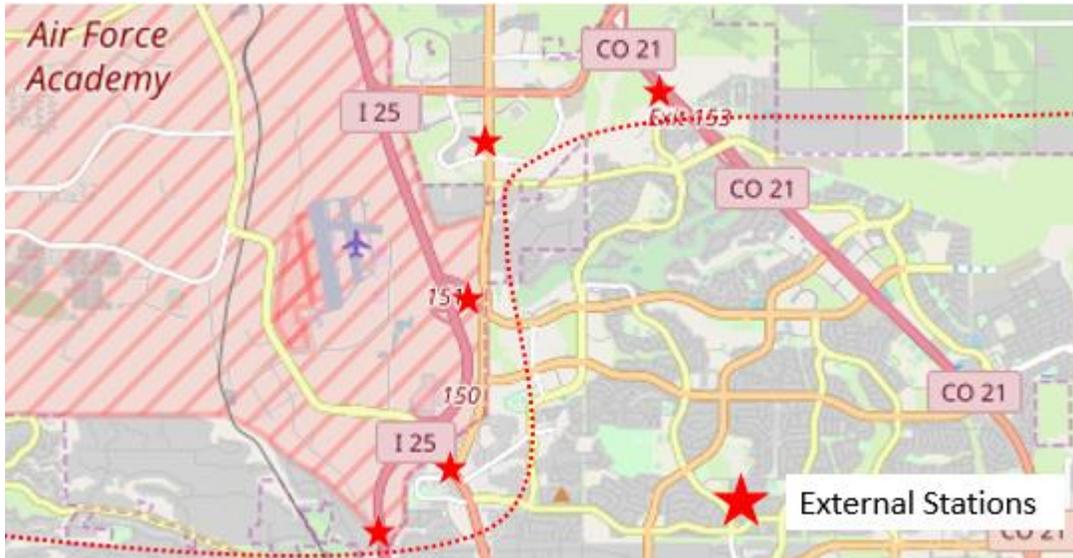


Source: OpenStreetMap

Figure 4 zooms in on the extension above and shows the locations of the new external stations; the I-25 Model includes external stations at:

- I-25;
- Academy Boulevard;
- Briargate Parkway;
- Voyager Parkway; and
- North Powers Blvd (SH-21).

Figure 4: External Stations for I-25 Model Network



Source: OpenStreetMap

Attribute Translation

In addition to joining the networks geographically, we translated several network attributes from the PPACG model links into the FOCUS network attributes, as follows:

- In many cases for the attributes that were needed (e.g. link length, number of lanes), it was possible to fill in the values directly from a PPACG attribute with a different name but an identical definition.
- Free flow speed and per-lane capacity retained the lookup tables from FOCUS which depend on link area type and facility type.
- Due to the lack of an analogous attribute in the PPACG model, SDG used best judgment for the 'AREA TYPE' attribute for the new links in El Paso County. The FOCUS model assigns all zones an area type on a scale from 1 (Downtown Denver CBD) to 5 (rural), and all links get assigned a value equal to that of the zone in which they reside. Most zones in northern El Paso County were assigned a value of 4. Zones in the town of Monument were assigned a 3, and some rural zones near SH-83 were assigned a 5.

The only attribute that required a more complex translation from one set of values to another was the "FACILITY TYPE" attribute. We determined the values for this attribute from the value of the TYPENO attribute from the PPACG model. Table 5 below documents how this was done.

Table 5: Translation of PPACG TYPENO to FOCUS FACILITY TYPE Attribute

PPACG TYPENO Value	FOCUS FACILITY TYPE	Roadway Type
10-19	1	Freeway
20-29	2	Major Regional Arterial
30-39	3	Principal Arterial
40-49	4	Minor Arterial
50-59	5	Collector
75	6	Ramp
Links in connector layer	8	Centroid Connector

Source: PPACG model and FOCUS

Trip Matrices

The FOCUS II and PPACG travel demand models provided the initial trip matrices that we combined into a single matrix.

Zone / External Station Renumbering

The first step was to create a zone system for the extended network of the I-25 Model. Table 6 summarizes how this was done.

Table 6: Combining FOCUS and PPACG Zones into the I-25 Model Zone System

	I-25 Model Zone Numbers	Source	Source Model Zone Numbers
Zonal Areas	1-2804	FOCUS	1-2804 (same as in FOCUS)
	2805-2869	PPACG	Various (shaded region in Figure 1 above)
	2870-2887	FOCUS	2805-2822
External Stations	2888-2893	FOCUS	2827-2832
	2894-2898	New External Stations in Figure 3 (above)	n/a

Source: Steer Davies Gleave

The I-25 Model (prior to subarea extraction) contained 2869 zones and 29 external stations, numbered from 2870-2898. The first 24 of these external stations (numbered 2870-2893) corresponded to 24 of the 28 external stations in FOCUS, numbered 2805-2822 and 2827-2832 in the FOCUS model. Note that four of the original FOCUS external stations, 2823-2826, were located at the southern edge of Douglas County, which we joined to the PPACG model. Thus, these were not external stations in the I-25 Model. Similarly, the four PPACG model external stations with which these line up also were not external stations in the I-25 Model.

Finally, the five external stations depicted above in Figure 4 above were numbered 2894-2898.

Development of Trip Matrix

Table 7 illustrates the steps taken to create the full 2898 x 2898 zone weekday trip matrices for the I-25 Model, consistent with the zone numbers in Table 6 and with the time periods in FOCUS. To assist in the creation of the trip matrices for the I-25 Model, we collected origin-destination from StreetLight Data, following a zone system that covered most of the two MPOs. The zone system was coarser than in the MPO models, due both to StreetLight Data’s pricing system (based on number of zones) and the greater level of confidence in the data with larger zones.

Table 7: Methodology for Extended Model Trip Matrix Development

Between TAZs...	And TAZs...	Description of Trips	Methodology
1-2804 or 2870-2893	1-2804 or 2870-2893	Within areas in the FOCUS model	Used trips from FOCUS model as is
	2805-2869	Between FOCUS and northern El Paso County	<ol style="list-style-type: none"> (1) Disaggregated trips to/from original FOCUS external stations at the MPO border into zones 2805-69, based on travel patterns in StreetLight Data (2) Disaggregated from StreetLight Data zones to MPO model zones
	2894-2898	Between FOCUS and new external stations in Briargate area	<ol style="list-style-type: none"> (1) Computed percent of the trips to/from original FOCUS external stations at the MPO border <i>not to/from</i> for zones 2805-2869, based on travel patterns in StreetLight Data (2) Disaggregated based on relative trips passing through stations 2894-2898, also based on StreetLight Data travel patterns (3) Scaled entire rows/columns of matrix to match traffic counts in the appropriate direction
2805-2869	2805-2869	Within areas in the portion of PPACG model used in project extended model	Used trips from PPACG model as is
	2894-2898	Between the portion of PPACG model used in project extended model and elsewhere in the PPACG model	<ol style="list-style-type: none"> (1) Computed the number trips to/from all other PPACG zones not used in the I-25 Model (2) Disaggregated based on relative trips passing through stations 2894-2898, also based on StreetLight Data travel patterns (3) Scaled entire rows/columns of matrix to match traffic counts in the appropriate direction
2894-2898	2894-2898	Between new external stations in Briargate area	Trips between I-25 and Academy Blvd or Voyager Pkwy used applicable ramp counts. Other O-D pairs assumed to be zero.

Source: Steer Davies Gleave

The general methodology followed these guidelines:

- As a preliminary step, we converted the PPACG matrices to the FOCUS model time periods, using

multiplicative factors that reflected both the comparative lengths of the time periods and traffic count patterns within each period.

- Trips that did not cross the border between the two MPOs were not changed.
- To calculate trips between the two MPOs, we started with the trips using the former FOCUS external stations at the MPO border (2823-2826) and distributed them among the PPACG zones, using travel patterns implied by the StreetLight Data OD data.
- Due to the more aggregate geography of the zones underlying the StreetLight Data, we disaggregated trips from StreetLight Data zones into MPO model zones based on the MPO model zones' relative trip data within their respective StreetLight Data zones.
- We estimated trips going to the five new external stations (2894-2898) in three steps:
 - We computed trips going to any PPACG zone *not* included in the I-25 Model (because any trip going to one of those zones would need to cross one of the new external stations).
 - We disaggregated into the individual external stations based on the relative numbers of trips crossing through each from the StreetLight Data.
 - We scaled each row and column to match the traffic counts collected in 2017 for each of the five stations in each direction and time period.

It should be noted that the first two columns of Table 7 are reversible, i.e. each methodology listed applies to trips either from TAZs in column 1 to TAZs in column 2, or from TAZs in column 2 to TAZs in column 1.

Weekend Trip Matrices

The MPO models are both designed for average weekday conditions. Thus they do not contain the necessary trip-making levels and patterns to model the increased delay often observed on I-25 on Friday afternoons and weekends. Thus, it was necessary to adjust these matrices in such a way to be able to model these conditions. Note that throughout the memo, the term “weekend” is used to describe both the Friday afternoon and Sunday model periods.

As discussed in the travel times section above, the desired modeling periods were Friday late afternoon / evening and Sunday early afternoon. To facilitate compatibility with FOCUS, we chose the following time periods, which aligned closely with the actual time periods of greatest observed delay on I-25:

- Friday 3-5 PM (PM1)
- Friday 5-6 PM (PM2)
- Friday 6-7 PM (PM3)
- Sunday 11:30 AM – 3:00 PM (OP3)

Technical details of the weekend trip matrix adjustments are discussed in the model calibration and validation section of this memo.

Model Calibration and Validation

List of Calibration Modifications

Network

Initially the model’s screenline traffic forecasts at the El Paso County Line lower than observed while screenline traffic forecasts very closely matched observed counts at Castle Rock and in the middle of the gap. Table 8 presents average weekday traffic volumes from the 2015 version of FOCUS and from the 2017 traffic counts collected on two weekdays in May of 2017 at three screenlines, corresponding roughly to the northern, middle and southern portions of the corridor being studied. The table shows the low model forecasts at the El Paso County Line, highlighting the challenge to increase this screenline traffic without disrupting the forecasted traffic levels at the other screenlines.

Table 8: Comparison of 2015 FOCUS volumes to 2017 Traffic Counts, Average Weekday

Location	2015 FOCUS	2017 Traffic Count
I-25, between exits 184 and 185	130,300	132,800
SH-105 (Perry Park Rd), south of US-85	10,100	3,100
SH-83, south of SH-86	11,300	9,100
Total Screenline – Castle Rock	151,700	145,000
I-25, between exits 174 and 181	78,000	79,000
SH-105 (Perry Park Rd), south of Tomah Rd	7,800	3,100
SH-83, south of Gillian Ave	4,400	5,700
Total Screenline – Middle of Gap	90,200	87,800
I-25, between exits 163 and 167	63,500	77,800
SH-83, north of SH-105	2,800	8,500
Total Screenline – El Paso County Line	66,300	86,300

Sources: DRCOG FOCUS model, All Traffic Data

As described previously, we collected more extensive traffic data in the study corridor, including on I-25 between every interchange, as well as all on-ramps, off-ramps and parallel routes, and used that data to establish a higher level of model accuracy within the corridor. The calibration includes four weekday time periods (AM peak, PM peak, mid-day and overnight) plus two weekend time periods (Friday PM peak and Sunday Mid-Day) which represent the periods of greatest delay observed on the weekends traffic counts were collected.

In addition to calibrating to traffic counts, we made efforts to ensure consistency with observed travel times and travel patterns, to the extent possible. It was sometimes necessary to make trade-offs to balance consistency with the various observed conditions, and to ensure that network and matrix assumptions didn’t deviate too far from reasonability.

Network and matrix calibration modifications are discussed below.

Network Modifications

Network free flow speeds were set to approximate weekday overnight travel time observations on I-25 (70 mph), eastern alternative SH-83 (between 45 and 60 mph), and western alternative CO-105 (between 37 and 52 mph). This practice at SDG ensures that the model reflects observed speed conditions of major roadways and routes in the study.

A persistent challenge in calibration was the model’s slow peak travel times between C-470 and Castle Rock, particularly on parallel routes. The model was sending traffic to the Eastern and Western alternative routes instead of I-25, resulting in mainline travel volumes consistently lower than observed, and certain portions of parallel routes being slower and more congested than observed. We calibrated the links to reflect observed volumes and travel times by making the following adjustments:

- Increasing capacity to 2200 vehicles per hour per lane on I-25 between Castle Pines Parkway and C-470
- Reducing the alpha volume-delay parameter on this same segment of I-25 (this increases the volume / capacity ratio threshold for delays to occur)
- Increasing capacity on C-470 and E-470 between I-25 and the alternate routes
- Incorporating a motorway bonus by adding a 20% time penalty to links not on highways or ramps to move volume from the alternate routes
- Changing the facility type from 3 (principal arterial) to 4 (minor arterial) on rural roads connecting I-25 to these parallel routes to decrease free flow speed from 55 mph to 47 mph and thus decrease trips switching between I-25 and the parallel routes.
- Significantly reducing capacity and free flow speed on the rural (mostly dirt) roads near the gap, also to decrease the number of trips switching between I-25 and parallel routes

Table 9 shows an example of the travel times in this section of the study corridor, and the improvements achieved by means of the calibration steps above.

Table 9: Average Weekday Southbound Travel Times (in minutes), 3:00 – 7:00 PM

Segment	Observed	Modeled – Pre-Calibration	Modeled – Post-Calibration
I-25 from Plum Creek to C-470	12.9	16.7	14.4
US-85 from SH-105 to C-470	17.1	23.2	19.8
SH-83 from SH-60 to E-470	12.7	18.3	14.7

Sources: INRIX, Steer Davies Gleave

We discovered some strange movements using ramps, with travelers avoiding longer loop ramps and instead turning around and taking shorter ramps in the wrong direction. In response, we updated the turn penalty databases to prohibit U-turns which helped ramp volumes become more consistent with the traffic counts.

Due to this study’s focus on the gap, we paid careful attention to match traffic volumes and travel times between Plum Creek Parkway and Palmer Divide Road. To simulate the hilliness and curvature and increased potential for congestion, we reduced capacity in the gap to 1700 vehicles per hour per lane, which was also produced slower travel times that were more consistent with the travel times observed in the INRIX data. We increased frontage road free flow speed to 50 mph to help match counts. We also increased free flow speed on Palmer Divide Road to 50 mph and hourly per lane capacity to 800, because modelled counts were significantly lower than observed.

As discussed in the section below, we took additional post-processing steps to distinguish between mainline

and frontage routes and to simulate the backups caused by vehicles re-entering I-25 from the exit 174 (Tomah Road interchange) on ramp in the southbound direction.

Trip Matrix

Weekday Trip Matrices

At a certain point in model calibration, if network modifications are not completely resolving the differences between modelled and observed traffic counts and travel times, it becomes necessary to modify trip matrices. The following trip matrix adjustments enabled us to bring model output more in line with observed data:

- Adjustment of origin-destination (OD) pairs using the gap section by time period and direction to match counts as precisely as possible (mostly increasing by 15% to 35%)
- Adjustment of OD pairs using SH-83 and not I-25, mostly downward
- Adjustment of OD pairs using US-85 and not I-25, mostly upward
- Reduction of trips to / from Larkspur TAZs, due to high volumes on the Spruce Mountain Rd ramps

Weekend Trip Matrices

For each of the weekend time periods we studied (Friday 3-7 PM and Sunday 11:30 AM – 3:00 PM), the differences between the weekend and weekday traffic counts were primarily along I-25, and fairly level (mostly between 7,000 and 9,000 vehicles per day on Friday and between 1,000 and 2,000 vehicles per day on Sunday in each direction) from the Baptist Rd interchange to Downtown Castle Rock. Furthermore, ramp counts did not differ significantly between weekday and weekend periods. Therefore, we modified the weekday trip matrices in the following manner:

- We created an indicator matrix of all origin-destination pairs in the model which would involve travel on I-25 between Castle Rock (or farther north) and Monument (or farther south). We called these “end to end” O-D pairs.
- For both the PM peak and early afternoon model time periods, we computed
 - The number of trips from the weekday calibrated matrices using end to end O-D pairs in each direction
 - The approximate number of additional vehicles counted on I-25 in each direction on Friday (in the case of the PM peak) and Sunday (in the case of early afternoon)
- Finally, we computed the ratio of the second item above (additional vehicles on I-25 on Friday or Sunday) to the first (end to end trips on the weekday), and applied that ratio as a multiplicative factor to all end to end O-D pairs in the appropriate weekday time period matrix.

Table 10 shows the ‘weekend adjustment factors’ to the end to end O-D pairs to produce Friday and Sunday matrices. Note that the same factors were used on Friday in each of the three PM subperiods.

Table 10: Weekday-to-Weekend Trip Matrix Adjustment Factors to “End to End” Origin-Destination Pairs

Weekend Period	Direction	Factor
Friday 3-7 PM	Northbound	1.43
	Southbound	1.32
Sunday 11:30 AM – 3:00 PM	Northbound	1.64
	Southbound	1.50

Source: Steer Davies Gleave

Note that this does not mean, for example, that Friday southbound traffic volumes are 32% higher than weekday, but that Friday *end to end* trips are estimated to be 32% higher as evidenced by fairly consistent higher traffic differences (by 7,000-9,000 vehicles per day) all through the gap.

We made a few minor modifications to the above process to assist in calibration / validation:

- In the case of Friday southbound, we used the factor derived from end to end trips and traffic counts from 1-7 PM, to coincide with the entire time period over which significant delay was observed. This enabled us to avoid a situation where the model was being calibrated to throughput volumes rather than demand volumes.
- The differences between weekday and weekend traffic counts were smaller at the southern end of the corridor, between Baptist Rd and Academy Blvd (exit 149). Thus, we created factors similar to those in Table 10 to apply to trips between the southern I-25 external station and zones immediately to the north. These factors ranged from 0.8 to 0.95 and improved the validation of traffic counts in the southern end of the corridor.
- We applied additional factors of 2/3 and 1/3 to truck trips on Friday and Sunday, respectively. These factors were based on count data as well.⁴

Post-Processing

The adjustments outlined above enabled the model to validate observed conditions in the corridor within acceptable levels in most cases, but the 3-7 PM conditions on I-25 between Exits 181 (Plum Creek Parkway) and 174 (Sky View Lane / Tomah Rd) in the southbound direction were a challenge for the model to reproduce and required some post-processing, as discussed below.

The 7-mile section of I-25 between exits 181 and 174 often experiences large southbound afternoon delays, especially on Friday; on the Friday of traffic count data collection (August 4th), travel times on this 7-mile section were as high as 25 minutes, corresponding to an average speed of 18 MPH. The travel demand model produced speeds only as low as 39 MPH. Conversely, the model was producing slightly slower speeds on this section on weekday afternoons than observed (49 MPH vs 60 MPH).

⁴ Although RV traffic is likely higher on Fridays and Sundays, RVs are counted and modeled with trucks, and the decrease in actual trucks more than offsets the increase in RVs.

Table 11: 3-7 PM Southbound Observed and Modeled Traffic Volumes and Speeds, Prior to Post-Processing

	Weekday		Friday		
	Observed	Modeled	Observed	Modeled	
Traffic Counts	I-25 North of Exit 181 (Plum Creek Pkwy)	15,700	15,800	15,800	16,600
	Exit 181 Off Ramp	5,000	3,700	6,800	3,500
	I-25 South of Exit 181 (at Lane Drop)	11,100	12,600	9,300	13,700
	Exit 174 On Ramp	1,100	400	3,900	300
	I-25 South of Exit 174 (Sky View Ln / Tomah Rd)	12,200	12,800	12,900	13,800
	Slowest Southbound PM Peak Travel Speed, Exits 181 to 174 (MPH)	60	49	18	39

Source: Steer Davies Gleave

As shown in Table 11, the model was producing volumes on I-25 immediately to the north of this section that very closely replicated observed traffic counts, both on weekdays and Fridays. However, it was not fully capturing the level of diversion to frontage roads, overestimating traffic remaining on I-25 and underestimating traffic on both the Exit 181 off ramp and Exit 174 on ramp by the same magnitude. The traffic split between two parallel routes such as these is often difficult for a link-based travel demand model to replicate, because it can't directly take micro-level traffic operations considerations, such as signals, queuing or reliability, into account.

The deviance between weekday modeled and observed traffic counts and travel speeds was consistent – modeled volumes on I-25 were high and travel speeds were slower than observed, whereas the Friday ones did not – volumes on I-25 were also high, yet travel speeds were faster than observed, and both differed from observations to a greater extent than on the weekday.

However, the comparison between modeled I-25 traffic volumes on Friday vs the weekday (13,700 vs 12,600) was consistent with the delays on Friday (delays on the weekdays are minimal). The reason the difference between modeled and observed volumes were so much higher on Friday than weekdays was that traffic counts were lower on Friday (9,300 vs 11,100).

At first, this would appear counterintuitive; how could Friday traffic counts be lower than on the weekday, yet delays be higher? Elsewhere along I-25, as discussed earlier in this memo, traffic counts were largely higher on Friday than on the weekdays⁵. Friday also had much higher counts on the Exit 181 off ramp, frontage roads, and Exit 174 on ramp. Thus, we concluded that the lower observed Friday traffic count on I-25 between exits 181 and 174 compared to the weekdays was not a case of reduced demand, but of reduced throughput due to queuing and turbulence.

⁵ In some cases near this critical section, Friday traffic counts from 3-7 PM were approximately equal to weekday traffic counts; we believe this is also due to the demand vs throughput effect, most easily observed just south of exit 181, but still present elsewhere. Friday delays began between 1 and 2 PM and end around 7 PM; traffic counts over the six hour 1-7 PM period, during which demand and throughput equalize, were uniformly higher on Friday than on the weekdays

The other reason that volumes could be too high on Friday and yet speeds could still be too fast is that the delays are caused not just by increased demand, but by the diversion to frontage roads itself. Travel speeds, obtained from INRIX data, on Friday August 4th, 2017, began to slow at the Exit 174 on ramp, i.e. the point where vehicles which diverted to the frontage roads merged back onto I-25, between 1:00 and 2:00 PM. These slower speeds gradually moved upstream, as far north as Exit 182, reaching their peak delay at about 5:00 PM, before gradually improving. Appendix B shows the INRIX data that illustrate this trend, and also shows that the level of delay moved closely in tandem with the observed traffic volumes on the Exit 174 on ramp.

Thus, we concluded that the delays and queuing in this direction were also a consequence of the high volumes and turbulence inducement of merging traffic from the frontage roads at the exit 174 on ramp. These delays can't easily be captured by a link-based travel demand model where individual links' delays are independent from each other, and queue spillback can't be modeled explicitly.

We determined that best way to enable the model to reproduce these conditions would be with two post-processing steps:

1. Shifting some traffic away from I-25 onto the Exit 181 off ramp, the frontage roads, and the Exit 174 on ramp, consistent with observed conditions
2. Reducing capacity on I-25, by an amount which depends on the amount of traffic on the Exit 174 on ramp to reflect the added friction caused by high levels of merging traffic.

Table 12 shows the capacity modifications that were necessary to validate the observed travel times and speeds on this 7-mile section of I-25, both on the weekdays and Friday.

Table 12: Calibration Post-Processing of Capacities on I-25, Southbound 3-7 PM

	Weekday			Friday		
	3-5 PM	5-6 PM	6-7 PM	3-5 PM	5-6 PM	6-7 PM
Exit 174 On Ramp PCE Per Hour	250	500	150	975	1050	875
I-25 Hourly Vehicle Capacity						
3-lane section s/o Exit 181	5,250	5,100	6,000	2,450	2,450	2,100
2-lane section s/o Exit 181 after lane drop	3,500	3,300	4,000	2,450	2,450	2,100
Immediate downstream of Exit 174 on ramp	4,000	4,000	4,000	3,400	3,400	3,400

Note: All capacities shown are total directional I-25 capacity per hour

Source: Steer Davies Gleave

This post-processing along with improved free-flow speeds enabled us to use a consistent methodology to both decrease the modeled speed on this portion of I-25 significantly on Friday, but also increase the modeled speed slightly on weekdays, to bring the modeled speed in line with observed speed in both cases. With the post-processing steps outlined above, shifting a large amount of traffic off I-25 on Friday increased delay, because that traffic was not shifted off the corridor completely, but instead shifted to a diversion route that involved merging back onto I-25 at Exit 174. Due to the capacity reduction for the mainline, this

caused more delay than if the traffic had stayed on I-25. Conversely, on the weekday, only a small amount of traffic was shifted in this way, so the Exit 174 merge had little effect, and travel speeds on I-25 improved slightly.

Making the changes to capacities shown in Table 12, along with shifting volumes from I-25 to the appropriate ramps and frontage roads (about 750 vehicles on the weekday and 3,500 on Friday) enabled us to validate both traffic counts and travel times. On the weekday, shifting a small number of vehicles to the frontage road increased speeds moderately, and not enough new vehicles were re-entering on the Sky View on ramp to warrant capacity reductions on I-25. On Friday, however, we shifted a much larger number of vehicles to the frontage road and subsequent Sky View re-entry, enough to warrant a significant capacity reduction on I-25. This more than offset the volume reduction from the shift, and decreased speeds significantly.

The post-processing was discussed in detail with a group that included travel demand modelers from both the DRCOG (Scott Ramming) and PPACG (Ken Prather) MPOs as well as CDOT (Erik Sabina) in a phone call on October 23rd, 2017. The group agreed that this approach was reasonable to deal with this unique set of conditions that apply to the southbound direction of I-25 in the afternoon and evening.

Model Validation Results

Traffic Counts

Table 13 and Table 14 present the traffic counts on the mainline for weekday peak periods and weekend (Friday and Sunday) periods, respectively. As a general rule, our target is for most modeled volumes on key highway facilities to fall within 10% of the observed volumes, with an emphasis in the areas of interest.

Validation between Palmer Divide Road and Plum Creek Parkway is particularly refined, reflecting how we prioritized calibrating the gap for this study. Appendix A contains complete volume calibration results for all segments of I-25 collected, as well as several critical frontage roads and parallel route segments. The modeled volumes on the frontage roads includes gap diversion adjustments.

Table 13: Weekday Peak Period Mainline Calibration Volumes

I-25 Location		Count		Model Volume		% Difference	
		6:30 – 9 AM	3-7 PM	6:30 – 9 AM	3-7 PM	6:30 – 9 AM	3-7 PM
N/O Baptist Rd	NB	7,700	12,900	8,400	14,000	9%	9%
	SB	7,200	13,800	7,400	14,800	3%	7%
N/O Palmer Divide Rd	NB	6,800	9,700	6,600	9,800	-2%	1%
	SB	4,700	11,600	4,800	11,600	2%	0%
S/O Plum Creek Pkwy	NB	7,200	9,900	7,500	10,600	4%	7%
	SB	4,800	11,100	5,200	11,900	8%	7%
N/O Founders Pkwy	NB	14,500	15,700	15,300	17,400	5%	11%
	SB	7,700	21,600	8,700	22,900	13%	6%

Source: Steer Davies Gleave

Table 14: Weekend Mainline Calibration Volumes

I-25 Location		Count		Model Volume		% Difference	
		Friday 3-7 PM	Sunday 11:30 AM – 3 PM	Friday 3-7 PM	Sunday 11:30 AM – 3 PM	Friday 3-7 PM	Sunday 11:30 AM – 3 PM
N/O Baptist Rd	NB	14,700	12,500	15,700	13,500	7%	8%
	SB	14,700	11,800	16,100	12,500	9%	6%
N/O Palmer Divide Rd	NB	11,800	11,100	11,400	10,900	-3%	-2%
	SB	12,100	10,200	12,700	10,000	5%	-2%
S/O Plum Creek Pkwy	NB	11,800	10,800	12,200	11,400	3%	6%
	SB	9,300	10,000	10,200	10,600	10%	6%
N/O Founders Pkwy	NB	17,100	16,200	18,900	16,100	10%	-1%
	SB	20,900	16,100	24,100	15,400	15%	-4%

Source: Steer Davies Gleave

Travel Times

We calibrated travel times to match INRIX travel time data on I-25 from Academy Blvd. to C-470, shown for the AM and PM periods northbound in Table 15 and southbound in **Error! Reference source not found.**

Table 15: Weekday Travel Time Calibration- Northbound

I-25 Location	Observed Travel Time (min)		Modeled Travel Time (min)	
	6:30 –9 AM	3-7 PM	6:30 –9 AM	3-7 PM
Academy to Palmer Divide	12.9	11.7	12.2	12.2
Palmer Divide to Upper Lake Gulch	8.3	7.2	8.9	8.5
Upper Lake Gulch to Plum Creek	8.3	7.8	9.9	9.1
Plum Creek to Castle Pines	8.4	6.6	7.9	6.9
Castle Pines to CO-470	8.2	5.0	6.6	5.2
CUMULATIVE	46.1	38.2	45.4	41.8

Source: Steer Davies Gleave

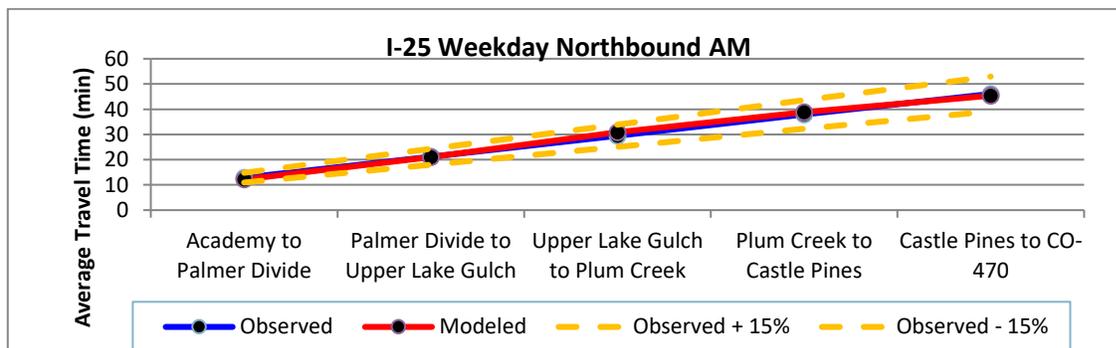
Table 16: Weekday Travel Time Calibration- Southbound

I-25 Location	Observed Travel Time (min)		Modeled Travel Time (min)	
	6:30 –9 AM	3-7 PM	6:30 –9 AM	3-7 PM
CO-470 to Castle Pines	5.7	5.6	5.3	6.3
Castle Pine to Plum Creek	6.8	6.6	6.7	8.2
Plum Creek to Upper Lake Gulch	8.4	9.4	8.4	9.5
Upper Lake Gulch to Palmer Divide	8.3	9.3	7.6	9.4
Palmer Divide to Academy	12.5	10.7	11.6	12.3
CUMULATIVE	41.7	41.5	39.5	45.7

Source: Steer Davies Gleave

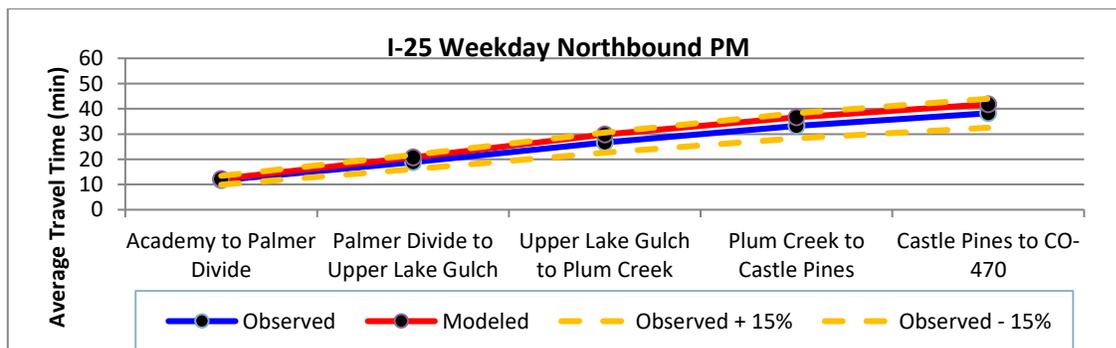
The next four figures show that the model falls within the target range of 15% for observed travel times in both directions during weekday peak periods.

Figure 5: I-25 Mainline Travel Time Calibration – Weekday Northbound AM



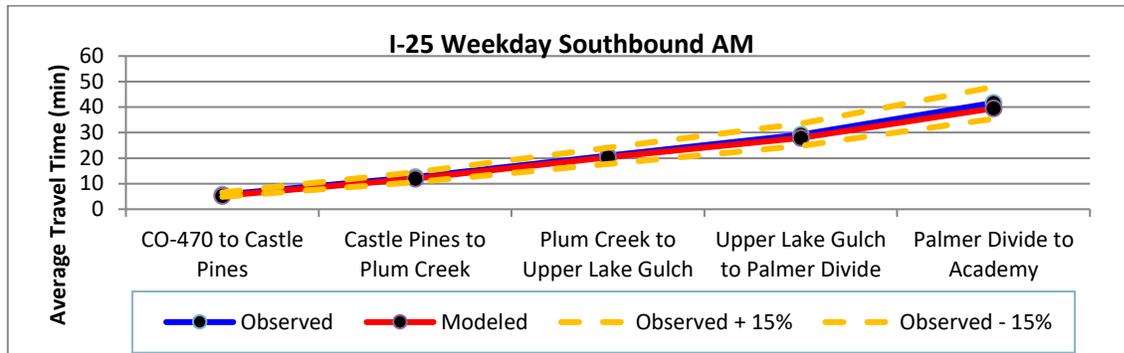
Source: Steer Davies Gleave

Figure 6: I-25 Mainline Travel Time Calibration – Weekday Northbound PM



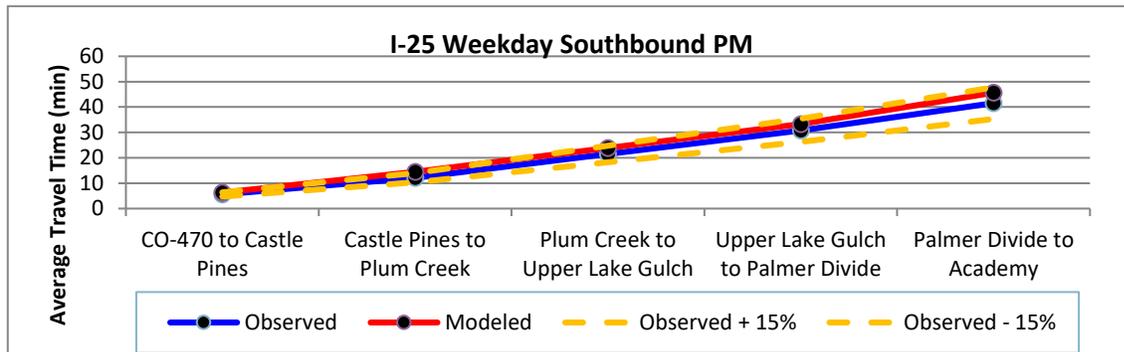
Source: Steer Davies Gleave

Figure 7: I-25 Mainline Travel Time Calibration – Weekday Southbound AM



Source: Steer Davies Gleave

Figure 8: I-25 Mainline Travel Time Calibration – Weekday Southbound PM



Source: Steer Davies Gleave

Weekend travel time calibration is presented in Table 17 and Table 18. We gave special attention to calibrating the PM peak southbound on Friday afternoons, given the high congestion in the gap during that period.

Table 17: Weekend Travel Time Calibration - Northbound

I-25 Location	Observed Travel Time (min)		Modeled Travel Time (min)	
	Fri 3-7 PM	Sun 11:30 AM – 3 PM	Fri 3-7 PM	Sun 11:30 AM – 3 PM
Academy to Palmer Divide	12.6	12.4	12.5	12.2
Palmer Divide to Upper Lake Gulch	7.8	8.6	9.5	9.9
Upper Lake Gulch to Plum Creek	8.9	11.4	10.1	10.4
Plum Creek to Castle Pines	6.5	6.5	7.0	6.8
Castle Pine to CO-470	5.1	4.8	5.2	5.1
CUMULATIVE	40.9	43.7	44.3	44.5

Source: Steer Davies Gleave

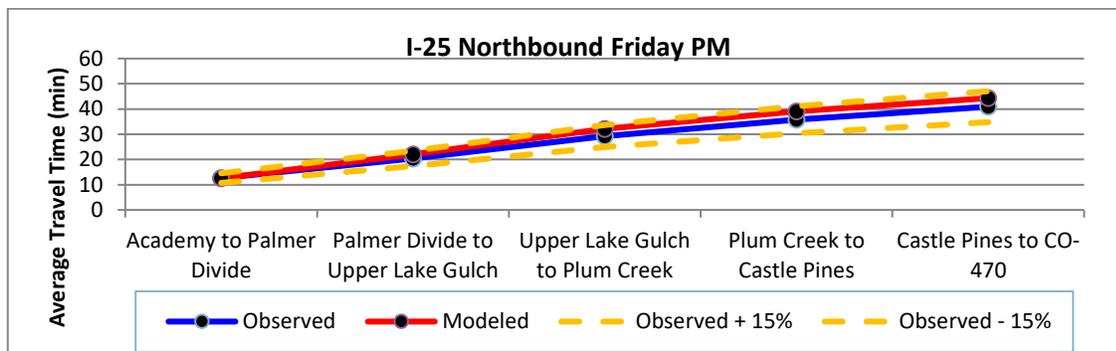
Table 18: Weekend Travel Time Calibration- Southbound

I-25 Location	Observed Travel Time (min)		Modeled Travel Time (min)	
	Fri 3-7 PM	Sun 11:30 AM – 3 PM	Fri 3-7 PM	Sun 11:30 AM – 3 PM
CO-470 to Castle Pines	6.4	5.1	6.4	5.4
Castle Pine to Plum Creek	7.2	6.4	8.3	6.9
Plum Creek to Upper Lake Gulch	25.9	8.6	20.6	9.8
Upper Lake Gulch to Palmer Divide	8.7	8.3	10.4	8.9
Palmer Divide to Academy	10.8	10.6	12.7	11.9
CUMULATIVE	59.0	39.0	58.3	42.8

Source: Steer Davies Gleave

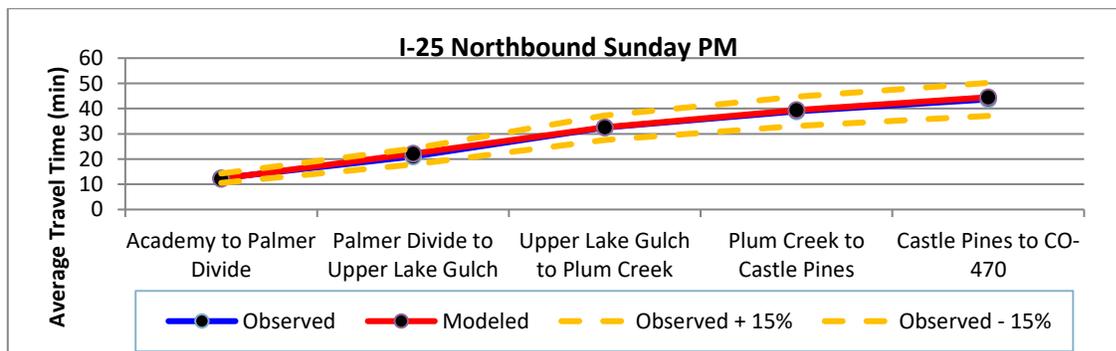
The next four figures show that the model falls within the 15% range of observed travel times in both directions during weekend peak periods.

Figure 9: I-25 Mainline Travel Time Calibration- Northbound Friday PM



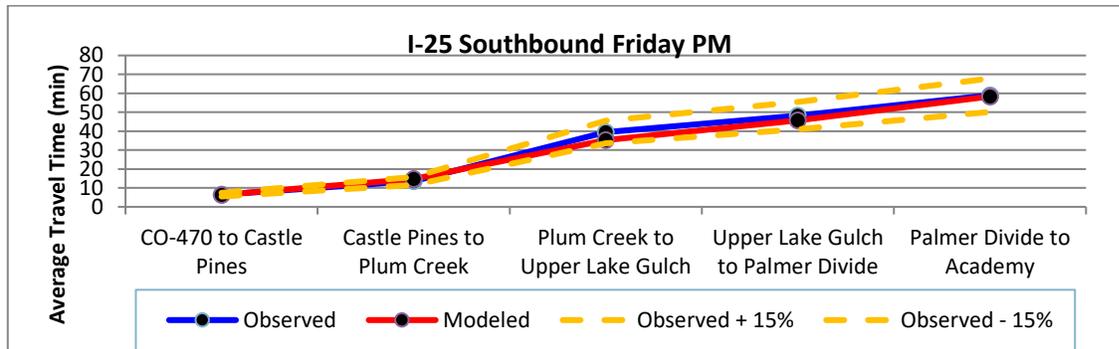
Source: Steer Davies Gleave

Figure 10: I-25 Mainline Travel Time Calibration- Northbound Sunday PM



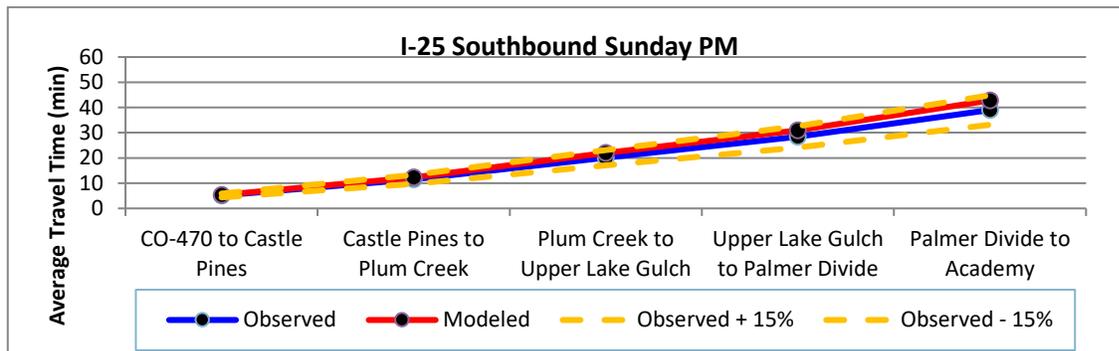
Source: Steer Davies Gleave

Figure 11: I-25 Mainline Travel Time Calibration- Southbound Friday PM



Source: Steer Davies Gleave

Figure 12: I-25 Mainline Travel Time Calibration- Southbound Sunday PM



Source: Steer Davies Gleave

Development of Forecast Year Model

SDG is developing both horizon year (2040) and opening year (2021) forecasts for the Environmental Assessment document.

Horizon Year (2040) Network

Starting with the calibrated 2017 network, we modified the following attributes to reflect their 2040 network values from the MPO model network files as received:

- Area type
- Facility Type
- Number of Lanes
- Toll Charge on C/E-470

Additionally, we included (in both the No Build and Build 2040 networks) all roadway facilities present in the 2040 networks but not in the 2015 networks. Those in the project study area included:

- Managed lanes on C-470 between I-25 and Kipling Pkwy
- New diamond interchange at I-25 and Crystal Valley Pkwy
- N/S connector between Lincoln Ave and Ridgeway Pkwy to the east of I-25 (and to the west of Peoria St)

- E/W connector from Sky Ridge Medical Center to Peoria St, between Lincoln Ave and Ridgeway Pkwy
- Two N/S connectors from Hess Ave to Crowfoot Valley Rd (one near I-25, one near SH-83)
- Woodlands Blvd N/S connector between Black Feather Tr and Scott Blvd (in Castle Rock)
- Valley Dr N/S connector between South St and Plum Creek Parkway (in Castle Rock)
- SH-21 northward / westward extension to new diamond interchange with I-25 between Interquest Pkwy and N Gate Rd

Horizon Year (2040) Trip Matrix

We derived 2040 trip matrices by applying a pivot method to the calibrated 2017 trip matrices, as follows:

- For all zones other than the five new external stations (2894-2898), we computed “target” total origins and destinations for each time period, by adding the difference between the 2040 and 2015 totals in the MPO model matrices to the 2017 calibrated matrix totals.
- For the five new external stations, we computed the targets by adding the origins from (or destinations to) all zones in the PPACG model other than those which were included in the I-25 and disaggregated to the relative traffic counts from (or to) each of the five stations in the StreetLight Data OD data.
- We applied an iterative “fratar” process to the 2017 matrices, using the 2040 row and column targets computed in the above steps.

To obtain 2040 weekend matrices, we applied the same adjustment factors used in the development of 2017 weekend matrices to the appropriate weekday time periods.

Value of Time Estimates (all forecasts with express lanes)

As described in the data collection section of this memo, the US-36 Express lanes transaction data provided continuous traffic count data and allowed us to estimate the portion of traffic using managed lanes and their travel time savings. In combination with toll rate data and managed lane share analysis, this enabled us to estimate value of time for the upper-percentile travelers, i.e. those with the highest values of time, who were willing to pay the toll to save travel time.

Table 19: Value of Time Classes

Class	Value of Time (\$/hour)
1	3.00
2	7.20
3	12.00
4	16.20
5	21.00
6	27.60
7	36.00
8	60.00

Source: Steer Davies Gleave

However, this only provides values of time for those travelers who are willing to pay the toll charged for the

time savings realized at some point during the day, which is roughly 24% of traffic⁶. We estimated the values of time for the remaining travelers based upon Denver MSA census income setting a median value of time of \$18 per hour, consistent with the original FOCUS model input and the upper-percentile results calculated from the US-36 toll transactions. Table 19 presents the results of the value of time analysis. The decision to use eight classes specifically was based on a trade-off between representing variation of traveler preferences and model run time. The classes represent the midpoints of eight equally sized groups, each with 12.5% of trips.

It is now well accepted that managed lanes users are not only paying for travel time savings, but are also paying for increased reliability and other perceived benefits such as safety improvements and comfort. The FOCUS model includes a “toll bonus” of 8 cents per mile on tolled facilities, and we retained this assumption for the I-25 managed lanes. This effectively reduces the cost of the managed lanes by 8 cents per mile during the highway assignment step, where travelers are assigned to routes based on minimizing their generalized trip costs.

Post-Processing (all forecasts, including No Build)

On I-25 between Plum Creek Parkway and Sky View Lane in the Weekday and Friday PM periods, SDG applied the two southbound post-processing steps from calibration discussed earlier in this memo, in the following manner:

1. Shifting the same volume of traffic away from I-25 onto the Exit 181 off ramp, frontage roads and Exit 174 on ramp that was shifted in base year calibration
2. Reducing capacity by amounts which vary, depending on the volumes on the Exit 174 on ramp. These reductions were equal to or greater than in the base year, due to the higher overall level of traffic. We ensured consistent capacities among the various alternatives we ran.

Opening Year (2021)

The assumptions supporting 2021 forecasts were:

- We interpolated trip matrices linearly between 2017 and 2021.
- As an exception to the above, we did not grow trips to or from the model zones containing planned development projects in several TAZs in southern Castle Rock near Crystal Valley Parkway and Plum Creek Parkway, i.e. we would assume those development projects not to begin before 2021.
- We retained the highway network from 2017 as is for the No Build forecasts, adding only the proposed I-25 improvements for the Build forecasts.

⁶ In other words, 24% is the highest capture rate observed on US-36, when the value of time threshold for choosing the express lanes is at its lowest. The remaining 76% of travelers do not have a high enough value of time for the express lanes ever to be an attractive route choice.

Appendix A: Traffic Count Calibration

Weekday Mainline Volumes Calibration Northbound

Start	End	Observed Volumes				Modeled Volumes				Percent Difference				Difference (GEH)			
		AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT
Exit 158 (Baptist)	Exit 161 (2nd St Monument/SH-105)	7,670	12,881	16,666	10,204	8,370	13,985	18,269	10,754	9%	9%	10%	5%	4.9	4.8	5.0	1.6
Exit 161 (2nd St Monument/SH-105)	Exit 163 (Palmer Divide)	6,254	9,681	13,728	8,830	6,668	10,701	14,633	8,999	7%	11%	7%	2%	3.3	5.1	3.1	0.5
Exit 163 (Palmer Divide)	Exit 167 (Greenland)	6,814	9,679	14,072	8,705	6,649	9,774	14,193	8,792	-2%	1%	1%	1%	1.3	0.5	0.4	0.3
Exit 167 (Greenland)	Exit 172 (Upper Lake Gulch)	6,929	9,753	14,022	8,735	6,818	9,909	14,421	8,859	-2%	2%	3%	1%	0.8	0.8	1.4	0.4
Exit 172 (Upper Lake Gulch)	Exit 173 (Spruce Mountain)	6,885	9,638	13,922	8,675	6,804	9,895	14,398	8,832	-1%	3%	3%	2%	0.6	1.3	1.6	0.5
Exit 173 (Spruce Mountain)	Exit 174 (Sky View)	7,281	10,054	14,594	8,941	7,523	10,699	15,422	9,193	3%	6%	6%	3%	1.8	3.2	2.8	0.8
Exit 174 (Sky View)	Exit 179 (Crystal Valley)	7,191	9,860	14,339	8,839	7,498	10,560	15,290	9,083	4%	7%	7%	3%	2.3	3.5	3.2	0.8
Exit 179 (Crystal Valley)	Exit 181 (Plum Creek)	7,191	9,860	14,339	8,839	7,498	10,560	15,290	9,083	4%	7%	7%	3%	2.3	3.5	3.2	0.8
Exit 181 (Plum Creek)	Exit 182 (Wilcox)	9,848	11,923	17,736	10,346	9,635	12,634	17,478	9,712	-2%	6%	-1%	-6%	1.4	3.2	0.8	1.9
Exit 182 (Wilcox)	Exit 184 (Founders/Meadows)	10,902	13,453	19,834	11,386	11,634	15,416	20,636	11,735	7%	15%	4%	3%	4.4	8.2	2.3	1.0
Exit 184 (Founders/Meadows)	Exit 185 (Castle Rock)	14,528	15,681	23,895	13,991	15,288	17,391	23,110	12,442	5%	11%	-3%	-11%	3.9	6.6	2.1	4.0
Exit 185 (Castle Rock)	Exit 187 (Happy Canyon)	15,373	15,511	24,298	13,105	16,581	18,659	24,819	13,185	8%	20%	2%	1%	6.0	12.0	1.4	0.2
Exit 187 (Happy Canyon)	Exit 188 (Hess/Castle Pines)	16,055	16,174	25,328	13,410	17,942	20,251	26,355	13,709	12%	25%	4%	2%	9.2	15.1	2.6	0.8
Exit 188 (Hess/Castle Pines)	Exit 192 (Ridgegate)	16,223	15,351	25,322	12,346	20,102	21,328	28,830	14,680	24%	39%	14%	19%	18.2	22.1	8.7	5.9
Exit 192 (Ridgegate)	Exit 193 (Lincoln)	16,885	16,557	27,639	14,634	19,685	20,638	29,032	16,166	17%	25%	5%	10%	13.1	15.0	3.4	3.6
Exit 193 (Lincoln)	Exit 195 (C-470/E-470)	19,408	23,389	34,676	19,262	22,238	24,239	33,191	18,067	15%	4%	-4%	-6%	12.4	2.8	3.3	2.6

Source: Steer Davies Gleave

Weekday Mainline Volumes Calibration Southbound

Start	End	Observed Volumes				Modeled Volumes				Percent Difference				Difference (GEH)			
		AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT
Exit 193 (Lincoln)	Exit 195 (C-470/E-470)	13,576	31,403	33,832	20,722	11,441	32,292	30,930	19,043	-16%	3%	-9%	-8%	12.1	2.5	6.6	3.5
Exit 192 (Ridgegate)	Exit 193 (Lincoln)	8,712	26,356	26,529	16,410	9,399	28,448	25,047	16,320	8%	8%	-6%	-1%	4.6	6.3	3.8	0.2
Exit 188 (Hess/Castle Pines)	Exit 192 (Ridgegate)	7,616	25,082	24,033	14,698	10,211	29,759	25,383	16,076	34%	19%	6%	9%	17.4	14.1	3.5	3.3
Exit 187 (Happy Canyon)	Exit 188 (Hess/Castle Pines)	8,301	24,489	24,151	14,312	10,170	27,090	23,563	14,588	23%	11%	-2%	2%	12.3	8.1	1.6	0.7
Exit 185 (Castle Rock)	Exit 187 (Happy Canyon)	7,895	23,506	23,256	13,800	9,202	24,928	22,157	13,889	17%	6%	-5%	1%	8.9	4.6	3.0	0.2
Exit 184 (Founders/Meadows)	Exit 185 (Castle Rock)	7,689	21,613	22,223	13,139	8,660	22,945	20,563	12,987	13%	6%	-7%	-1%	6.8	4.5	4.6	0.4
Exit 182 (Wilcox)	Exit 184 (Founders/Meadows)	6,754	17,874	19,666	11,702	7,787	18,721	18,493	12,154	15%	5%	-6%	4%	7.7	3.1	3.5	1.2
Exit 181 (Plum Creek)	Exit 182 (Wilcox)	5,739	15,699	17,154	10,652	5,949	15,715	15,058	9,854	4%	0%	-12%	-7%	1.7	0.1	6.7	2.3
Exit 179 (Crystal Valley)	Exit 181 (Plum Creek)	4,810	11,083	13,846	9,036	5,213	11,882	13,301	9,234	8%	7%	-4%	2%	3.6	3.7	1.9	0.6
Exit 174 (Sky View)	Exit 179 (Crystal Valley)	4,810	11,083	13,846	9,036	5,213	11,882	13,301	9,234	8%	7%	-4%	2%	3.6	3.7	1.9	0.6
Exit 173 (Spruce Mountain)	Exit 174 (Sky View)	4,904	12,171	14,050	9,089	5,307	12,782	13,465	9,346	8%	5%	-4%	3%	3.6	2.7	2.0	0.8
Exit 172 (Upper Lake Gulch)	Exit 173 (Spruce Mountain)	4,690	11,465	13,377	8,776	4,865	11,817	12,590	8,871	4%	3%	-6%	1%	1.6	1.6	2.8	0.3
Exit 167 (Greenland)	Exit 172 (Upper Lake Gulch)	4,746	11,579	13,545	8,826	4,885	11,826	12,619	8,881	3%	2%	-7%	1%	1.3	1.1	3.3	0.2
Exit 163 (Palmer Divide)	Exit 167 (Greenland)	4,745	11,549	13,465	8,715	4,829	11,555	12,438	8,779	2%	0%	-8%	1%	0.8	0.0	3.7	0.2
Exit 161 (2nd St Monument/SH-105)	Exit 163 (Palmer Divide)	5,003	11,155	13,330	8,802	5,279	11,373	12,820	8,673	6%	2%	-4%	-1%	2.4	1.0	1.8	0.4
Exit 158 (Baptist)	Exit 161 (2nd St Monument/SH-105)	7,165	13,839	16,983	10,130	7,375	14,778	16,852	10,465	3%	7%	-1%	3%	1.6	3.9	0.4	1.0

Source: Steer Davies Gleave

Weekend Mainline Volumes Calibration Northbound

Start	End	Observed Volumes		Modeled Volumes		Percent Difference		Difference (GEH)	
		FR	SU	FR	SU	FR	SU	FR	SU
Exit 158 (Baptist)	Exit 161 (2nd St Monument/SH-105)	14,654	12,522	15,654	13,533	7%	8%	4.1	4.7
Exit 161 (2nd St Monument/SH-105)	Exit 163 (Palmer Divide)	11,758	10,940	12,317	11,158	5%	2%	2.5	1.1
Exit 163 (Palmer Divide)	Exit 167 (Greenland)	11,798	11,124	11,433	10,872	-3%	-2%	1.7	1.3
Exit 167 (Greenland)	Exit 172 (Upper Lake Gulch)	11,854	11,088	11,565	10,995	-2%	-1%	1.3	0.5
Exit 172 (Upper Lake Gulch)	Exit 173 (Spruce Mountain)	11,759	10,988	11,554	10,986	-2%	0%	0.9	0.0
Exit 173 (Spruce Mountain)	Exit 174 (Sky View)	12,219	11,450	12,281	11,519	1%	1%	0.3	0.3
Exit 174 (Sky View)	Exit 179 (Crystal Valley)	11,845	10,845	12,173	11,447	3%	6%	1.5	3.0
Exit 179 (Crystal Valley)	Exit 181 (Plum Creek)	11,845	10,845	12,173	11,447	3%	6%	1.5	3.0
Exit 181 (Plum Creek)	Exit 182 (Wilcox)	14,283	13,109	14,111	12,696	-1%	-3%	0.7	1.9
Exit 182 (Wilcox)	Exit 184 (Founders/Meadows)	15,866	14,574	16,913	14,616	7%	0%	4.1	0.2
Exit 184 (Founders/Meadows)	Exit 185 (Castle Rock)	17,130	16,207	18,880	16,076	10%	-1%	6.5	0.5
Exit 185 (Castle Rock)	Exit 187 (Happy Canyon)	18,130	16,655	20,134	17,041	11%	2%	7.2	1.6
Exit 187 (Happy Canyon)	Exit 188 (Hess/Castle Pines)	-	-	21,704	17,959	-	-	-	-
Exit 188 (Hess/Castle Pines)	Exit 192 (Ridgegate)	19,781	17,289	22,815	19,275	15%	11%	10.4	7.9
Exit 192 (Ridgegate)	Exit 193 (Lincoln)	-	-	22,156	19,366	-	-	-	-
Exit 193 (Lincoln)	Exit 195 (C-470/E-470)	-	-	25,439	21,800	-	-	-	-

Source: Steer Davies Gleave

Weekend Mainline Volumes Calibration Southbound

Start	End	Observed Volumes		Modeled Volumes		Percent Difference		Difference (GEH)	
		FR	SU	FR	SU	FR	SU	FR	SU
Exit 193 (Lincoln)	Exit 195 (C-470/E-470)	-	-	33,170	22,288	-	-	-	-
Exit 192 (Ridgegate)	Exit 193 (Lincoln)	-	-	29,606	18,480	-	-	121.7	102.8
Exit 188 (Hess/Castle Pines)	Exit 192 (Ridgegate)	24,157	17,183	30,917	18,722	28%	9%	20.4	6.1
Exit 187 (Happy Canyon)	Exit 188 (Hess/Castle Pines)	-	-	28,184	17,379	-	-	-	-
Exit 185 (Castle Rock)	Exit 187 (Happy Canyon)	23,060	16,995	26,047	16,488	13%	-3%	9.5	2.1
Exit 184 (Founders/Meadows)	Exit 185 (Castle Rock)	20,899	16,074	24,082	15,400	15%	-4%	10.6	2.9
Exit 182 (Wilcox)	Exit 184 (Founders/Meadows)	17,936	13,529	19,736	13,878	10%	3%	6.6	1.6
Exit 181 (Plum Creek)	Exit 182 (Wilcox)	15,765	12,095	16,697	11,696	6%	-3%	3.7	2.0
Exit 179 (Crystal Valley)	Exit 181 (Plum Creek)	9,277	9,996	10,182	10,595	10%	6%	4.6	3.2
Exit 174 (Sky View)	Exit 179 (Crystal Valley)	9,277	9,996	10,182	10,595	10%	6%	4.6	3.2
Exit 173 (Spruce Mountain)	Exit 174 (Sky View)	12,821	10,499	13,844	10,705	8%	2%	4.4	1.1
Exit 172 (Upper Lake Gulch)	Exit 173 (Spruce Mountain)	12,108	10,126	13,011	10,117	7%	0%	4.0	0.0
Exit 167 (Greenland)	Exit 172 (Upper Lake Gulch)	12,231	10,206	13,018	10,134	6%	-1%	3.5	0.4
Exit 163 (Palmer Divide)	Exit 167 (Greenland)	12,096	10,170	12,748	10,009	5%	-2%	2.9	0.9
Exit 161 (2nd St Monument/SH-105)	Exit 163 (Palmer Divide)	12,059	10,197	12,574	10,188	4%	0%	2.3	0.0
Exit 158 (Baptist)	Exit 161 (2nd St Monument/SH-105)	14,721	11,811	16,075	12,524	9%	6%	5.5	3.5

Source: Steer Davies Gleave

Table 1: Weekday Surface Roads Volumes Calibration

Location		Observed Volumes				Modeled Volumes				Percent Difference				Difference (GEH)			
		AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT	AM	PM	MD	NT
Frontage Rd W Side of I-25 just south of Plum Creek Pkwy	NB	750	610	1,079	390	407	695	606	222	-46%	14%	-44%	-43%	9.0	1.7	6.7	2.8
Frontage Rd W Side of I-25 just south of Plum Creek Pkwy	SB	690	2,146	1,711	967	678	2,021	1,196	699	-2%	-6%	-30%	-28%	0.3	1.4	5.5	2.7
Frontage Rd E Side of I-25 just south of Crystal Valley Pkwy	NB	365	438	627	180	85	133	165	88	-77%	-70%	-74%	-51%	11.8	9.0	9.5	2.3
Frontage Rd E Side of I-25 just south of Crystal Valley Pkwy	SB	173	782	578	242	71	394	143	98	-59%	-50%	-75%	-59%	5.8	8.0	9.4	3.2
Santa Fe Dr (US-85) just south of C-470	NB	4,981	7,304	9,036	3,605	5,342	7,595	8,808	3,641	7%	4%	-3%	1%	3.2	1.7	1.0	0.2
Santa Fe Dr (US-85) just south of C-470	SB	4,619	6,842	9,054	4,416	4,677	7,702	8,103	4,563	1%	13%	-10%	3%	0.5	5.0	4.2	0.6
Perry Park Rd (SH-105) just south of Tomah Rd	NB	415	312	582	209	451	380	381	194	9%	22%	-35%	-7%	1.1	1.8	3.7	0.3
Perry Park Rd (SH-105) just south of Tomah Rd	SB	119	725	523	247	192	809	352	189	62%	12%	-33%	-23%	3.7	1.5	3.3	1.2
SH-83 Just south of E-470	NB	6,854	7,109	9,376	5,968	7,472	7,680	10,400	5,328	9%	8%	11%	-11%	4.6	3.3	4.2	2.5
SH-83 Just south of E-470	SB	4,617	10,942	11,466	5,761	4,422	11,001	12,527	6,337	-4%	1%	9%	10%	1.8	0.3	4.0	2.2
SH-83 just south of Gillian Ave	NB	559	822	889	467	640	636	773	320	15%	-23%	-13%	-31%	2.1	3.4	1.6	2.2
SH-83 just south of Gillian Ave	SB	431	1,194	907	519	265	993	552	302	-38%	-17%	-39%	-42%	5.6	3.0	5.4	3.1

Source: Steer Davies Gleave

Table 2: Weekend Surface Roads Volumes Calibration

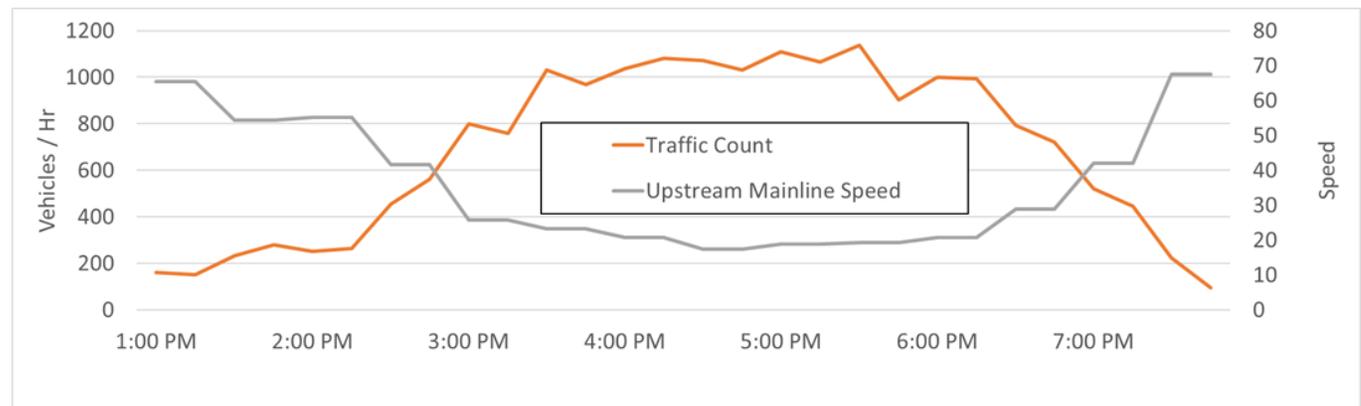
Location		Observed Volumes		Modeled Volumes		Percent Difference		Difference (GEH)	
		AM	PM	AM	PM	AM	PM	AM	PM
Frontage Rd W Side of I-25 just south of Plum Creek Pkwy	NB	750	610	407	695	-46%	14%	9.0	1.7
Frontage Rd W Side of I-25 just south of Plum Creek Pkwy	SB	690	2,146	678	2,021	-2%	-6%	0.3	1.4
Frontage Rd E Side of I-25 just south of Crystal Valley Pkwy	NB	365	438	85	133	-77%	-70%	11.8	9.0
Frontage Rd E Side of I-25 just south of Crystal Valley Pkwy	SB	173	782	71	394	-59%	-50%	5.8	8.0
Santa Fe Dr (US-85) just south of C-470	NB	4,981	7,304	5,342	7,595	7%	4%	3.2	1.7
Santa Fe Dr (US-85) just south of C-470	SB	4,619	6,842	4,677	7,702	1%	13%	0.5	5.0
Perry Park Rd (SH-105) just south of Tomah Rd	NB	415	312	451	380	9%	22%	1.1	1.8
Perry Park Rd (SH-105) just south of Tomah Rd	SB	119	725	192	809	62%	12%	3.7	1.5
SH-83 Just south of E-470	NB	6,854	7,109	7,472	7,680	9%	8%	4.6	3.3
SH-83 Just south of E-470	SB	4,617	10,942	4,422	11,001	-4%	1%	1.8	0.3
SH-83 just south of Gillian Ave	NB	559	822	640	636	15%	-23%	2.1	3.4
SH-83 just south of Gillian Ave	SB	431	1,194	265	993	-38%	-17%	5.6	3.0

Source: Steer Davies Gleave

Appendix B: Illustration of Southbound PM Delays on Friday August 4th, 2017

Friday August 4, 2017															
Location	1:00 PM	1:30 PM	2:00 PM	2:30 PM	3:00 PM	3:30 PM	4:00 PM	4:30 PM	5:00 PM	5:30 PM	6:00 PM	6:30 PM	7:00 PM	7:30 PM	
US-85 to Wolfensberger Rd	70	53	67	69	65	60	50	66	67	70	69	66	69	69	
Wolfensberger Rd Exit 182	70	66	65	69	66	65	61	61	67	62	66	67	69	69	
Wolfensberger Rd to Plum Creek Pkwy	70	66	66	66	63	60	58	34	53	36	57	67	68	69	
Plum Creek Pkwy Exit 181	72	61	65	39	50	21	28	16	21	26	35	59	65	66	
Plum Creek Pkwy to Tomah Rd	64	51	52	40	23	21	18	15	16	17	18	26	41	68	
Tomah Rd Exit 174	69	69	65	34	22	19	17	17	17	17	22	21	26	68	
Tomah Rd to Spruce Mtn Rd	69	67	64	49	41	40	39	39	41	42	41	39	43	68	
Spruce Mountain Rd Exit 173	67	68	64	62	57	61	61	59	61	62	61	62	61	65	
Spruce Mtn Rd to Upper Lake Gulch Rd	68	68	64	63	58	62	62	59	61	62	61	61	62	66	
Upper Lake Gulch Rd Exit 172	67	67	56	64	58	62	63	60	62	63	62	62	62	65	
Upper Lake Gulch Rd to Greenland Rd	70	66	57	52	65	65	65	66	65	56	63	55	44	36	
Greenland Rd Exit 167	72	66	55	42	70	61	65	66	65	62	60	49	29	34	
Greenland Rd to County Line Rd	71	63	53	53	66	56	64	66	63	61	49	44	45	50	

Exit 174 On Ramp
PM Detail →



Source: Steer Davies Gleave

Appendix C: Modeling Approach Memo

Memo

To Mandy Whorton
Cc Zeke Lynch, David Cuneo
From Mark Feldman
Date 3 January 2017
Project I-25 C-470 to Monument PEL

Project No. 23029301

Introduction

Steer Davies Gleave’s role in task order 1 for the I-25 PEL is to give an overview of the available models and recommendation of the preferred model to use for performing the project’s travel demand forecasting.

In a prior memo, dated November 14th, 2016, we recommended the use of a version of DRCOG’s 4-step travel demand model (COMPASS) that was modified to incorporate high speed rail transit for the ongoing Interregional Connectivity Study (ICS). We primarily recommended this model because of its inclusion of high speed rail, and because of its faster run time compared to activity based models such as DRCOG’s newer FOCUS model.

In the past month, however, conversations among the project team and with technical stakeholders and advisors have brought to light a desire to use the most current forecasting tools in the region, i.e. FOCUS, as the newest version (FOCUS II) has substantially reduced run times. We have also learned that the ability to include and analyze high speed rail is not essential for the PEL.

Discussions have also highlighted interest in using a TransModeler (microscopic) model to potentially analyze elements of alternatives. The TransModeler model has also been identified as a critical component of work downstream of the PEL.

Therefore, we have revised our recommendation to 1) develop a macroscopic travel demand model as the main tool for analysis ; the macroscopic model will use FOCUS II as its core component and be supplemented by an interregional trip distribution model, and 2) develop a Transmodeler model to provide more detailed analysis of some alternatives. The details of the development of these models are described in the subsequent sections of this memo.

Macroscopic Travel Demand Model

We will use the latest version of the DRCOG model, FOCUS II, as the core travel demand forecasting tool.

DRCOG FOCUS Component

We will run the full FOCUS II model as the starting point of our model. This will provide the initial trip matrices that we will then modify using the trip distribution model described below.

We will perform a detailed calibration and validation of the model’s forecasts to observed traffic counts in the study corridor. According to the FOCUS Model Calibration memo⁷, the model overstates traffic by about

⁷ FOCUS Model Calibration 1.0, DRCOG, 8/5/2010, Pages 47-48, Figure 13 and Table 52

21% on a screenline which passes through the northern edge of the I-25 PEL study corridor (Castle Rock), and includes counts on two links as part of that screenline. We would collect more extensive traffic data in the study corridor, including on I-25 between every interchange, as well as all on ramps, off ramps and parallel routes, and use that data to establish a higher level model accuracy within the corridor.

After running and calibrating the FOCUS model for our study area, we will extend the highway network for this project since the PEL study corridor extends beyond the FOCUS model project limits. Specifically, the FOCUS model network extends as far south as Palmer Divide Road at the northern edge of Monument, whereas the study corridor extends a mile further south.

To provide a minimal number of likely entrances / exits to the model network for trips passing between the DRCOG and PPACG regions, we propose extending the FOCUS model network. Figure 3 depicts this extension, as compared to the current FOCUS model edge:

Figure 13: Comparison of Southern Edges of FOCUS Model to Proposed Extended Model

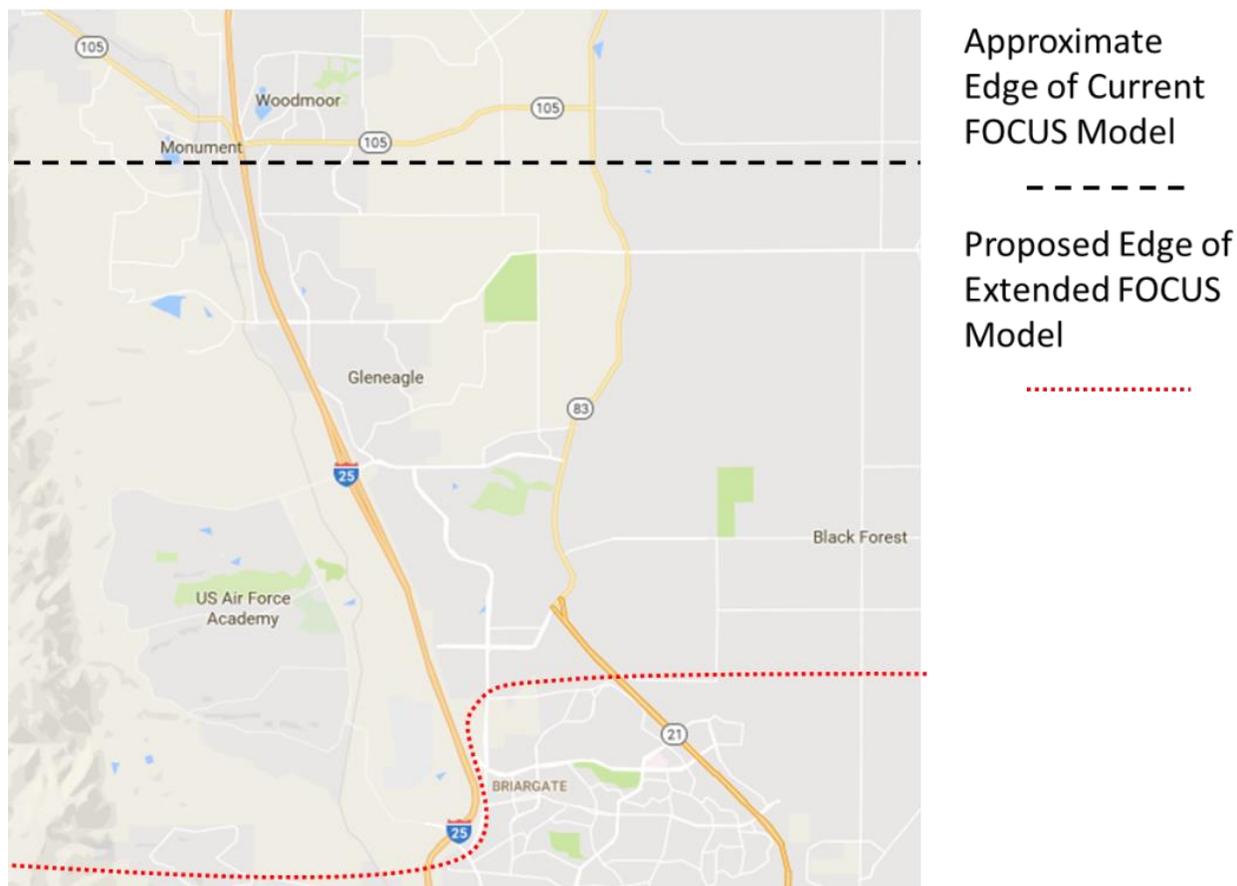
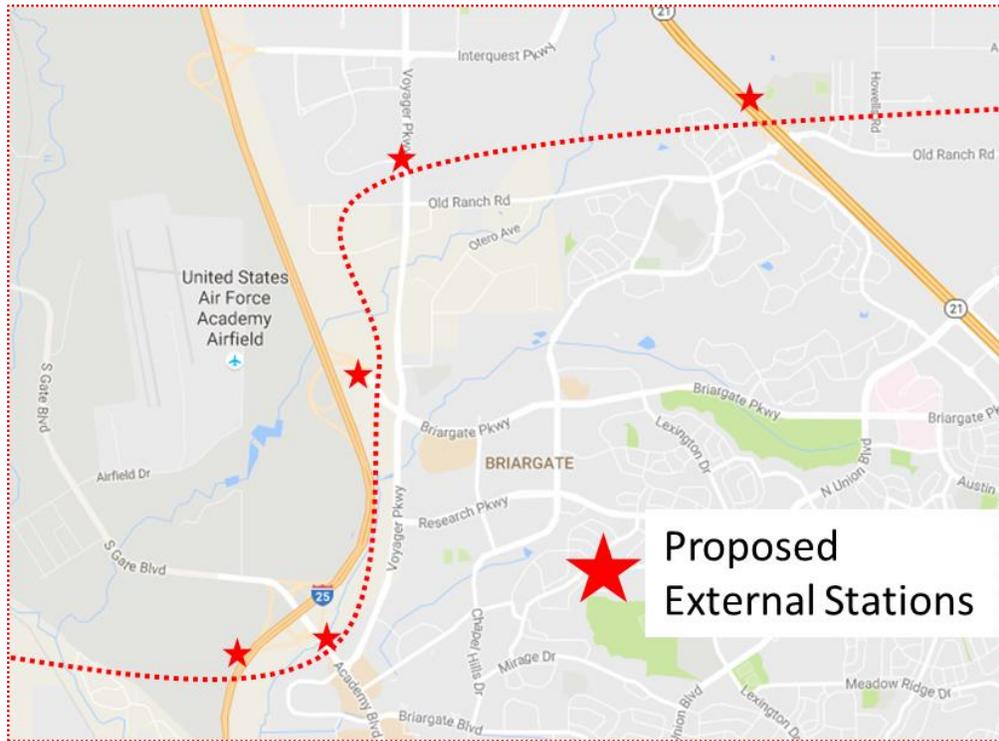


Figure 4 zooms in on the proposed extension above and shows the locations of the proposed external stations, from :

- I-25
- Academy Boulevard
- Briargate Parkway

- Voyager Parkway
- North Powers Blvd (C-21)

Figure 14: Proposed External Stations for Extended FOCUS Model Network



We will obtain network attributes for the extended network (beyond the FOCUS model) and land uses for zones in the appended model region from the most current available version of the PPACG region’s travel demand model.

Interregional Trip Distribution Model

To simply incorporate socioeconomic projections in the appended PPACG region is not sufficient by itself, because it can distort the model’s trip distribution (allocation of zonal trip generation into origin-destination (OD) pairs using relative zone pair travel impedances) by treating all trip ends in the PPACG region as occurring at the same zone.

To address this issue while still including a method to analyze how an alternative may impact trip patterns, we will develop an interregional trip distribution model. We will estimate the parameters of this trip distribution model to fit observed regional travel patterns, which we will establish from one or more of the following sources:

- Interregional trip matrices from the Interregional Connectivity Study (ICS);
- AirSage cell phone OD data obtained in 2013 for the ICS;
- Streetlight OD data: either already collected by CDOT or new data obtained for this project; and
- Front Range regional household survey data

Note that the OD data will have larger zones than the FOCUS model. If new data is obtained, we propose to define zones with the FOCUS model boundaries along the study corridor.

The observed travel patterns will be fit to a trip distribution gravity model, which relates interzonal impedance to a set of factors that monotonically decrease with increasing impedance.

We will develop future trip matrices using the zone system of the OD data source from the above list and each zone's total trips generated as estimated from the regional travel demand models, socioeconomic projections from local sources and/or projections from national sources such as Moody's.

After calibrating the trip distribution gravity model, we will use it as an input to running alternative improvements. We will update future network impedances corresponding to the alternative being run, and then rerun the distribution model to figure out how much to adjust the external zone traffic. We will apply the gravity model to each zone's trip total, with a procedure known as iterative proportional fitting (IPF).

Finally, the future trip matrix will be adjusted to the zone system in the slightly-extended DRCOG focus model, as follows:

- Trip ends to the south of the model (in the PPACG region or further south) will be assigned to one of the external gateway zones in Figure 3, with most being assigned to the I-25 gateway.
- Trip ends within the model and on the study corridor will be disaggregated from the coarser OD data zone system into the more refined FOCUS zone system (if new data is collected, the OD data zones will be the same as the FOCUS zones on the study corridor and this won't be necessary).
- Outside the study corridor, the FOCUS zones will be aggregated into the OD data zone system, to reduce model run time.

Microsimulation

Along with the macroscopic travel demand model, we will develop a microsimulation in TransModeler. This tool will be used primarily to analyze details that are not covered by macroscopic travel demand models, such as roadway geometry, steepness, weaving and merging, and traffic signal operations.

When we embark on this TransModeler model development, we will determine the geographical extent of the model as it could cover the entire corridor, or may focus on the key sections with the most significant operational concerns.

A further motivation for developing the TransModeler model now, is that the data collection and set-up time of these sorts of models can be considerable, and thus it would be better to begin developing in advance of when the model could be needed for activities downstream of the PEL. Therefore, data collection can occur in parallel with that for the macroscopic model, and development can occur in tandem to the extent possible, to increase project efficiency, reduce costs, and enable the project to stay on schedule for the purposes of eventually obtaining financing.

Appendix A4
Pel Traffic Reliability Assessment

I-25 PEL: Colorado Springs to Denver Travel Reliability – Existing Conditions Assessment

PREPARED FOR: Colorado Department of Transportation
 COPY TO: Project file, Mandy Whorton/Peak Consulting, Shane Binder/APEX
 PREPARED BY: Zeke Lynch/CH2M
 DATE: March 24, 2017 (revised April 24, 2018)
 REVISION NO.: 0

1. Introduction

With an influx of residents and increasing number of drivers on our roadways, nearly all Colorado drivers experience, and plan around traffic congestion as part of their daily activities. However, on many primary roadways like interstates, travel times vary greatly from day to day, and travelers are less tolerant of unexpected delays. Comments and complaints received from I-25 corridor travelers nearly every day suggest that unexpected congestion and delays from incidents, weather, work zones, special events, fluctuations in demand, or other factors are especially frustrating. Travelers remember the “bad days” and report that bad days are becoming more common, even the norm.

2. Methodology

When drivers have a predictable travel time they are able to plan their trips and be relatively confident that they will arrive on time. However, as shown in Figure 1, it is the heavily congested days that they remember such as when they are late for work, miss an important appointment, or freight deliveries are impacted.

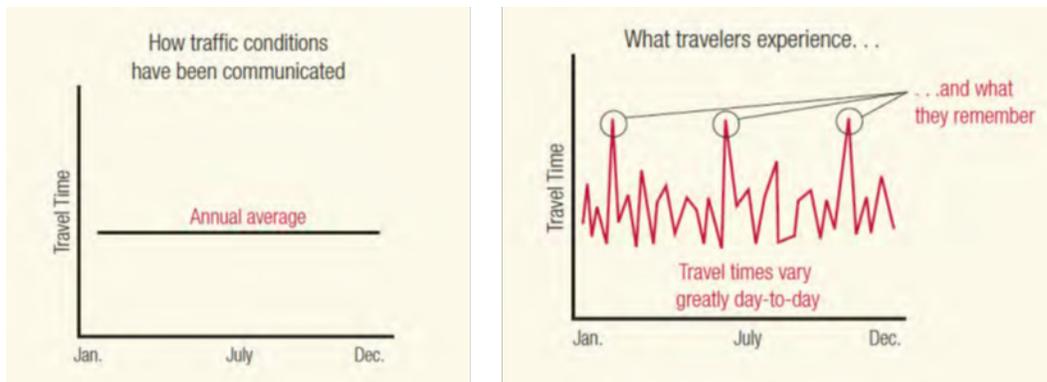


Figure 1. Typical Travel Times – Source: FHWA. 2016. Travel Time Reliability Overview

Because drivers on I-25 between Denver and Colorado Springs regularly and sometimes unexpectedly experience congestion, delay, and turbulence in the traffic stream not commonly found on rural freeways, the study team assessed corridor travel time reliability to best represent these known corridor conditions. This assessment helped understand the level of congestion, changing traffic conditions, and factors contributing to delay.

Travel times on I-25, both northbound and southbound, were obtained from INRIX, which collects roadway speeds from over 250 million real-time anonymous cell phones worldwide. Raw point-to-point travel times between exits were compiled from the INRIX database in 15-minute time periods over 2015 and 2016. CDOT has completed an assessment of INRIX travel time data and has validated its reliability.

For travel time purposes, the corridor was divided into two segments based on facility context. As shown in Figure 2, the southern segment, traversing a primarily rural area with few exits, is from Baptist Road to US 85 and the northern segment transitions into a suburban context, extending from US 85 to C-470.

Figure 3 illustrates the color gradient from green to yellow to red which was used to display the Good, Fair, and Poor travel times as compared to free-flow travel time (posted speed limit). The data was plotted on three-dimensional graphs, with the X-axis representing the day of the year, the Y-axis representing the time of day, and the Z axis representing the calculated travel time.

For the most recent two years of data, 2015 and 2016, the travel time reliability was assessed separately by direction for northbound and southbound I-25. The data was further analyzed by day of the week to understand the effects of typical commuter travel during the week as well as recreational travel occurring on weekends. The data was stratified into weekdays (Monday through Thursday), and weekends (Friday through Sunday). Initial analysis indicated that the travel on Fridays more closely matched that of the weekends, so Fridays were included with the weekend data.

Finally, days with Fair or Poor travel time reliability were investigated to determine what specific factors contributed to the decreased reliability. Recorded stalled vehicles, police enforcement, and crashes were categorized into the incidents event type. Impacts due to weather were determined from CDOT logs and NOAA data. The planned and special events category included events such as the Renaissance Festival, Air Force Academy athletics, Denver Broncos football games, and scheduled CDOT maintenance activities. Those

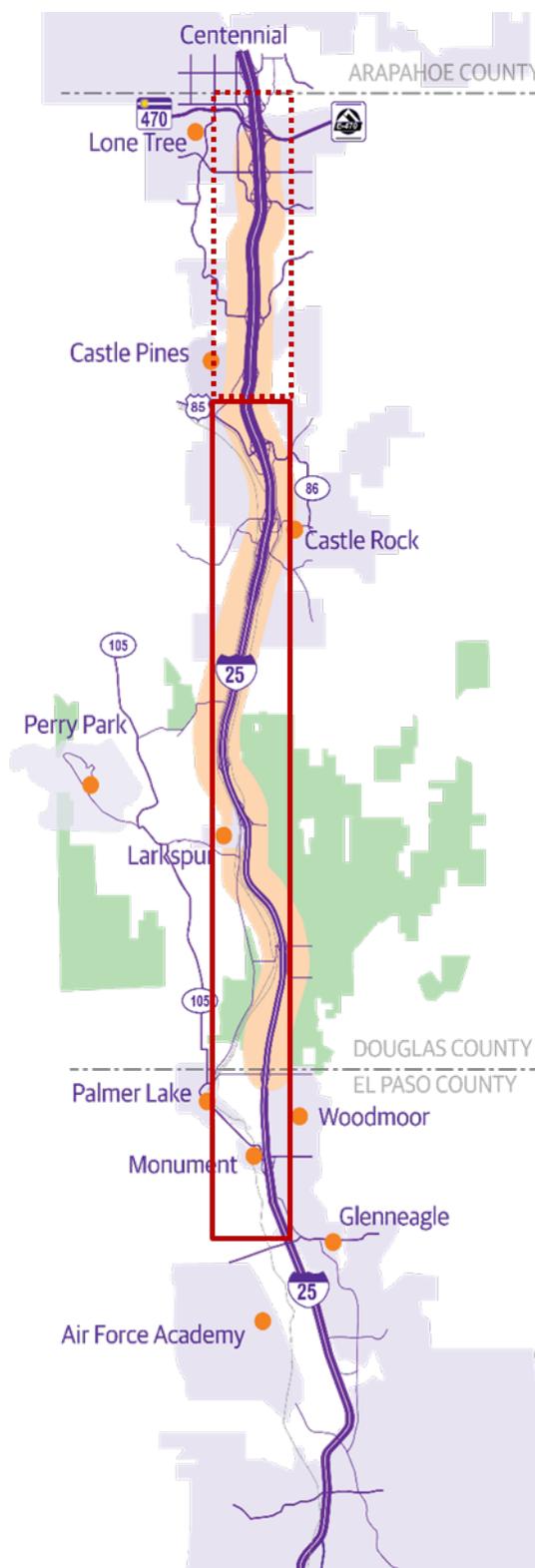


Figure 2. Corridor Travel Time Segments

days that did not have an apparent causal factor were grouped into the unaccounted type when shear traffic volume was the most likely cause of the degraded travel time reliability. When days had multiple 15-minute periods of Fair or Poor travel time reliability and there was more than one apparent contributing factor, those days were listed in the incident event type.

Threshold	Compared to Free Flow Travel Time	Southern Segment Travel Time (minutes)	Northern Segment Travel Time (minutes)
Free flow	1.0	21	9
Good	< 1.4x	<29	<12.5
Fair	1.4x to 2.0x	29 to 42	12.5 to 18
Poor	>2.0x	>42	>18

Figure 3. Travel Time Thresholds

3. Corridor Travel Time Reliability Assessment

A. Southern Segment

Figures 4 through 17, display the travel time reliability analysis for the southern segment. As described in Figure 4, most of the Poor travel times (more than twice as long as free-flow travel time) in the southern segment occurred in the evening hours and are more prominent in the summer months. While this same trend appears in 2016, as shown in Figure 5, the duration of Poor travel reliability and number of Poor travel days increased when compared to 2015.

As shown in Figures 6 and 7, Poor travel time reliability southbound in the southern segment was greater in both 2015 and 2016 than the northbound direction. Like the northbound, more Poor travel time reliability occurred in the evening hours and the southbound also experienced more Poor travel time reliability in 2016 than 2015.

The weekday data shown in Figures 8 through 11, indicate that travel time reliability was worse in the southbound than in the northbound direction and 2016 was worse in both directions than 2015.

Travel time reliability was worse on the weekends than on the weekdays in both 2015 and 2016. There were more occurrences of Poor travel time reliability southbound than northbound and both southbound and northbound saw a greater number of Poor occurrences in 2016 than 2015.

Figures 16 and 17 show the causal factor analysis for 2015 and 2016, respectively. In 2015, nearly two thirds of the days of the year (243) had at least one 15-minute period experiencing Fair or Poor travel time reliability. This number significantly increased to 261, or 72% of the days of the year in 2016. Incidents (44%) and typical congestion (44%) comprised most of the Fair and Poor travel time reliability days in 2015. Incidents were a prevalent causal factor in both 2015 and 2016 of Fair and Poor travel time reliability on weekends.

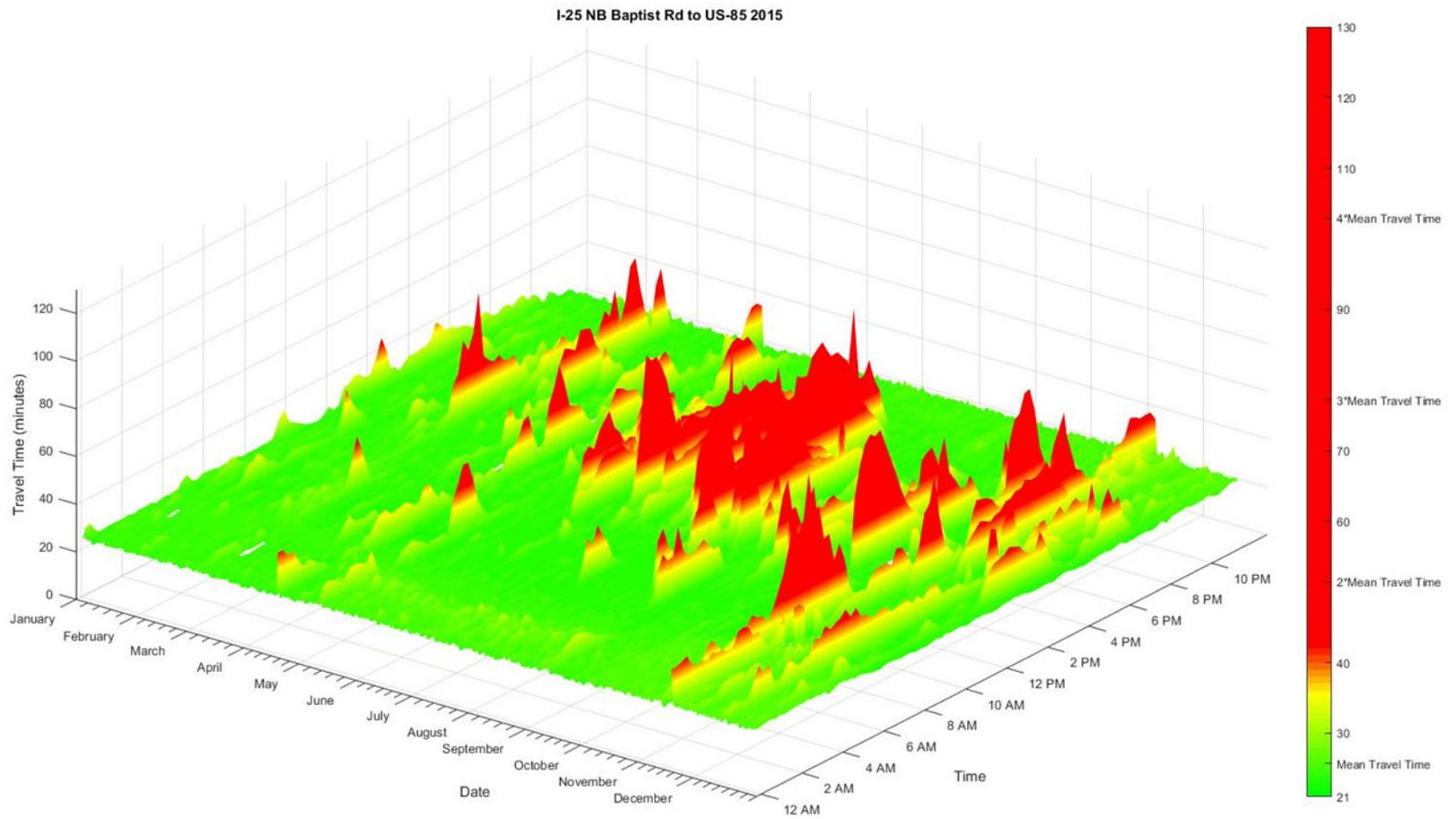


Figure 4. Southern Segment – Northbound All Days in 2015

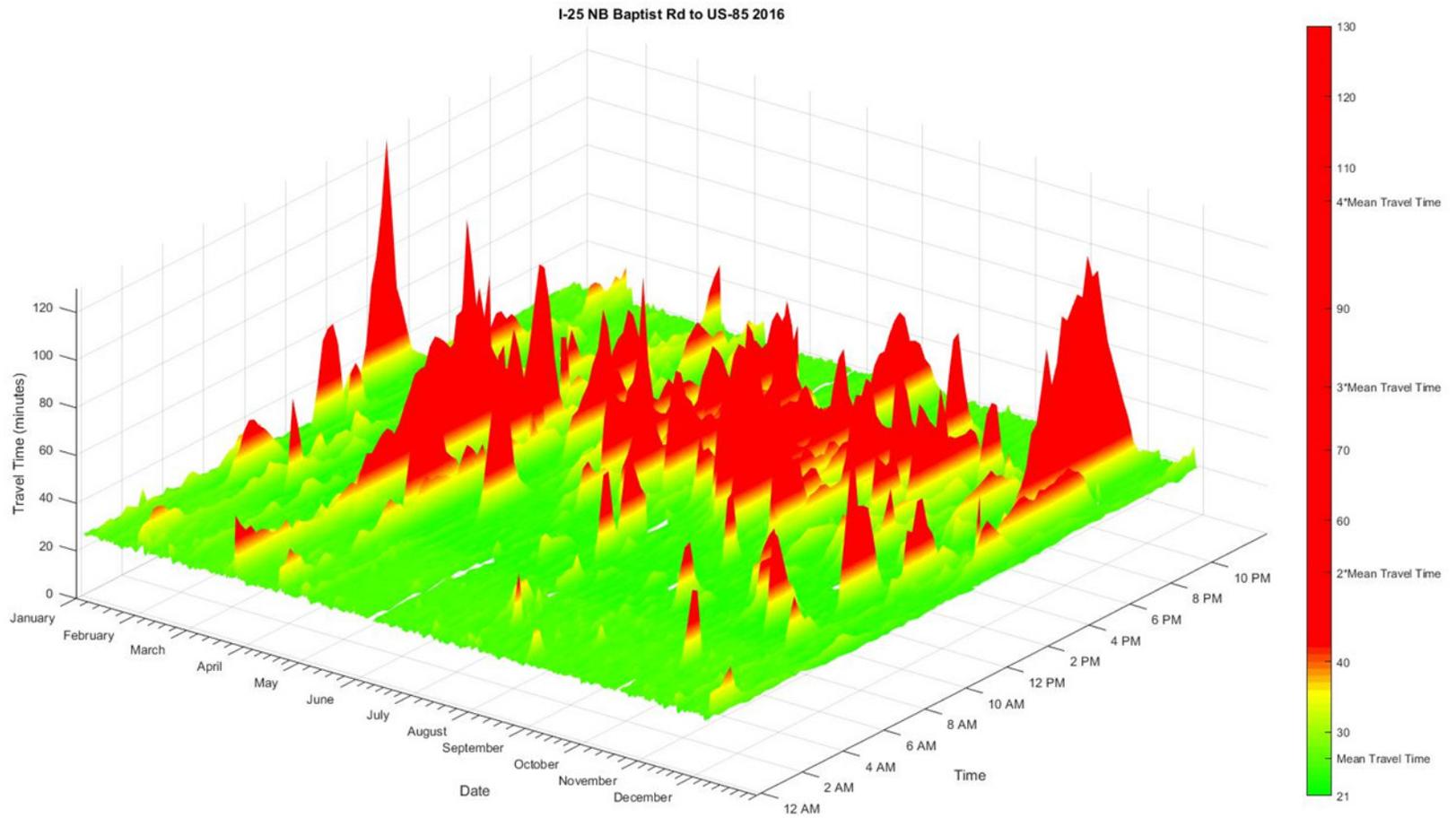


Figure 5. Southern Segment – Northbound All Days in 2016

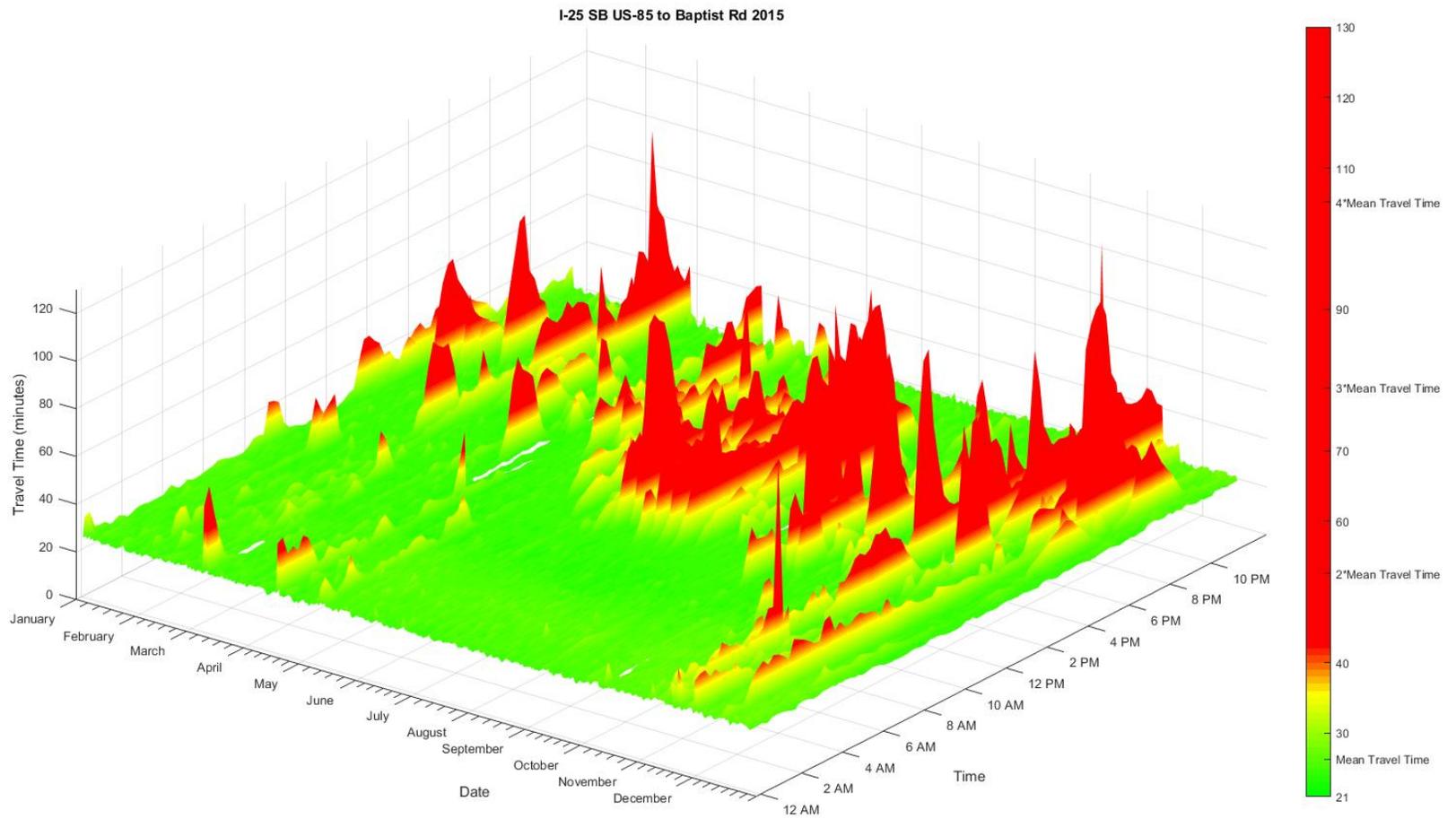


Figure 6. Southern Segment – Southbound All Days in 2015

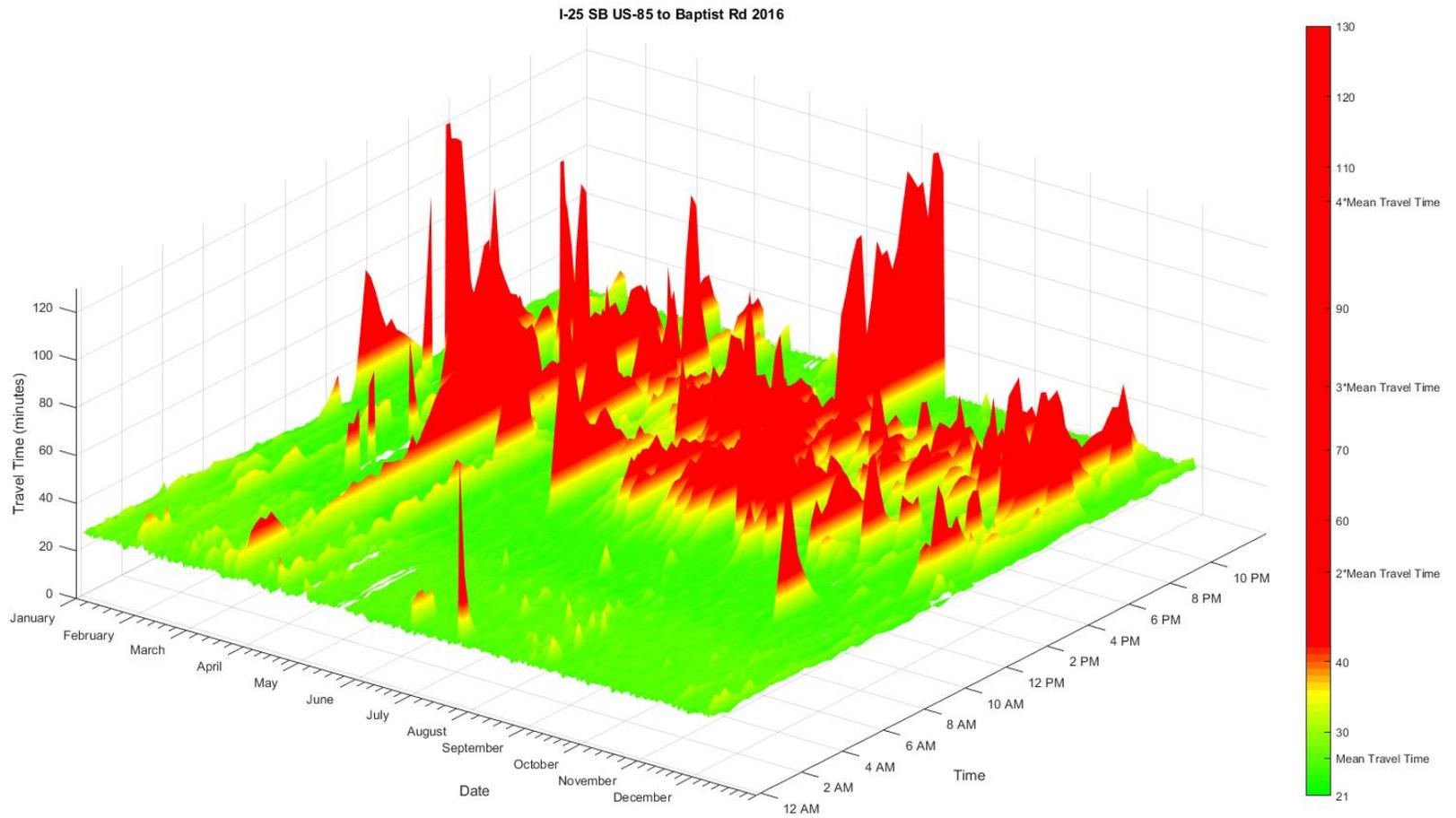


Figure 7. Southern Segment – Southbound All Days in 2016

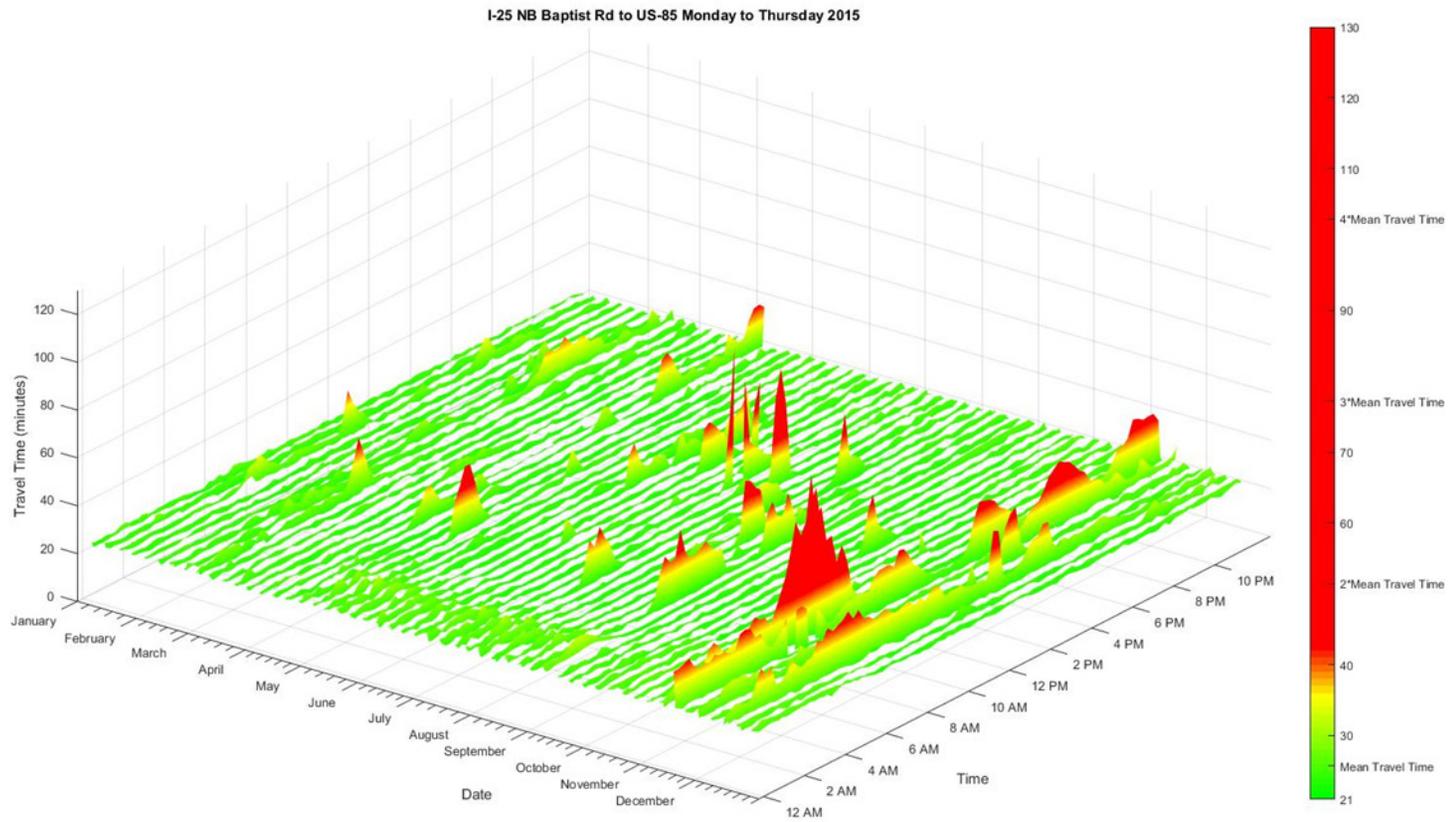


Figure 8. Southern Segment – Northbound Weekdays (M-R): 2015

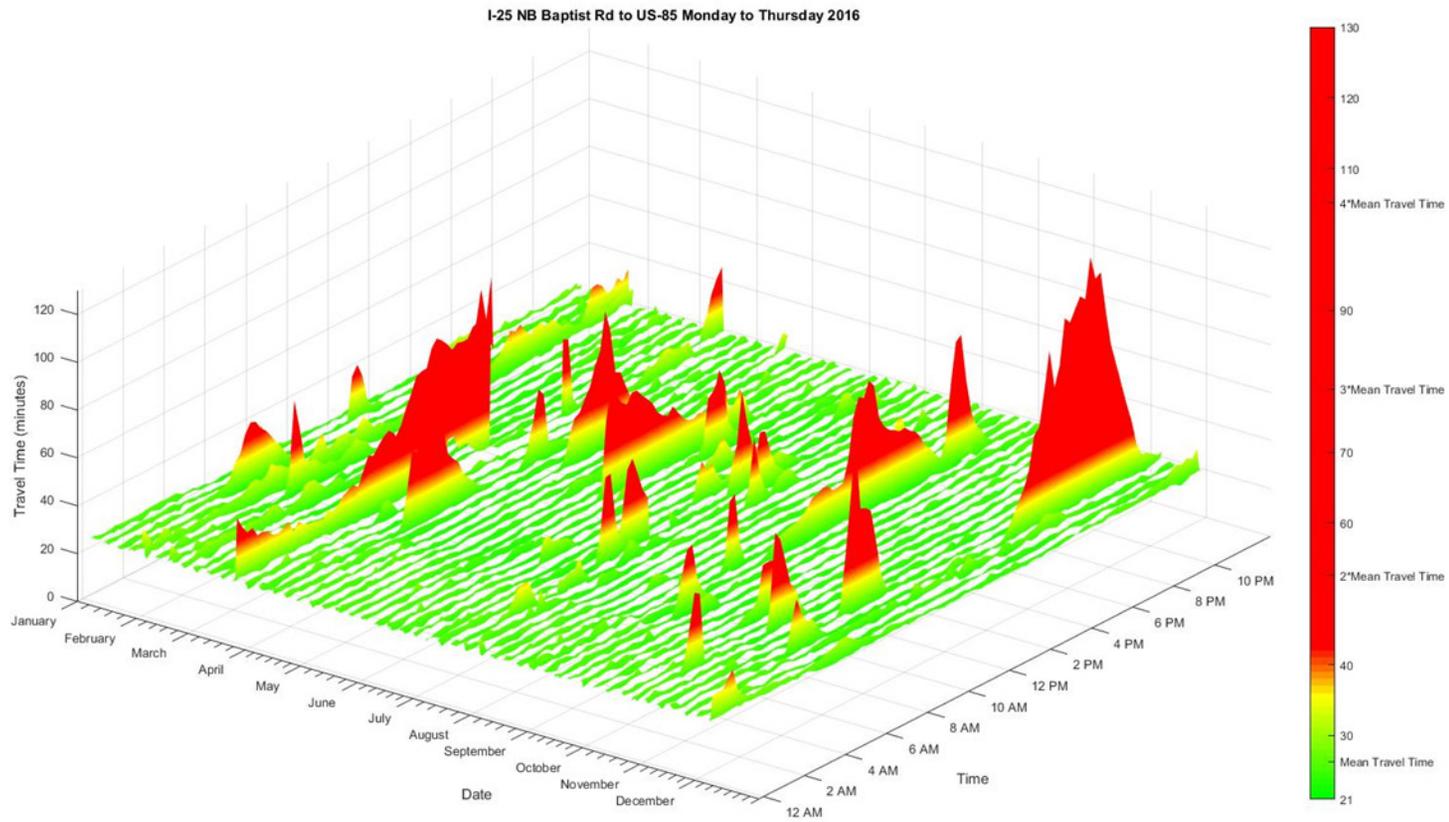


Figure 9. Southern Segment – Northbound Weekdays (M-R): 2016

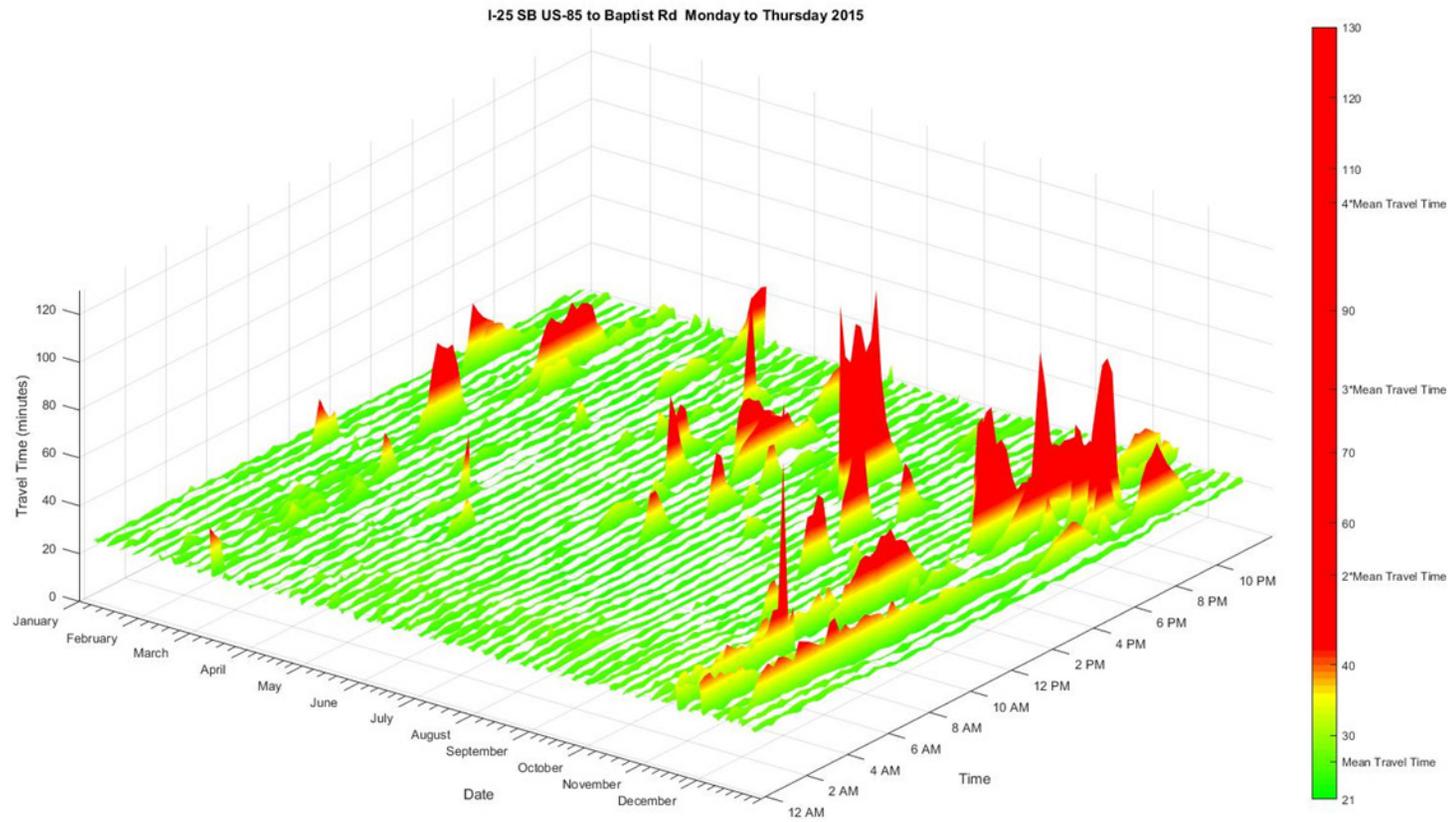


Figure 10. Southern Segment – Southbound Weekdays (M-R): 2015

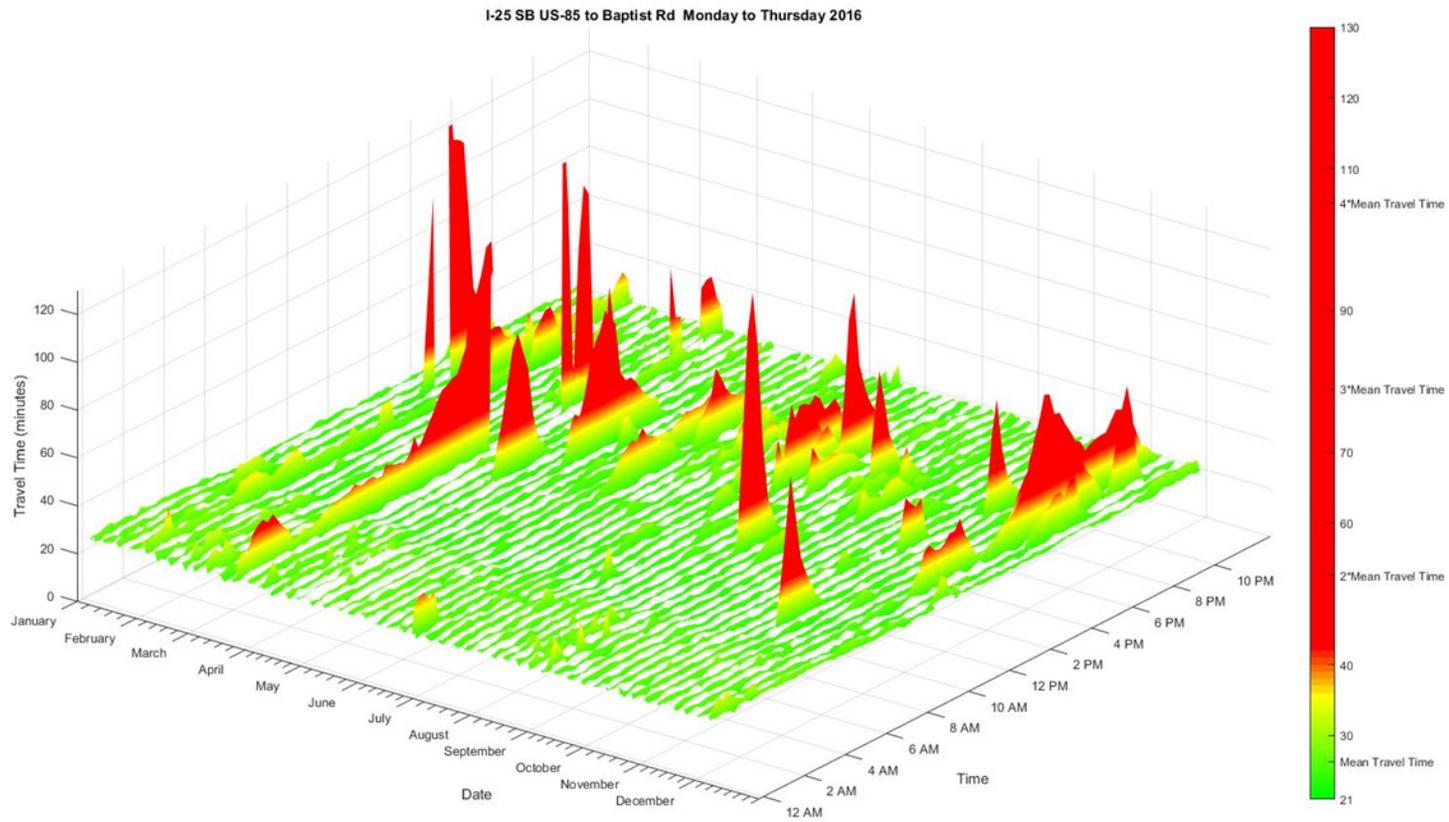


Figure 11. Southern Segment – Southbound Weekdays (M-R): 2016

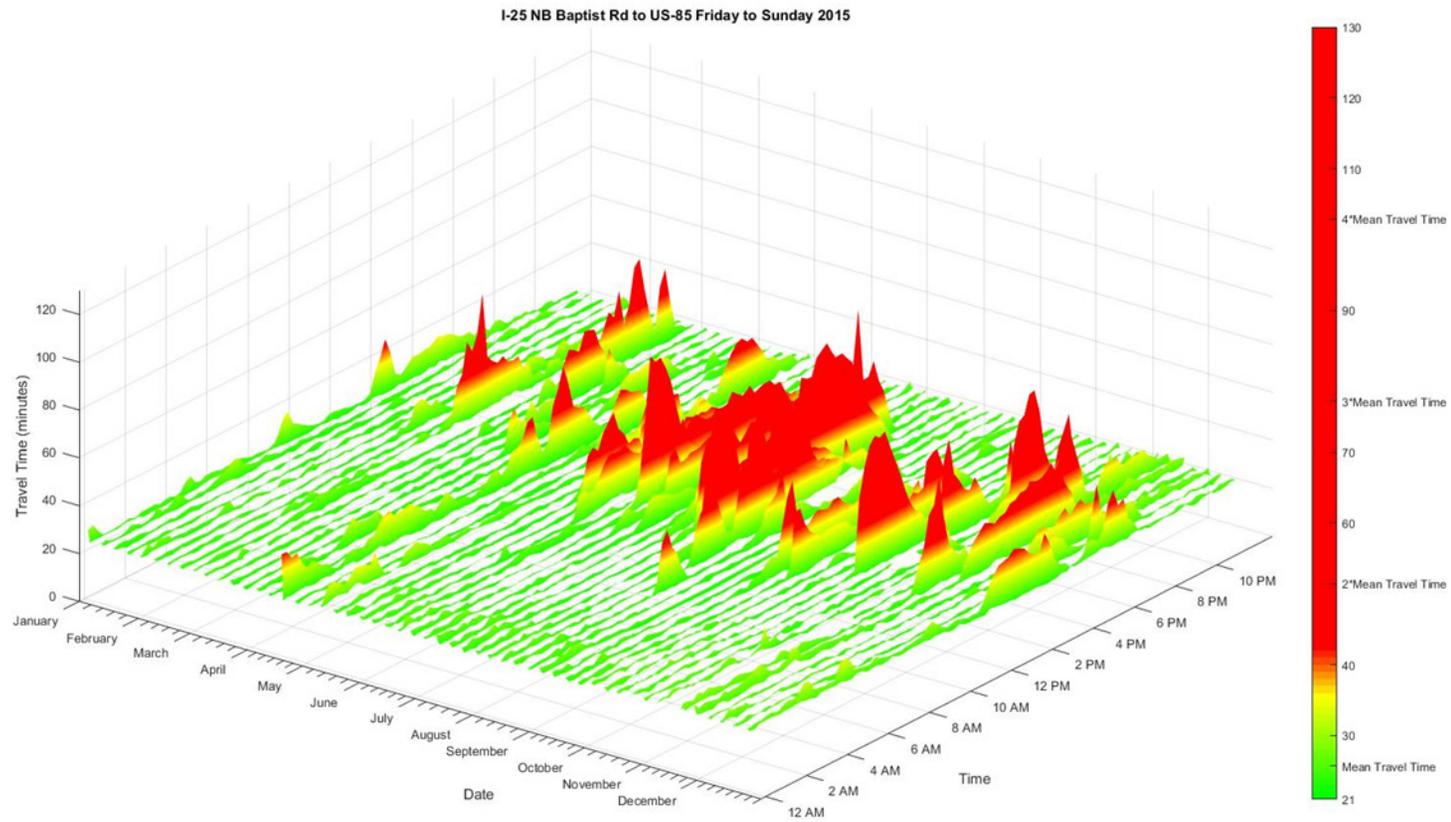


Figure 12. Southern Segment – Northbound Weekends (Fri-Sat-Sun): 2015

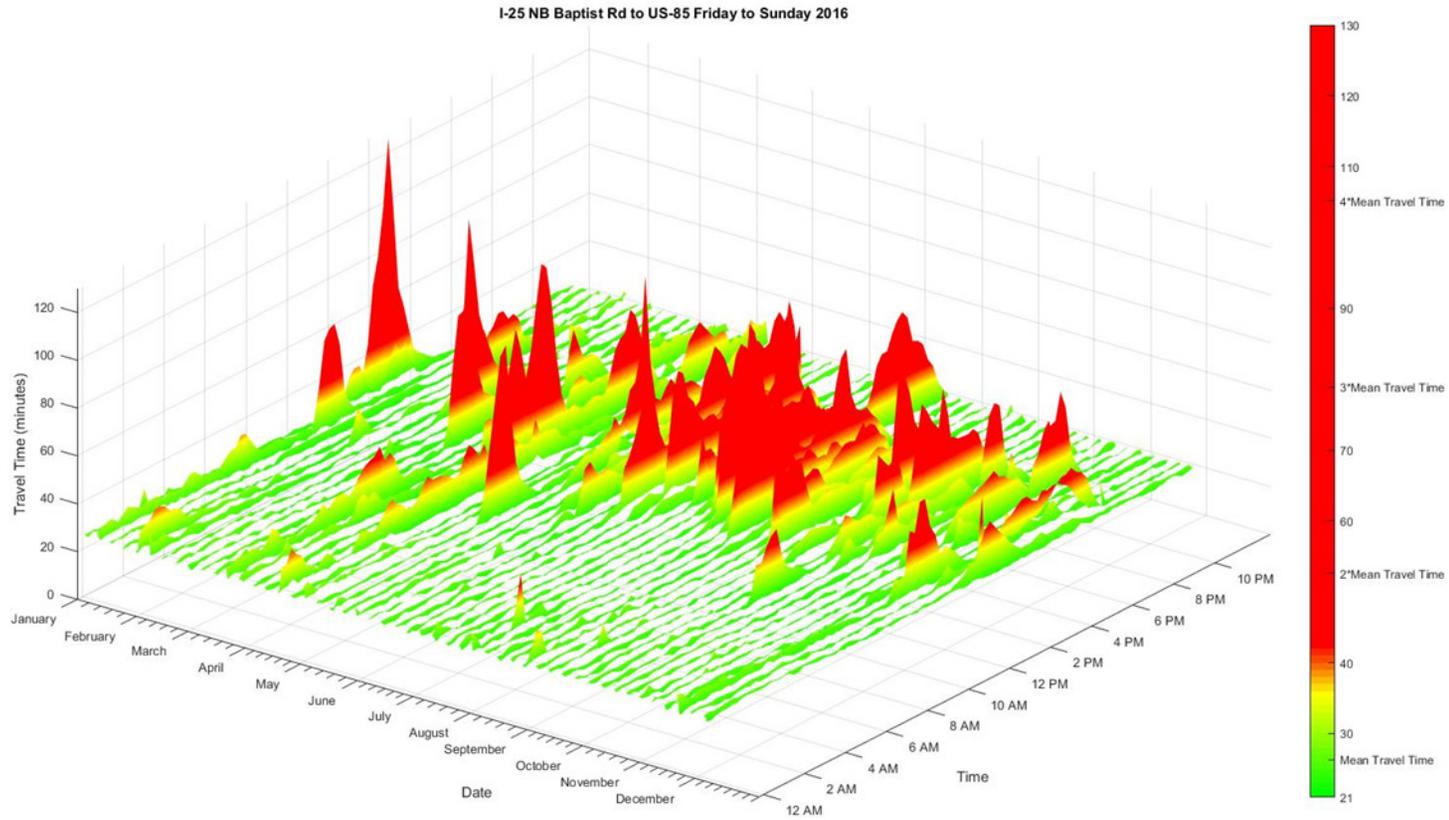


Figure 13. Southern Segment – Northbound Weekends (Fri-Sat-Sun): 2016

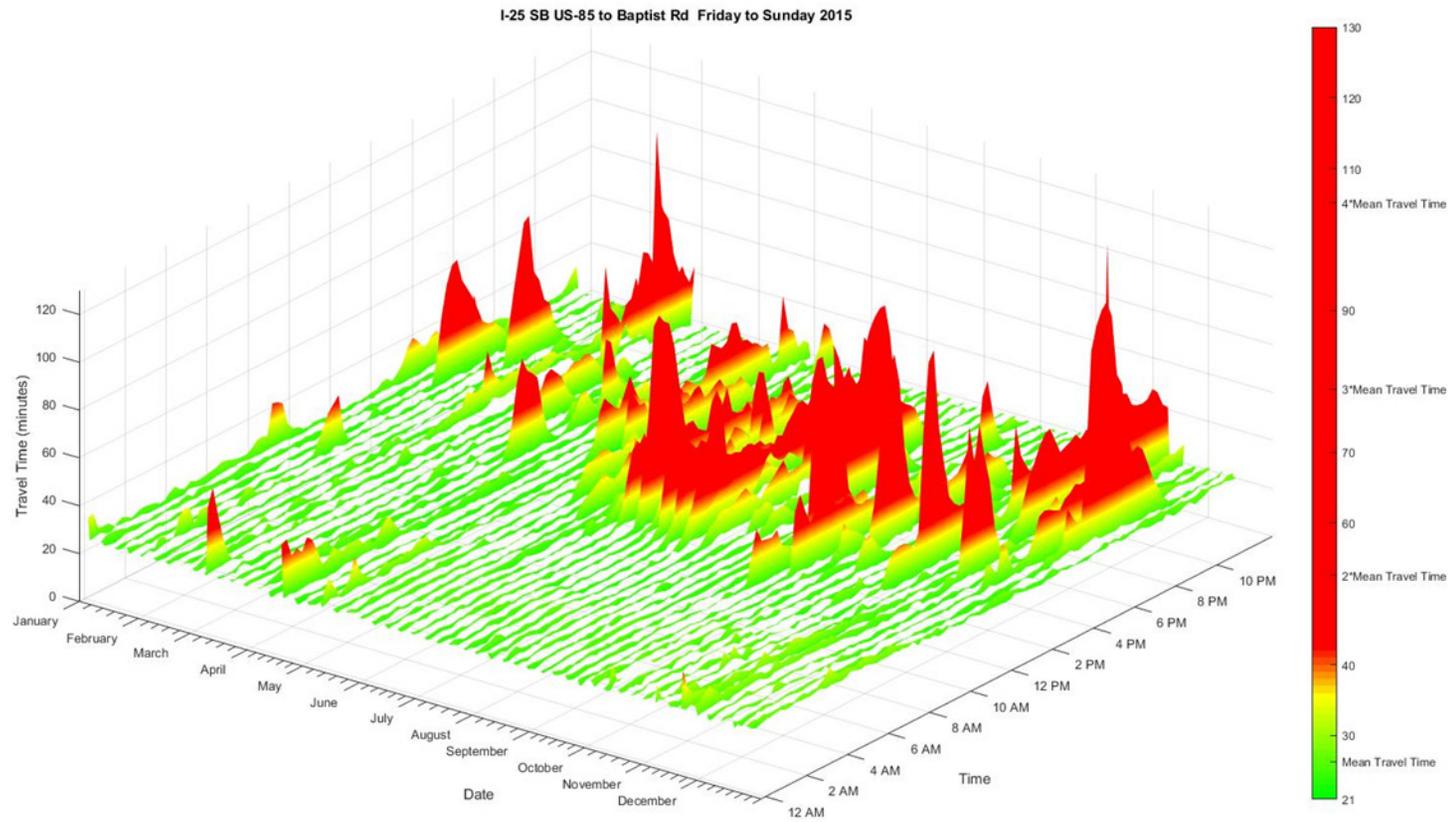


Figure 14. Southern Segment – Southbound Weekends (Fri-Sat-Sun): 2015

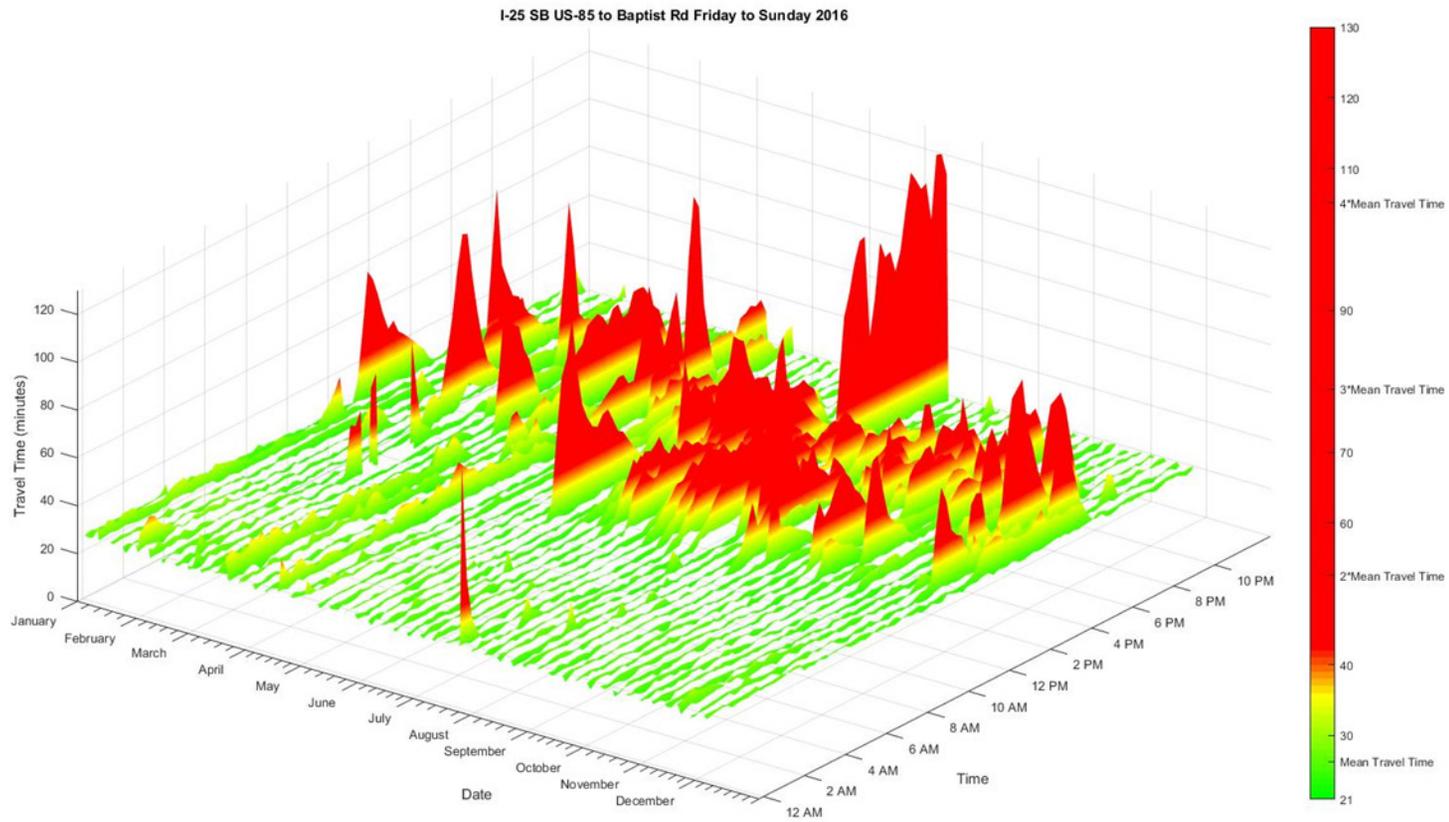


Figure 15. Southern Segment – Southbound Weekends (Fri-Sat-Sun): 2016

Causal Factor Analysis for Fair or Poor Days of 2015

Event Type		Total Year				Day of Week							
		Count		% of Total Number of Events		Count				% of Total for Event Type			
						Weekday		Weekend		Weekday		Weekend	
Incidents		106		43.6%		44		62		41.5%		58.5%	
Weather		17		7.0%		10		7		58.8%		41.2%	
Planned/Special Event		12		4.9%		9		3		75.0%		25.0%	
Unaccounted/ Congestion	Monday	13	108	5.3%	44.4%	13	59	-	49	12.0%	54.6%	-	45.4%
	Tuesday	16		14.8%		16		-		14.8%		-	
	Wednesday	13		5.3%		13		-		12.0%		-	
	Thursday	17		7.0%		17		-		15.7%		-	
	Friday	17		7.0%		-		17		-		15.7%	
	Saturday	10		4.1%		-		10		-		9.3%	
	Sunday	22		9.1%		-		22		-		20.4%	
TOTAL		243		100.0%		122		121		50.2%		49.8%	

Figure 16. Southern Segment Causal Factors 2015

Causal Factor Analysis for Fair or Poor Days of 2016

Event Type		Total Year				Day of Week							
		Count		% of Total Number of Events		Count				% of Total for Event Type			
						Weekday		Weekend		Weekday		Weekend	
Incidents		96		36.8%		34		62		35.4%		64.6%	
Weather		10		3.8%		6		4		60.0%		40.0%	
Planned/Special Event		25		9.6%		20		5		80.0%		20.0%	
Unaccounted/ Congestion	Monday	20	130	7.7%	49.8%	20	73	-	57	15.4%	56.2%	-	43.8%
	Tuesday	16		12.3%		16		-		12.3%		-	
	Wednesday	20		7.7%		20		-		15.4%		-	
	Thursday	17		6.5%		17		-		13.1%		-	
	Friday	21		8.0%		-		21		-		16.2%	
	Saturday	15		5.7%		-		15		-		11.5%	
	Sunday	21		8.0%		-		21		-		16.2%	
TOTAL		261		100.0%		133		128		51.0%		49.0%	

Figure 17. Southern Segment Causal Factors 2016

B. Northern Segment

Unlike the southern segment, the northern segment of the corridor operated more like a typical suburban commuter corridor with predominant northbound AM peak period traffic and southbound PM peak period traffic. Figures 18 through 31, display the travel time reliability analysis for the northern segment. As shown in Figure 18, most of Poor travel times (more than twice as long as free-flow travel time) in the northbound northern segment occurred in the morning hours and were fairly equally distributed throughout the year. The same general trend appeared in 2016, as shown in Figure 19, except the duration of Poor travel reliability and number of Poor travel days decreased in 2016. This decrease from 2015 to 2016 can be attributed to the completion of the “I-25 lane balance” project which constructed an additional lane on I-25 in each direction from Ridge Gate Parkway to C470. A similar, but less significant, decrease also occurred southbound between 2015 and 2016 as shown in Figures 20 and 21

Like the southern segment, the travel time reliability data was further stratified into weekdays (Monday through Thursday), and weekends (Friday through Sunday) for the northern segment. The effects of the lane balance project were equally clear upon review of the weekday travel time reliability as shown in Figures 22 through 25. The commuter peak periods were still observed but the occurrence of Fair and Poor travel time reliability was reduced from 2015 to 2016. There were a few exceptions which were attributable to incidents, weather, or planned/special events.

As shown in Figures 26 through 29, there was not the same type of weekend travel time reliability issue in the northern segment that were observed in the southern segment.

Figures 30 and 31 show the causal factor analysis for 2015 and 2016, respectively. The days with Fair or Poor travel time reliability were investigated to determine if specific factors contributed to the changes in reliability. The same methodology used for the southern segment analysis was applied to the northern segment.

In 2015, more than 85% of the days of the year (312) had at least one 15-minute period experiencing Fair or Poor travel time reliability. This number decreased to 256, or 71% of the days of the year in 2016. This decrease is attributable to the completion of the lane balance project. On a percentage basis, the causal factors remained about the same between 2015 and 2016 with congestion comprising approximately 50% of the days and incidents accounting for roughly 36% of the days of Fair or Poor travel time reliability.

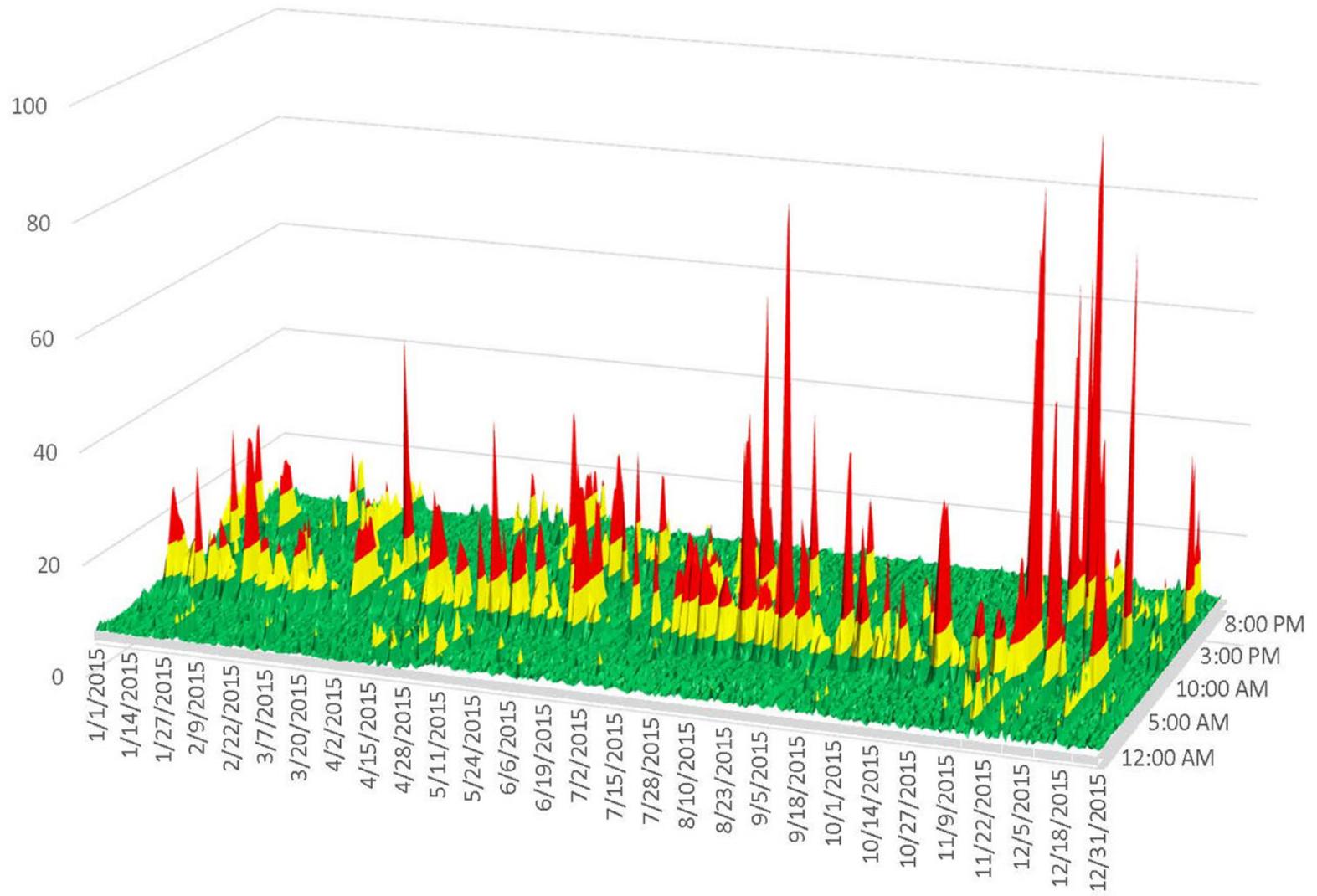


Figure 18. Northern Segment – Northbound All Days in 2015

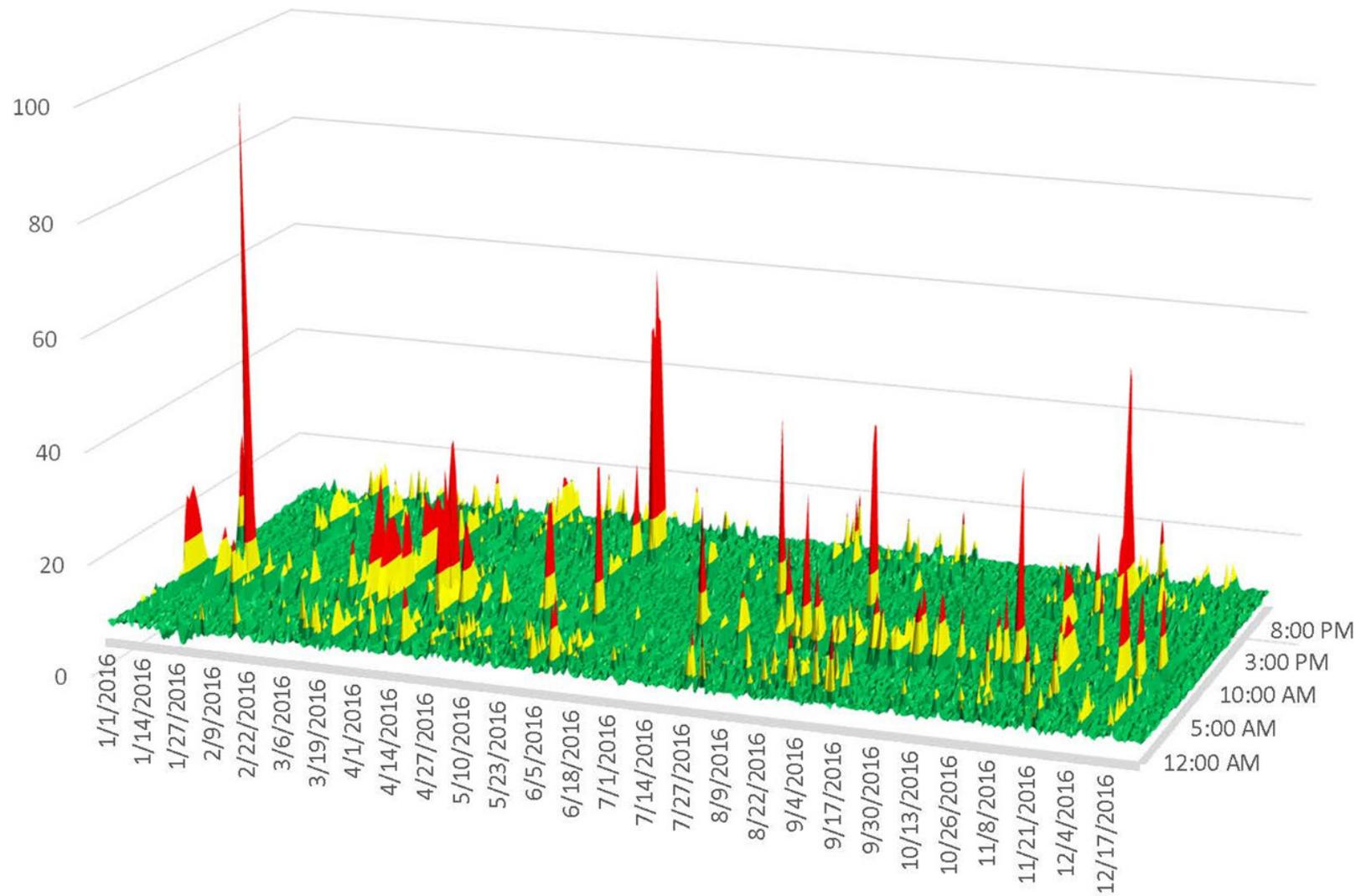


Figure 19. Northern Segment – Northbound All Days in 2016

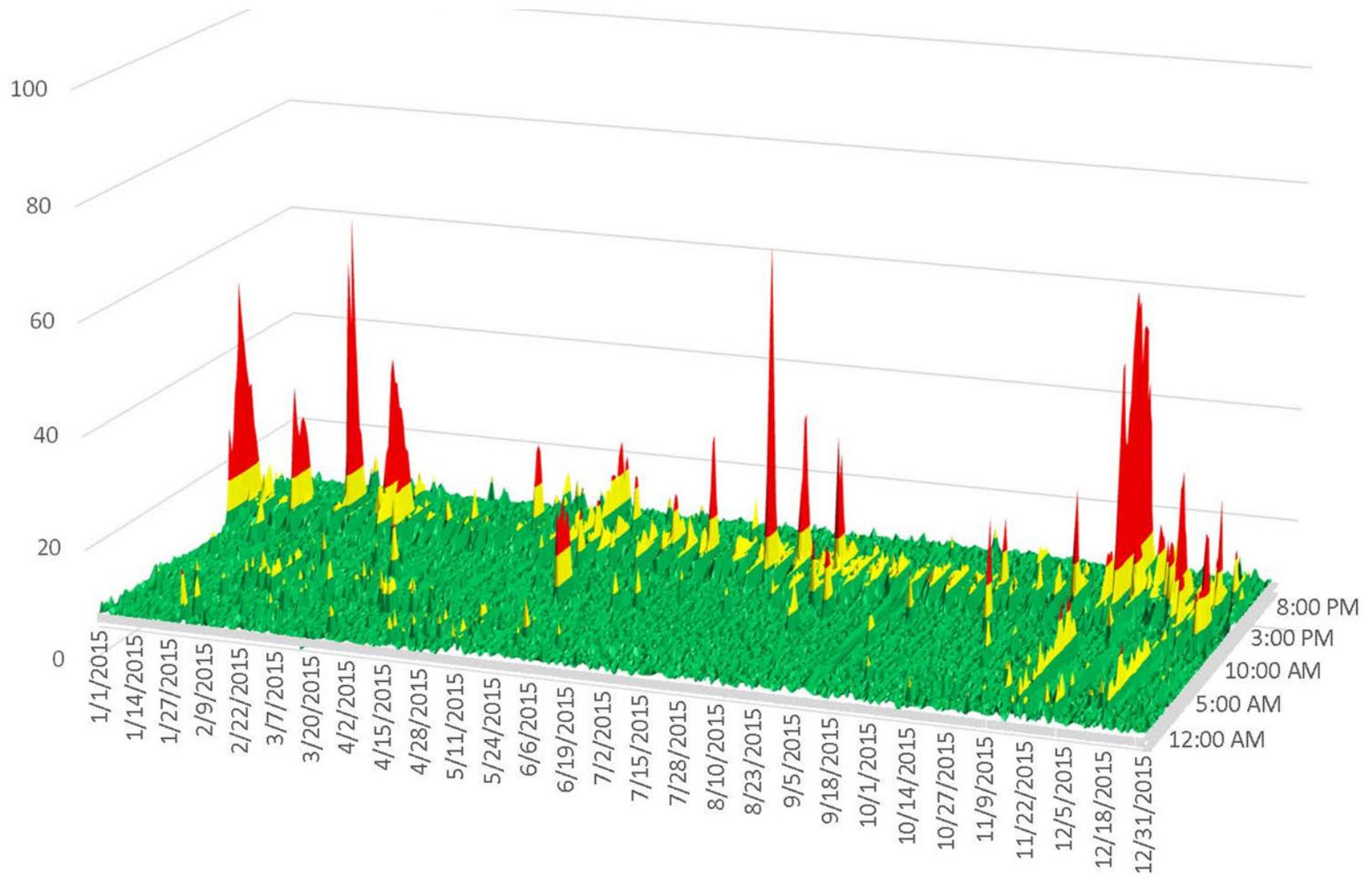


Figure 20. Northern Segment – Southbound All Days in 2015

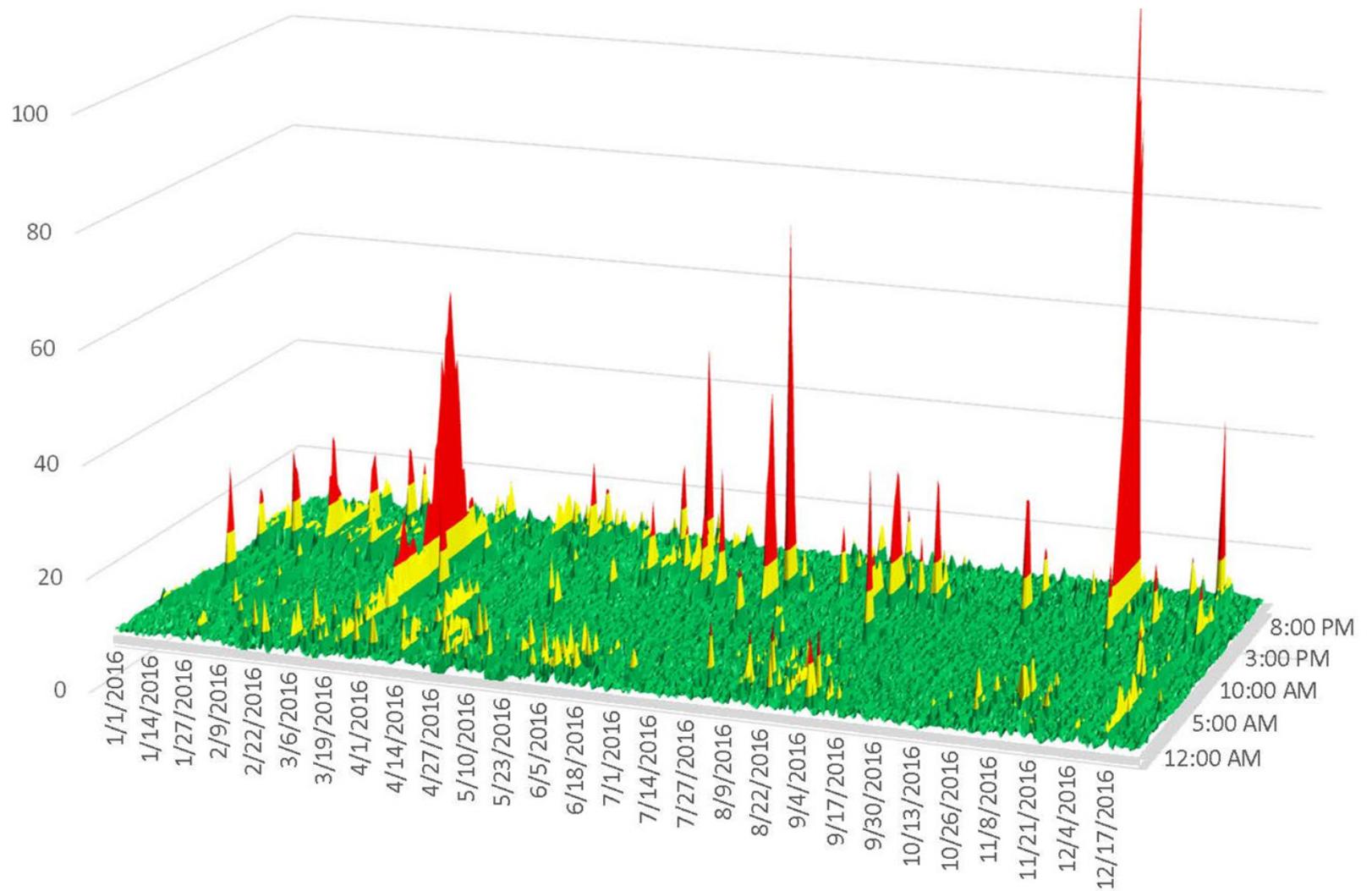


Figure 21. Northern Segment – Southbound All Days in 2016

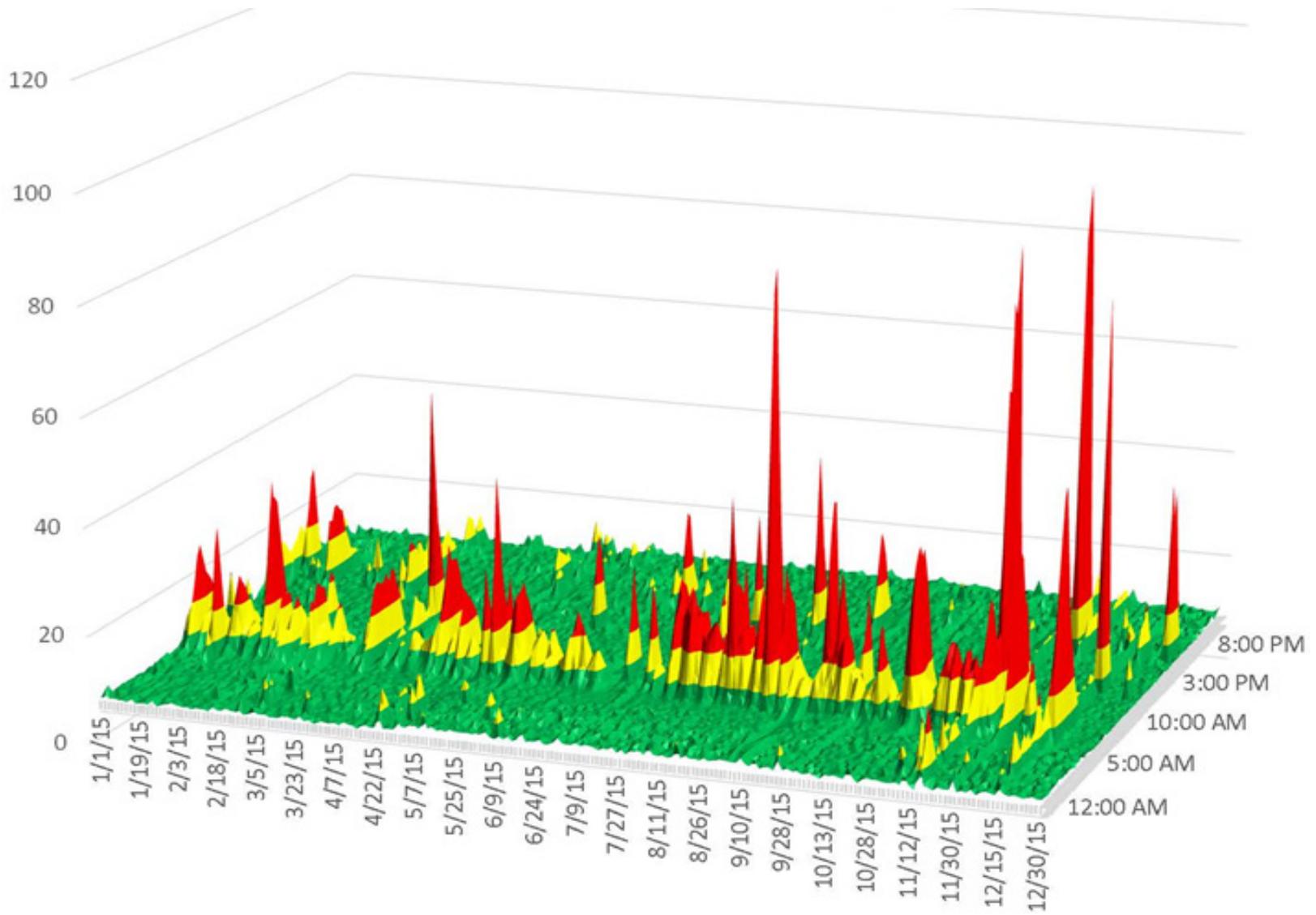


Figure 22. Northern Segment – Northbound Weekdays (M-R): 2015

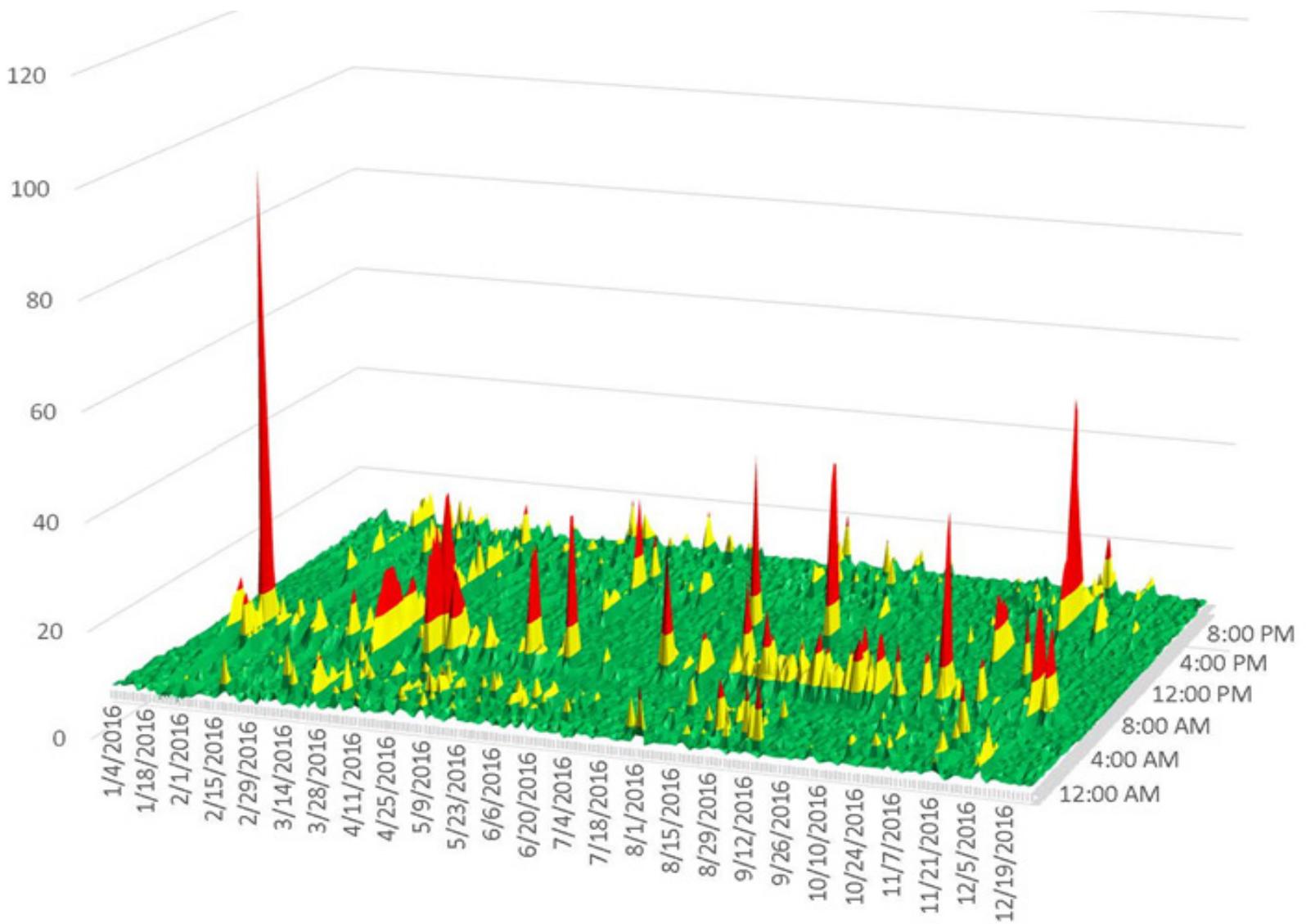


Figure 23. Northern Segment – Northbound Weekdays (M-R): 2016

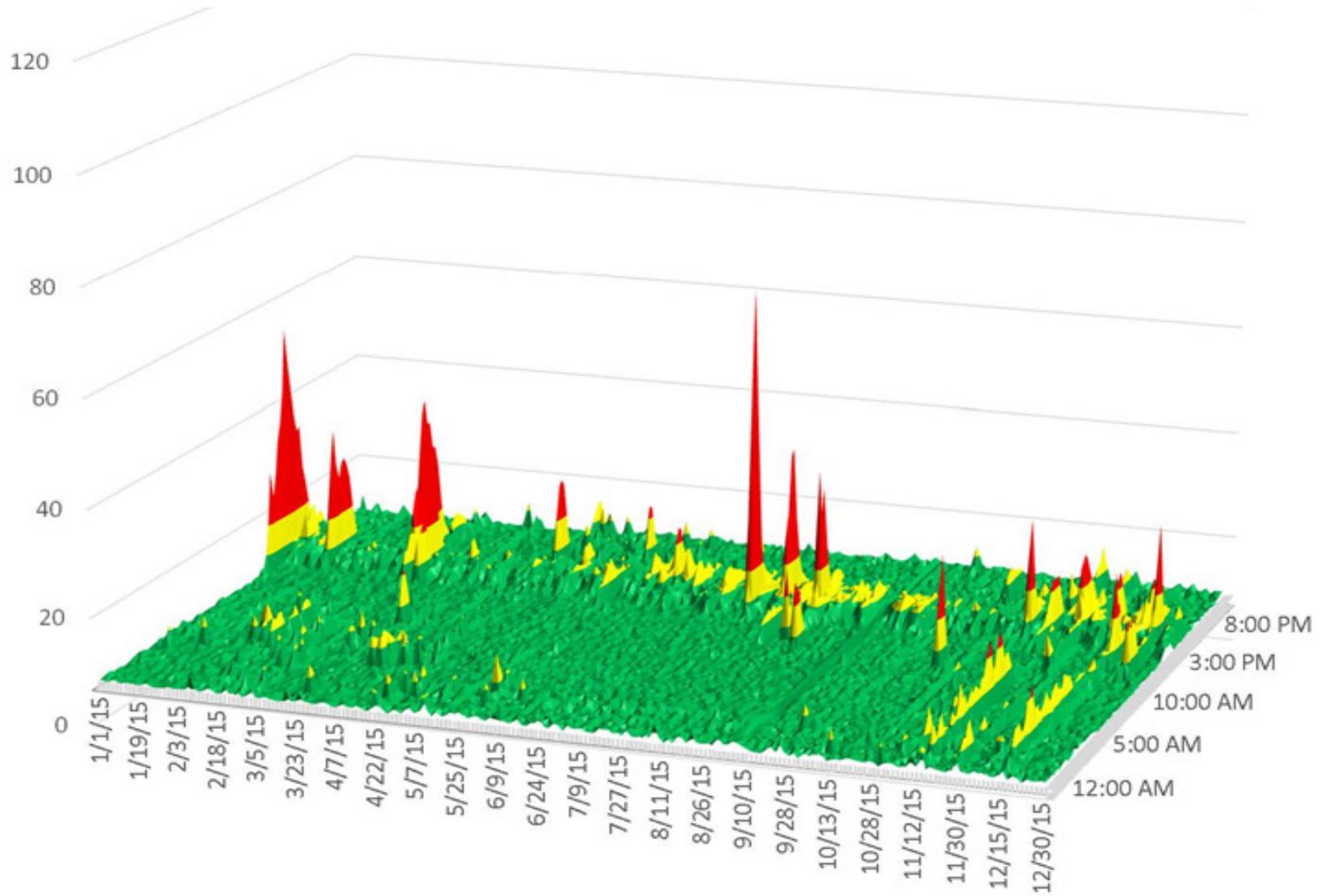


Figure 24. Northern Segment – Southbound Weekdays (M-R): 2015

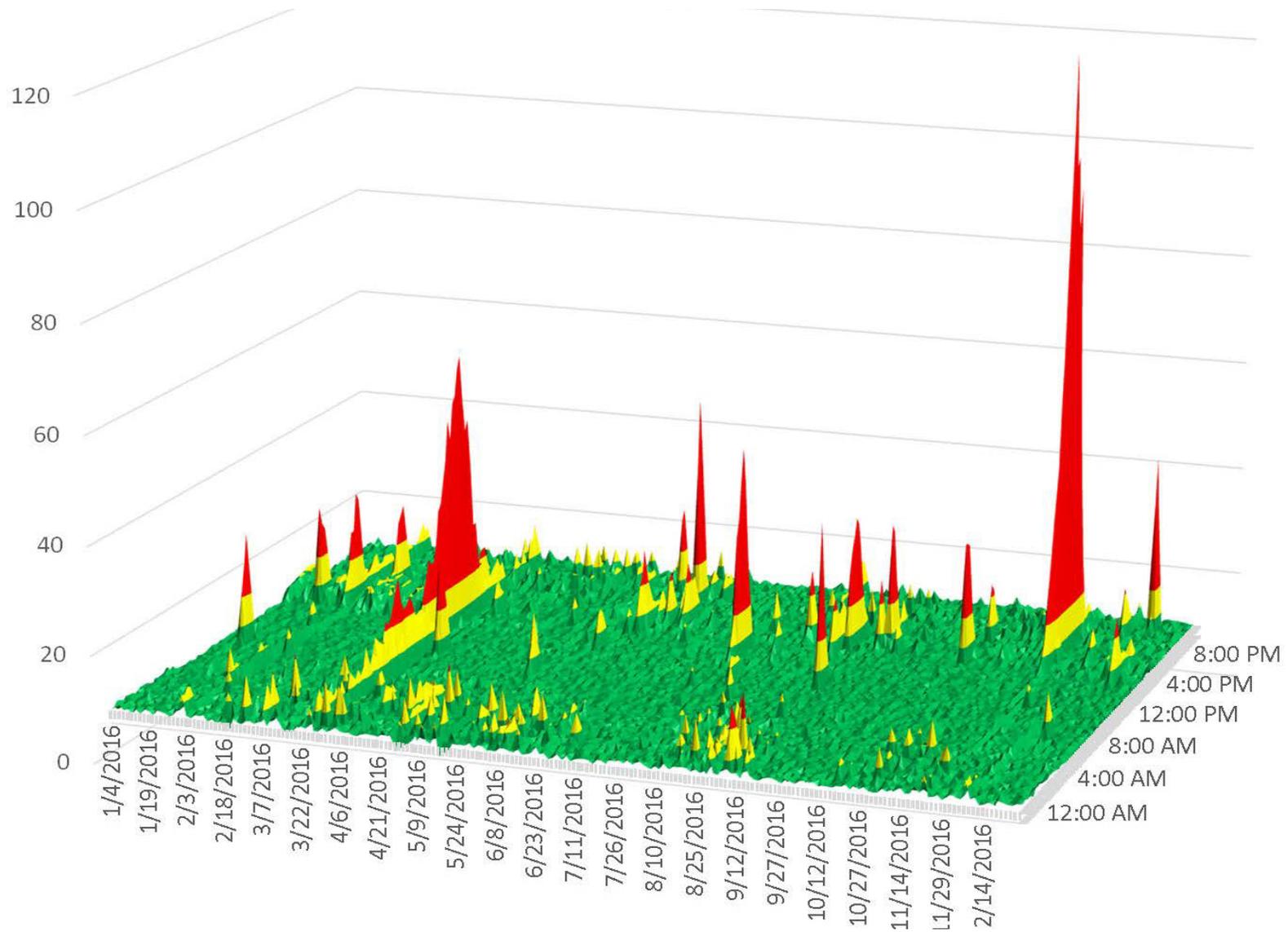


Figure 25. Northern Segment – Southbound Weekdays (M-R): 2016

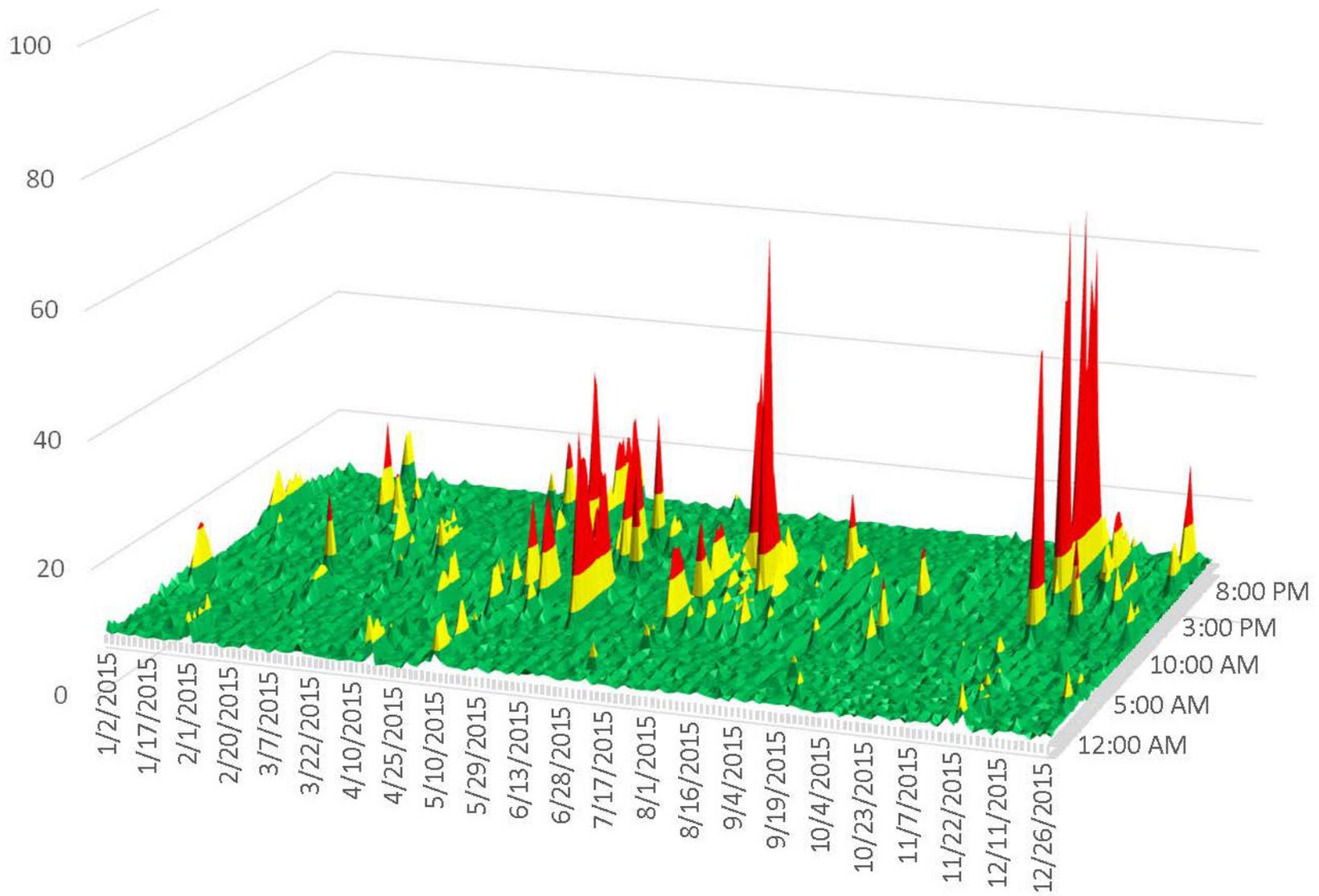


Figure 26. Northern Segment – Northbound Weekends (Fri-Sat-Sun): 2015

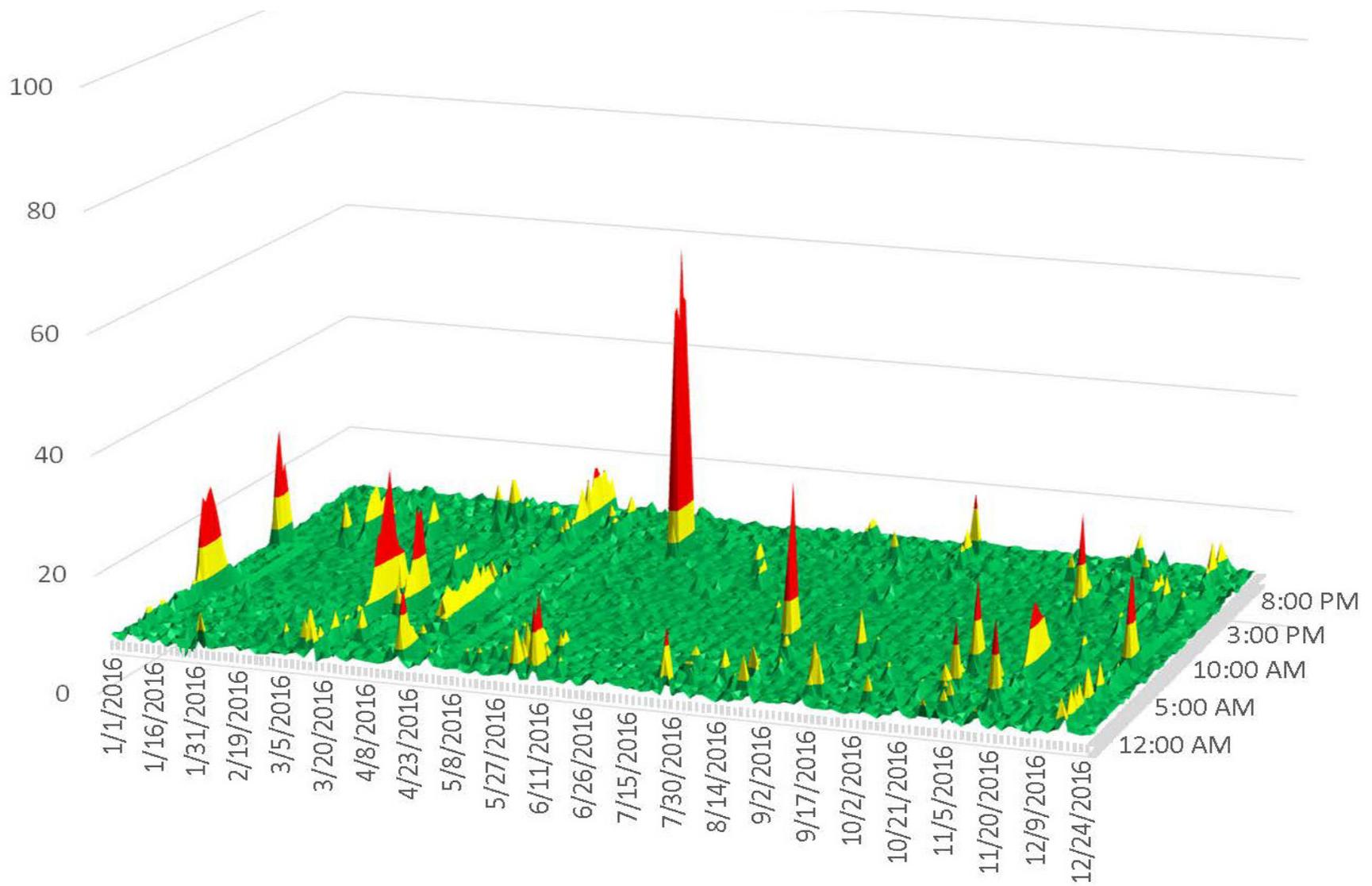


Figure 27. Northern Segment – Northbound Weekends (Fri-Sat-Sun): 2016

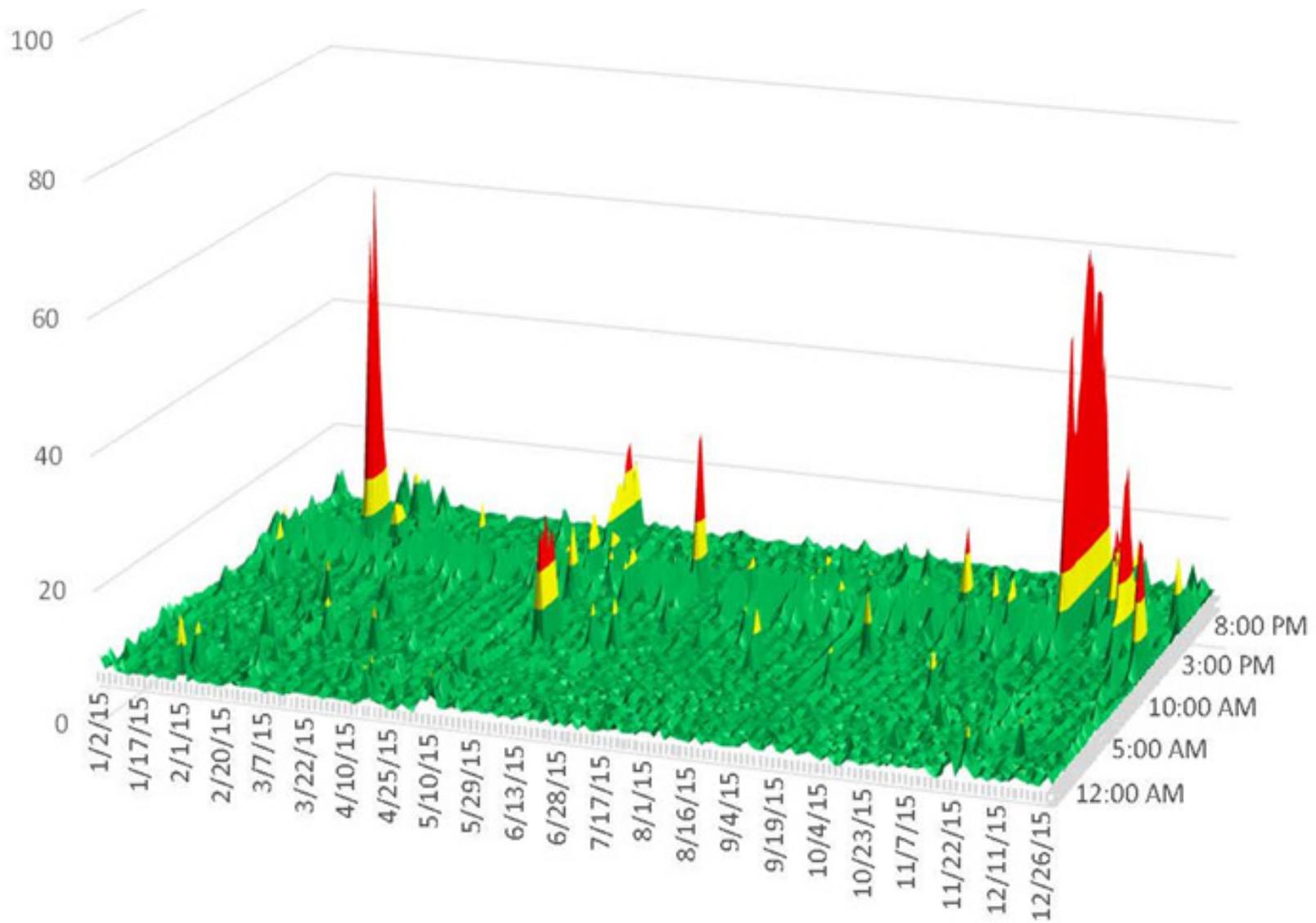


Figure 28. Northern Segment – Southbound Weekends (Fri-Sat-Sun): 2015

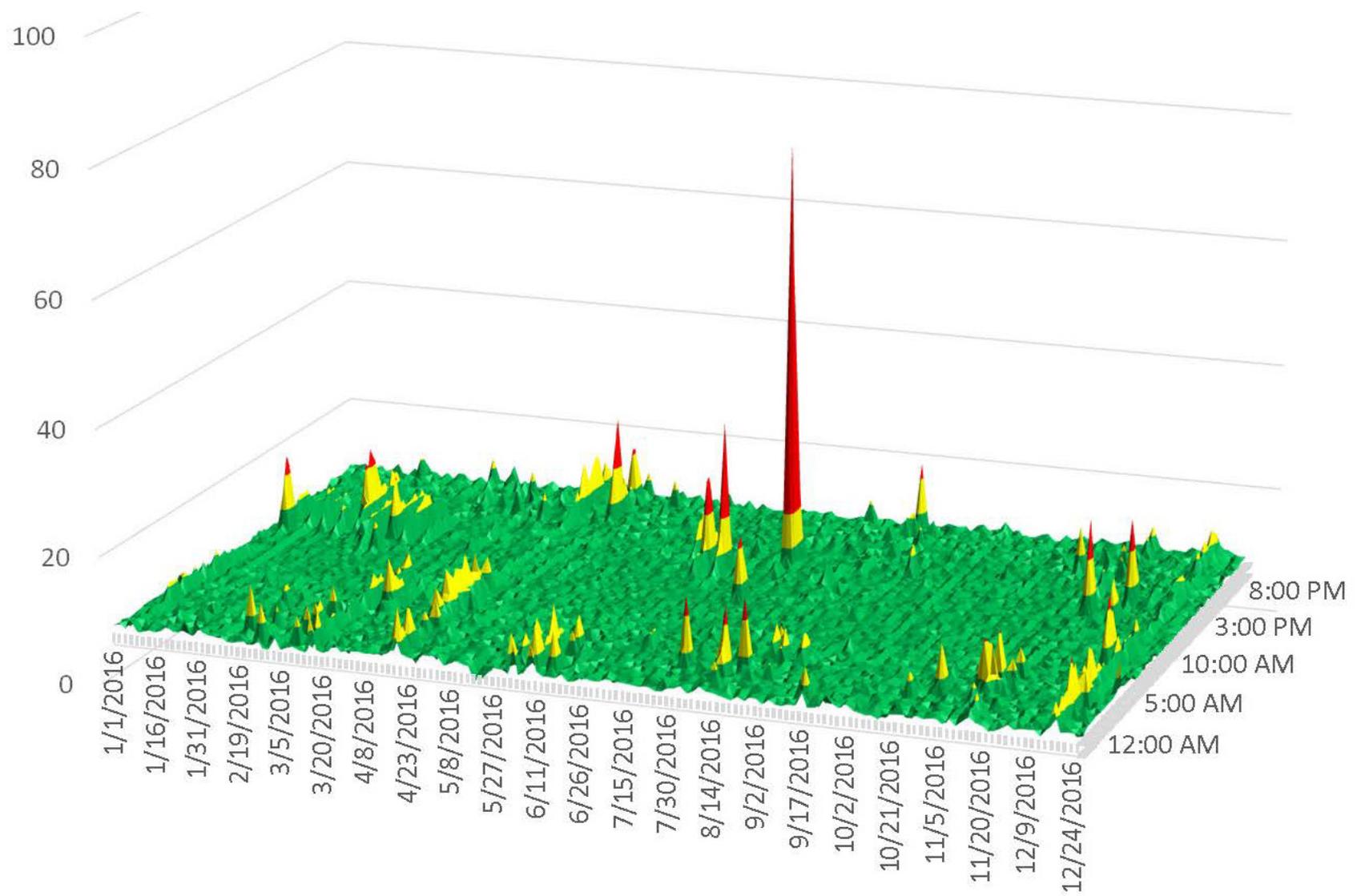


Figure 29. Northern Segment – Southbound Weekends (Fri-Sat-Sun): 2016

Causal Factor Analysis for Fair or Poor Days of 2015

Event Type		Total Year				Day of Week							
		Count		% of Total Number of Events		Count				% of Total for Event Type			
						Weekday		Weekend		Weekday		Weekend	
Incidents		111		35.6%		69		42		62.2%		37.8%	
Weather		11		3.5%		4		7		36.4%		63.6%	
Planned/Special Event		37		11.9%		28		9		75.7%		24.3%	
Unaccounted/ Congestion	Monday	22	153	7.0%	49.0%	22	109	-	44	14.4%	71.2%	-	28.8%
	Tuesday	28		9.0%		28		-		18.3%		-	
	Wednesday	37		11.9%		37		-		24.1%		-	
	Thursday	22		7.0%		22		-		14.4%		-	
	Friday	17		5.5%		-		17		-		11.1%	
	Saturday	16		5.1%		-		16		-		10.5%	
	Sunday	11		3.5%		-		11		-		7.2%	
TOTAL		312		100%		210		102		67.3%		32.7%	

Figure 30. Northern Segment Causal Factors 2015

Causal Factor Analysis for Fair or Poor Days of 2016

Event Type		Total Year				Day of Week							
		Count		% of Total Number of Events		Count				% of Total for Event Type			
						Weekday		Weekend		Weekday	Weekend		
Incidents		91		35.5%		60		31		65.9%	34.1%		
Weather		1		0.4%		0		1		0%	100%		
Planned/Special Event		36		14.1%		29		7		80.6%	19.4%		
Unaccounted/ Congestion	Monday	19	128	7.4%	50.0%	19	85	-	43	14.8%	66.4%	-	33.6%
	Tuesday	27		21.1%		27		-		21.1%		-	
	Wednesday	17		6.7%		17		-		13.3%		-	
	Thursday	22		8.6%		22		-		17.2%		-	
	Friday	22		8.6%		-		22		-		17.2%	
	Saturday	6		2.3%		-		6		-		4.7%	
	Sunday	15		5.7%		-		15		-		11.7%	
TOTAL		256		100%		174		82		68.0%	32.0%		

Figure 31. Northern Segment Causal Factors 2016

4. Conclusions

For the southern segment, the Poor travel time reliability occurred northbound on weekends during summer and fall afternoons as well as southbound weekday evenings. From 2015 to 2016, there was a 7% increase in the total number of days with Fair or Poor travel times and congestion-related increases in travel time grew by 20%. The increased travel times as a result of incidents predominantly occurred on weekends. Figure 32 summarizes the causal factors evaluation for the southern segment.

Event Type	Number of Days		Percentage of Days		Change
	2015	2016	2015	2016	From 2015 to 2016
Incidents	106	96	29%	28%	
Weather	17	10	5%	3%	
Special Events	12	25	3%	7%	
Congestion	108	130	30%	38%	
Uncongested	122	83	33%	24%	
Total Days Analyzed	365	344	100%	100%	

Figure 32. Causal Factor Summary: Southern Segment

For the northern segment, the Poor travel time reliability occurred northbound during AM weekday peaks and southbound PM weekday peaks. From 2015 to 2016, there was an 18% decrease in the total number of days with Fair or Poor travel times and a 16% decrease in the number of congested days. Both of these decreases can be primarily attributed to the completion of the I-25 Lane Balance Project. Figure 33 summarizes the causal factors evaluation for the northern segment.

Event Type	Number of Days		Percentage of Days		Change
	2015	2016	2015	2016	From 2015 to 2016
Incidents	111	91	30%	25%	
Weather	11	1	3%	1%	
Special Events	37	36	10%	10%	
Congestion	153	128	42%	35%	
Uncongested	53	107	15%	29%	
Total Days Analyzed	365	363	100%	100%	

Figure 33. Causal Factor Summary: Northern Segment

Appendix A5
EA Alternatives Analysis
and Comparison

Acronyms and Abbreviations

C-470	Colorado State Highway 470
CDOT	Colorado Department of Transportation
DRCOG	Denver Regional Council of Governments
EA	Environmental Assessment
EL	Express Lane
FHWA	Federal Highway Administration
GP	general-purpose lane
I-25	Interstate 25
INFRA	Infrastructure for Rebuilding America
MP	milepost
mph	mile per hour
NEPA	National Environmental Policy Act
NB	northbound
N/O	north of
OP	off peak (time period)
PEL	Planning and Environmental Linkages
SB	southbound
S/O	south of

I-25 South Gap Environmental Assessment Alternatives Analysis

PREPARED FOR: Colorado Department of Transportation
COPY TO: Federal Highway Administration
PREPARED BY: Peak Consulting Group and Jacobs Engineering
DATE: April 7, 2018
REVISION NO.: 1

This memorandum describes how alternatives were developed for the I-25 South Gap Project and presents the analysis of the two primary operating alternatives considered for the project:

- General-Purpose Lane Alternative: operate the new capacity as a general-purpose lane available to all traffic.
- Express Lane alternative: operate the new capacity as Express Lanes with conditions for their use.

The assessment determined how the new lane would operate: whether as a general-purpose lane or an Express Lane. These operational scenarios are referred to as the General-Purpose Lane alternative and the Express Lane alternative. Although these are presented and viewed by stakeholders as different alternatives, most of the project elements between the two alternatives are the same; the main difference is in how the proposed new third travel lane in each direction would operate.

1. Background

The I-25 South Gap: Monument to Castle Rock Project stemmed from an ongoing Planning and Environmental Linkages (PEL) study, which is considering long-term solutions for travel needs along Interstate 25 (I-25) between Colorado Springs and Denver. The I-25 PEL corridor extends from Monument, which is located north of Colorado Springs, to Colorado State Highway 470 (C-470) in the south Denver metropolitan area. The southern segment of the PEL corridor is an 18-mile bottleneck along I-25 that is often referred to as “the Gap” because it is a narrow, four-lane section (two lanes in each direction) of the interstate adjoining the urban areas, which are served by six or more travel lanes. Figure 1 details the project location and I-25 PEL and South Gap corridors.

In response to significant public and political interest from the onset of the PEL study, in January 2017, the Colorado Department of Transportation (CDOT) and the Federal Highway Administration (FHWA) announced they would initiate design and environmental studies for a project in the Gap segment concurrently with the broader PEL study to allow project construction to be accelerated to November 2019 if funding for a Gap project could be secured. By October 2017, a potential funding package of state and local sources had been preliminarily identified for a comprehensive construction project in the Gap segment. A federal notice of funding availability for a new/revamped grant program (Infrastructure for Rebuilding America or INFRA) was announced in July 2017 with a November 2017 deadline. Taking advantage of the potential to bridge and complete the funding for the project, CDOT committed to further advancing the Gap project design and environmental clearance to have a construction-ready project by November 2018 or sooner to be in a position to accept these federal funds should the grant application be successful. To meet this deadline, CDOT focused all project development efforts on the

Gap project and extended the PEL study schedule to allow more time to determine regional corridor priorities beyond the Gap project.

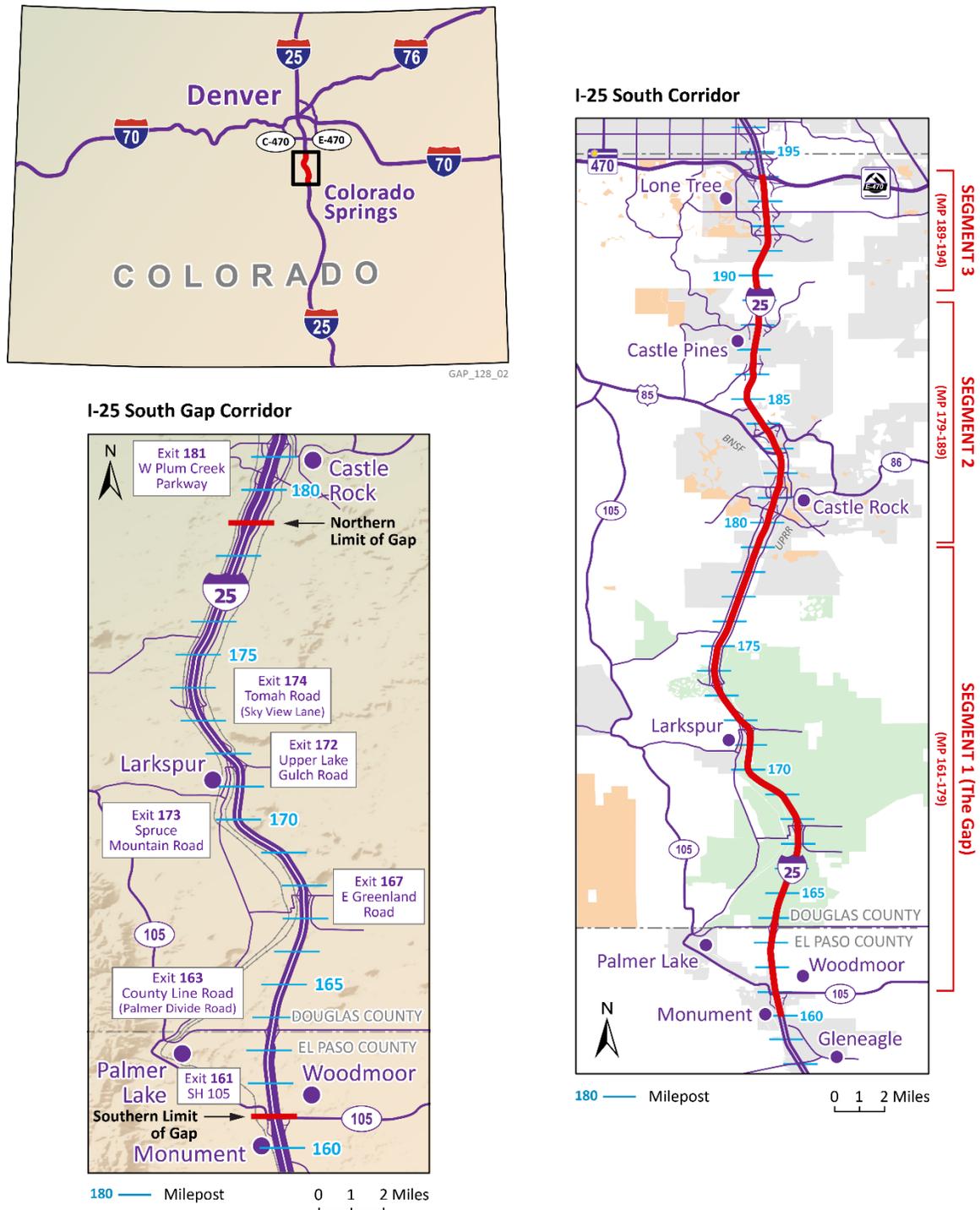


Figure 1. I-25 South PEL Corridor Location

A. I-25 South Gap Purpose and Need

The I-25 South Gap: Monument to Castle Rock Project purpose and need was refined from the PEL purpose and need to focus on the specific issues of travel through the Gap. Safety and reliability needs were the same, but the mobility criterion was refined to address travel delays, and incident management was elevated as a separate need. Both these refinements recognized the unique issues associated with

traveling through the Gap because of the lack of alternate routes, frontage roads, and safe locations for drivers or workers to pull off the interstate.

1. Purpose

The purpose of the I-25 South Gap Project is to enhance safety, reduce delays, and improve reliability for travel on I-25 through the 18-mile, four-lane bottleneck (the Gap) between Colorado Springs and the Denver South region.

2. Needs

Improvements are needed on I-25 to address issues with safety and efficiency of travel through the Gap. This vital segment of the Colorado interstate system remains as it was configured when it was originally constructed in the 1960s, when the population was measured in thousands, not millions, and suburban neighborhoods and towns south of Denver and north of Colorado Springs were yet to be envisioned.

The two lanes in each direction, combined with narrow shoulders, create a bottleneck for travel between the adjoining three-lane sections to the north and south. As traffic volumes have grown, delays have increased substantially and are often measured in hours. On average, crashes in the corridor occur daily. When crashes occur, the narrow shoulders limit the ability to move disabled vehicles from the travel lanes, often resulting in a single lane of travel or full closure and contributing to large delays. Responders require more time to reach incident scenes, lengthening the time required to return traffic to normal flow and delaying care for those involved in crashes. Specific needs are summarized below. The steady southbound climbing grades of approximately 1,000 feet in elevation that is gained between Castle Rock and the crest of Monument Hill (elevation 7,352 feet) presents challenges for slow-moving vehicles and causes issues with speed differentials and passing.

3. Improve safety and reduce crashes

A high number of crashes occur on the corridor—averaging nearly one per day. Crashes have consistently increased over the past 5 years, and CDOT’s safety analysis suggests the entire length of the Gap has the potential for safety to be improved. Fatalities in the corridor have also consistently increased, which is a substantial concern for both CDOT and the public. Higher than expected crashes occur due to weather, conflicts, and darkness.

Crashes also have a substantial effect on travel delays in the corridor. When crashes occur, the narrow shoulders, coupled with guardrail, limit the ability to move disabled vehicles from the travel lanes, which often results in lane or even highway closures as crashes are investigated and cleared. The inability to move crashes outside the travel lanes also contributes to secondary crashes (crashes that occur in the congested conditions that result from the initial crash).

4. Enhance safety and effectiveness of incident management, enforcement, and maintenance

Emergency response, roadway maintenance, and law enforcement activities in the Gap are challenging because the narrow shoulders do not provide adequate space to stage operations safely, store or recover disabled vehicles, or park vehicles sheltering from severe weather. The result is that highway workers are at risk, the traveling public is inconvenienced by lane closures, and law enforcement cannot patrol effectively. Further, the lack of emergency detours makes it difficult to provide or communicate reliable information to travelers so they could avoid incident scenes and associated disruption to travel flow.

5. Reduce travel delays

The Gap segment of I-25 does not function like a typical commuting corridor, which experiences the weekly peak traffic demand during weekday morning and evening rush-hour periods. Rather, peak traffic occurs on the weekends (Friday to Sunday). Unexpected and unreasonably long traffic delays are increasingly common, and delays are getting longer, particularly on weekends when the demand is greater. With no reasonable alternate routes or other reliable travel options, drivers have little option

but to be stuck in backups, some of which last for hours. Delays increase in congested conditions, which inhibit free-flow speed and the ability to maneuver around slower-moving vehicles.

6. Improve travel reliability

Travel times in the corridor are highly variable, ranging from approximately 20 minutes in free-flow traffic to several hours after a serious crash has occurred. The lack of reliability or predictability of travel times is especially frustrating for time-sensitive trips, such as travelling to Denver International Airport for a flight. In planning time-sensitive trips, travelers spend a lot of time waiting, whether in traffic if volumes are high or an incident occurs, or at their destination if traffic is flowing efficiently. Compromised roadway and weather conditions also contribute to variability in travel time through the Gap because of the need to adjust speeds to safely navigate this rural area and steady elevation change. Fluctuating volume demands (congestion and the mix of vehicle types within the traffic stream) can restrict the ability to maneuver around other vehicles and maintain a consistent rate of speed, leading to variable travel times.

B. Logical Termini and Independent Utility

To ensure that transportation projects are not improperly segmented, FHWA regulations require that project segments have logical termini (rational start and end points), independent utility (be usable and reasonable even if no additional transportation improvements are made), and not restrict the consideration of alternatives for other reasonably foreseeable transportation improvements (not create problems up- or downstream and/or force additional improvements outside the project area).

In separating out improvements for the Gap, the project limits were reviewed to establish logical termini or end points for starting and ending improvements. The review confirmed the Gap segment limits identified in the PEL were logical based on traffic and land use patterns in the Gap.

The I-25 South Gap Project would add capacity and improve infrastructure deficiencies along the southern segment of the PEL corridor of I-25 between milepost (MP) 161 (State Highway [SH] 105) and MP 179, as defined by the extents of the existing narrowing roadway section, where the highway tapers from three to two lanes (**Figure 1**).

- On the southern end of the project corridor, the narrowing of the roadway from three to two lanes occurs just north of SH 105 in the southbound direction; the northbound direction of the interstate is wider in some locations between MP 161 and MP 163 due to an existing truck climbing lane. The project would tie into the recently completed (2014) expansion of I-25 from two to three lanes south of SH 105 into Colorado Springs.
- On the northern end of the corridor, the project would also tie into existing three lanes at approximately MP 179, south of the Plum Creek Parkway interchange. The northern project limit is defined by the transition in pavement from the original asphalt construction of the Gap area to the more recently improved concrete segment through Castle Rock to the north.

For the Express Lane alternative, the project limits would extend farther north of the proposed capacity improvements to account for advanced signing and pavement markings that would be installed to inform drivers about the presence of and the entrance into the Express Lane. The advance signing and pavement markings would not be required for the General-Purpose Lane alternative.

Eliminating the bottleneck through the Gap has independent utility and would not create issues that would force improvements north or south. Therefore, the I-25 South Gap meets the conditions as an independent project segment to advance into the National Environmental Policy Act (NEPA) process.

2. Alternatives Analysis

The range of alternatives considered in the Environmental Assessment (EA) relied on the range of Core Concepts developed in the PEL Level 2 evaluations, which included four basic categories for adding capacity to both directions of I-25 within the Gap: add an Express Lane, add a general-purpose lane, add a peak period shoulder lane, and add two or more travel lanes (encompassing the varying combinations of two or more travel lanes and/or travel lane and peak period shoulder). Each alternative category was compared to the purpose and need to determine which would be further developed and evaluated in the EA. Through this qualitative comparison and for consistency with regional transportation plan amendments and funding applications, CDOT determined that Express Lanes would be the proposed action evaluated in the EA. The PEL Level 2 evaluation matrices provide additional details regarding the assessment of the lane alternatives from the PEL. The matrices and more information on the PEL alternatives analysis are included in the **PEL Alternatives Analysis Technical Memorandum**, which is included as **Appendix A1** to the EA.

Based on feedback at and after the public meetings, stakeholders, including elected officials, opposed the Express Lane concept and favored different alternatives, either 1) three general-purpose lanes or 2) three general-purpose lanes plus an Express Lane (four lanes). In response, CDOT agreed to evaluate an alternative for three general-purpose lanes along with the Express Lane alternative. This memorandum documents the Express Lane and General-Purpose Lane alternatives evaluation.

A fourth lane was not considered as an EA alternative but will continue to be evaluated when the PEL study resumes. Adding one travel lane in each direction through the Gap best meets the project's purpose of removing the 18-mile bottleneck and addresses the safety, travel delay, incident management, and trip reliability needs of the interstate. The three-lane alternative could be constructed within the CDOT right-of-way. Additionally, many of the challenges that occur in the corridor are incident specific and could be remedied with the addition of wider shoulders to give first responders the ability to move some of these incidents off the main travel thoroughfare. CDOT is proposing to add wider shoulders as part of the Gap improvements to address these issues. While adding a fourth lane is outside the scope and budget for the I-25 South Gap Project and would present problems for travel north and south (would not meet conditions of segmentation), the I-25 South Gap Project does not preclude adding a fourth lane in the future, and, where feasible and practical, the project is being designed to accommodate a fourth travel lane should it be needed in the future.

Why not build four lanes now?

Major travel corridors will not improve over the long term by adding lanes. With continuous population growth and development, simply adding lanes is not a feasible long-term solution. With the limited funds that are available for transportation, CDOT has limited resources to apply to improvements in the Gap. Other needs along the corridor and within the state would need to be evaluated against adding additional capacity to this 18-mile stretch.

CDOT acknowledges the support for a four-lane alternative and has collected many comments on this topic during public meetings/listening sessions, and directly from local government officials. Widening the Gap to four lanes would greatly increase the \$350 million project cost, would likely delay the project's schedule, require right-of-way acquisitions, and cause new transportation problems in the Castle Rock area.

Although not an alternative for the current EA, CDOT is analyzing an alternative that includes four lanes in each direction as part of the long-term vision in the PEL study. The PEL study will resume in Summer 2018 and will help CDOT prioritize the next improvements in the I-25 corridor and identify funding needs for the improvements.

A. Common Elements of the General-Purpose Lane and Express Lane Alternatives

CDOT evaluated two alternatives for the I-25 South Gap Project: an Express Lane alternative and a General-Purpose Lane Alternative. The two alternatives are very similar with common infrastructure elements. The alternatives differ in how they would operate.

The **Express Lane alternative** would involve widening I-25 through the Gap from approximately MP 161 to 179 to add, in each direction, a new 12-foot tolled Express Lane with a 4-foot painted buffer to separate the Express Lane from the general-purpose (non-tolled) lanes. The existing two general-purpose lanes in each direction would remain toll-free.

Transitions for the Express Lane would expand the project north to approximately MP 181 to account for advanced signing and striping to develop the Express Lane; on the southern end of the project, the Express Lane would operate north of County Line Road (MP 163) due physical constraints with the weigh station and grades at Monument Hill. Motorists would also be able to travel in the Express Lanes if they choose to pay a toll or would be able to use the lane for free if they ride the bus or carpool with three or more people (with a switchable transponder). As currently designed, the Express Lane alternative includes 15 miles of Express Lane operation, and approximately 6 miles of operation in the general-purpose lanes. This is due to the operational and physical constraints for developing and transitioning between the Express Lane and existing general-purpose lanes.

The **General-Purpose Lane alternative** would involve widening I-25 through the Gap from approximately MP 161 to 179 to add a new 12-foot general-purpose lane in each direction to provide three general-purpose lanes in each direction. There would be no tolled lanes under this alternative.

B. Infrastructure Components Common to Both Alternatives

Most of the components of the I-25 South Gap Project are the same under both alternatives. Both the Express Lane and General-Purpose Lane alternatives would nearly double the I-25 pavement width in each direction from 36 feet to 67 feet, on average. Widening would occur primarily to the inside of the existing travel lanes to maximize the use of existing right-of-way. Although the Express Lane alternative includes a 4-foot buffer between the Express Lane and existing general-purpose lanes that is not required in the General-Purpose Lane alternative, the General-Purpose Lane alternative does not require less space because the construction requires “inside-out” phasing where the pavement in the center needs to accommodate two lanes of traffic to keep the existing number of lanes of traffic open during construction.

1. Structures

The I-25 bridges over Plum Creek, Greenland Road, and Upper Lake Gulch Road would be replaced, as would the Spruce Mountain Road bridge over I-25. While the bridges are generally old, their primary deficiencies relate to vertical clearance, meaning that they are too short to accommodate trucks. Except for the bridge over Plum Creek, the roadway bridges are associated with interchanges with I-25 and will include lengthened ramps to improve merge distances for traffic entering or exiting I-25 in these locations. The new structures would be both taller and wider to accommodate the larger roadway footprint. The bridge spans (widths) are being designed to allow flexibility for future expansion if additional lanes are warranted in the future.

2. Wildlife Crossings

The two alternatives also include four new wildlife underpasses and improvement of the only existing underpass at the I-25 bridge over Plum Creek (at MP 172.2). A multidisciplinary group of structural engineers, drainage engineers, and wildlife biologists advised on the location, dimensions, and design of the crossings. Based on their recommendations, the four new wildlife crossings included in the project

are located at MPs 162.5, 164.0, 167.7, and 170.6. The crossings are in locations of high wildlife-vehicle collisions and are spaced to provide the best opportunity for wildlife to use the crossings. Wildlife fencing will be installed between the crossing locations to encourage deer and other animals to use the new crossings and deter them from crossing the interstate.

3. Walls

The corridor has intermittent stretches of split-grade profile, totaling about 6 miles, where the northbound and southbound travel lanes are at different elevations. In these locations, walls would be required in the median. The height of the walls varies based on the degree of the offset; most range from 3 to 5 feet tall, with two locations over 9 feet tall. In addition to the walls, median barriers would be provided throughout to provide physical separation between the northbound and southbound travel lanes. Locations for breaks in the median for emergency crossovers (and construction access) would be considered in final design. Along most of the corridor, widening would also be required to the outside of the existing pavement; retaining walls would be needed in several of these locations to keep the highway within right-of-way.

4. Lighting, Power, and Intelligent Transportation System Infrastructure

Both alternatives include improved lighting along the mainline in the urban section through Monument (from approximately MP 161 to MP 164) and spot lighting at interchanges. Lighting already exists in these locations but would be upgraded and modernized to improve safety, reduce costs, and minimize light pollution for surrounding open space properties. Lighting near the Greenland Road interchange would be designed to minimize light intrusion for the nearby wildlife crossing.

The two alternatives include communications and power systems to equip the roadway to leverage current and future technology, such as variable messaging signs to provide driver information, dynamic wildlife detection signs, and solar lighting. The Express Lanes alternative requires additional intelligent transportation system equipment to provide driver information, monitor driver compliance with the Express Lane operations, and collect tolls.

C. Operational Characteristics of the Alternatives

Although both alternatives include adding one new travel lane in each direction to expand I-25 through the Gap corridor from two to three lanes in each direction, they vary in how they operate.

- With the General-Purpose Lane alternative, all vehicles would travel in the new third lane or the existing two general-purpose lanes for free at all times.
- With the Express Lane alternative, the new third lane would operate as an Express Lane, where drivers would have two options: either continue to use the two existing general-purpose lanes for free or pay a fee (toll) to drive in the Express Lane. Based on current planning and assumptions, high-occupancy vehicles with three or more passengers, including transit and vanpools, and motorcycles would also be able to use the Express Lane for free. All drivers would always be able to use the existing general-purpose lanes for free.

D. Comparison between the General-Purpose and Express Lane Alternatives

CDOT evaluated and compared the projected performance of the General-Purpose Lane and Express Lane alternatives in more detail. The remainder of this memorandum presents the comparison of the two operating scenarios. Based on the consideration of how each of these alternatives met the purpose and need for the project, CDOT determined that the differences between the two alternatives were minimal. However, because the Express Lane alternative meets the purpose and need for reliability better and offers future management flexibility, it is CDOT's preferred operating alternative.

E. Safety and Incident Management

The safety and incident management benefits of the alternatives are essentially the same in large part because those needs are met by Supplemental Elements such as wider shoulders, lighting, interchange ramp and bridge improvements, and wildlife crossings that are common to both alternatives. The **Safety Analysis Technical Memorandum** included as **Appendix A2** to the EA includes an expanded discussion of the safety benefits and the expected safety performance of the Gap with the additional lane.

F. Travel Delays and Reliability

Both alternatives will similarly address some aspects of the existing turbulence in the traffic stream that contributes to reductions in free-flow speed associated with turbulence and incident response. Increasing capacity with a third lane reduces density and turbulence between drivers, which promotes more harmonious traffic flow and higher travel speeds. Increasing ramp lengths will allow drivers to accelerate or decelerate primarily on the ramps, and not in the mainline traffic flow, which will help to maintain homogeneous travel speeds through the interchange influence areas and reduce delays associated with slowing for turbulent conditions.

The operational differences between the two alternatives result in different projected travel times and speeds through the Gap. The variations in performance projected by the travel demand model are correlated to volumes: the higher volume periods result in more congestion, lower speeds, and longer travel times across all lanes. As expected, congestion-related delays are most prominent with the No Action when the existing two-lane section is over capacity. The three-lane sections under both the Express Lane and General-Purpose Lane alternatives provide additional capacity, and for much of the time, especially in the short-term, operate well in terms of travel times and speeds. In low-volume periods when there is little advantage for travel in the Express Lane (and little incentive for drivers to pay to use the lane), the share of volumes in the two general-purpose lanes are expected to be higher. Additionally, travel times and speeds in the general-purpose lanes in the Express Lane alternative are projected to be slightly longer/slower than in the three general-purpose lanes in the General-Purpose Lane alternative. However, this situation occurs in the low-volume periods where all lanes, regardless of operating scenario, are operating well at relatively high speeds. As volumes increase in the three-lane section, traffic also distributes across the lanes differently. More people are projected to use the Express Lane, freeing up more capacity in the general-purpose lanes. However, in the high-demand periods, demand exceeds capacity across the system; therefore, as more people want to travel, speeds decrease, and general-purpose lane travel times are longer under both the General-Purpose Lane and Express Lane alternatives (although still lower than No Action). In high-demand periods (peak travel periods), the Express Lane is the only lane that CDOT can manage to control congestion and provide consistent travel speeds.

The expected operating conditions in the opening year (2021) and horizon planning year (2040) for both alternatives and a No Action alternative were estimated with a travel demand model constructed for this project. The **Travel Demand Forecasting Technical Memorandum** in **Appendix A3** includes an expanded discussion of the modeling process, assumptions, results, and conclusions. Because the I-25 South Gap corridor exhibits high weekend volumes and delays and the regional travel demand models are designed for average weekday conditions, some adjustments to the project's travel demand model trip matrices were required to model the increased delay often observed on I-25 on Friday afternoons and weekends. Based on traffic counts and analysis of travel patterns and speeds from INRIX data, weekend peaks for the corridor were defined as Friday afternoon and Sunday mid-day. The following (weekday) time periods in the Denver Regional Council of Government's FOCUS model aligned closely with the actual time periods of greatest observed delay (from historical data and 2017 traffic counts) on I-25: Friday 3 – 5 PM (PM1), Friday 5 – 6 PM (PM2), Friday 6 – 7 PM (PM3), and Sunday 11:30 AM – 3:00 PM (OP3). For other

times on the weekends, travel is expected to be better than these periods but cannot be precisely quantified because the FOCUS model is not a weekend model and therefore reference data are limited.

Forecasts of travel times and speeds of the two alternatives are presented in detail in the subsequent sections of this memorandum. Travel times and speeds were modeled for the presumed opening year of the project, 2021, and for the horizon year of 2040. **Tables 6 through 9** project travel times by year using a linear growth projection between the two modeled times of 2021 and 2040 (that is, assuming that growth happens evenly over the 20-year period).

1. Traffic Flow in Peak Demand Periods

Four different time periods were modeled to capture the highest volume demand periods: weekday morning and evening peak commute periods, Friday late-afternoon, and Sunday mid-day. **Table 1** summarizes the travel times estimated by the model for these four time periods in each analysis year. The “2GP+1 Express Lane” column/category shows the estimated travel time for the Express Lane and for the two general-purpose lanes adjacent to the Express Lane for the Express Lane alternative. The model results indicate that both alternatives improve travel times and reduce congestion-related delays as compared to No Action in both the opening and horizon years. **Table 2** is similar to **Table 1** but shows the average travel speeds (in miles per hour [mph]) that correspond to the travel times in **Table 1**.

Table 1. Summary of Gap Travel Time Forecasts in Peak Periods

Time Period / Direction	Existing 2017 Travel Time (Minutes)	Year	Travel Time (Minutes)			
					2 GP + 1 Express Lane	
			No Action (2 GP)	3 GP Lanes	Express Lane	2 GP Lanes (with EL)
Weekday 6:30 – 9:00 AM Northbound	20	2021	22	18	18	19
		2040	43	26	23	31
Weekday 3:00 – 7:00 PM Southbound	21	2021	24	19	19	21
		2040	79	37	26	43
Friday 3:00 – 7:00 PM Southbound	37	2021	50	23	23	25
		2040	119	50	32	69
Sunday 11:30 AM – 3:00 PM Northbound	24	2021	26	19	19	22
		2040	40	26	23	29

Source: Steer Davies Gleave, 2018; Note: GP = general purpose

Table 2. Summary of Gap Average Speeds Forecasts in Peak Periods

Time Period / Direction	Existing 2017 Average Speed (mph)	Year	Average Speed (mph)			
			No Action	3 GP Lanes	2 GP + 1 Express Lane	
					Express Lane	GP Lanes (with EL)
Weekday 6:30 – 9:00 AM Northbound	62	2021	56	70	70	64
		2040	33	52	57	45
Weekday 3:00 – 7:00 PM Southbound	59	2021	52	67	67	61
		2040	20	41	49	36
Friday 3:00 – 7:00 PM Southbound	34	2021	25	55	55	49
		2040	10	28	40	20
Sunday 11:30 AM – 3:00 PM Northbound	53	2021	48	64	66	57
		2040	31	49	53	43

Source: Steer Davies Gleave, 2018

In the planned opening year of 2021, the Express Lane would operate similarly to the three lanes in the General-Purpose Lane alternative because free-flow travel conditions would be experienced in all these lanes. Presumably, the demand for the Express Lane would be lower with nearly free-flow conditions in the three general-purpose lanes. Lower use of the Express Lanes results in higher volumes in the two general-purpose lanes, which results in slightly slower projected speeds. It is noted that small differences in speeds (less than 5 mph) and travel times (less than 3 minutes) are within the margin of error for the model between the actual and modeled travel times.

The General-Purpose Lane alternative would operate slightly better in 2021 (i.e., faster travel times and less delay) than the two general-purpose lanes in the Express Lane alternative. The projected travel time savings for the three general-purpose lanes compared to the general-purpose lanes in the Express Lane alternative are within 3 minutes and travel speeds within 7 mph over the two general-purpose lanes.

The Express Lane within the Express Lane alternative provides travel time savings, increased speeds, and reduced delay in the horizon year 2040 during the peak periods. The time savings in the general-purpose lanes in the General-Purpose Lane and Express Lane alternative vary from 6 minutes to 37 minutes, depending upon the peak period and the route choice selected. The Express Lane provides the most significant advantage in the southbound direction on Friday afternoon, which is the highest-volume demand period, with the time savings realized by an average travel speed that is 20 mph higher than in the parallel general-purpose lanes. The general-purpose lanes in the Express Lane alternative (the existing lanes adjacent to the new Express Lane) experience travel time and speed savings compared to the No Action but not as much savings as the three general-purpose lanes in the General-Purpose Lane alternative. Outside of the Friday afternoon peak period where the difference is 19 minutes, the variation between travel times in the general-purpose lanes in the General-Purpose Lane alternative and Express Lane alternative is minimal at 3 to 6 minutes.

2. Average Daily Volume Forecasts

The travel demand model forecasted daily volumes north and south of the Gap, and through the Gap in 2021 and 2040. These volumes represent the number of trips served and are the basis of the operations assessment of travel time and corresponding speed. **Table 3** summarizes the average weekday traffic forecasts. As the table shows, while the General-Purpose Lane alternative is projected to accommodate

slightly more volume than the Express Lane alternative, there is relatively little difference (about 1 percent) through the Gap between the two build alternatives. For the Express Lane and General-Purpose Lane alternatives, there is a notable difference between the SH 105 and SH 83 volume forecasts among the alternatives, especially on a percentage basis. This result is expected because the congestion that will perpetuate through the Gap with the No Action encourages demand to divert to these other two routes. With the additional capacity provided by the third lane in the build alternatives, more drivers opt to stay on I-25 and the two highway forecasts are lower. The improved performance and reliability of I-25 under the Express Lane alternative is expected to attract more trips from the alternate routes. The Express Lane alternative attracts more trips to SH 83 and/or SH 105 than the General-Purpose Lane alternative.

Table 3. Average Weekday – Daily Bi-Directional Traffic Volumes

I-25 Location	2017	2021 No Action	2021 3 GP Lanes	2021 2 GP + EL	2040 No Action	2040 3 GP Lanes	2040 2 GP + EL
I-25 N/O Baptist Rd	95,540	99,830	100,540	100,170	138,540	140,980	140,380
I-25 N/O Greenland Rd	78,140	83,740	85,270	GP: 75,720 EL: 9,330 85,050	110,670	119,650	GP: 92,710 EL: 25,940 118,650
I-25 S/O Plum Creek Pkwy	79,000	84,730	87,430	86,950	146,170	153,030	149,980
I-25 N/O Founders Pkwy	132,750	139,760	140,550	140,200	189,940	191,530	190,860
SH 105 S/O Tomah Rd	3,130	4,320	3,290	3,430	13,110	8,080	9,510
SH 83 S/O Gillian Ave	5,790	7,750	6,770	7,200	16,130	12,220	13,100

Source: Steer Davies Gleave, 2018

3. Travel Time Forecasts

Table 4 shows the travel time forecasts (in minutes) by time period for the Gap with 2021 volume forecasts. **Table 5** shows predicted times for 2040 volume forecasts. Note that a 17.7-minute travel time corresponds to the average recorded travel speed of 70 mph; actual speeds for individual trips may be higher during uncongested time periods. Travel times for existing conditions (2017) are also presented in **Tables 4 and 5** to provide a familiar frame of reference. For future conditions, the No Action travel times equate to the existing conditions modeled for 2021 and 2040 travel demand.

The longest travel times are projected for the No Action, regardless of the time period. The General-Purpose Lane alternative would provide marginally faster travel times than the general-purpose lanes in the Express Lane alternative in non-congested conditions during off-peak periods in both the opening and horizon years. The projected travel time savings differs by only a few minutes.

With the Express Lane alternative, the slightly reduced speeds in the general-purpose lanes are anticipated for these time periods because use of the Express Lanes during free-flow conditions would be low (drivers may not elect to pay when traffic is flowing well in the general-purpose lanes). The similar time projections are further explained with recognition that the impact of slowing traffic due to decision point-induced turbulence would have little effect in these off-peak demand periods when the Express Lanes are not anticipated to have high use.

Table 4. 2021 Forecast Travel Times by Time of Day (Minutes)

Scenario	Direction	6:30-7A	7-8A	8-9A	9-11:30A	11:30 A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30 A-3P
Existing Conditions (2017)	NB	18.7	20.6	20.3	17.9	18.1	17.7	17.7	17.2	18.1	18.2	21.7	18.4	18.2	23.6
2021 No Action	NB	20.0	24.6	21.4	19.3	19.4	19.0	18.7	17.7	18.1	18.2	22.8	19.7	19.0	25.8
2021 3 GP	NB	17.7	17.9	17.7	17.7	17.7	17.7	17.7	17.7	18.1	18.2	17.7	17.7	17.7	19.3
2021 2GP (with EL)	NB	17.7	20.2	19.3	17.7	18.5	17.7	17.7	17.7	18.1	18.2	18.7	17.7	18.6	21.8
2021 EL	NB	17.7	17.7	17.7	17.7	17.7	17.7	17.7	17.7	18.1	18.2	17.7	17.7	17.7	18.7
Existing Conditions (2017)	SB	19.1	20.4	18.9	18.5	19.1	22.4	21.9	18.0	18.9	19.1	36.4	40.1	35.3	19.3
2021 No Action	SB	19.6	21.1	19.4	18.9	20.0	25.7	26.2	20.0	18.9	19.1	49.3	52.0	47.7	20.5
2021 3 GP	SB	18.5	19.4	18.2	17.7	17.7	19.3	17.9	17.7	18.8	19.1	23.3	22.0	21.8	17.7
2021 2GP (with EL)	SB	19.5	20.8	19.1	18.6	19.3	21.7	20.5	18.4	18.9	19.1	26.0	25.5	24.0	18.9
2021 EL	SB	18.5	19.4	18.2	17.7	17.7	19.3	17.9	17.7	18.7	19.1	23.2	21.9	22.2	17.7

Source: Steer Davies Gleave, 2018

Table 5. 2040 Forecast Travel Times by Time of Day (Minutes)

Scenario	Direction	6:30-7A	7-8A	8-9A	9-11:30A	11:30 A-3P	3-5P	5-6P	6-7P	7-11P	11P-6:30A	3-5P	5-6P	6-7P	11:30 A-3P
Existing Conditions	NB	18.7	20.6	20.3	17.9	18.1	17.7	17.7	17.2	18.1	18.2	21.7	18.4	18.2	23.6
2040 No Action	NB	28.7	63.2	30.5	29.5	30.4	29.8	28.6	23.8	18.4	18.2	36.0	30.8	27.8	40.2
2040 3 GP	NB	18.1	35.8	20.8	18.5	19.5	18.4	18.8	18.0	18.1	18.2	21.5	18.7	19.5	25.5
2040 2GP (with EL)	NB	20.8	43.3	23.1	22.8	23.3	20.9	21.4	20.2	18.4	18.2	24.0	21.2	21.8	28.6
2040 EL	NB	17.7	28.7	19.3	17.7	17.7	17.7	17.7	17.7	18.1	18.2	20.1	17.4	18.3	23.4
Existing Conditions	SB	19.1	20.4	18.9	18.5	19.1	22.4	21.9	18.0	18.9	19.1	36.4	40.1	35.3	19.3
2040 No Action	SB	26.7	31.7	26.4	25.2	28.6	82.2	120.1	31.2	20.0	19.1	120.2	128.0	108.4	30.7
2040 3 GP	SB	19.7	21.9	19.5	18.8	19.4	30.4	68.5	18.8	18.9	19.1	53.6	64.5	26.5	19.2
2040 2GP (with EL)	SB	21.5	23.9	21.2	21.4	22.3	41.7	68.4	19.0	19.7	19.1	70.0	97.0	40.1	21.6
2040 EL	SB	18.6	20.0	18.3	17.7	17.7	28.9	30.2	17.7	18.7	19.1	34.0	34.0	24.4	16.6

Source: Steer Davies Gleave, 2018

4. Travel Speed Forecasts

Travel speeds (in mph) are derived from the travel time forecasts through the Gap. The slowest travel speeds are projected for the No Action alternative, regardless of the time period. Between the two build alternatives, the General-Purpose Lane alternative would provide slightly higher travel speeds than the

general-purpose lanes than the general-purpose lanes in the Express Lane alternative in non-congested conditions during off-peak periods in both the opening and horizon years, whereas, the Express Lane would provide better speeds in peak conditions. As noted previously, the travel times and travel speeds present the same modeling data, as speeds are converted from travel times.

Tables 6 through 9 show average speed forecasts through the Gap for the existing (2017) conditions and for the 2021 and 2040 forecasts, with the interim years linearly interpolated. The colors refer to speed ranges, with green being fastest and red being slowest. Within the Express Lane alternative, the average speeds are shown for both the two general-purpose lanes and the one Express Lane. These tables also include Express Lane capture rates by Passenger Car Equivalents based on the assumption that three-axle trucks correspond to three passenger vehicles. Capture rates are equal to the percentage of the total (GP+EL) traffic volume that choose to use the Express Lane. Although trucks are not expected to use the Express Lane the total volume includes some three-axle trucks, which are counted as three automobiles for the purposes of this calculation.

Tables 6 through 9 represent an aggregate set of time periods and contain the periods of highest congestion only. To obtain speeds for the 6:30 – 9:00 AM morning peak period, travel times were weighted across the AM1, AM2 and AM3 periods and converted to speeds. A similar process was followed for the evening peak period. The speeds in **Tables 6 through 9** represent averages across the entire peak period, and if peak spreading occurs, may be a more accurate representation of typical conditions.

The speeds shown in **Tables 6 through 9** represent the average speed through the 20.7-mile I-25 South Gap Project area, of which about 15 miles includes Express Lanes. As a result, for the remaining 5.7 miles of the Express Lane Alternative, travel would be in general-purpose lanes. Average speeds in the Express Lane eventually drop below 45 mph in Table 9 because the remaining 5.7 miles of general-purpose lane travel cannot be managed to the minimum 45 mph speed required for the Express Lane operations. Thus, the overall average travel speed for the whole length of the Gap reflects the congestion that will be experienced in the general-purpose lane within the Express Lane alternative. The full 20.7-mile Gap is reported so that results can be compared easily between the modeling efforts.

Table 6 shows average weekday travel speeds in the northbound direction during the morning peak period (6:30 – 9 AM). **Table 7** shows average weekday travel speeds in the southbound direction during the evening peak period (3 – 7 PM). **Tables 8 and 9** display weekend travel speeds, defined as Friday afternoon in the southbound direction and Sunday afternoon (11:30 AM – 3 PM) in the northbound direction.

Per the speeds shown in **Tables 6 through 9**, both the Express Lane and General-Purpose Lane alternatives provide higher travel speeds (which results in shorter travel times) than the No Action. For most travel periods in the first 5 years, the speeds are relatively consistent among the lanes in both the General-Purpose Lane and Express Lane alternatives. The Express Lanes are projected to provide marginally higher travel speeds than the three general-purpose lanes after about 2 years of opening in 2021; however, outside of the highest-volume period on Friday afternoons, speeds vary less than 5 mph among the Express Lanes and general-purpose lanes under both action alternatives, indicating little short-term difference in the performance of the alternatives.

The capture rate, shown as a percentage in the far-right column of each table in **Tables 6 through 9**, for the Express Lane increases as the average speed differential between the two general-purpose lanes and the Express Lane increases. In higher volume periods, both the Express Lane and general-purpose lanes carry more volume but the Express Lane has a higher capture rate (share of the volume) in higher demand periods because more people elect to use the Express Lanes. Because speeds in the Express Lanes can be managed (i.e., capture rate stays below 33 percent), speeds are projected to be higher, and the differences in speeds between the Express Lane and the general-purpose lanes are also higher. In the General-Purpose Lane alternative, as available capacity is reduced with higher volumes, traffic fills up and

is distributed more evenly among the lanes. Further, as volumes increase, congestion and delays increase, and travel speeds decrease across the lanes.

Table 6. Weekday Morning Northbound Year-by-Year Average Speeds (mph) from Monument to Plum Creek

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	62				
2018	60				
2019	59				
2020	57				
2021	56	70	70	64	15%
2022	55	69	69	63	16%
2023	53	68	69	62	17%
2024	52	67	68	61	17%
2025	51	66	67	60	18%
2026	50	65	67	59	19%
2027	49	64	66	58	19%
2028	47	63	65	57	20%
2029	46	62	65	56	21%
2030	45	61	64	55	22%
2031	44	60	63	54	22%
2032	42	59	62	53	23%
2033	41	58	62	52	24%
2034	40	57	61	51	25%
2035	39	56	60	50	25%
2036	38	55	60	49	26%
2037	36	54	59	48	27%
2038	35	53	58	47	28%
2039	34	53	58	46	28%
2040	33	52	57	45	29%

Source: Steer Davies Gleave, 2018

Table 7. Weekday Evening Southbound Year-by-Year Average Speeds (mph) from Plum Creek to Monument

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	59				
2018	57				
2019	55				
2020	53				
2021	52	67	67	61	18%
2022	50	66	66	59	19%
2023	48	64	65	58	19%
2024	47	63	64	57	20%
2025	45	62	63	55	20%
2026	43	60	62	54	20%
2027	42	59	61	53	21%
2028	40	58	60	51	21%
2029	38	56	60	50	22%
2030	37	55	59	49	22%
2031	35	54	58	48	22%
2032	33	52	57	46	23%
2033	32	51	56	45	23%
2034	30	50	55	44	24%
2035	28	48	54	42	24%
2036	27	47	53	41	24%
2037	25	45	52	40	25%
2038	23	44	51	38	25%
2039	22	43	50	37	26%
2040	20	41	49	36	26%

Source: Steer Davies Gleave, 2018

The weekend projections also indicate that the Express Lane will provide higher average speeds than either the three general-purpose lanes or the two general-purpose lanes. As with the weekday peak periods, the share of travel in the general-purpose lane will be higher in the General-Purpose Lane alternative than in the Express Lane alternative.

Table 8. Weekend Southbound (Friday afternoon) Year-by-Year Average Speeds (mph) from Plum Creek to Monument

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	34				
2018	31				
2019	29				
2020	27				
2021	25	55	55	49	21%
2022	24	54	54	47	21%
2023	24	52	53	46	22%
2024	23	51	53	44	22%
2025	22	49	52	43	22%
2026	21	48	51	41	23%
2027	20	46	50	40	23%
2028	20	45	50	38	23%
2029	19	44	49	37	24%
2030	18	42	48	35	24%
2031	17	41	47	34	24%
2032	17	39	46	32	24%
2033	16	38	46	31	25%
2034	15	37	45	29	25%
2035	14	35	44	27	25%
2036	14	34	43	26	26%
2037	13	32	42	24	26%
2038	12	31	42	23	26%
2039	11	30	41	21	27%
2040	10	28	40	20	27%

Source: Steer Davies Gleave, 2018

Table 9. Weekend Northbound (Sunday mid-day) Year-by-Year Average Speeds (mph) from Monument to Plum Creek

Year	2 GP	3 GP	2 GP + Express Lane		
			EL	GP	EL %
2017	53				
2018	51				
2019	50				
2020	49				
2021	48	64	66	57	15%
2022	47	64	66	56	16%
2023	46	63	65	55	17%
2024	45	62	64	55	17%
2025	44	61	64	54	18%
2026	44	60	63	53	19%
2027	43	59	62	53	19%
2028	42	59	61	52	20%
2029	41	58	61	51	21%
2030	40	57	60	50	22%
2031	39	56	59	50	22%
2032	38	55	59	49	23%
2033	37	54	58	48	24%
2034	36	54	57	48	25%
2035	35	53	57	47	25%
2036	35	52	56	46	26%
2037	34	51	55	45	27%
2038	33	50	54	45	27%
2039	32	50	54	44	28%
2040	31	49	53	43	29%

Source: Steer Davies Gleave, 2018

5. Delay Associated with Incidents

The travel demand model projects travel times and speeds related to volume. Delays related to incidents, which account for roughly half of the current delays through the Gap corridor, are addressed partially by capacity improvements (reduction in congestion-related crashes) but also through improved incident management and ability to move crashes from the travel lanes. The shoulder improvements provided with both alternatives will similarly reduce delays related to incident management by providing the following benefits:

- More recovery area for drivers to avoid crashes, especially secondary crashes.
- Adequate space to move disabled vehicles from the travelway and conduct investigations and cleanup without requiring lane closures.
- Room for vehicles to get over to allow quicker access for emergency vehicles to reach the incident and get it cleared.
- More space for vehicles to temporarily maneuver around crashes, even if lanes are blocked.

6. Reliability of Travel Due to Delays and Incidents

While both alternatives would reduce travel delays in the near term, by providing increased capacity and maneuverability, the Express Lane alternative better meets the purpose and need for improving 2040 travel times and reliability because it provides the opportunity to manage travel as the corridor becomes congested again in the future. The Express Lane alternative provides flexibility to respond to changing travel patterns and needs, while encouraging travel in off-peak periods and/or different travel choices, such as transit or carpooling, which is consistent with CDOT's overall strategy for managing congestion and reliability on congested highways. Additionally, Express Lanes provide more consistent travel speeds, which improve reliability and promote a safer and more comfortable driving experience.

Figures 2 through 7 compare average daily travel speeds among the No Action, Express Lanes, and General-Purpose Lanes through the Gap in the weekday and Friday afternoon (weekend) peak travel periods in 2021 and 2040.¹ As conditions become more congested in the highest peak period (Friday afternoons) and for all peak time periods in 2040, the Express Lane offers the highest and most consistent travel speeds, substantially outperforming all other scenarios. Consistent travel speeds result in trip time reliability. The information presented in the line graphs is the same as the travel time data presented in tabular format earlier.

Table 10 compares the projected travel times and travel time savings among the alternatives for trips through the Gap and longer regional trips that traverse through the Gap but originate in or are destined for Colorado Springs, Monument, or Denver South. Regional trips account for 75 percent of the trips through the I-25 South Gap corridor. By addressing the bottleneck of the narrowed roadway through the Gap and adding lane capacity, travel in the peak periods will improve notably over the No Action (and existing conditions) under all scenarios in the opening year of 2021. By 2040, the travel time savings associated with the Express Lane alternative are substantial, ranging from 11 to 87 minutes. While both the Express Lane and General-Purpose lane alternatives improve travel times through the Gap, the benefit is diluted over the broader travelshed due to persisting congestion north and south along the regional route. The PEL study is continuing to evaluate alternatives to reduce congestion and improve travel on the regional I-25 route between Colorado Springs and Denver.

¹ Hourly average travel times / speeds for the northbound weekend peak period (Sundays from 11:30 AM to 3:00 PM) are not available due to the travel demand model's limitations in modeling weekend conditions, as described earlier. Also, for the Friday afternoon weekend period, the data presented assume that outside of the Friday peak from 3:00 PM to 7:00 PM, the rest of the day operates like an average weekday.

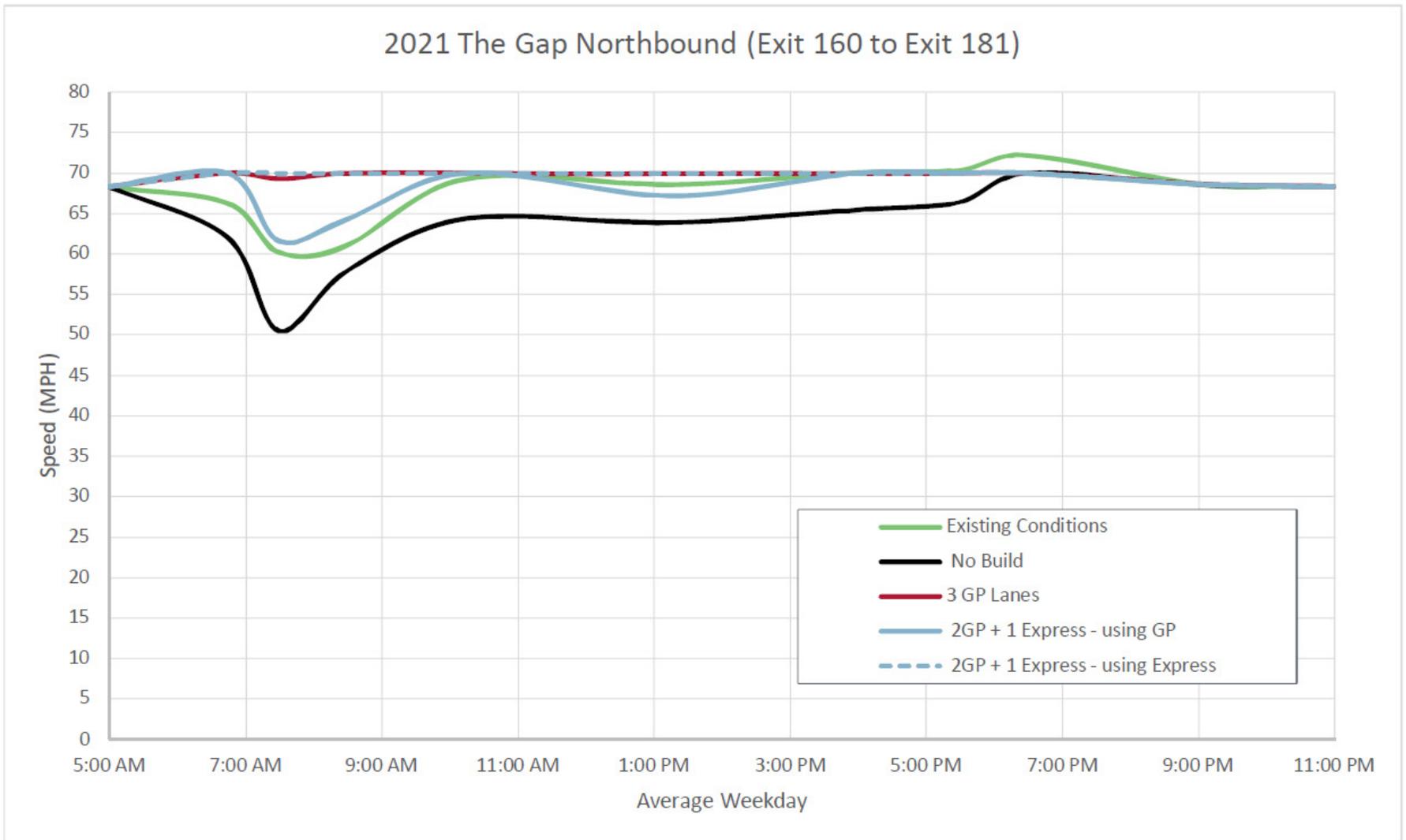


Figure 2. Comparison of Projected Travel Times, Northbound Weekday AM, 2021

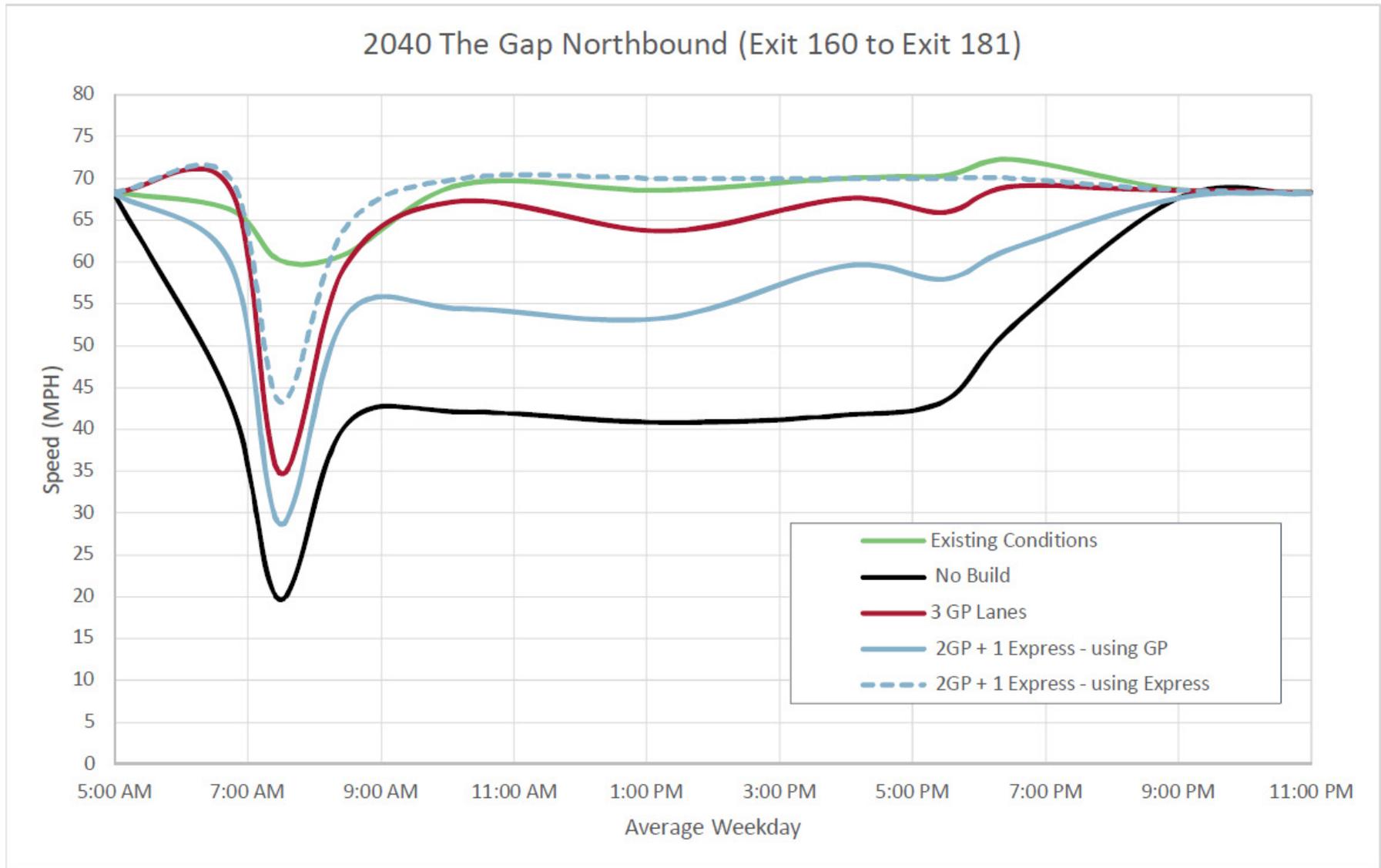


Figure 3. Comparison of Projected Travel Times, Northbound Weekday AM, 2040

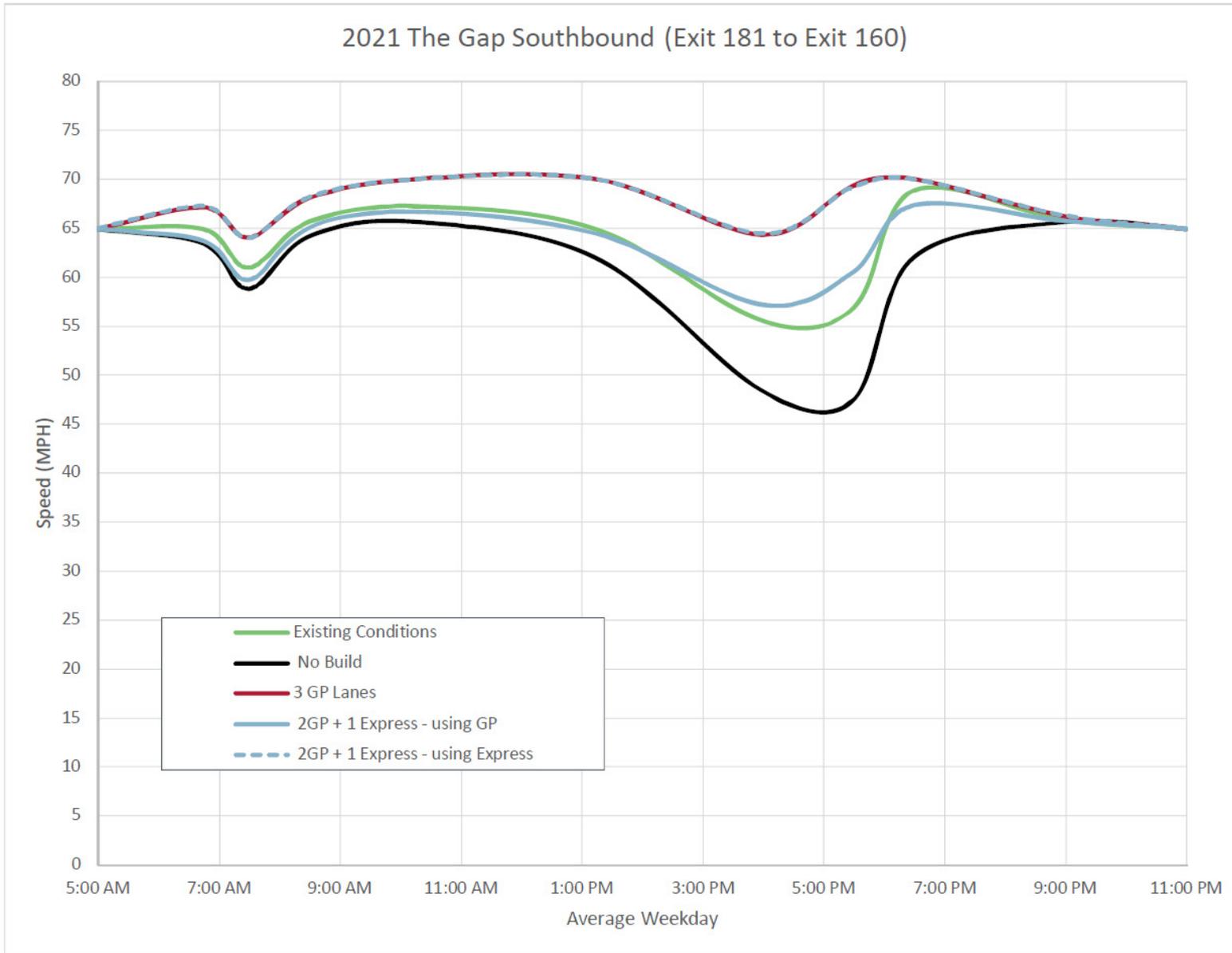


Figure 4. Comparison of Projected Travel Times, Southbound Weekday AM, 2021

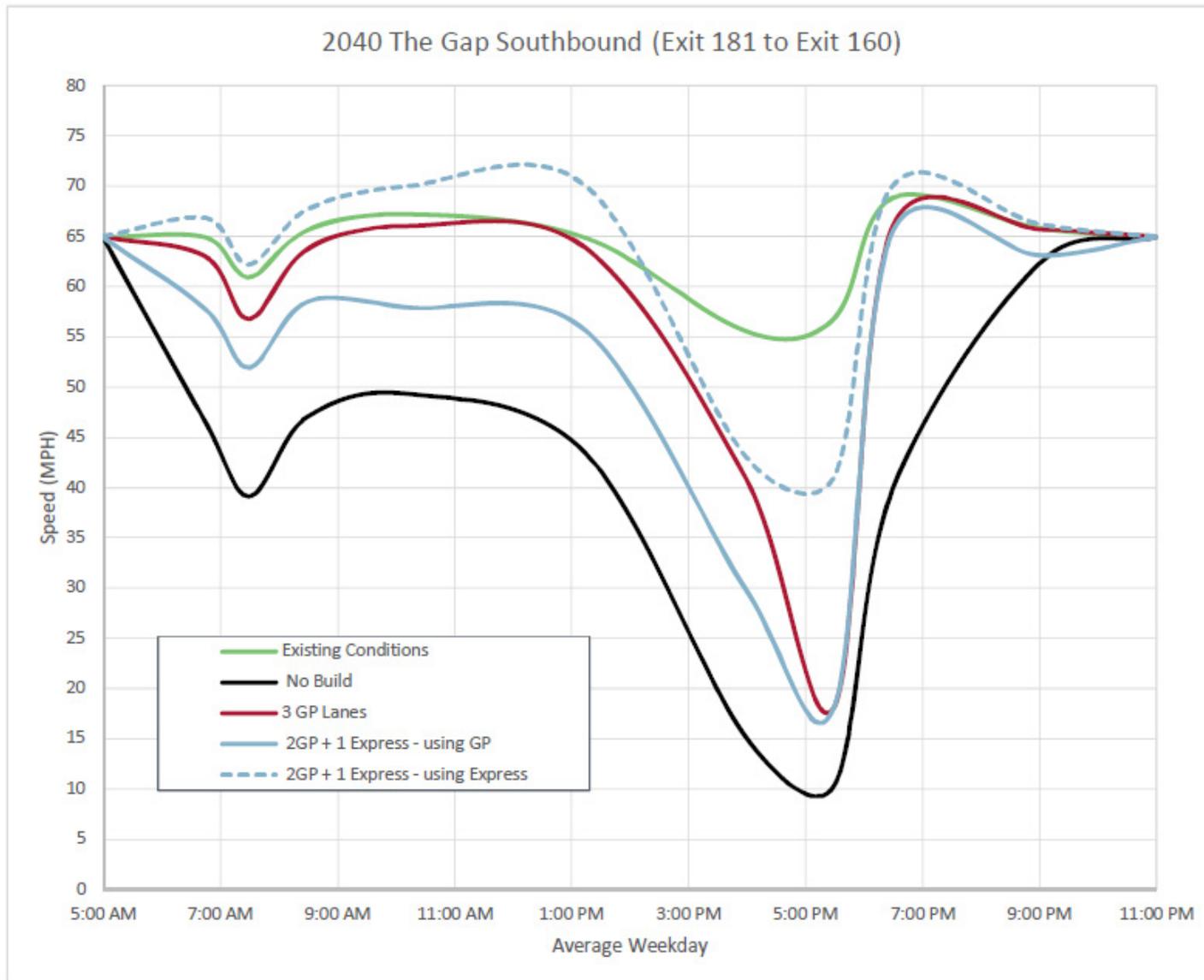


Figure 5. Comparison of Projected Travel Times, Southbound Weekday PM, 2040

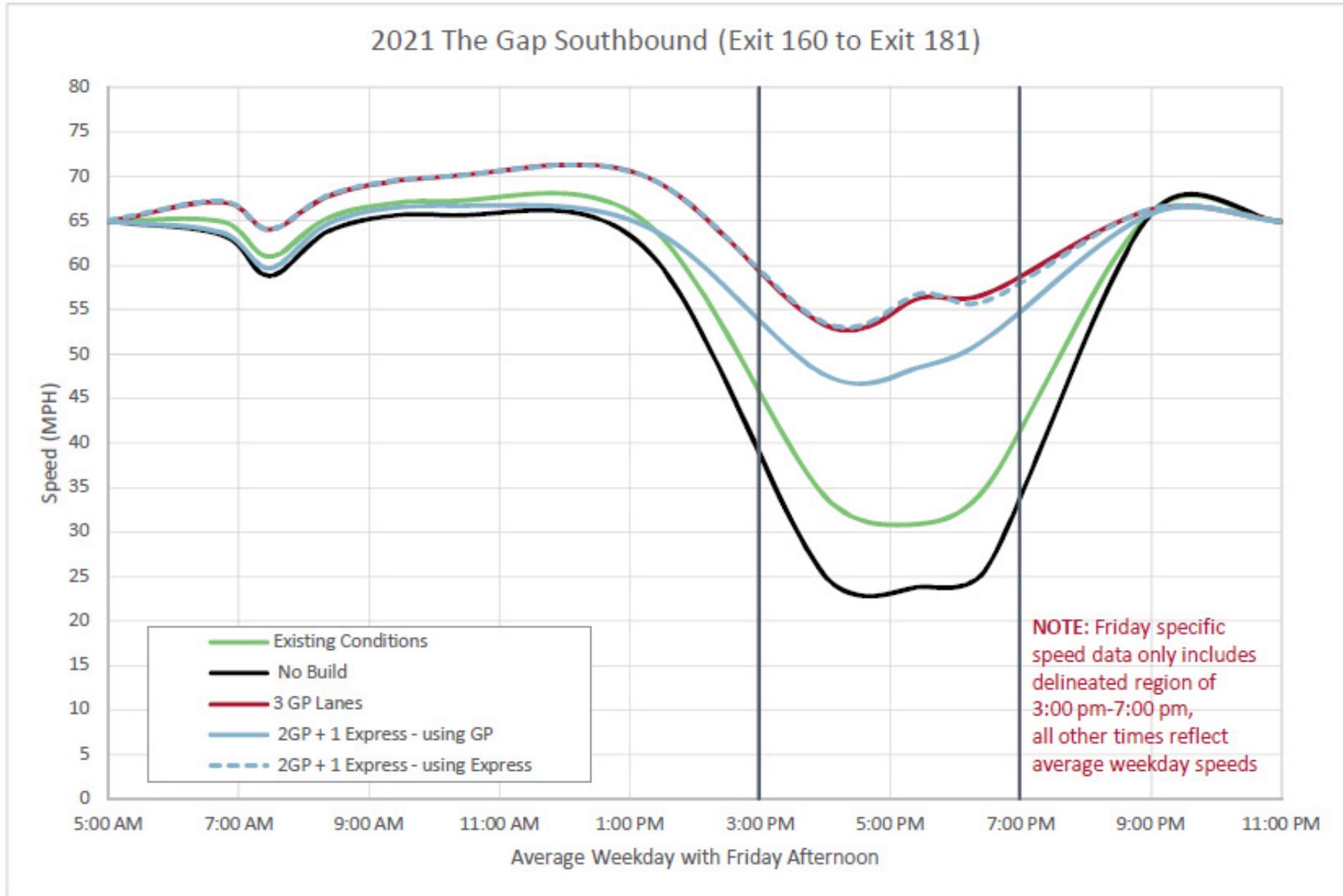


Figure 6. Comparison of Projected Travel Times, Southbound Weekend (Friday PM), 2021

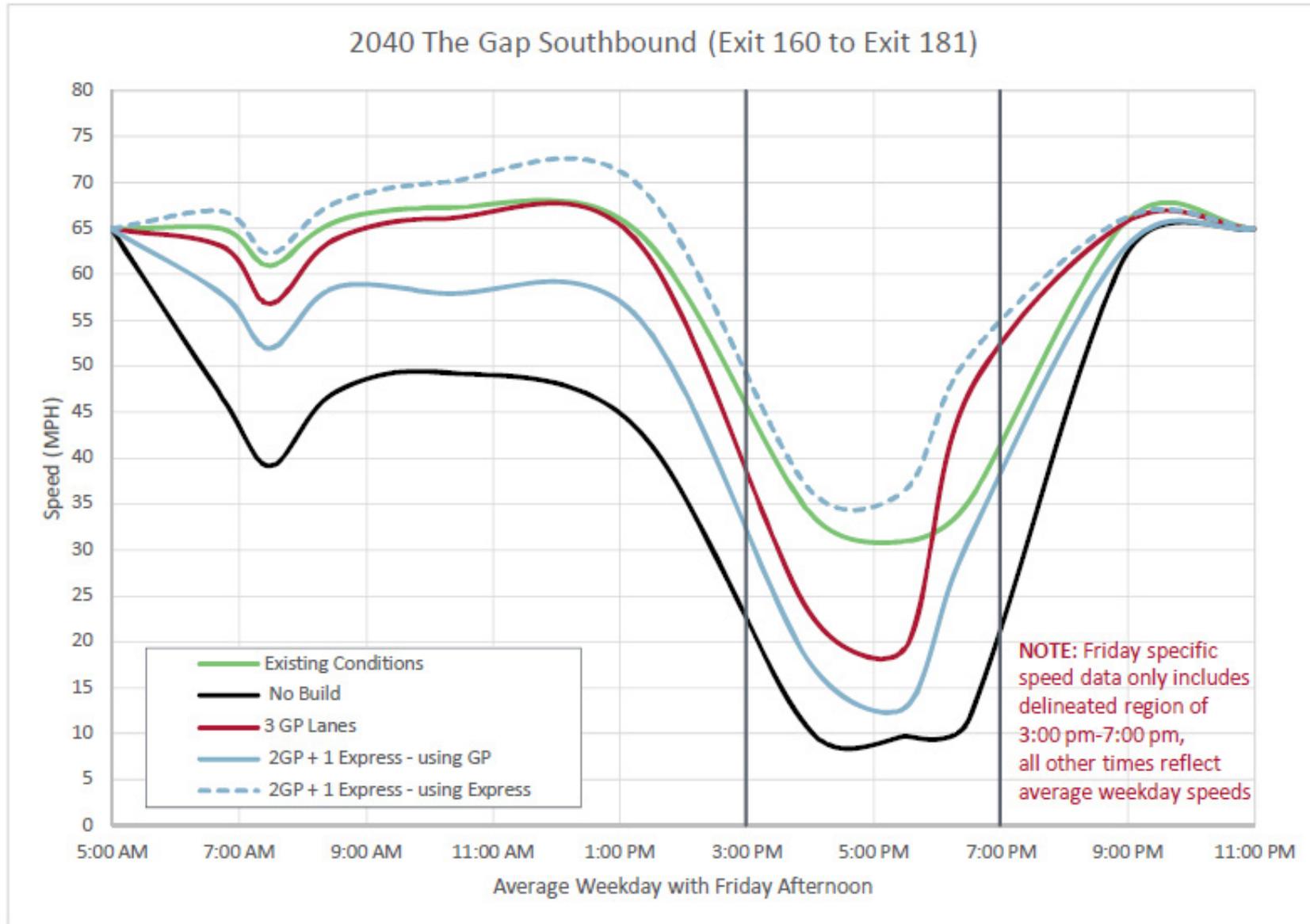


Figure 7. Comparison of Projected Travel Times, Southbound Weekend (Friday PM), 2040

Table 10. Comparison of Travel Times and Projected Travel Time Savings for No Action, General Purpose Lane and Express Lane Alternatives in 2021 (anticipated opening) and 2040 (design year)

Time Period	Travel Times	2021 No Action	2021 3 GP	2021 2 GP (with EL)	2021 EL	2040 No Action	2040 3 GP	2040 2 GP (with EL)	2040 EL
Weekday Northbound	Total Travel Time	22	18	19	18	43	26	31	23
6:30-9:00 AM	Travel Time Savings		4	3	5		16	12	20
Weekday Southbound	Total Travel Time	24	19	21	19	79	37	43	26
3:00-7:00 PM	Travel Time Savings		5	3	5		42	36	53
Friday Southbound	Total Travel Time	50	23	25	23	119	50	69	32
3:00-7:00 PM	Travel Time Savings		27	25	27		69	50	87
Sunday Northbound	Total Travel Time	26	19	22	19	40	26	29	23
11:30 AM-3:00 PM	Travel Time Savings		7	4	7		14	11	17

Notes:

EL = Express Lanes; GP = general-purpose lane

The Gap corridor extends 21 miles between SH 105 (MP 160) and Plum Creek Parkway (MP 181). Based on the current design of the Express Lane transitions, the Express Lanes would operate for approximately 15 miles within the Gap corridor. Therefore, the travel times shown for the Express Lane include travel within the general-purpose lanes.

G. Environmental and Social impacts

CDOT reviewed environmental and social resources within the I-25 South Gap Project area to determine which resources could be affected by the project generally and then which resources might have differing potential impacts due to the change in operations. Because the project elements are the same between the two alternatives, the footprint is similar, and the impacts associated with ground-disturbing construction activities do not distinguish project impacts for most resources. The Express Lane alternative includes a 4-foot buffer, additional signage, and tolling equipment that is not required in the General-Purpose Lane Alternative. The pavement difference due to the Express Lane buffer is negligible because of construction phasing requirements. Enough pavement width is needed to allow traffic to shift to the center and keep two lanes in each direction open during construction. Because of this, the alternatives have the same construction footprint – i.e., require the same amount of space to construct – so most environmental impacts associated with ground disturbance or direct impacts to sensitive resources are the same. For both alternatives, construction is planned to occur within CDOT’s right-of-way, further limiting the potential for environmental impacts. Where impacts are projected, mitigation measures are included or designed into the project to avoid or minimize impacts.

For several social resources, CDOT determined that the two alternatives may have the potential to result in differing impacts. The following describes the environmental impacts of the two alternatives for the four resources evaluated: transportation (freight and transit), socioeconomics, environmental justice, and visual.

- **Transportation Resources**
- **Freight.** Approximately 9 percent of current traffic through the Gap and 12 percent of projected traffic in 2040 is comprised of freight trucks. The General-Purpose Lane alternative provides a greater benefit to freight, as compared to the Express Lane alternative. Travel times and average speeds in the general-purpose lanes are better when use of the third travel lane is not restricted, and passing movements become easier for cars and trucks when all three lanes can be used at any point in the project area. However, compared to the No Action, the Express Lane alternative provides a benefit to freight. While trucks are not expected to use the Express Lanes directly, the increased system capacity is projected to improve travel times and speeds in the general-purpose lanes.
- **Transit.** The Express Lane alternative would improve transit service in the corridor by providing improved travel times and schedule reliability for CDOT’s regional Bustang service and other private transit providers, such as airport shuttles.
- **Socioeconomics.** Both alternatives would improve safety and flow of travel through the Gap. Under both alternatives, travel times, trip reliability, and access would improve, improving the flow of people and goods and benefitting businesses, residents, and freight locally and throughout the Front Range. Improved travel conditions would better support the development plans of corridor communities. Reliability of travel, which is better supported by the Express Lane alternative, is a key factor for attracting economic development, employers, and employees. For the I-25 South Gap corridor and other regional corridors, Express Lanes support reliable transportation choices for commuters that choose to live farther from employment centers. The Express Lane alternative would also benefit transit users by improved travel times and schedule reliability for CDOT’s regional Bustang service and other private transit providers, such as airport shuttles.

Environmental Justice (Low-Income and Minority Populations). The addition of a new tolled lane to the interstate would not represent a disproportionately high and adverse impact to low-income or minority communities in the corridor. The Express Lane alternative would benefit all users, whether they chose to travel in the Express Lane or the improved existing general-purpose lanes, which would remain free and would experience improved performance. In addition to being able to continue to travel toll-free in the general-purpose lanes, the Express Lane is expected to be toll-free for HOV 3+ users and transit,

providing additional toll-free options for travel. Both alternatives include expanded shoulders, improved interchanges, wildlife underpasses, and improved lighting that would create a safer and better functioning road for all users, including low-income and minority populations. A noise wall is recommended for a mobile home park in Monument; if approved by residents, the wall would mitigate adverse traffic noise for low-income residents.

- Visual. Both alternatives would cause some changes to the visual setting in the corridor through the introduction of new roadway and structural elements. The primary difference between the two would be the additional overhead signage required for the Express Lane alternative at ingress and egress locations. Where visible, these signs could reduce the rural feel of the drive for interstate users and may briefly interfere with open views of landscapes in the mid and far background. From surrounding vantage points, most of these elements will be too far from the viewer to create a noticeable change. The ingress and egress locations are limited to three locations, leaving long stretches of I-25 through the Gap without Express Lane signage. The introduction of several new signs in the corridor is not a substantial visual change.

Neither the differences in impacts nor the severity of impacts for either alternative is a distinguishing factor for selecting a preferred alternative. The impacts and included mitigation for the project are discussed in more detail in Chapter 5 of the EA.

H. Conclusion – Selection of the Preferred Alternative

The review of the Express Lane and General-Purpose Lane alternatives found similar impacts between the two alternatives due to their common infrastructure components and comparable construction footprints. The primary difference between the alternatives is in travel reliability on I-25 now and into the future as volumes on the interstate increase. By 2040, the General-Purpose Lane alternative will have congestion and consequently lack reliable travel times across all lanes, while the Express Lane alternative offers the choice for predictability and reliability by managing volumes and speeds in the Express Lanes. Therefore, while performance between the two alternatives is similar, CDOT determined that Express Lanes are a better choice because of the improved travel time reliability offered by Express Lanes, which better meets the purpose and need and public concerns regarding trip reliability into the future.

Express Lanes provide a reliable trip through the Gap. Travel times that can range from 15 minutes to several hours now will only get more unpredictable as population and traffic growth continues and I-25 becomes increasingly congested. Express Lanes offer a choice for users to bypass congestion and take advantage of reliable and predictable travel times in Express Lanes now and into the future, and provide additional system capacity that also improves travel times in the general-purpose lanes.

CDOT has used Express Lanes for more than a decade as a proven way to enhance capacity and travel time reliability and encourage higher density travel (serving more trips with fewer vehicles) by promoting carpooling and transit use in order to reduce overall congestion by reducing the number of vehicles in the corridor. Express Lanes support transportation demand management, provide opportunities to leverage emerging technology, and allow flexibility to adapt highway operations to changing travel demands and patterns. For the I-25 South Gap corridor and other regional corridors, Express Lanes provide reliable transportation choices for commuters that choose to travel farther to employment centers. Express Lanes are especially effective for long, regional corridors like the I-25 Gap segment that serve a high percentage of through trips where travelers can stay in the Express Lane to arrive at their destinations at a predictable time. Bustang, regional bus transit, van pools, and carools also benefit from the ability to use the Express Lanes, avoid congestion, and keep reliable schedules.

In December 2012, the Colorado Transportation Commission adopted Policy Directive 1603.0 requiring that managed lanes (including tolled Express Lanes) be strongly considered during the NEPA phase of planning and developing capacity improvements on state highway facilities that are or will likely become

congested. In nearly all cases since the Policy Directive was adopted, evaluations of added capacity on congested highways have recommended and included Express Lanes.

Currently, the Express Lane network includes more than 80 miles on I-25, US Highway (US) 36, C-470, and I-70 in operation or under construction. Data from corridors like US 36 and the I-70 Mountain Corridor demonstrate that existing Express Lanes provide a trip benefit to all corridor users, not just users of the Express Lane. For example, in the first year of operation of the I-70 Mountain Corridor in 2016, throughput increased 14 percent, and travel times in the general-purpose lanes improved by 38

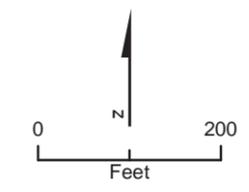
percent. The same pattern followed the opening of the US 36 Express Lane Project, where both travel times and safety improved. The Colorado State Patrol reported that between July and December 2017, the US 36 corridor experienced 33 percent fewer crashes compared to the previous 3 years before the Express Lane project. Additionally, operation of Express Lanes has demonstrated that users are willing to pay for travel time savings and reliability. In the DRCOG region where Express Lanes are an available choice for many travelers, the recorded use of Express Lanes documents that on similar toll facilities in the region, a certain percentage of travelers choose to use tolled lanes over free lanes for travel time savings, increased trip reliability, and other perceived benefits such as safety and comfort.

By building the I-25 South Gap Project with Express Lanes, CDOT has more options to provide I-25 users travel choices and reliable travel times. Active management of Express Lane operations provides CDOT flexibility to adapt highway operations for new technologies, such as driverless cars, or changing travel behaviors, such as increased use of rideshares or longer commutes between housing and employment centers. Roadway capacity can be optimized to move more people more efficiently, safely, and reliably.

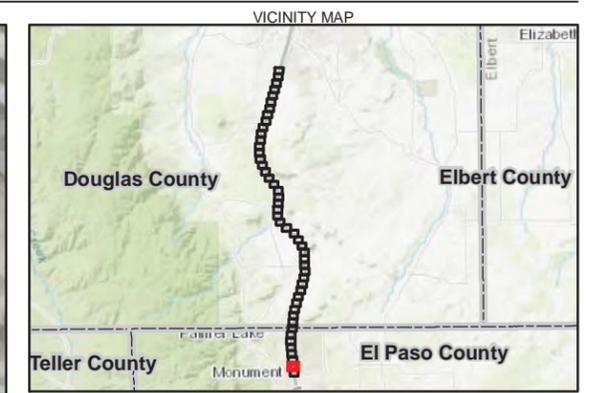
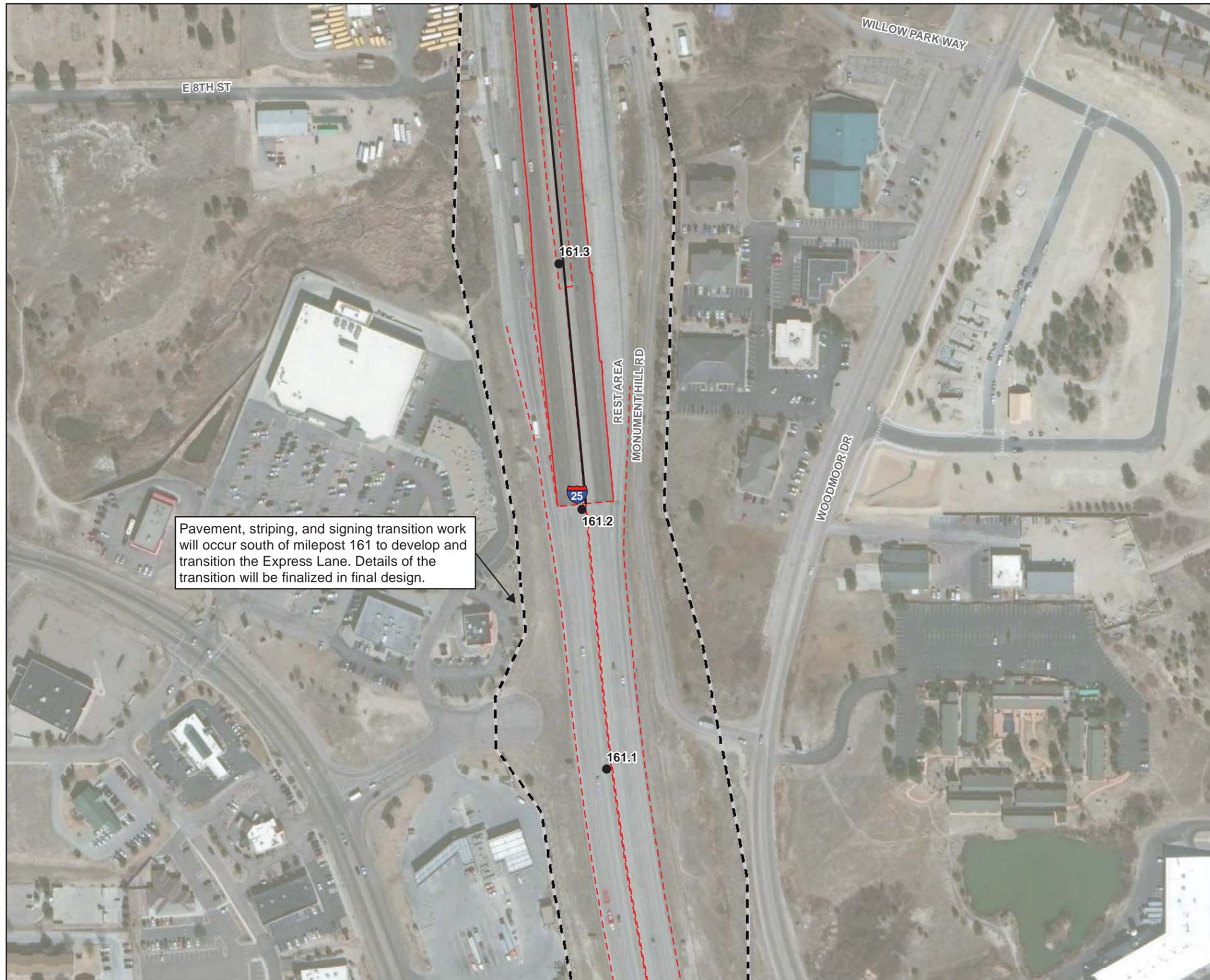
Appendix A6
Roadway Conceptual Design Drawings



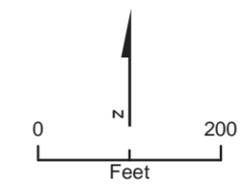
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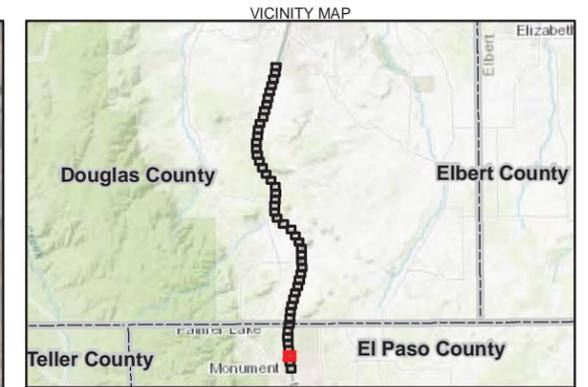


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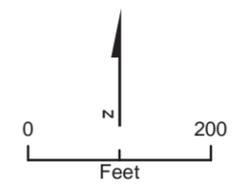


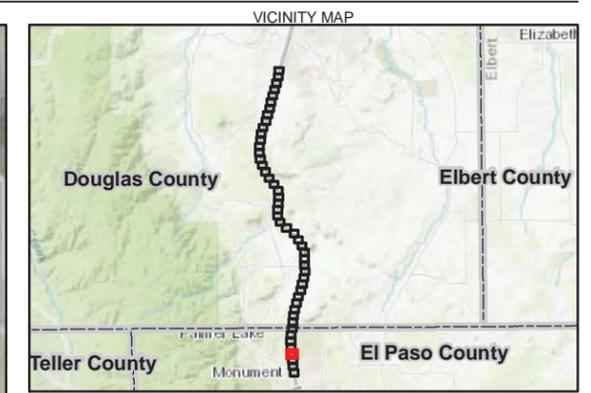
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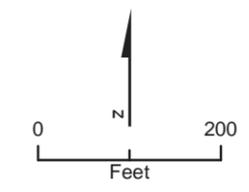


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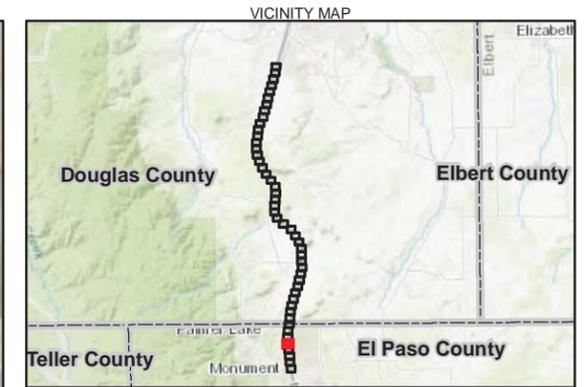
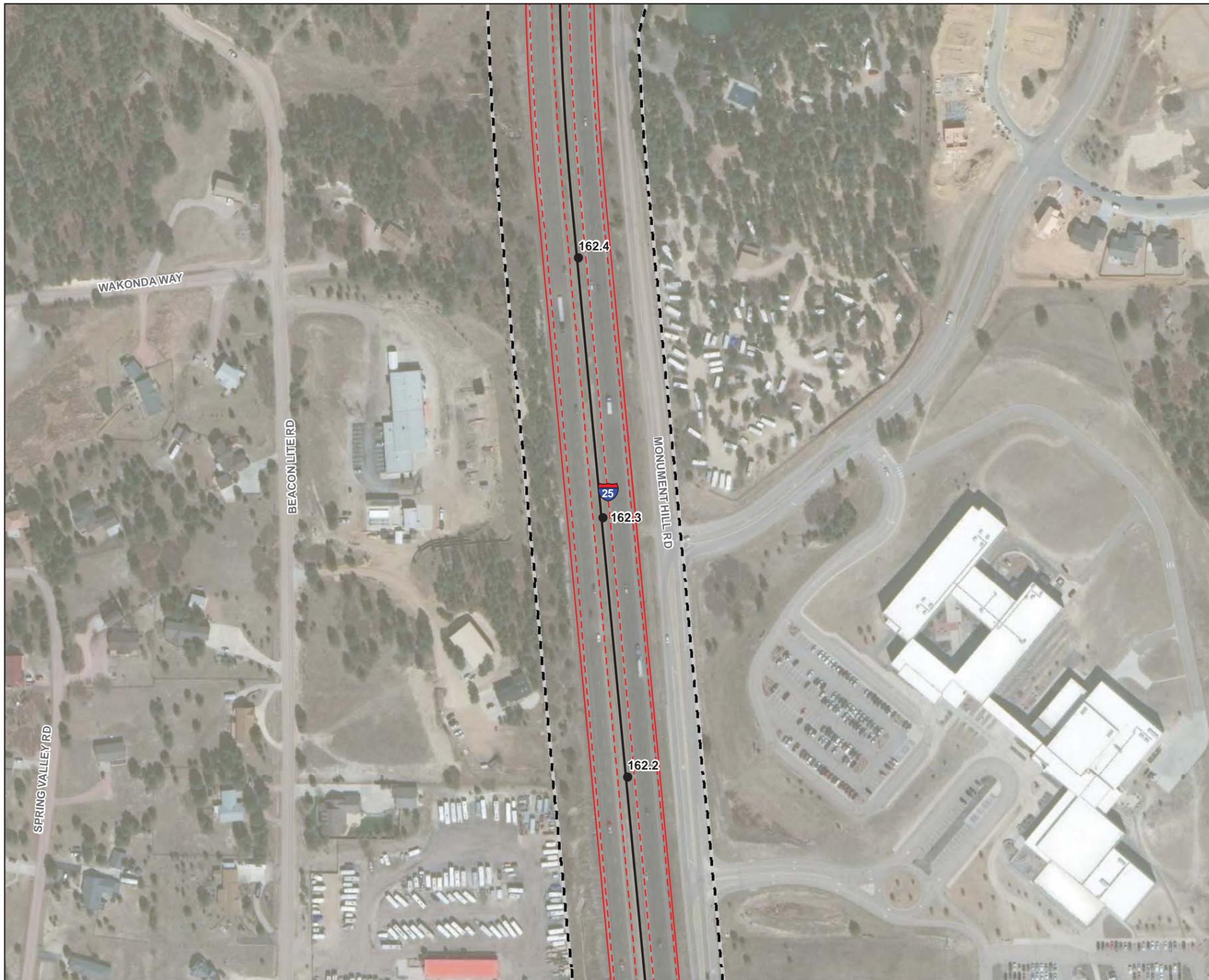




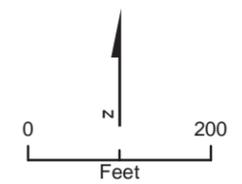
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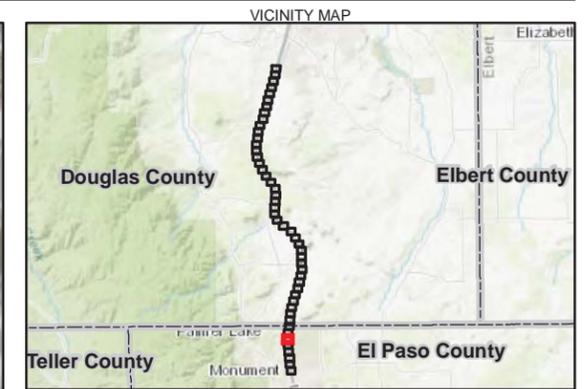


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Proposed Roadway Footprint
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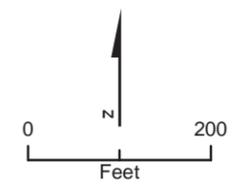


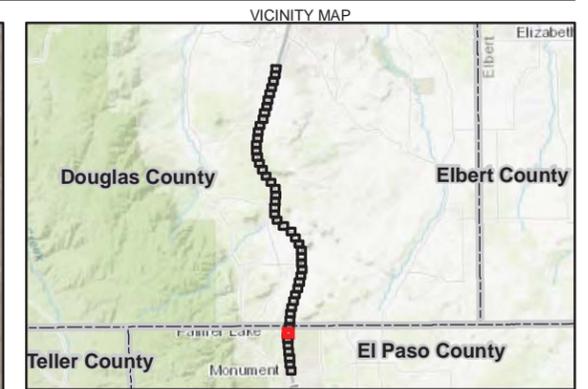
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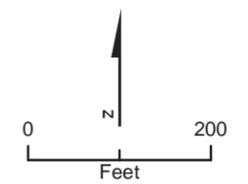
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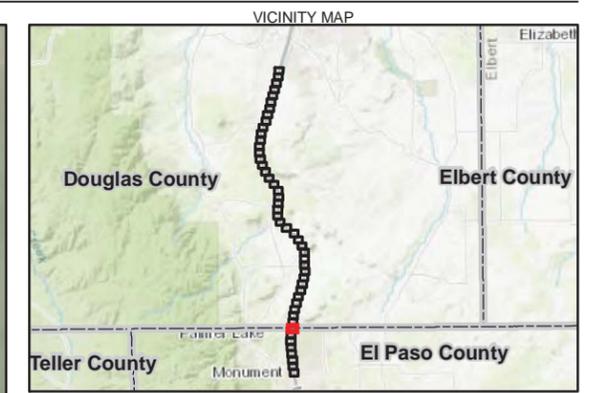
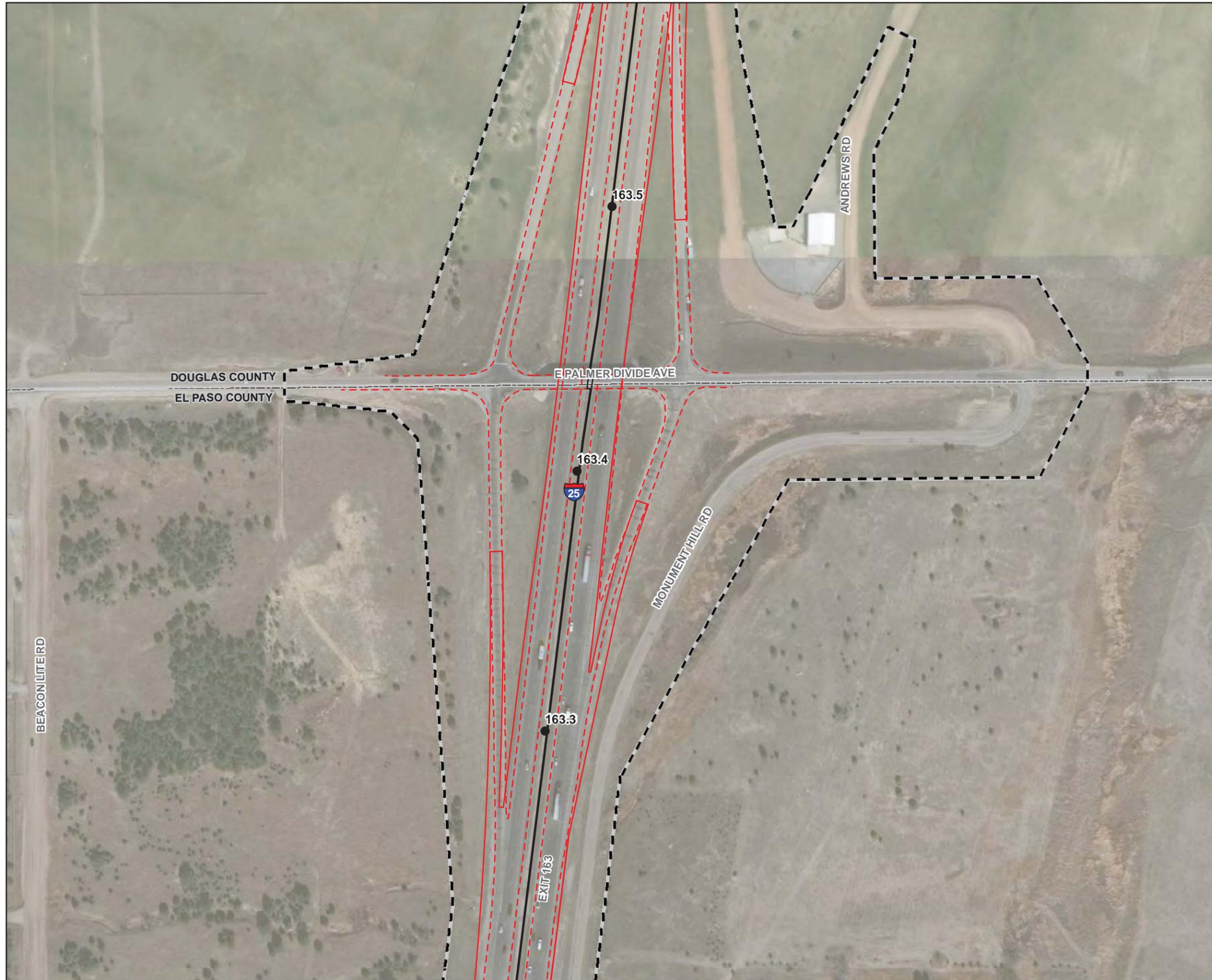




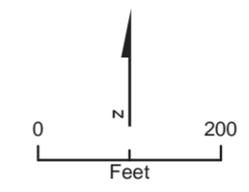
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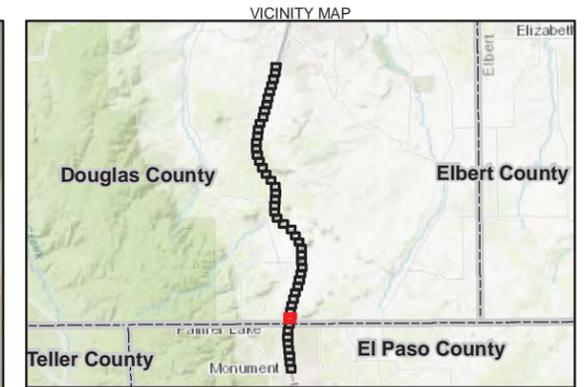
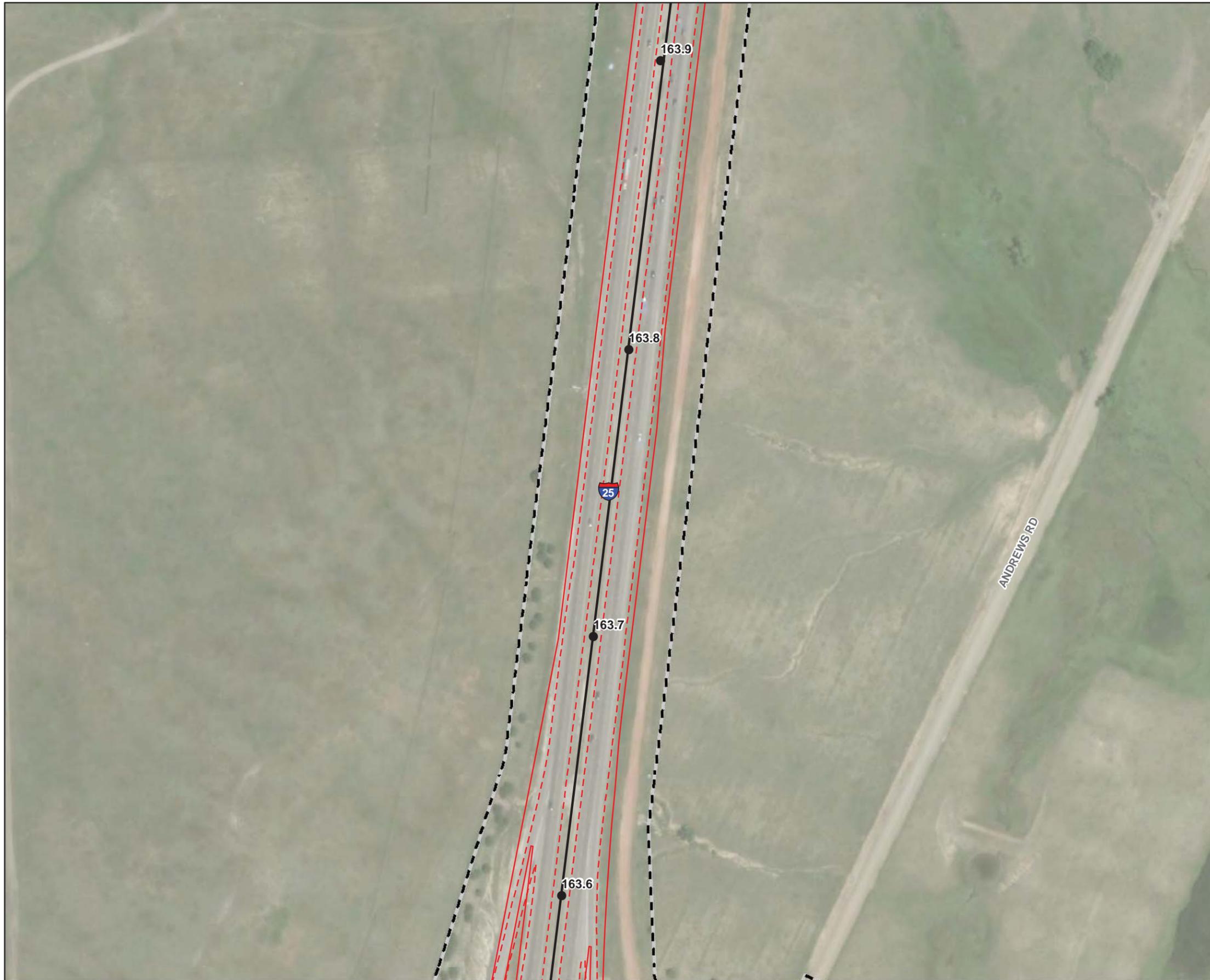
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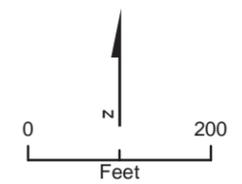


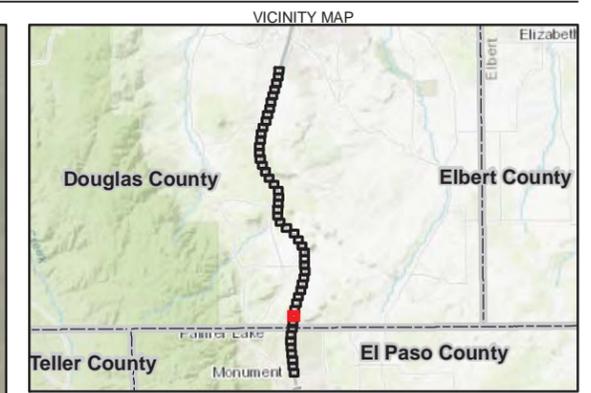
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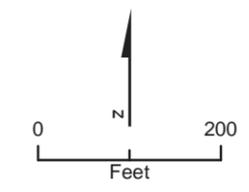


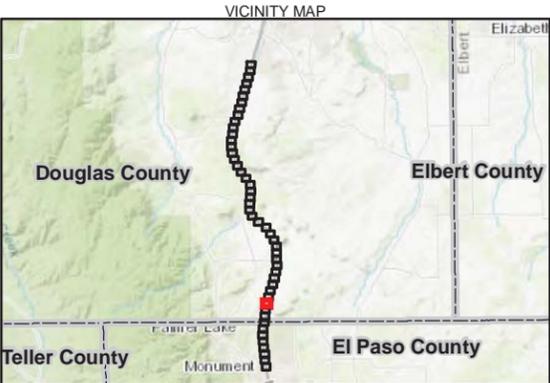
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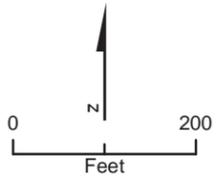


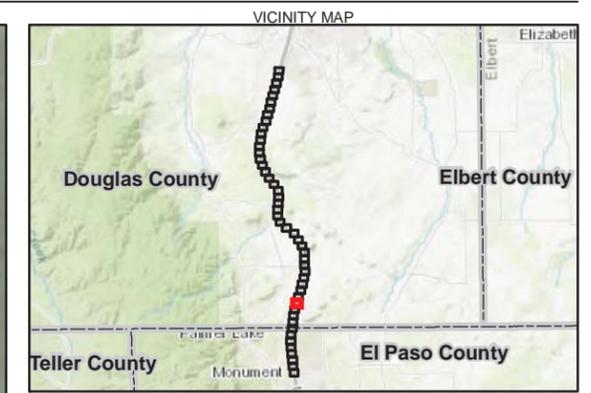
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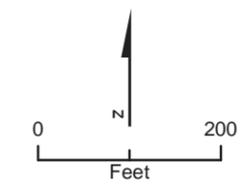


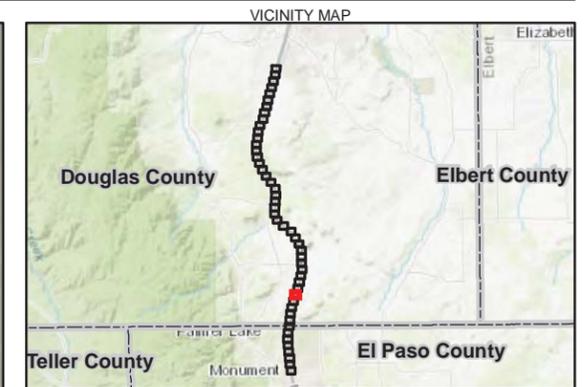
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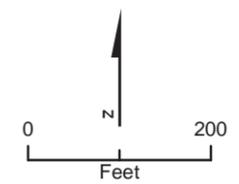


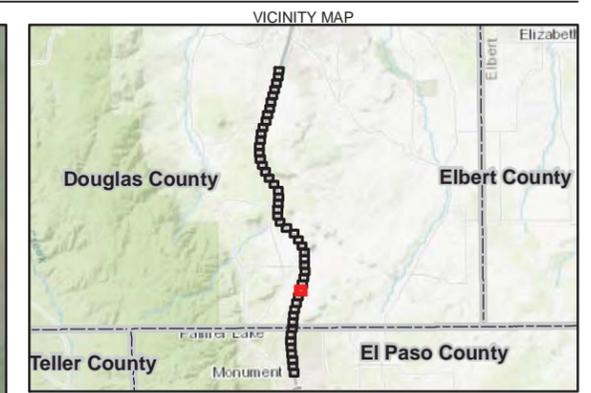
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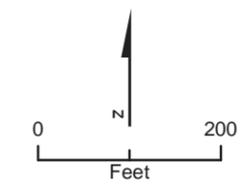


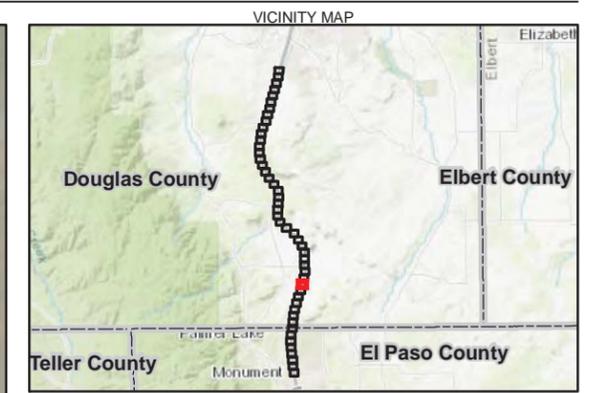
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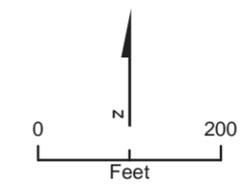


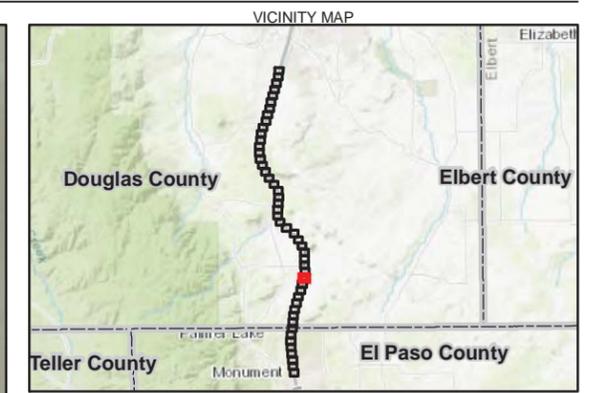
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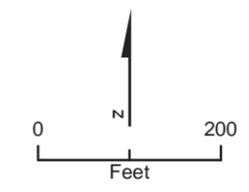


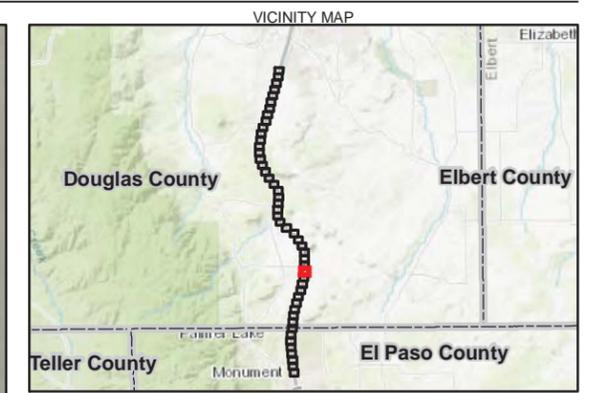
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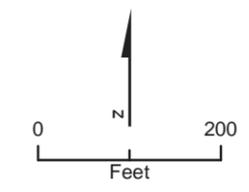


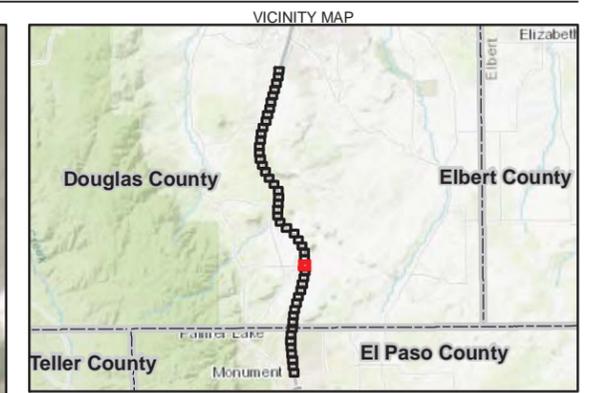
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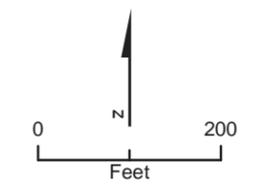


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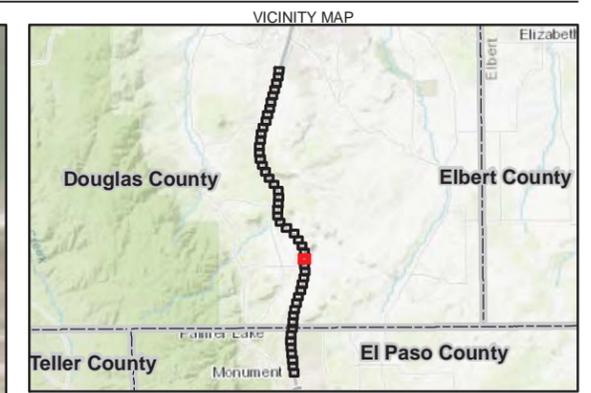




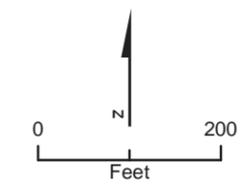
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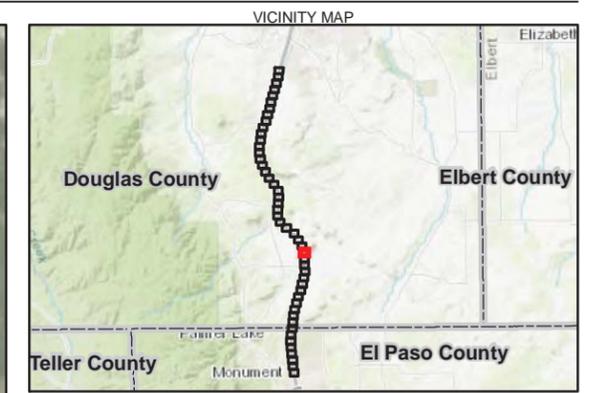


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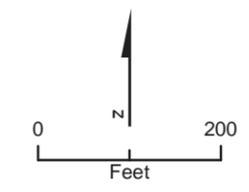


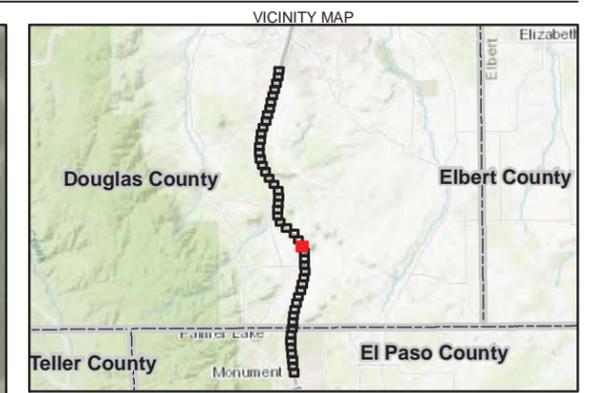
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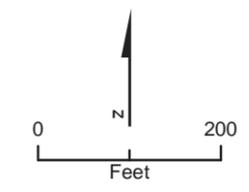


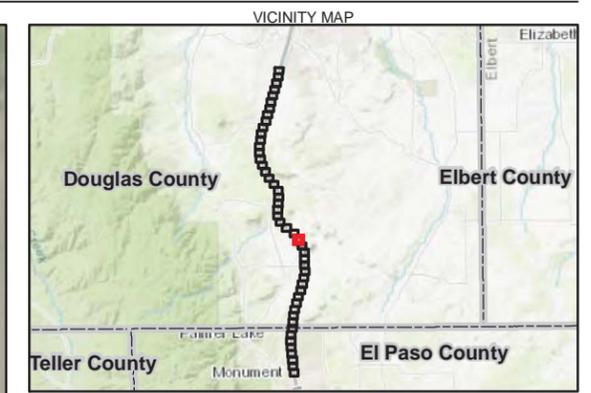
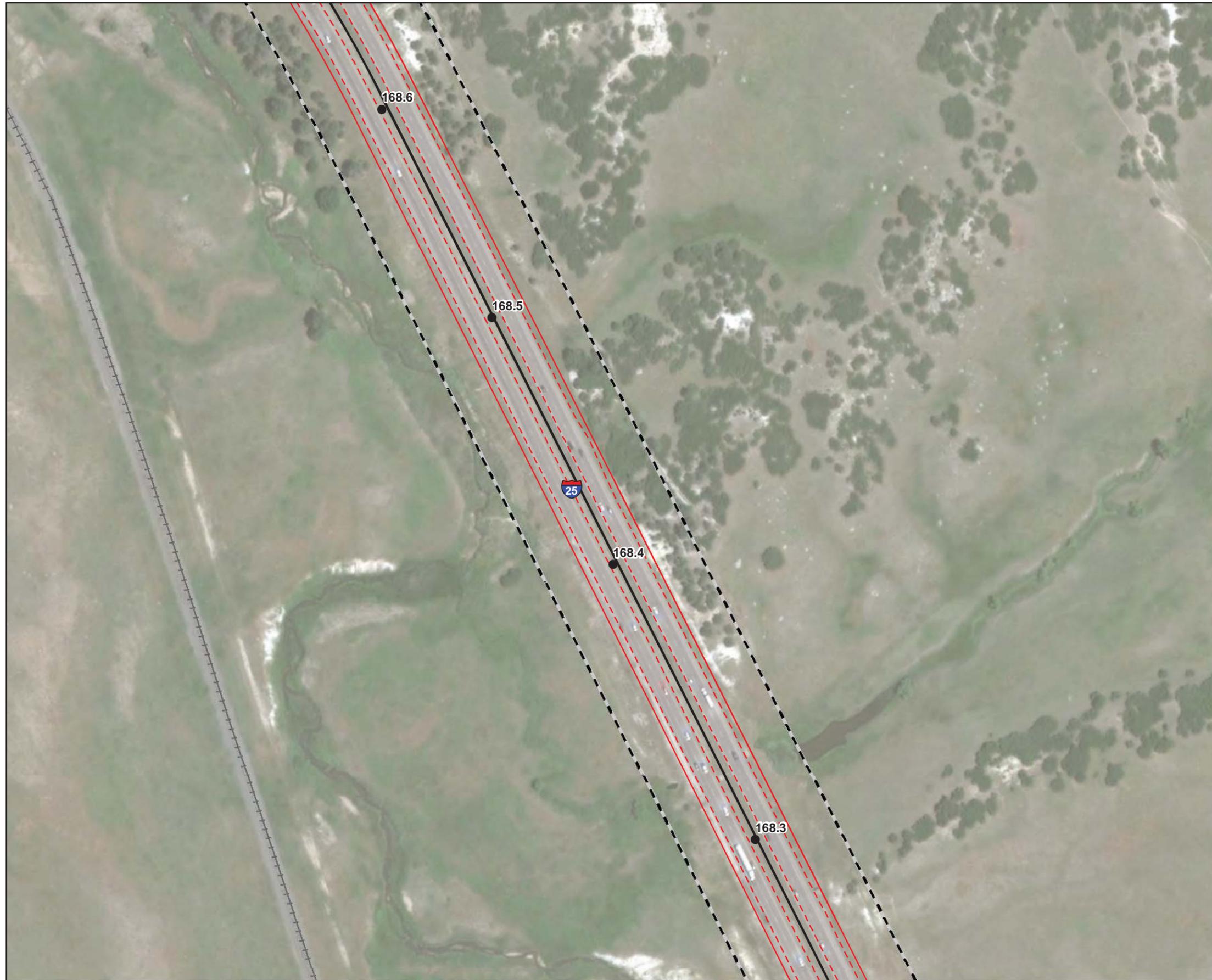
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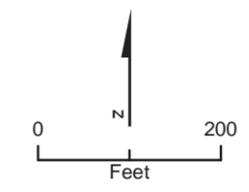


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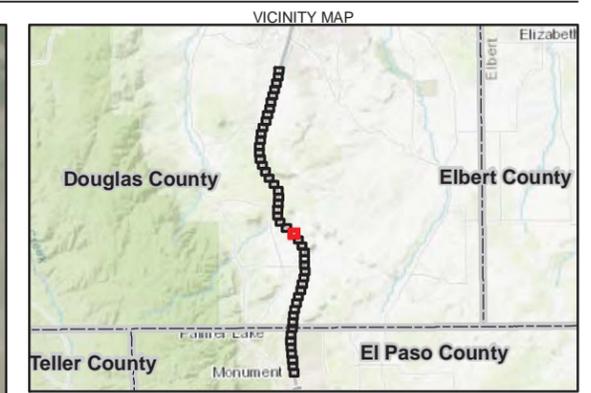




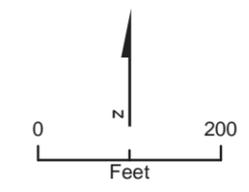
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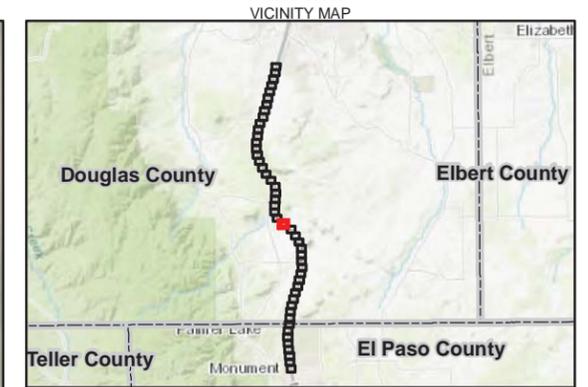
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Proposed Roadway Footprint
I-25 South Gap EA
 March 2018



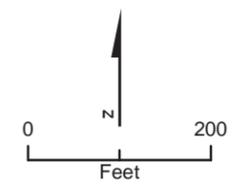
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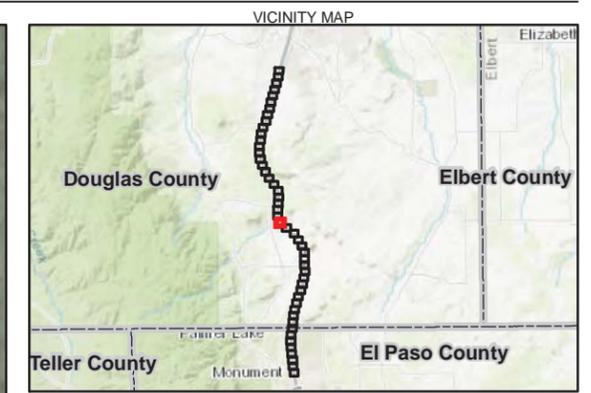


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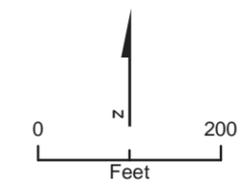


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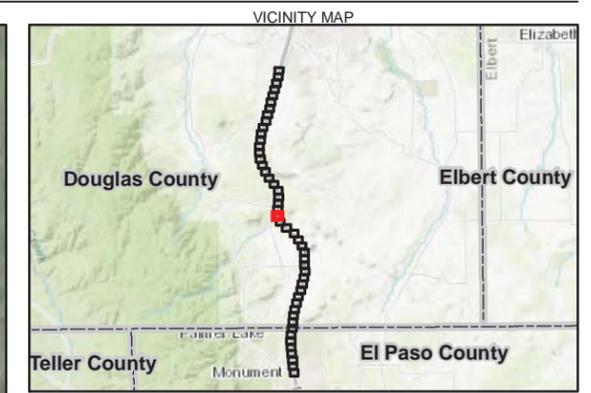




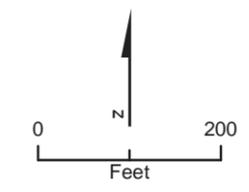
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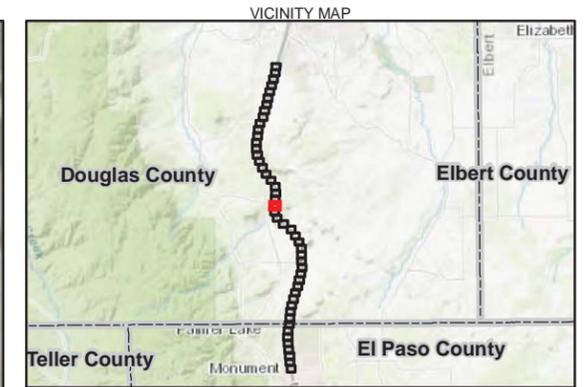
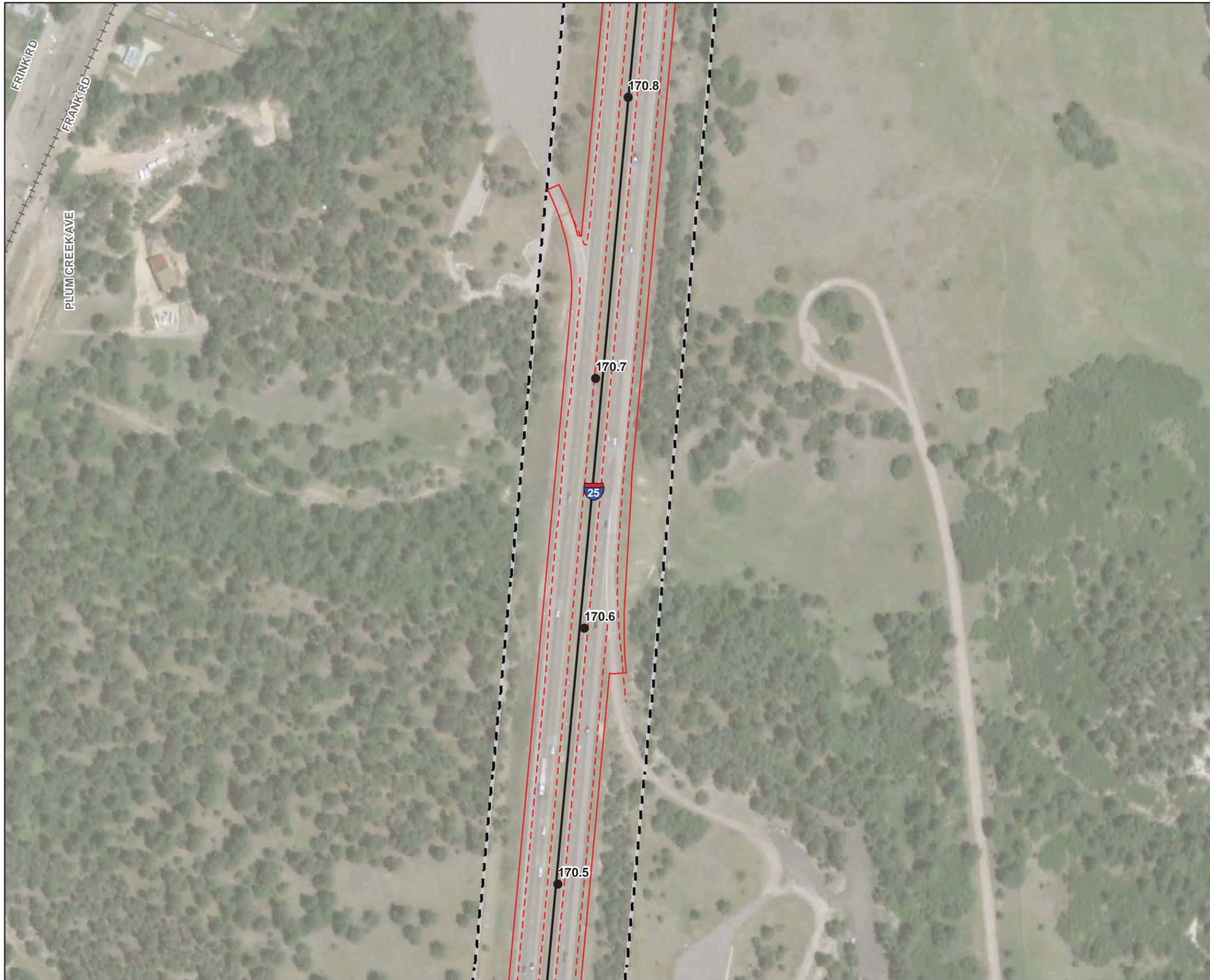


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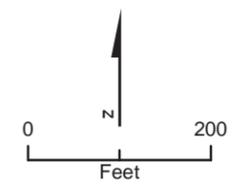


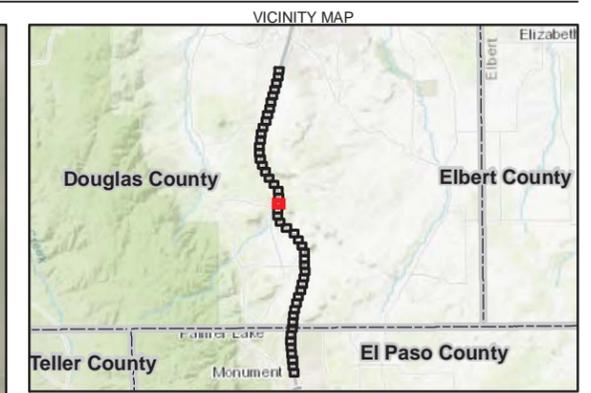
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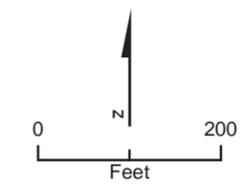


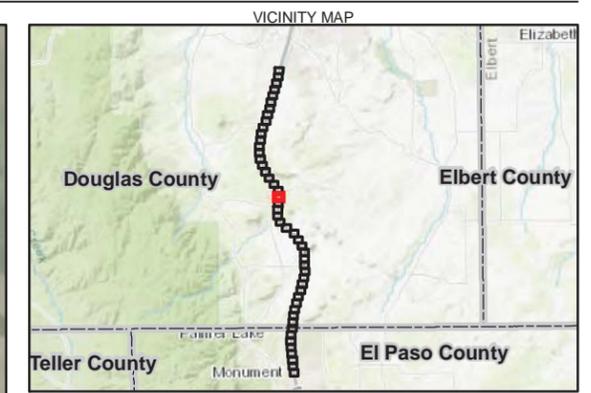
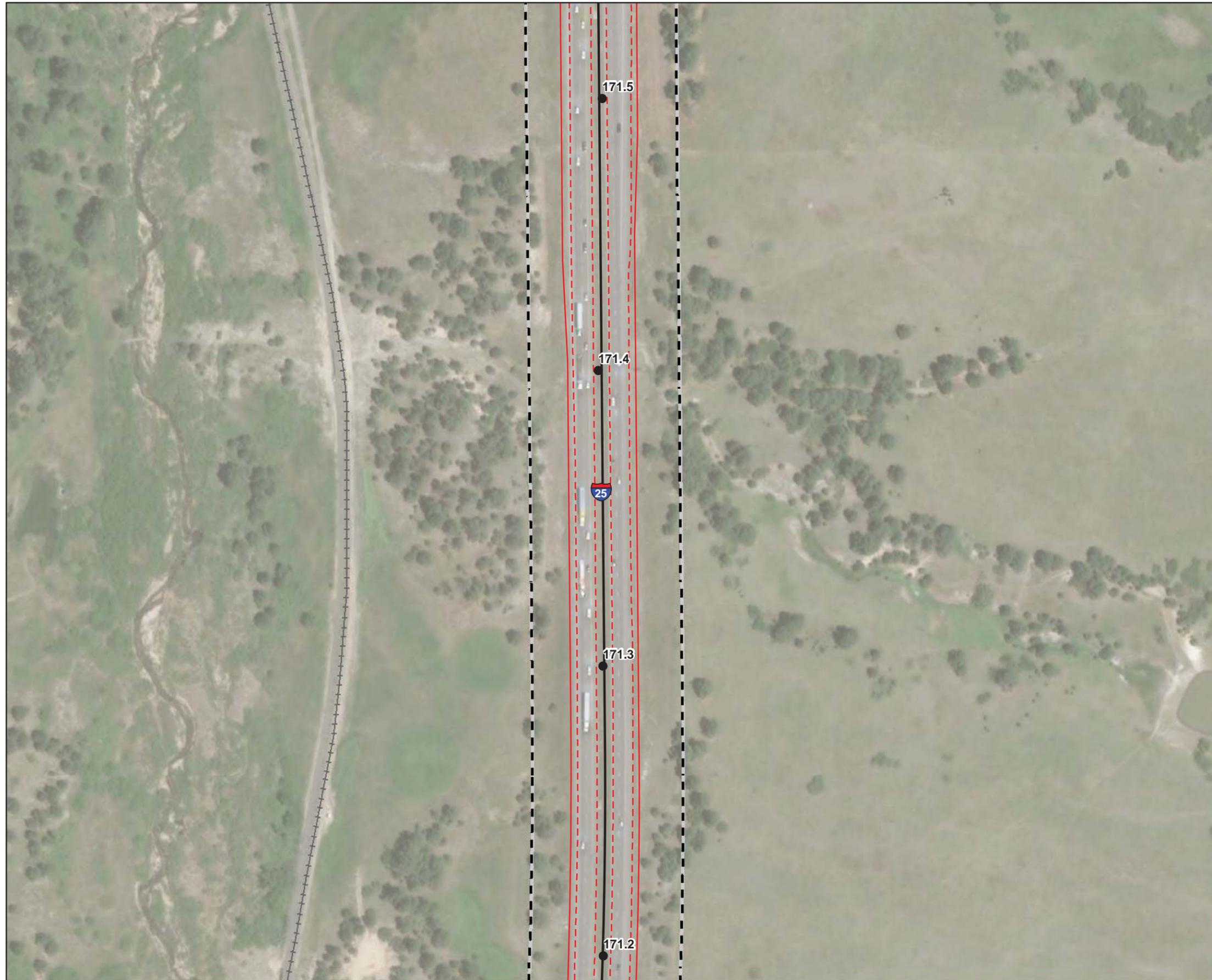
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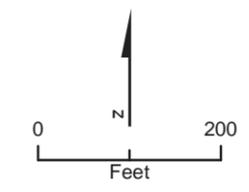


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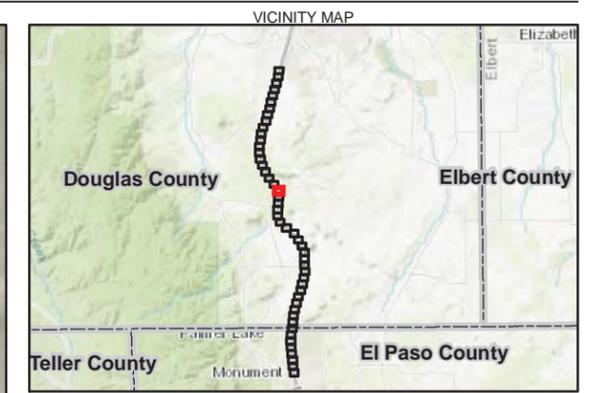




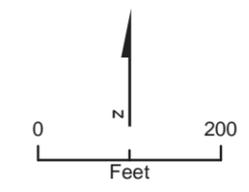
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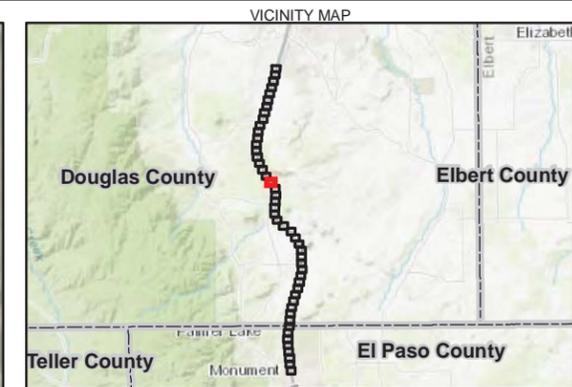
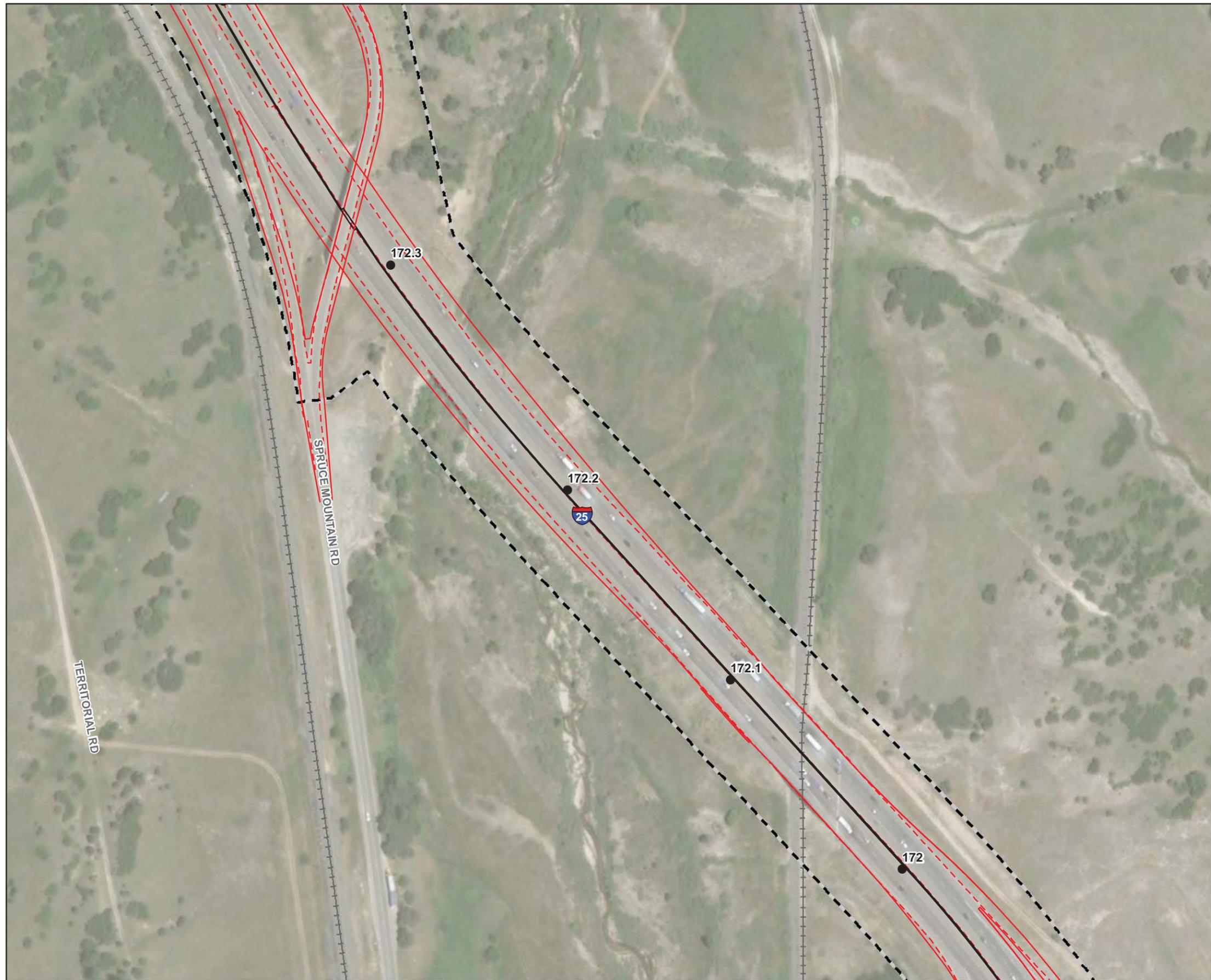


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Proposed Roadway Footprint
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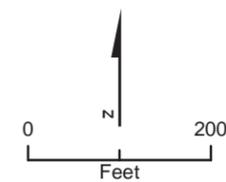


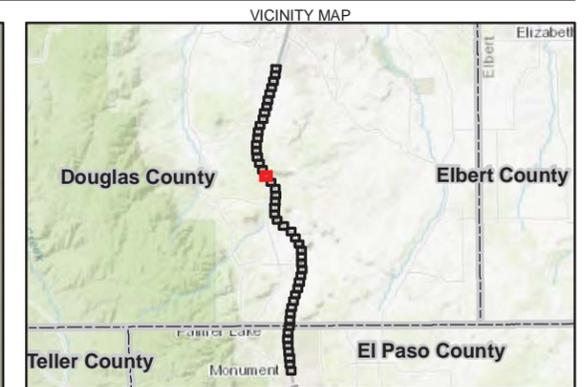
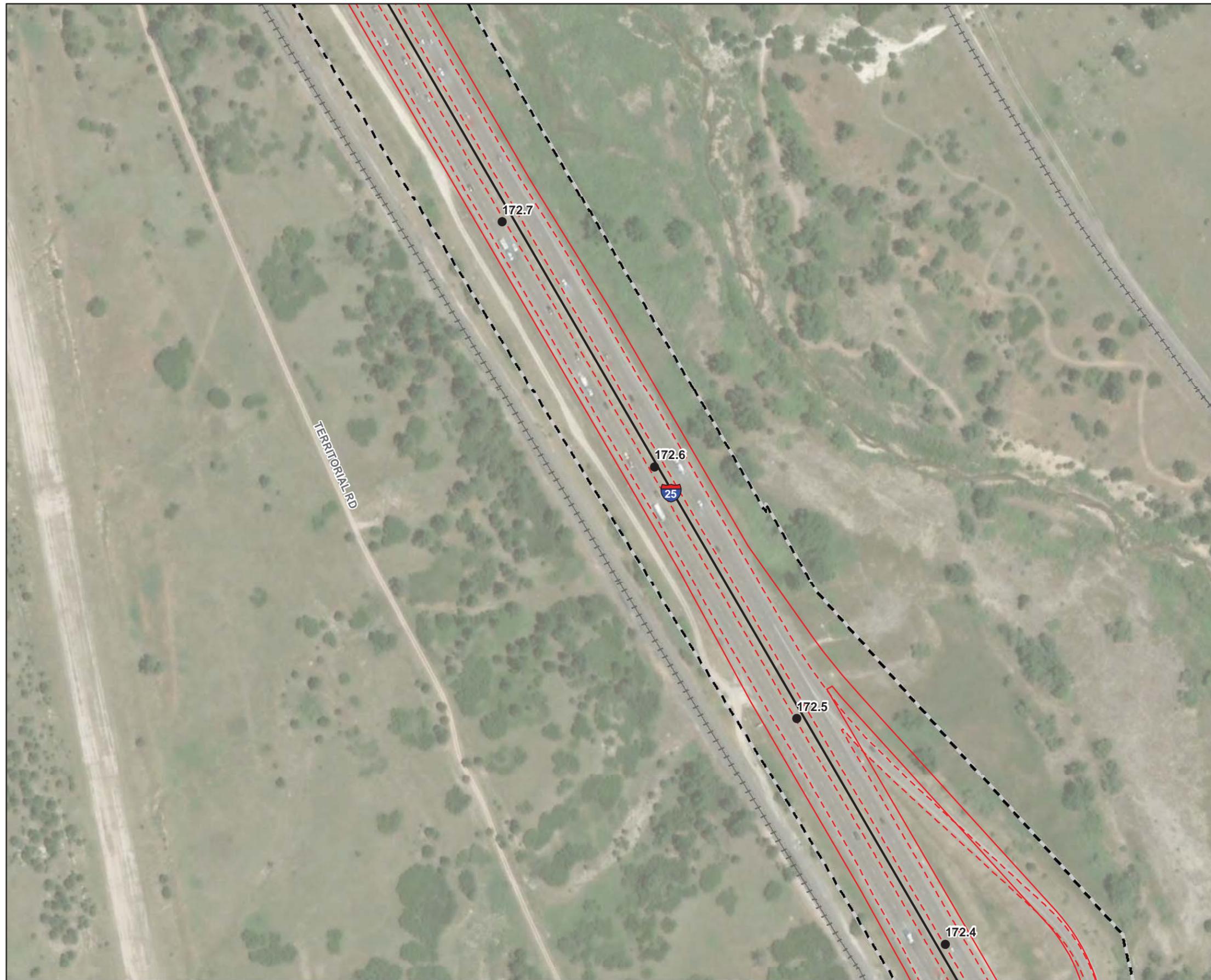
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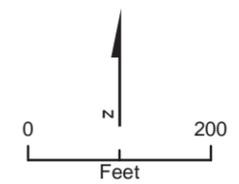


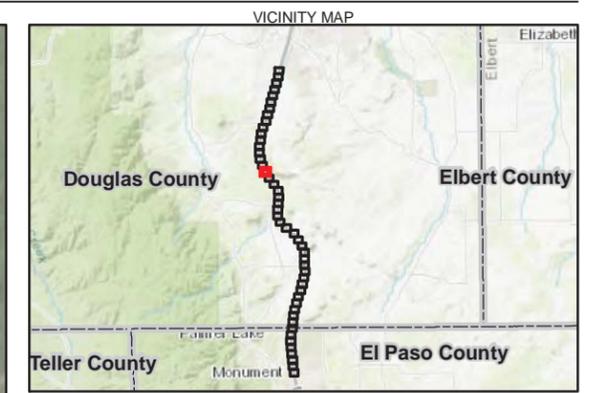
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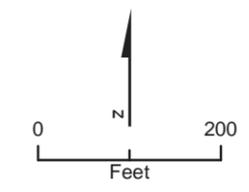


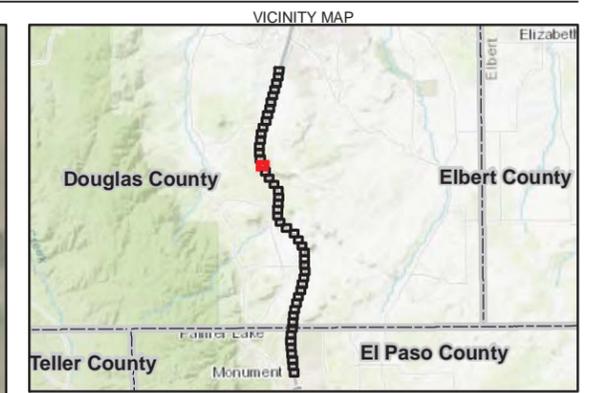
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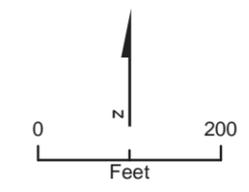


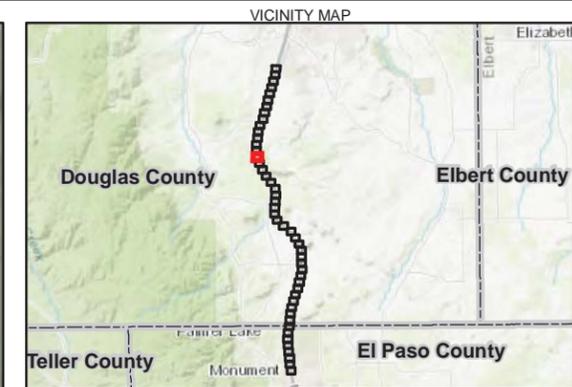
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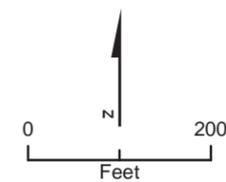


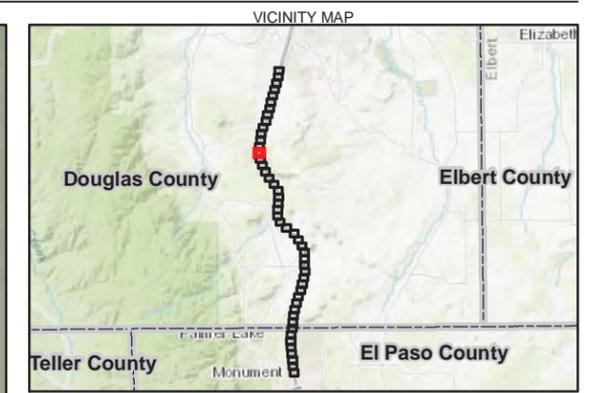
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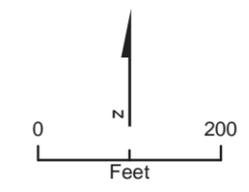


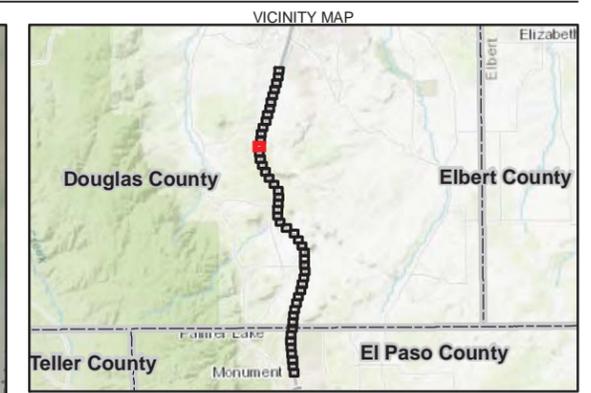
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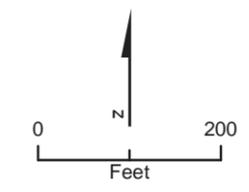


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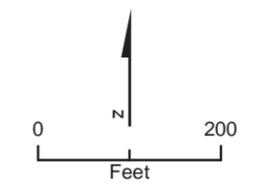


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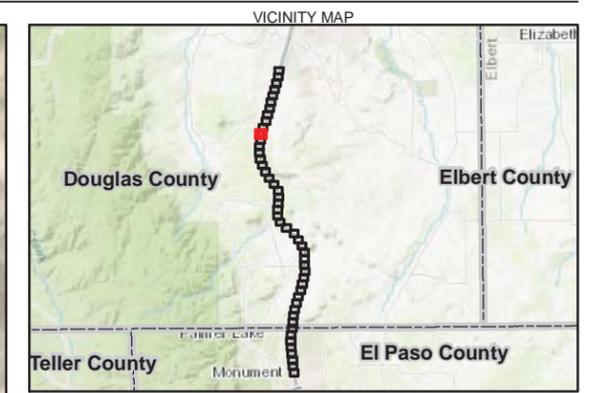
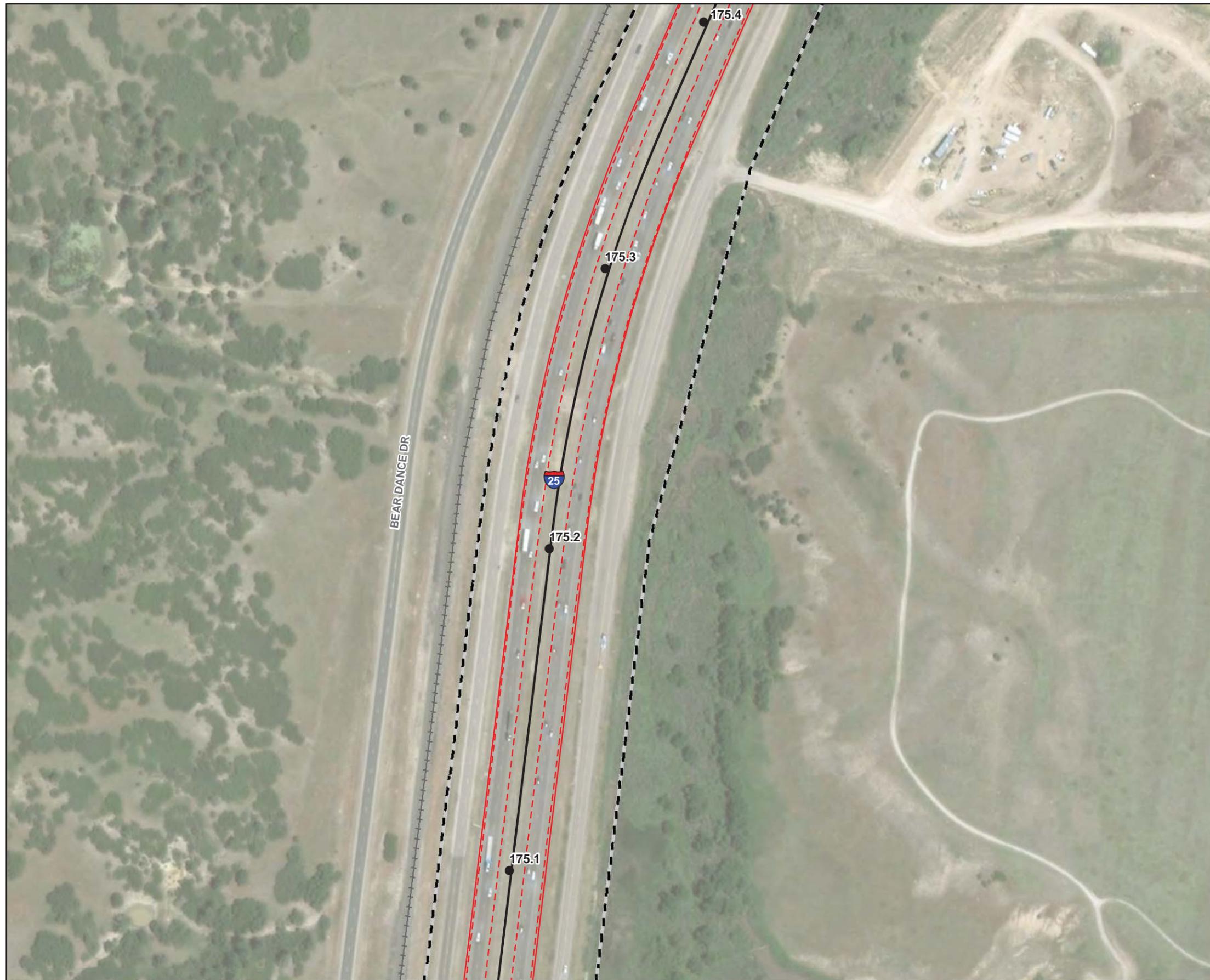




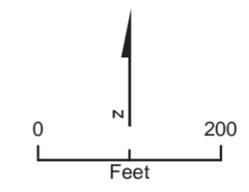
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Proposed Roadway Footprint
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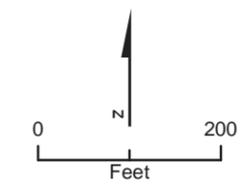


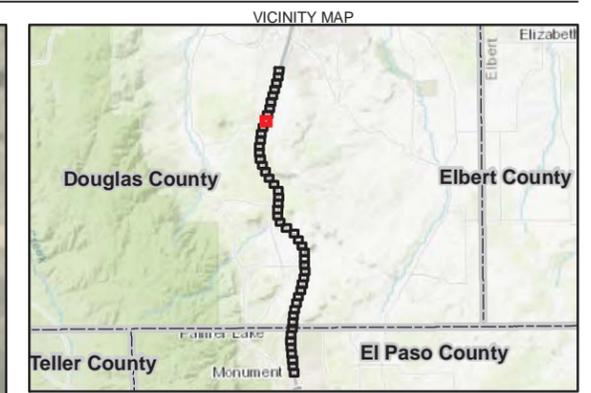
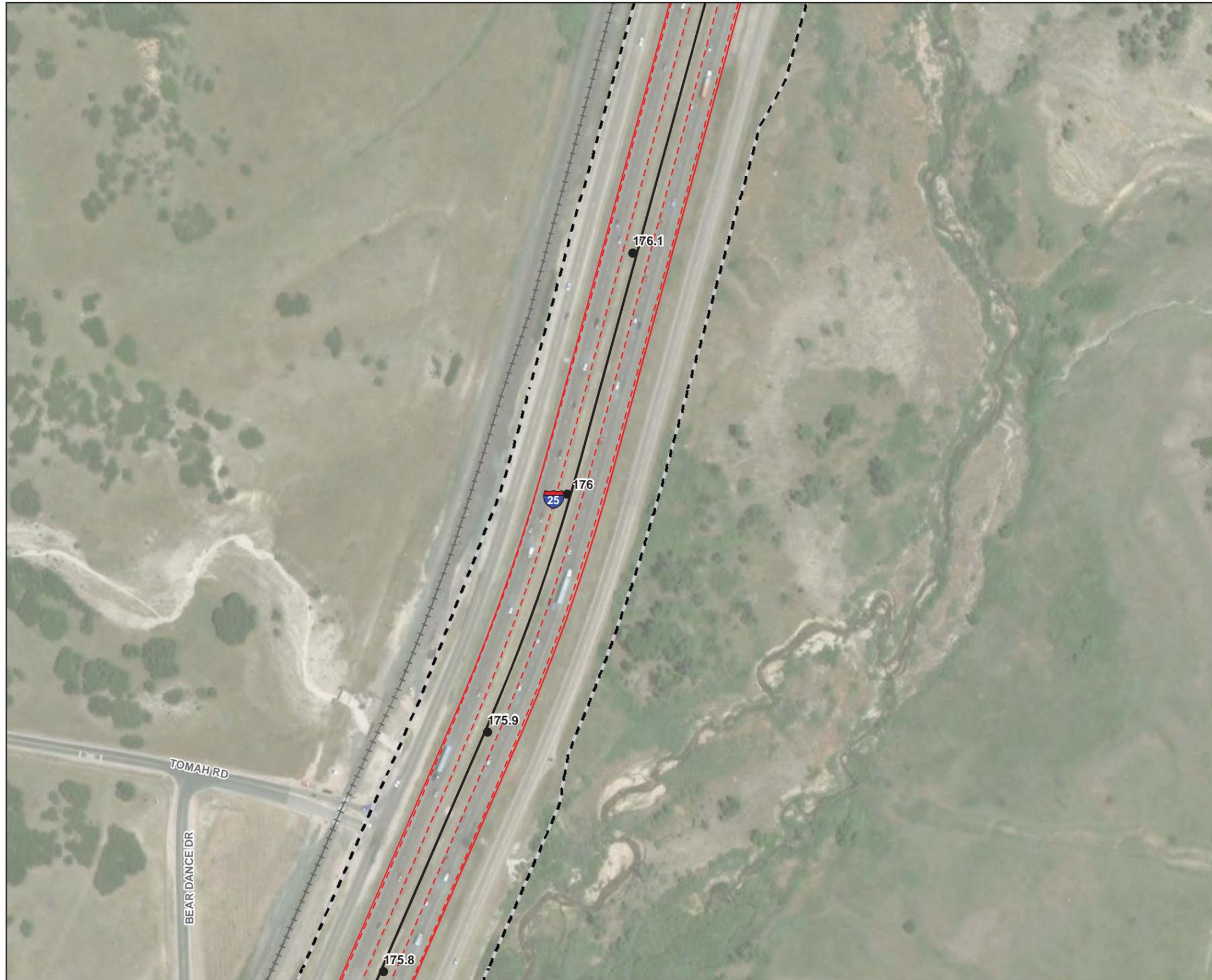
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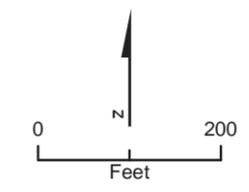


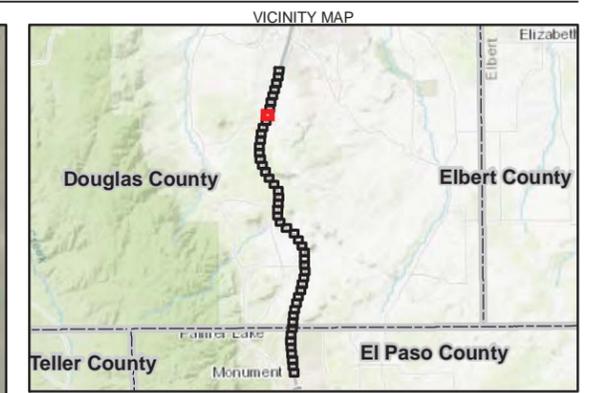
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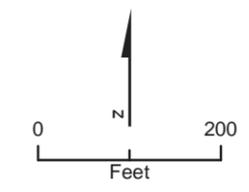


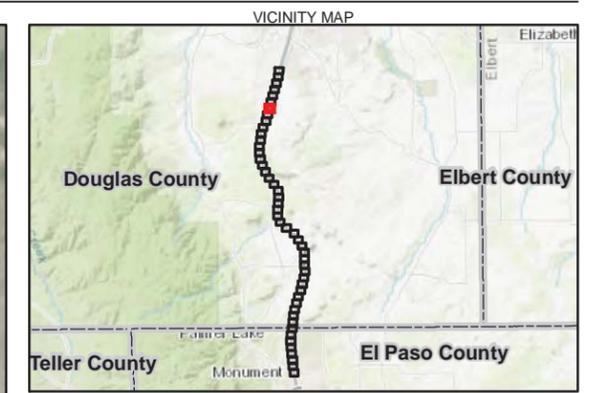
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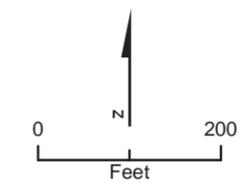


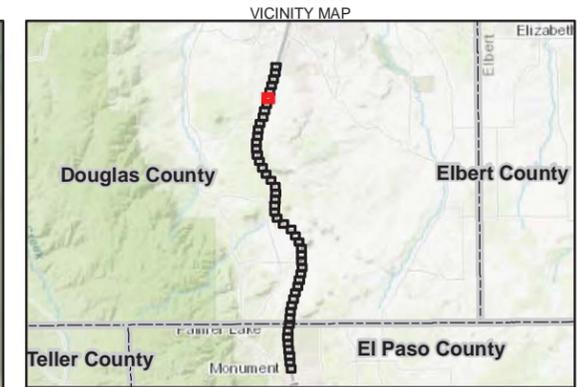
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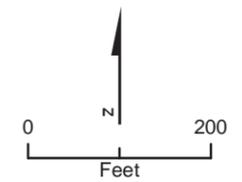


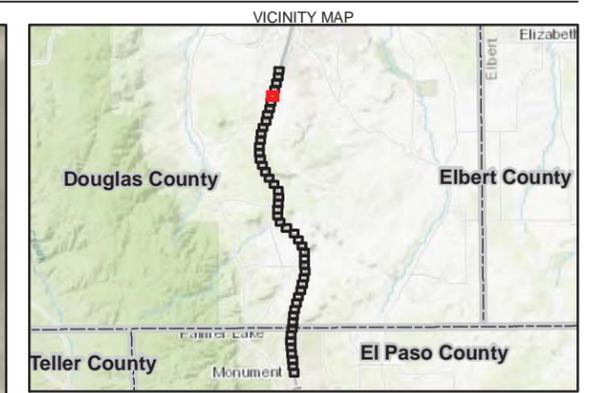
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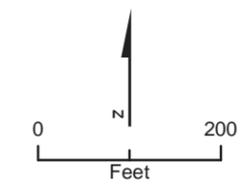


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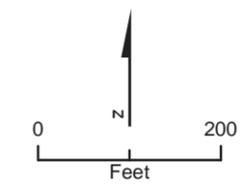


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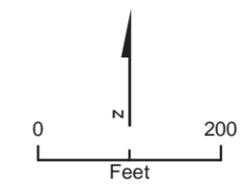


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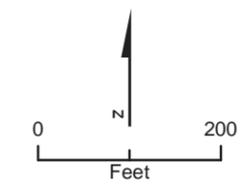


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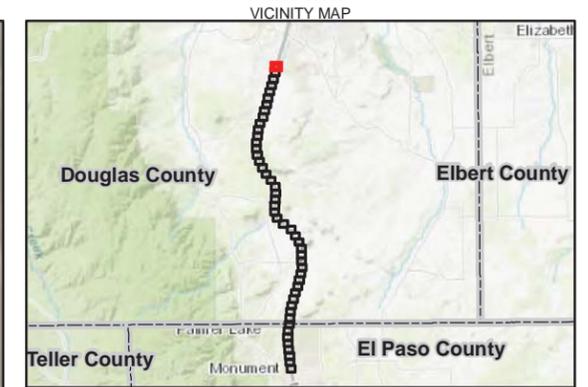


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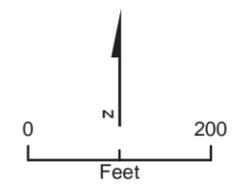


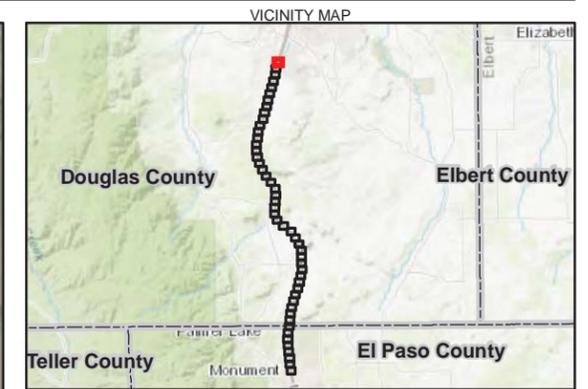


Pavement, striping, and signing transition work will occur north of milepost 179 to develop and transition the Express Lane. Details of the transition will be finalized in final design.

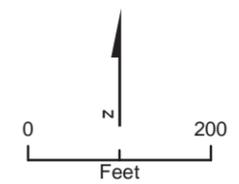


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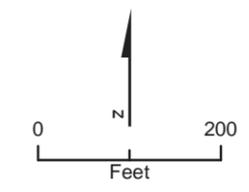


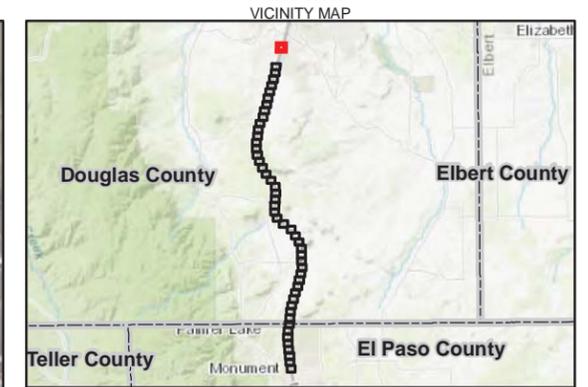
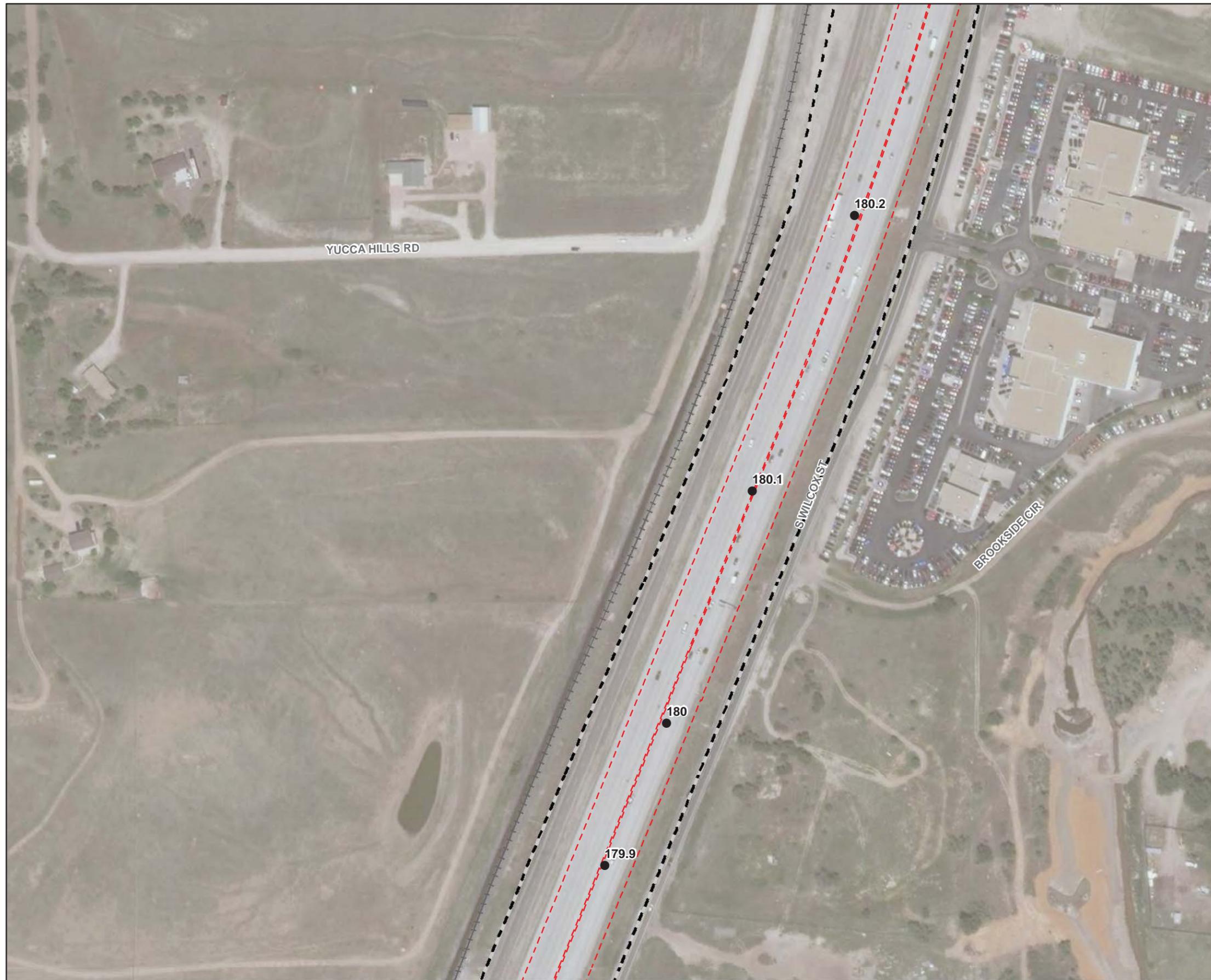
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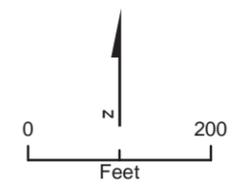


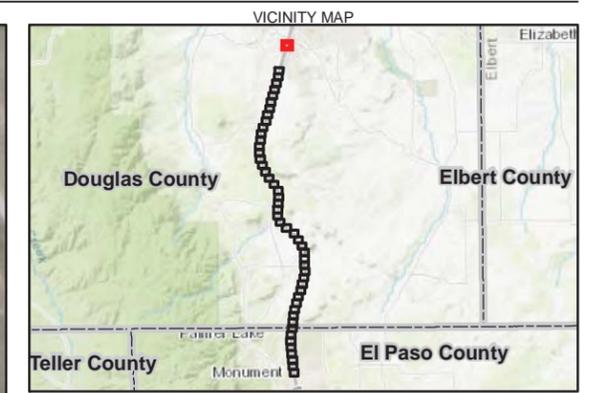
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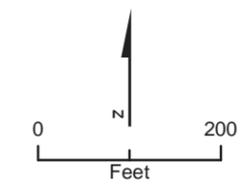


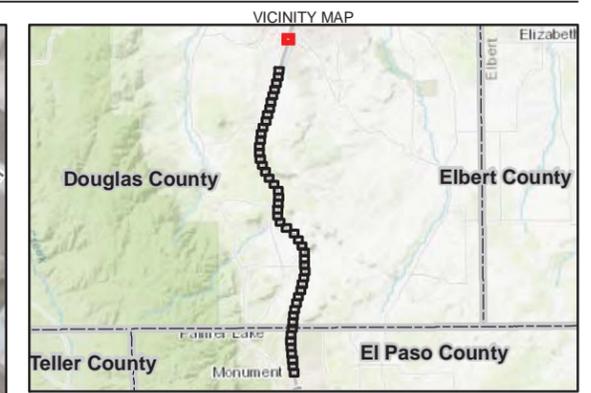
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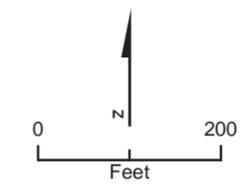


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Appendix A7 Project Description

Project Description

The Interstate 25 (I-25) South Gap Project is located along an 18-mile segment of I-25 between the City of Denver and the City of Colorado Springs in the Colorado Front Range (Figure 1). The physical corridor limits stretch from State Highway 105 (SH 105) in the Town of Monument (milepost [MP] 161) to south of the Town of Castle Rock (MP 179), south of Plum Creek Parkway (Figure 2).

Operationally, the Gap corridor extends farther north to account for transition needed to develop the new Express Lane. The project corridor falls within El Paso and Douglas Counties and spans the Denver Regional Council of Governments and Pikes Peak Area of Council Governments planning areas. This project is a high priority for the Colorado Department of Transportation (CDOT). An Executive Oversight Committee, comprising CDOT and Federal Highway Administration executive management, provides executive guidance and leadership for the project. The project delivery is managed by CDOT Region 1, with support from CDOT Region 2 and other CDOT Divisions.

The Project would widen I-25 between Monument and Castle Rock from 2 to 3 lanes and widen shoulders (Figure 3). Other important project elements include widening or replacing up to seven bridges, constructing wildlife underpass structures and wildlife fencing, constructing median and retaining walls to minimize right-of-way needs, replacing minor culverts, and installing supporting infrastructure, including lighting, concrete barrier and guardrail, and right-of-way fencing. Express lane infrastructure would be installed, including variable toll signage and tolling point equipment, additional video and vehicular detection coverage, and accompanying power and communications systems.

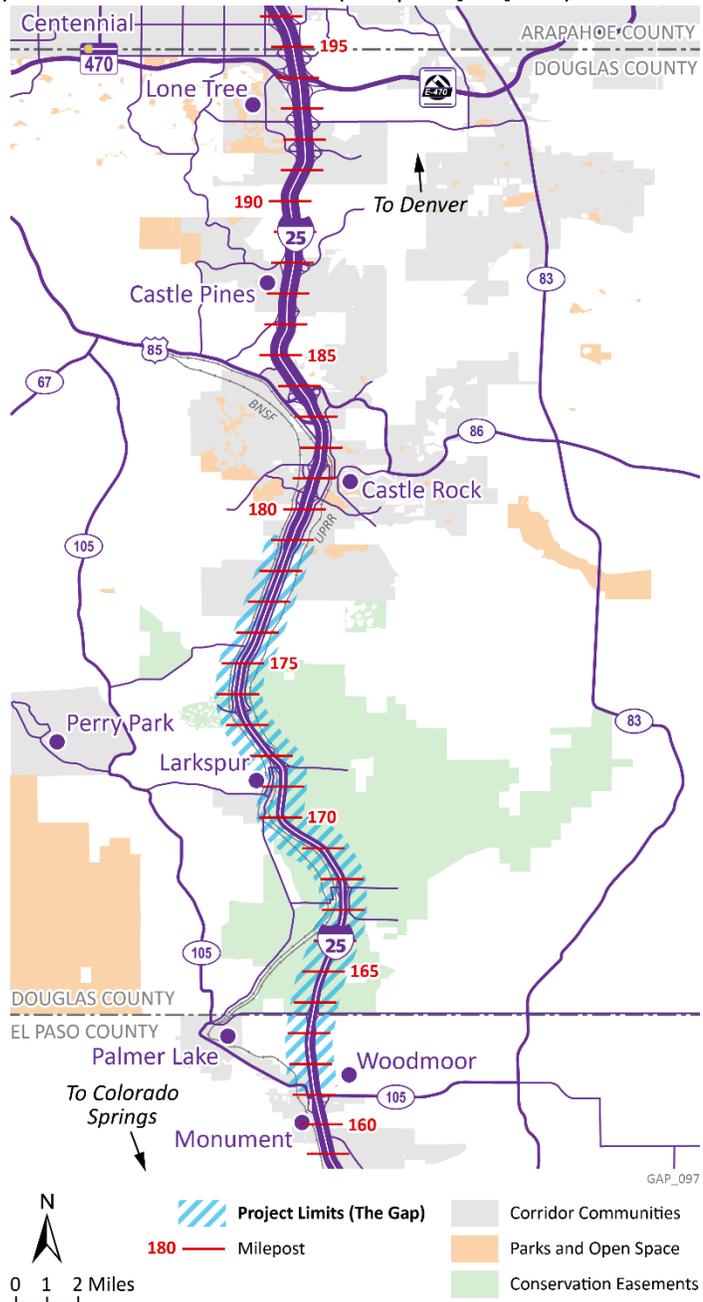


Figure 1. Project Location

All improvements are planned to be within CDOT’s existing right-of-way and the project’s \$350 million budget. This project description coincides with the 30 percent design concepts presented in the Conceptual Design Drawings. As the design progresses, elements of the project may be reduced based on right-of-way, budget, or other constraints. For the purposes of assessing environmental impacts, however, the full design elements are described.

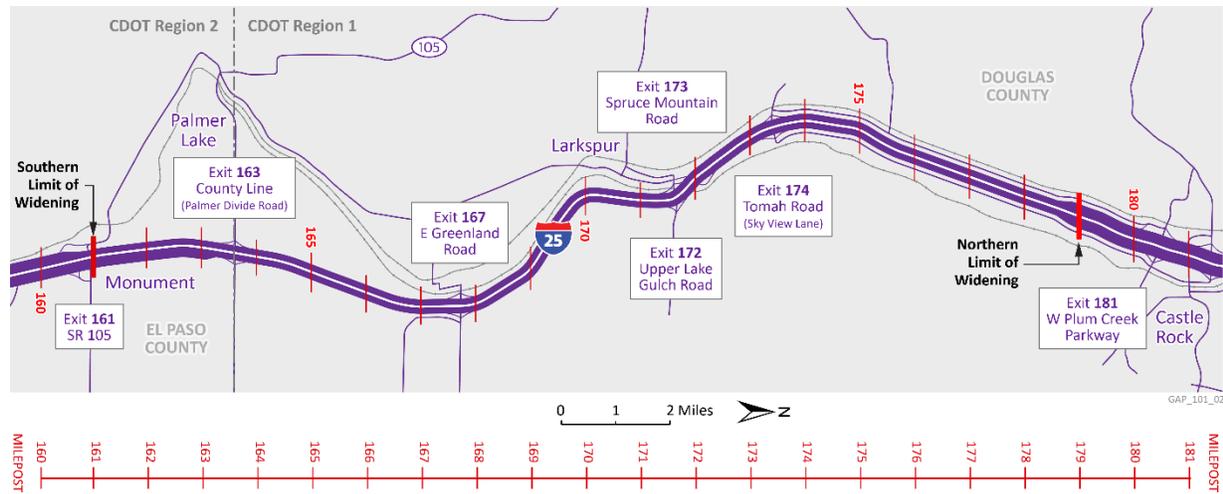


Figure 2. Project Area by Milepost

Roadway

The proposed roadway typical section consists of a 15-foot inside shoulder, new 12-foot Express Lane, 4-foot buffer, two 12-foot general-purpose lanes, and a 12-foot outside shoulder in each direction (Figure 3). In some areas where right-of-way or infrastructure is constrained, shoulder widths would be reduced.

Approximately 6 miles, or one-third, of the 18-mile project has split vertical grades (northbound and southbound lanes at different profiles). For the split grades greater than 3 feet, median walls varying from approximately 5 to 15 feet in height would be required. The 6 miles of split grade are not contiguous; split grades occur on I-25 throughout the 18-mile length of the project. To minimize the project’s footprint, widening would occur toward the center of the interstate, eliminating the existing width of grassy median. Because the existing median varies in width, widening to the outside of the existing pavement would also be required through much of the project. Widening to the outside of the existing pavement is anticipated for the northern 16 miles of the project from approximately County Line Road (MP 163.4) to the northern terminus of the project. Existing pavement will be reused, milled, and overlaid with approximately 2 inches of new asphalt. In areas of bridge replacements, some of the pavement will be fully reconstructed. New pavement in the widened sections and reconstructed areas will be asphalt at full depth of 12 inches.

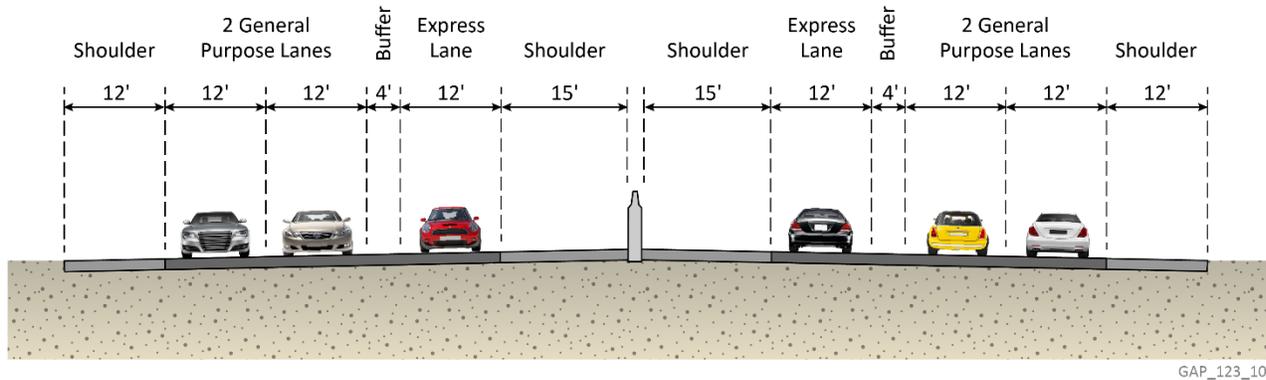
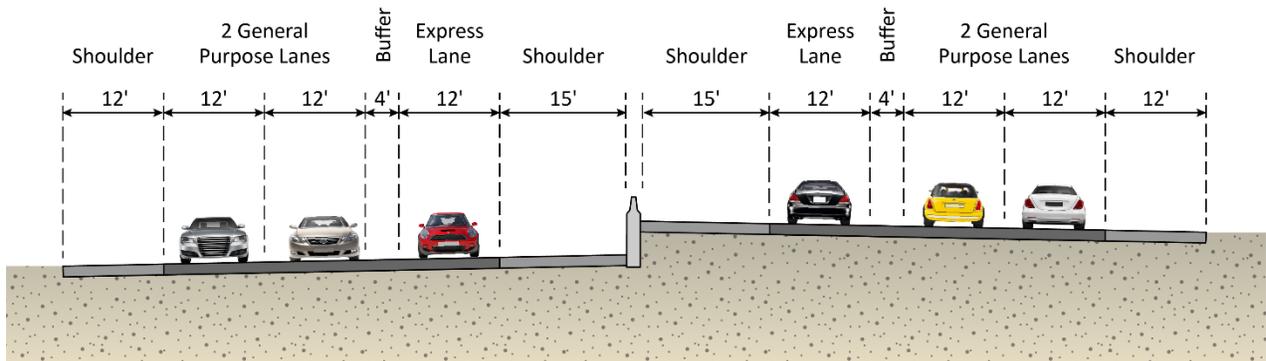
Proposed (Typical – Flat Grade)**Proposed (Typical – Split Grade)**

Figure 3. Proposed Typical Sections

Structures

The following describes the structure condition and engineering recommendations for replacing structures along the Gap corridor. As noted previously, the scope of the project may be reduced through design but for the purpose of the Environmental Assessment, the maximum footprint is reviewed and described here.

Interchanges:

County Line Road (Structure No. H-17-CF)

At MP 163.4 of I-25, County Line Road is carried east-west over I-25 (Figure 4). The existing bridge structure, originally constructed in 1964, has a sufficiency rating of 66 and is nearing the end of its service life. The structure itself is in fair condition, with the deck, super and substructure all having a condition rating of 5. Bridge inspection has identified deck deterioration, heavily rusted girders, and spalling and exposed reinforcing at several locations of the concrete piers and columns of the substructure. In addition to the wear, the structure lacks the width needed to accommodate the proposed roadway section. Due to the sufficiency rating, age, and lack of horizontal clearance, the County Line Road structure would be replaced with a new, wider bridge capable of passing the proposed roadway template. Because adequate vertical clearance is already present, the replacement structure would match the existing structure's height, and the profiles of both I-25 and County Line Road would be maintained. The onramps and offramps for both the southbound and northbound directions would be lengthened up to approximately 520 feet to allow for adequate acceleration and deceleration distances to merge on and off I-25. Ramp terminal locations would remain at their existing locations.



Figure 4. Looking North from MP 163.4

Greenland Road

At MP 167.3, I-25 is carried over Greenland Road via a single-cell concrete box culvert (CBC) (Figure 5) originally constructed in 1948. The existing 14-foot-wide single-cell CBC has a sufficiency rating of 58. The CBC structure serves two-way traffic on Greenland Road and has neither sufficient vertical or horizontal clearance. Commercial trucks pass through with virtually no clearance, as is evidenced by the scrapes and residual paint on the ceiling and walls of the culvert. The existing culvert is a physical and operational bottleneck for Greenland Road traffic and limits the interchange's ability to be used in incident management.

The Greenland Road single-cell CBC would be replaced with a twin-bridge structure with sufficient clearance to accommodate two-way traffic on Greenland Road. Except for the southbound I-25 offramp, which would be shortened approximately 90 feet, the onramps and offramps would be lengthened between 200 and 1,000 feet to allow for adequate acceleration and deceleration distances to merge on and off I-25. Both ramp terminals would be shifted approximately 160 feet toward the I-25 centerline to improve operation of the adjacent Best Road/Greenland Road intersection and to avoid ramp encroachments into the frontage road (east) and creek (west).

The vertical profile of I-25 would be raised in this location to account for the increased vertical clearance of Greenland Road under the interstate. Reconstruction and flattening of vertical curves along the I-25 mainline would be required for a total of approximately 4,000 feet of I-25 north and south of the new bridge.



Figure 5. Looking Across I-25 from East Greenland Road at MP 167.3

Upper Lake Gulch Road (Structure No. H-17-CH)

I-25 is carried over Upper Lake Gulch Road at MP 171.1 via twin bridges constructed in 1964. Both bridges have a sufficiency rating of 77. The decks are considered Structurally Deficient due to deterioration, spalling, and concrete delamination. In addition, the existing twin bridges do not have adequate vertical clearance over Upper Lake Gulch Road and do not have adequate width to accommodate the roadway section. Additionally, the bridges are located along a curve in the roadway that needs to be realigned to meet horizontal clearance requirements.

The project would replace the twin bridges at Upper Lake Gulch Road. The new bridge will be wider and at a higher profile than the existing bridges, and additional reconstruction of I-25 would be north and south of the bridge (within the roadway prism). Except for the northbound offramp, which would remain at its current length, the onramps and offramps would be lengthened between approximately 200 feet and 2,000 feet. Ramp terminals would remain in their existing locations.



Figure 6. Looking Across I-25 from Upper Lake Gulch Road at MP 171.1

Spruce Mountain Road (Structure No. H-17-CG)



Figure 7. Plan View of Spruce Mountain Road Flyover at MP 172.3

At MP 172.3 of I-25, onramps and offramps connect to Spruce Mountain Road, with the southbound offramp merging at-grade to southbound Spruce Mountain Road, and northbound Spruce Mountain Road connecting to I-25 on a bridge that functions as a one-way flyover onramp to northbound I-25 (Figure 7). This onramp was built in 1964 and has a sufficiency rating of 73. The bridge is identified as Functionally Obsolete due to its narrow width. In addition, the bridge is sub-standard for the vertical clearance distance between the bridge and the highway underneath. The steel girders are heavily rusted and the concrete substructure elements are spalled with exposed reinforcing

(Figure 8). Asphalt on the bridge deck is experiencing cracking and the expansion joints are also in poor condition. With modifications, the wider I-25 template could likely be accommodated under the structure but the vertical clearance would remain substandard.

The project would replace the bridge and raise the profile of the I-25 onramp from Spruce Mountain Road. The road/ramp geometry would need to be adjusted to tie into the higher vertical profile of the bridge.



Figure 8. Looking North from MP 172.3 Sky View Lane/Tomah Road (Structure No. G-17-AJ)

At MP 173.8 of I-25, Sky View Lane (also referred to as Tomah Road at this location) is carried over I-25 via a 1965-constructed bridge with a sufficiency rating of 79. The bridge is identified as Functionally Obsolete due to its narrow width (Figure 9). This bridge is in fair condition with the substructure experiencing minor concrete spalls with exposed reinforcing. However, the bridge has a substandard vertical clearance over I-25, and the span is not wide enough to accommodate the wider roadway template. Compounding the vertical clearance issues with this structure, the parabolic arches used in the bridge's original construction do not provide for easy expansion. As the roadway expands under the bridge – widening to the inside and outside – the shape of the arches increases the effective depth of the bridge deck and further reduces the vertical clearance. As a result, the bridge is also too narrow to accommodate the proposed roadway section.

The project would replace the bridge at Sky View Lane with a bridge wide enough and tall enough to accommodate the proposed roadway section. All four onramps and offramps would be extended between approximately 20 feet and 600 feet to allow for adequate acceleration and deceleration distances to merge on and off I-25. Ramp terminals would remain in their existing locations.



Figure 9. Looking North from MP 173.8

Other Structures

Union Pacific Railroad (Structure No. H-14-CQ)



Figure 10. UPRR Bridge

Constructed in 1980, and with a sufficiency rating of 93, this bridge carries I-25 over a single Union Pacific Railroad (UPRR) track at approximately MP 172.1 (Figure 10). The bridge is in good condition but does not provide vertical clearance required by the UPRR. Additionally, the bridge is located along a curve that needs to be reconstructed (flattened), which, combined with the wider roadway section, requires more width than is available with widening of the structure. Due to the lack of horizontal and

vertical clearances, the UPRR bridge would be replaced with a longer-span bridge that could accommodate future UPRR improvements, such as a second track and maintenance road, and meet the vertical clearance requirements specific to railroad bridges.

Plum Creek (Structure No. H-17-AH)

The Plum Creek bridge carries I-25 over Plum Creek at approximately MP 172.3 via a two-span structure built in 1950 as two bridges (Figure 11). In 1980, both structures were widened to the inside to create a single, 106-foot-wide bridge. The bridge has a Sufficiency Rating of 83. Given its age, the structure has seen little deterioration; however, the bridge is not wide enough to accommodate the proposed roadway section. Because the structure lacks the necessary horizontal clearance, the structure would be either widened or replaced. The improved, single span bridge



Figure 11. Plum Creek Bridge

would also be more attractive for large animals, such as elk and deer, to use as a safe underpass of the highway.

Wildlife Underpasses

Four new wildlife underpasses (Figures 12 through 15) would be constructed to facilitate the safe movement of animals underneath the interstate. Dimensions are considered minimum, and are listed as height by width by length:

- MP 162.5 – An existing 24-inch culvert would be replaced with three parallel bridges. The total underpass dimension beneath I-25 would be 10 feet by 50 feet by 200 feet. The third bridge (10 feet by 50 feet by 22 feet) would carry Monument Hill Road.

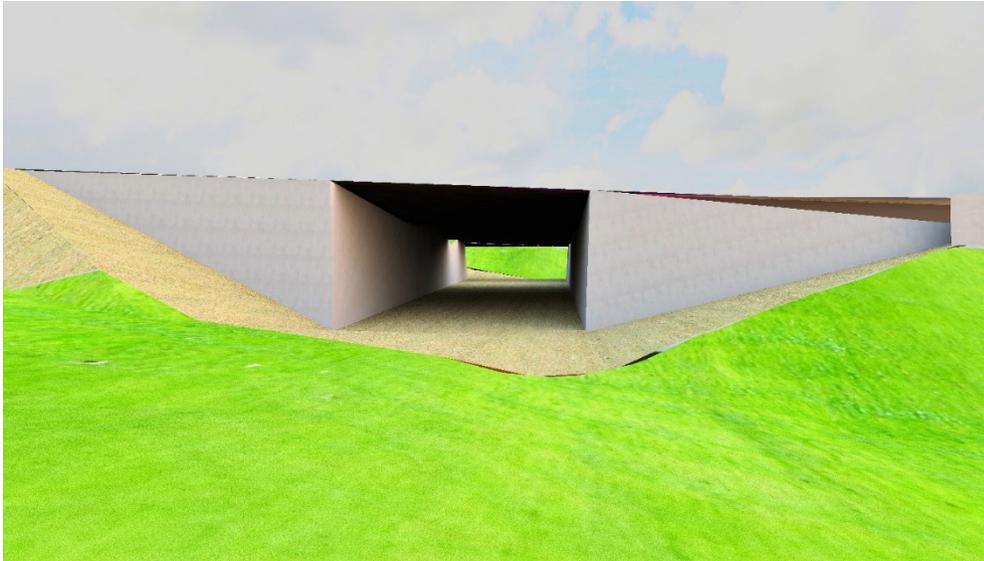


Figure 12. Simulation of Wildlife Underpass at MP 162.5, looking east.

- MP 164.0 – An existing 14-foot by 14-foot concrete culvert would be replaced with two parallel bridges. The total underpass dimension beneath I-25 would be 14 feet by 100 feet by 140 feet.

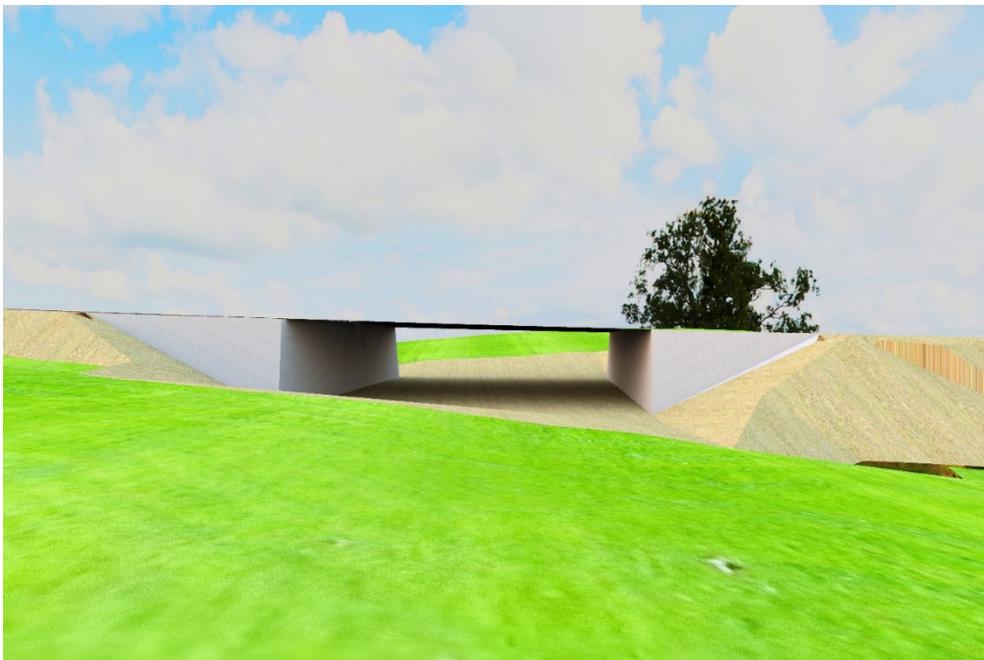


Figure 13. Simulation of Wildlife Underpass at MP 164.0, looking west.

- MP 167.7 – An existing 14-foot by 10-foot box culvert would be replaced with two parallel bridges. The total underpass dimension beneath I-25 would be 15 feet by 100 feet by 150 feet.

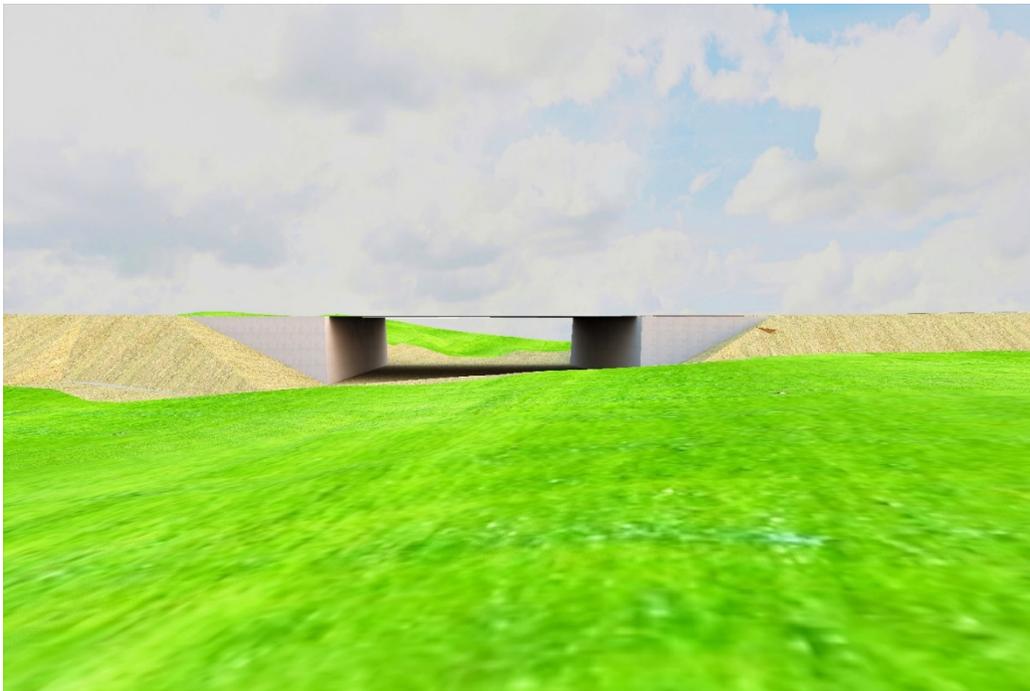


Figure 14. Simulation of Wildlife Underpass at 167.7, looking east.

- MP 170.6 – The 24- and 42-inch culverts would be replaced with two parallel bridges. The total underpass dimension beneath I-25 would be 16 feet by 100 feet by 160 feet.

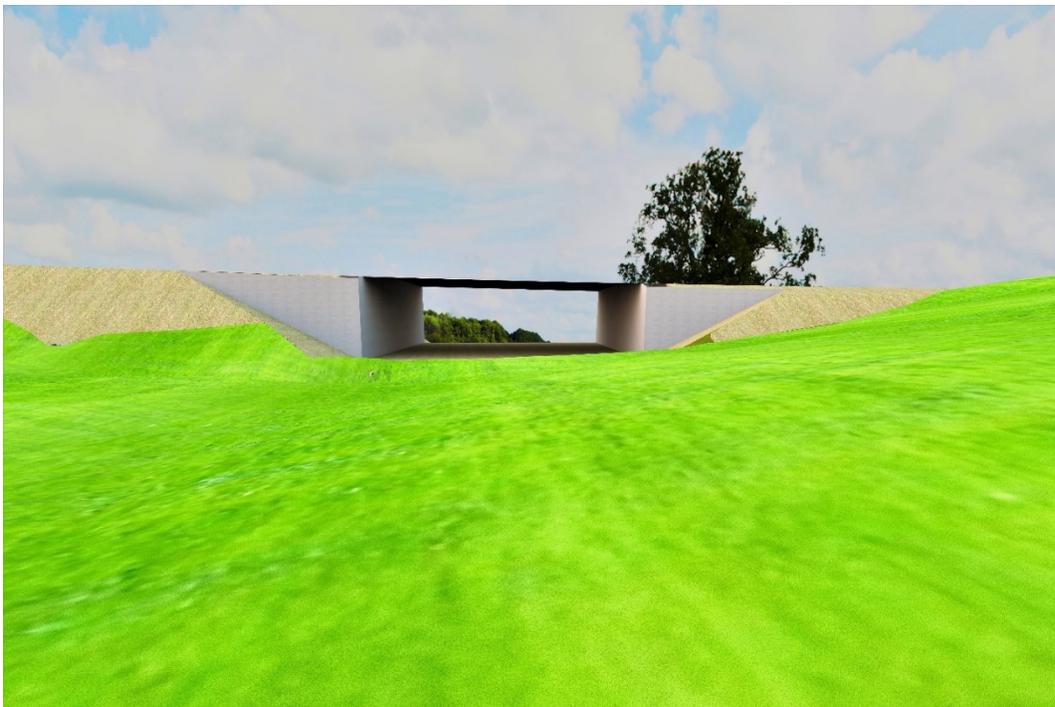


Figure 15. Simulation of Wildlife Underpass at MP 170.6, looking west.

Walls

Median Walls

Median walls would be used to address the split profile of the interstate, allowing widening to the inside of the interstate while keeping the project within the existing right-of-way and minimizing impacts to sensitive environmental resources. Median walls would be used throughout the project with concentrations of walls north of the Sky View Lane (Tomah Road) interchange and between the Greenland Road and Upper Lake Gulch Road interchanges. Wall heights would vary from approximately 5 feet to approximately 15 feet, with lengths extending up to approximately three-quarters of a mile.

Outside Walls

Outside fill and cut walls would be used where outside widening is required in areas without sufficient space for grading, or in areas where slope grading would cause impact to adjacent sensitive properties and environmental resources. Outside walls would be concentrated near the Greenland Road, Spruce Mountain Road, and Sky View Lane (Tomah Road) interchanges. Wall heights vary from approximately 5 feet to approximately 20 feet, with wall lengths extending up to approximately a half-mile.

Other features

Lighting

Portions of the existing I-25 are currently lit, including from the southern end of the Project (MP 161) to County Line Road at approximately 164.3. This area is in an urban setting of the Town of Monument. North of County Line Road, through the rural portion of the project, existing lighting is limited to interchange locations. The project would replace the urban setting lighting at the southern end of the project and improve lighting at interchange locations. Mainline I-25 would remain unlit through most of the project area, including through the Douglas County portions of the project adjacent to conservation easements. Dark Sky compliant lighting would be implemented at the interchange locations. In locations where proposed lighting is in proximity to a wildlife underpass, design features (e.g., shielding, dimming) would be considered to minimize the potential negative effects of lighting on the willingness of animals to use the underpass.

Minor Drainage Structures

Minor drainage structures of steel construction would be replaced with concrete drainage structures and would be lengthened to accommodate the greater width of the proposed roadway. Minor drainage structures that are not hydraulically sufficient would be replaced with hydraulically sufficient concrete drainage structures and lengthened to accommodate the greater width of the proposed roadway. Minor structures that are less than 36 inches and are hydraulically sufficient will remain in place. Details are to be determined during final design.

Wildlife Fencing, Jump-outs, and Deer Guards

In addition to wildlife underpasses, a minimum of 8-foot-high wildlife fencing would be constructed on both sides of I-25 extending from approximately the southern end of the project to Crystal Valley Parkway (MP 178). Fencing would serve to connect proposed wildlife underpass structures, as well as existing structures located within the gap, and deter animals from entering the highway. Fencing would funnel wildlife to the underpass locations, allowing wildlife to cross under I-25 safely in either direction. Further, wildlife friendly fence is recommended to be installed in areas where cattle may access wildlife underpass structures. Wildlife friendly fence is designed to inhibit cattle from entering the wildlife

underpasses, while allowing other wildlife to access those areas. Wildlife jump-out ramps and deer guards would be constructed in conjunction with the underpasses and fencing. The final wildlife fencing extent and design options for wildlife jump-outs and deer guards will be determined on a location-by-location basis during final design.

Guardrail

In areas where the slope does not meet requirements for a recovery zone, guardrail would be used. Guardrail locations generally correspond with outside fill and cut wall locations and would be confirmed during final design.

Intelligent Transportation System Infrastructure

Intelligent transportation system (ITS) infrastructure would be constructed to support driver communication, incident management, and operation of the new lane. Based on preliminary design and tolling assumptions, the project includes approximately four variable toll signs and three variable message signs. Final sign types, numbers, and locations would be determined during final design. Additional ITS elements include a weather station, cameras, fiber backbone, toll points, microwave detection, and dedicated short-range communication to communicate with connected/automated vehicles.

Permanent Water Quality

Within the Municipal Separate Storm Sewer System (MS4) areas, the facility will contain the required water quality capture volume and will meet the requirements of the CDOT's MS4 permit using drainage vaults. Outside of the MS4 areas, the project may incorporate drainage improvements, such as unpaved roadside ditches, grass swales, and media filters drains, where feasible, to minimize the amount of roadway pollutants entering adjacent habitat and surface waters. Details are to be determined during final design.

Appendix A8
Economic Impact Study

I-25 South Gap Environmental Assessment Economic Assessment of I-25 South

I-25 South Communities and I-25 South Transportation Investments



PREPARED FOR

Colorado Department of Transportation



April 23, 2018

JACOBS

and

ARLAND
Land Use Economics

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Executive Summary

I-25 is a critical transportation corridor and planned corridor improvements represent a strategic transportation investment for the Colorado Department of Transportation. I-25 has been an important “artery” or conduit for regional economic growth. Originally constructed in the 1960s, as the economy has grown, traffic has increased, and the highway has aged, I-25’s deficiencies between the Denver and Colorado Springs metropolitan areas have become glaring. In this section of the highway, shoulder widths are too narrow, climbing grades present challenges, interchanges are deficient, and an increase in the number and severity of accidents make for an increasingly unreliable trip hampering mobility with consequences for economic vitality and quality of life for the region.

In order to address these deficiencies, CDOT has commissioned two projects currently underway: the I-25 Gap Project between Monument and Castle Rock which is intended to address immediate needs and the I-25 PEL project which is intended to identify longer term improvements for the area between southern Denver and northern Colorado Springs. This study focuses on the economic benefits of I-25 to the communities between the southern Denver metropolitan area and Colorado Springs. It also examines the potential economic benefits of transportation improvements.

Community Profile:

- *Study Area:* The Study Area has been defined broadly to include Arapahoe County, Douglas County and El Paso County with a focus on those areas in close proximity to I-25. Southern Arapahoe County is specifically called out in certain circumstances where data could be stratified by geographic area as Arapahoe County stretches to the east. The Gap project area is more narrowly defined as the I-25 corridor area between Castle Rock to Monument.
- *Huge residential growth seen:* Since 2000, the population of Arapahoe, Douglas and El Paso Counties has grown by over 400,000 residents. While household sizes have generally decreased or remained flat in most of the area jurisdictions, Monument and Castle Rock have seen household sizes increase. Larkspur has seen a relatively dramatic decrease in household sizes. It has also aged at a greater rate than the other jurisdictions in the study area. The median ages in all communities have generally increased.
- *Residents are well educated and affluent:* The majority of Douglas County communities near I-25 have median household incomes over \$100,000. Woodmoor, Monument, and Black Forest residents in El Paso County also enjoy high household incomes. This corresponds to high education levels with those communities with higher incomes also having a significant percentage of residents (50% or more) with at least a Bachelor’s Degree.
- *Longer commutes for residents between Castle Rock and Monument:* Forty percent (40%) of residents in the area between Castle Rock and Monument have commutes of over 30 minutes, while residents in and around south Denver and in and around Colorado Springs have lower percentages of their population with long commute times.
- *Significant population growth forecast:* Arapahoe, Douglas, and El Paso Counties are forecast to grow by over 1% per year through 2040. Combined the three counties are forecast to grow by almost 700,000 people with El Paso County forecast to add the most residents at more than 300,000 persons. There are a large number of planned developments and thousands of acres of land slated for development. There is a significant amount of planned growth between Castle Rock and Monument.

Study Area Economy

- *The 3 county region (Arapahoe, Douglas and El Paso Counties) generates 30% of Colorado's Gross State Product:* Colorado's Gross State Product (GSP) is estimated at approximately \$313 billion in 2017. The GSP is a measure of all goods and services produced in the state. The Gross Regional Product for Arapahoe, El Paso and Douglas Counties is estimated at \$94 billion, approximately 30% of GSP.
- *The 3 county region employs 27.5% of Colorado's workers:* There are about 700,000 jobs in Arapahoe, Douglas and El Paso Counties representing 27.5% of all state jobs in 2016.
- *Location of major employers significant to the State:* There are a number of major employers in and near the corridor. In south Denver, Douglas County Schools and Charles Schwab employ the greatest numbers of people. In El Paso County, Fort Carson and Peterson Air Force Base are the largest employers at 32,000 and 9,900 employees respectively. The largest private employer in El Paso County is the UC Health Memorial Health System at 6,200 employees.
- *Location of major industrial clusters significant to the State:* Clusters significant to the state located in this area include Aerospace and Defense; Information Technology/Computer Software/Cybersecurity; Healthcare and Medical Technologies. All clusters are heavily reliant on the freight transportation system, in particular for receipt of supplies and deliveries of product.
 - *Defense and Aerospace:* El Paso County military facilities include Fort Carson, Peterson Air Force Base, Schriever Air Force Base, Cheyenne Mountain, and the Air Force Academy. Both El Paso and Arapahoe Counties receive a significant percentage -- over two-thirds of the State's Department of Defense contracts. Companies like The Harris Corporation, Lockheed Martin, Raytheon, Sierra Nevada Corporation, and United Launch Alliance are all located in the study area.
 - *Information Technology/Software and Cybersecurity:* There are an estimated 11,000 businesses in this cluster in the study area. Both the Denver Metro and the Colorado Springs area are strong in this cluster and the range of activities is broad ranging from support of other business activities such as e-business, telecommunications, gaming, finance and aerospace and defense. Colorado Springs, in particular, is developing a strong niche in the Cybersecurity arena with the recent opening of the National Cybersecurity Center. Many businesses in this cluster provide services to the defense and aerospace cluster.
 - *Healthcare and Medical Technologies:* This cluster not only includes healthcare providers, it also includes research institutions, as well as firms working in medical innovation and biotechnology. The cluster has an estimated 15,000 entities in the study area.
- *Significant growth forecast for employment:* Like the residential growth forecasts, the study area is poised for a significant amount of employment growth. According to the local Council of Governments forecasts, the 3 county region of Arapahoe, El Paso, and Douglas Counties are forecast to add an additional 430,000 jobs by 2040. El Paso County is forecast to add the most at 185,000 jobs with many of the jobs forecast for infill areas along I-25 in Colorado Springs.
- *Workforce demand is currently at all time high:* This study is being conducted during a time of huge labor force needs both in the study area as well as the region. Unemployment is at historic low levels with 5 year forecast demand heaviest for Health Care and Social Assistance, Government, Professional, Scientific and Technical Services. Top current unfilled jobs include truck drivers, registered nurses, and retail sales salespersons.
- *The rising price of housing is encouraging longer commuting:* The Denver metro area has grown tremendously in the past 6 years while the production of single family housing has not kept pace. While apartment production has dramatically increased in recent years, the production of single

family housing has not matched growth, resulting in constrained supply in the Denver metro area and high prices. The average single family house in Douglas County is priced at \$515,000 while the average single family house in El Paso County is \$255,000, making the south Denver single family housing market unattainable except for the highest paid workers. It is encouraging longer commutes in this area. Anecdotally, current broker marketing materials are encouraging and reflecting this trend.

- *Future cross commuting is likely to increase:* While maps of commuting times seem to indicate that the Denver metropolitan and the Colorado Springs markets are two different labor sheds, forecast growth and origin-destination maps indicate that more cross-commuting is likely to occur in the future.

Freight

- *Truck traffic is significant:* Truck traffic is estimated to comprise up to 12% of all traffic traveling the I-25 corridor in the study area. I-25 is a Congressionally designated High Priority Corridor and is also part of the Primary Highway Freight System which identifies it as one of the most critical highway portions of the U.S. freight system.
- *Area freight movement is valued at an estimated \$60 billion:* The study area is a significant part of the State's import/export economy. Major state trading partners include Wyoming, Nebraska, Texas, California and Utah for a wide variety of Colorado goods. Major international trading partners include Canada and Mexico, with I-25 being a conduit for movement of goods. The State's industrial clusters, particularly those that are the strongest in the study area, are reliant on long distance trucks for the movement of supplies and deliveries.
- *Trucks service a growing retail and industrial inventory:* Mirroring the growth of the economy, there has been a tremendous increase in retail and industrial activity, the great majority of which is serviced by trucks. In the last ten years, the retail and industrial inventory in the study area has increased by 7.4 million square feet. There is currently about 100 million square feet of industrial and retail space in the study area.
- *E-commerce growth points to a future increase in truck traffic:* E-commerce is becoming an ever expanding portion of total retail sales and it is forecast to result in an increase in industrial warehouse space and the need for more trucks to deliver goods to residents. At the same time, there has not been a significant decrease in travel for household shopping trips.

Tourism

- *Colorado is becoming a national destination:* Formerly, more of a regional destination, the state has transitioned to being a national destination. The Pikes Peak Region welcomed 23 million people in 2016 while Denver welcomed 31.5 million visitors. The nature of travel has changed with more leisure visitors (versus business visitors) and those who visit because they want to (versus because of a family or business obligation).
- *The corridor is a conduit for visitors:* Late week and weekend traffic volumes indicate that the corridor is an important conduit for travel and leisure. There are several retailers in Douglas County (Ikea / Cabela's) that serve a multi-state market. There are a wide variety of special events particularly in the summer like Larkspur's Renaissance Festival. Major professional sporting events encourage travel throughout the year. Sports leagues and school sporting events are also prevalent and I-25 is a conduit to the outdoors particularly in the southern parts of the state.
- *Travel in the study area contributes to the state's economy:* In 2016, direct travel spending in the State of Colorado was estimated at nearly \$20 billion and supported 165,000 jobs. Direct travel spending in Arapahoe, Douglas and El Paso Counties is estimated at \$2.6 billion and employed approximately 24,000 workers.

Travel and Mobility

- *Travel reliability is worsening:* Increasingly, corridor travelers are experiencing long traffic delays, and the need to plan daily activities around traffic congestion is becoming the new norm. Travel delays are the result of congestion and events (e.g. accidents, weather, and sporting events).
- *Safety is a central issue:* This is particularly important for the I-25 South corridor because of the number and type of crashes (accidents) that occur along this stretch of the interstate. From 2011 to 2015 there were 4,710 total crashes, or 2.5 crashes per day on average. Most of these (72 %) resulted in property damage. About 28 % of all crashes during the five-year period resulted in human injury or death. There were 13 fatal crashes during this time, or approximately two to three per year. In the Gap, there were 1,809 crashes between 2011 to 2015 with half occurring in 2014 and 2015 alone, indicating worsening conditions.
- *Truck / Freight traffic comprise a significant portion of traffic in this area:* Heavy trucks account for 8 to 11% of corridor traffic and must deal with climbing grades in both directions in the corridor. Climbing is difficult for heavy trucks and they often must slow down anywhere from 10 to 20 miles per hour depending on the grade.
- *I-25 Gap Project begins to address the most critical challenges:* The 18-mile Gap section between Monument and Castle Rock has been identified through the Planning and Environmental Linkage process as the most critical section of the I-25 Corridor between south Denver and Colorado Springs to be addressed. The project begins to address travel reliability, safety, and freight/truck traffic concerns. There are two build alternatives. One alternative provides for the addition of an express lane and the second provides for the addition of a general-purpose lane.
- *Travel time savings account for the greatest benefit provided by the Gap project.* By 2040, for morning peak period drivers, time savings are estimated at 12 to 20 minutes through the Gap. The time savings is most significant for Friday southbound traffic where time savings would range from 50 to 87 minutes. By 2040, weekday morning northbound travel through the Gap will take 43 minutes. Express lane travelers (the fastest option) will need just 23 minutes. Friday southbound travelers will need nearly 2 hours to get through the Gap by 2040 without improvements. Express lane travelers will need just 32 minutes.
- *The Benefit Cost Analysis shows that either of the build alternatives provides a \$1.7 billion positive benefit to the regional economy.* The net present value of the costs of the project is estimated at \$270 million. The net present value of benefits is estimated at \$1.5 billion. Benefits include the value of travel time savings, operating cost savings, safety benefits, and the residual value of the project (remaining useful life) of the project after a 20 year life cycle. Costs include initial capital and ongoing maintenance and operations costs. Benefits and costs are discounted at a 7% rate.

1.0 Introduction

In its 2040 Statewide Transportation Plan, *Transportation Matters*, the Colorado Department of Transportation (CDOT) articulated its goals for safety, mobility, economic vitality, and system maintenance. Its goal for economic vitality includes improving the competitiveness of the state economy through strategic transportation investments.

I-25 is a critical transportation corridor and planned corridor improvements represent a strategic transportation investment. I-25 South connects two of the primary metropolitan areas in the State of Colorado: the Denver metropolitan area and the Colorado Springs metropolitan area. It is the only continuous north-south freeway in Colorado, and the only north-south interstate freeway within roughly 500 miles.

- *Jobs*: There are more than 700,000 jobs in El Paso, Douglas, and Arapahoe Counties, nearly 30% of the State's workforce. The three counties connect the two major metropolitan areas. Major industry clusters important to the state are supported in these counties including Health Care and Medical Technology; IT, Software, and the growing Cybersecurity cluster, in addition to Defense and Aerospace.
- *Freight*: I-25 is part of the National Highway Freight Network supporting both interstate and international commerce and trade. Up to 12% of traffic along I-25 South is truck traffic moving goods and services for area residents and businesses.
- *National Security*: I-25 supports national defense, linking the state's eight major military installations, five of which are in El Paso County. It is part of the National Defense Highway System providing critical defense logistics support for military installations in Colorado. The military installations and supporting industry sectors are also major area employers.
- *Recreation and Tourism*: I-25 helps support the state's growing recreation and tourism industry by providing access to recreational and tourist destinations along the Front Range, as evidenced by the high volumes of weekend traffic.

I-25 has been an important "artery" or conduit for regional economic growth. Originally constructed in the 1960s, as the economy has grown, traffic has increased, and the highway has aged, I-25's deficiencies has become glaring. In this section of the highway, shoulder widths are too narrow, climbing grades present challenges, interchanges are deficient, and an increase in the number and severity of accidents make for an increasingly unreliable trip hampering mobility with consequences for economic vitality and quality of life for the region.

In order to address these deficiencies, CDOT has commissioned two projects currently underway: the I-25 Gap Project between Monument and Castle Rock which is intended to address immediate needs and the I-25 PEL project which is intended to identify longer term improvements for the area between southern Denver and northern Colorado Springs.

The I-25 GAP project begins to address some of the critical infrastructure needs along the corridor. There are three alternatives currently contemplated: The No-Build alternative, the Managed Lane alternative and the General-Purpose Lane alternative.

The alternatives are being contemplated in light of a state economy that is growing and changing. This analysis explores some of the past changes and current trends occurring in the region with a particular focus on the southern Denver metropolitan area and the northern Colorado Springs metropolitan area, and how transportation improvements can continue to help facilitate mobility within the context of Colorado's dynamic growth.

The analysis is divided into the following topic areas:

- *Community Profile:* The southern Denver / northern Colorado Springs area has experienced significant population and employment growth and change in recent years. Forecasts indicate continued growth in this region with potentially greater linkage between the two metropolitan areas.
- *Economy, Jobs and Workforce:* Arapahoe, Douglas and El Paso Counties generate a significant portion of the State Gross Domestic Product. Southern Denver and Northern Colorado Springs are two different labor markets currently. The analysis will explore past jobs and workforce trends, forecasts for the future, and discuss how these two labor markets are increasingly linked. This section will also discuss the role of the military, which is particularly important to the El Paso County/Colorado Springs economy.
- *Freight:* Truck traffic is a significant portion of overall traffic volumes on I-25 South, with increasing concerns about congestion and inability to move goods through the state. Freight's role in the state economy in moving goods to market will be discussed.
- *Travel and Tourism:* Travel and tourism is a huge and growing part of the state economy. The high volumes of weekend traffic attest to the role that tourism plays in the local economy.
- *Transportation Mobility and Cost Benefit Analysis:* The final section will discuss current conditions in the corridor, the costs and potential benefits of transportation improvements in the Gap Project Area. It will discuss potential travel time savings, the forecast increase in reliability, the potential decrease in accidents and compare the benefits and costs of the two "build" alternatives compared to the "no-build" scenario.

2.0 Community Profile

2.1 Study Area

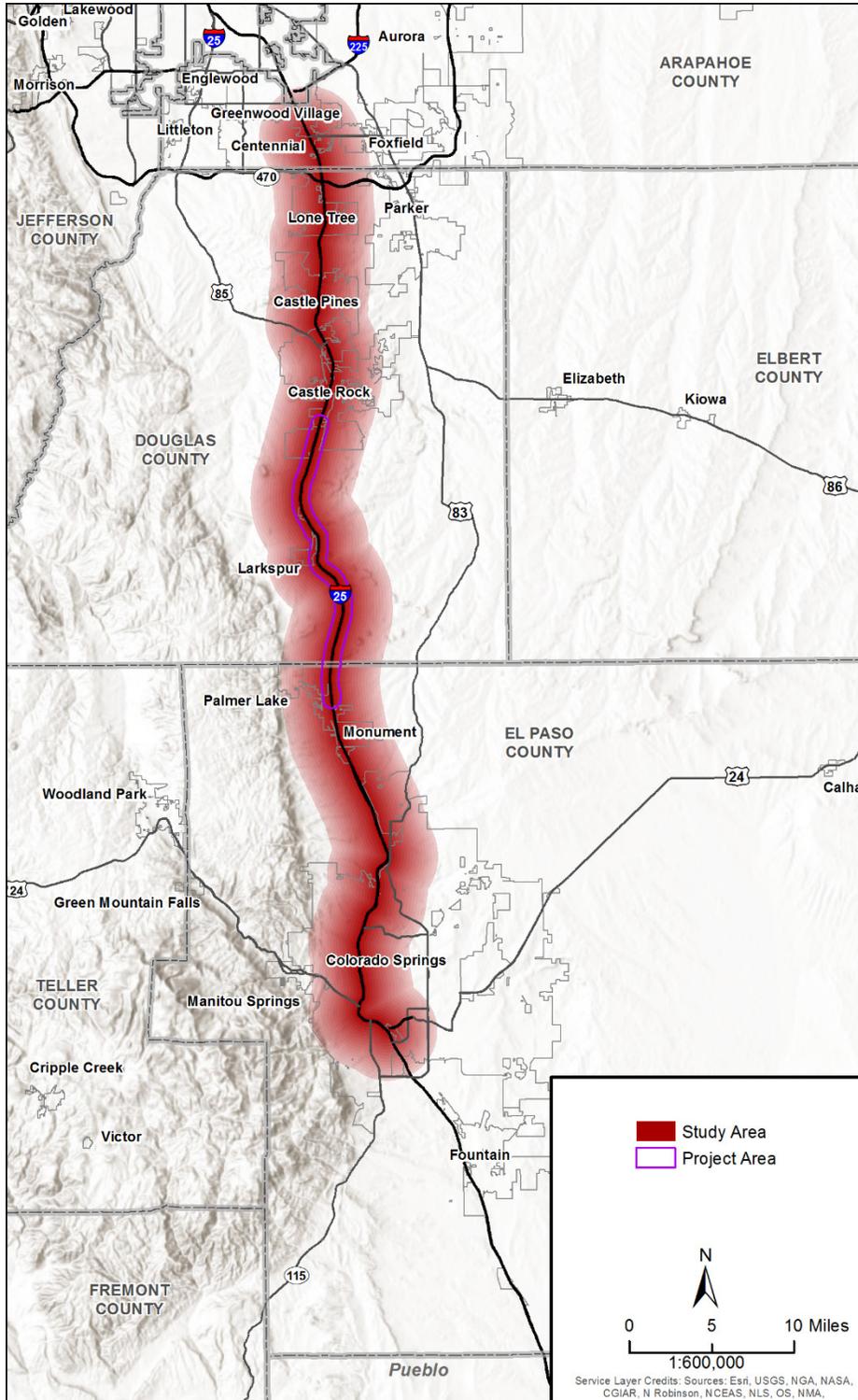


Figure 1. I-25 Gap Project Area and the I-25 South Regional Impact Area

Interstate 25 (I-25) is the only north-south interstate route through Colorado. It connects the state's largest population centers and provides access to recreation and cultural amenities. The stretch of I-25 from the southern Denver metropolitan area near C-470 to Colorado Springs is a particularly well-traveled roadway and a major transportation artery fueling the regional economy and greatly affecting the state's economy. The study area has been defined broadly to include Arapahoe County, Douglas County, and El Paso County adjacent to I-25. Communities within those counties in proximity to the corridor are also shown. Figure 1 depicts the study area from Denver to Colorado Springs. It also shows the Gap Project area which is located between Castle Rock in Douglas County and Monument in El Paso County.

2.2 Trends and Existing Conditions

2.2.1 Population and Households

The estimated combined population of Arapahoe, Douglas, and El Paso Counties in 2016 was about 1.6 million persons, an increase of over 400,000 persons since the year 2000. As seen in Table 1, all communities in proximity to I-25 South experienced population increases since 2000 except for Larkspur and the Air Force Academy. Douglas County grew by about 3.7% annually, a much higher rate than in Arapahoe and El Paso Counties. At the jurisdictional level, Monument, Castle Rock, and Lone Tree saw the greatest annual percentage gains, growing by more than 6% per year since 2000. Changes in the number of households essentially mirrored population change. The exception is Larkspur where the number of households is estimated to have increased while the population declined slightly, indicating a decrease in household size as seen in Figure 2.

Table 1. Population and Households, 2000-2016

	2000 ^a	2010 ^a	2016 ^b	Growth 2000-2016	CAGR ^c 2000-2016 (percent)
Population					
Arapahoe County	487,967	571,914	617,668	129,701	1.5
Centennial	--	100,377	107,862	--	--
Douglas County	175,766	285,465	314,238	138,472	3.7
Lone Tree	4,873	10,218	12,808	7,935	6.2
Castle Pines	--	10,360	10,389	--	--
Castle Rock	20,224	48,231	53,789	33,565	6.3
Larkspur	234	183	221	-13	-0.4
El Paso County	516,929	622,263	665,171	148,242	1.6
Palmer Lake	2,179	2,420	2,558	379	1.0
Woodmoor CDP	7,177	8,741	8,587	1,410	1.1
Monument	1,971	5,530	6,346	4,375	7.6
Black Forest CDP	13,247	13,116	13,506	259	0.1
Air Force Academy CDP	7,526	6,680	5,957	-1,569	-1.5
Colorado Springs	360,890	417,335	448,759	87,869	1.4
Households					
Arapahoe County	190,909	223,958	231,844	40,935	1.2
Centennial	--	37,449	39,222	--	--

	2000 ^a	2010 ^a	2016 ^b	Growth 2000-2016	CAGR ^c 2000-2016 (percent)
Douglas County	60,924	102,018	114,017	53,093	4.0
Lone Tree	1,848	4,023	5,184	3,336	6.7
Castle Pines	--	3,493	3,653	--	--
Castle Rock	7,226	16,688	18,739	11,513	6.1
Larkspur	92	81	122	30	1.8
El Paso County	192,409	235,959	249,279	56,870	1.6
Palmer Lake	843	955	1,052	209	1.4
Woodmoor CDP	2,374	3,107	2,865	491	1.2
Monument	725	1,802	1,988	1,263	6.5
Black Forest CDP	4,494	4,674	4,837	343	0.5
Air Force Academy CDP	1,128	532	527	-601	-4.6
Colorado Springs	141,516	167,788	177,774	36,258	1.4

Source: U.S. Census, ArLand

^aCensus 2000 and 2010 Census, Table DP-1

^b2012-2016 ACS 5-year estimates, Tables S0101 and S1101

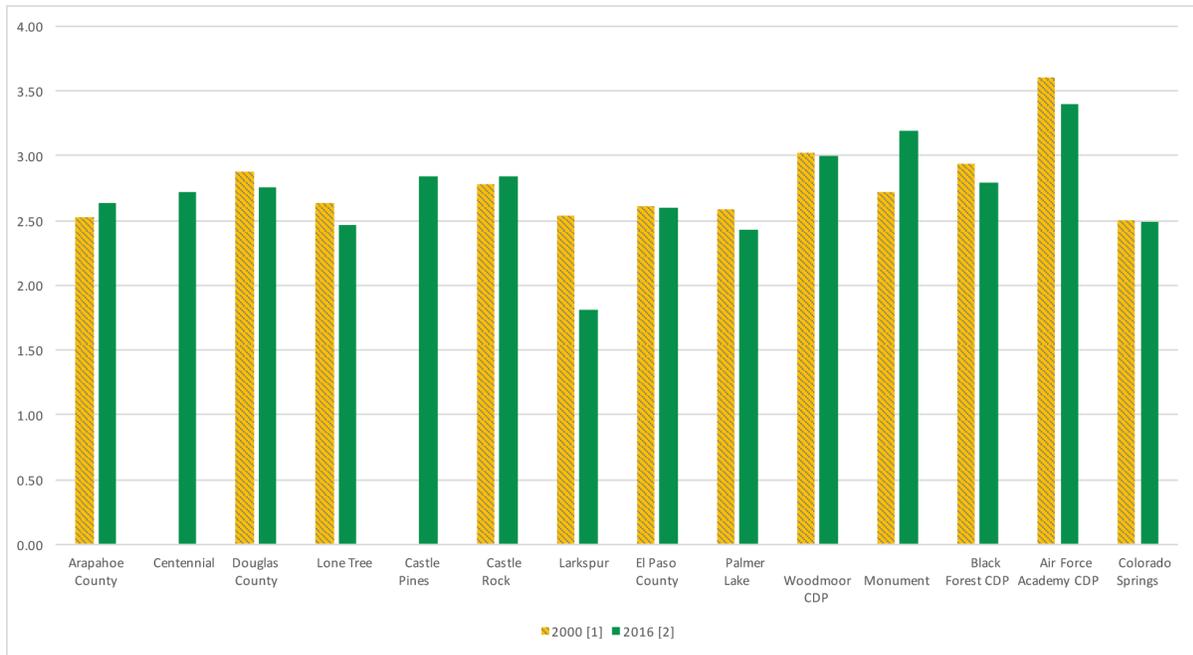
^cCompound Annual Growth Rate

Note:

CAGR=Compound Annual Growth Rate

2.2.2 Average Household Size

Household size has decreased or remained flat in most jurisdictions, as seen in Figure 2. Declines have generally not been sizeable, except for Larkspur where the average household size decreased from 2.54 persons in 2000 to 1.81 in 2016. Arapahoe County, Monument, and Castle Rock experienced increases in average household sizes between 2000 to 2016. Within the study area, average household size is generally between 2 and 3 persons, although Air Force Academy and Monument have average household sizes over 3. The Air Force Academy household sizes can be partially attributed to group living while increases in Monument can be attributed to an increase in family households in the area.

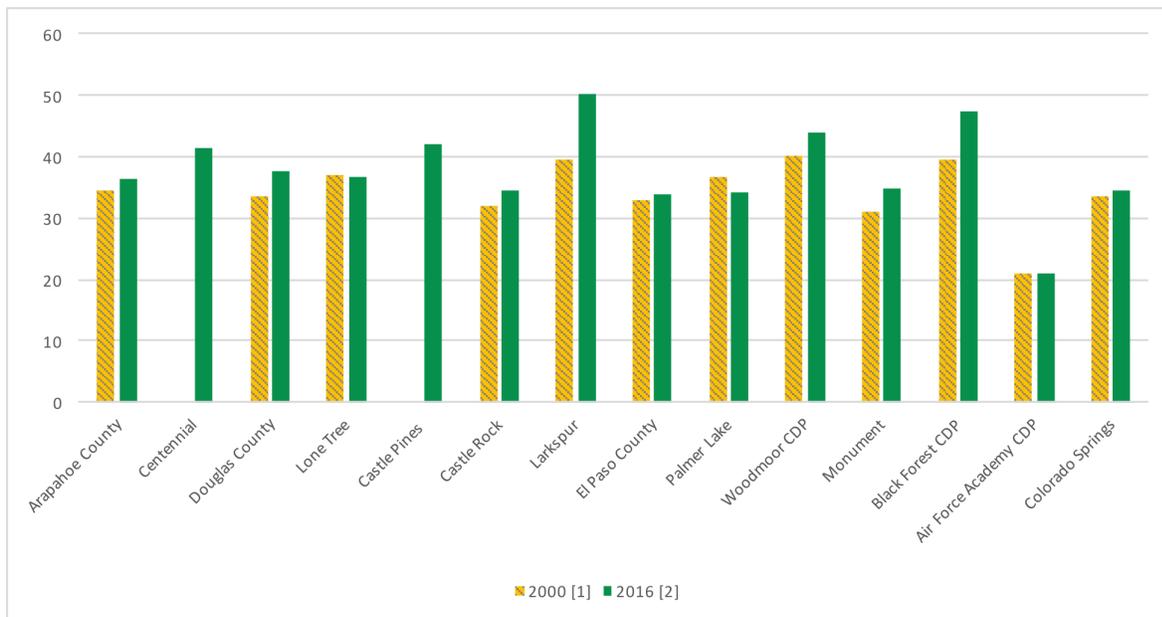


Source: U.S. Census, ArLand

Figure 2. Average Household Size, 2000 and 2016

2.2.3 Median Age

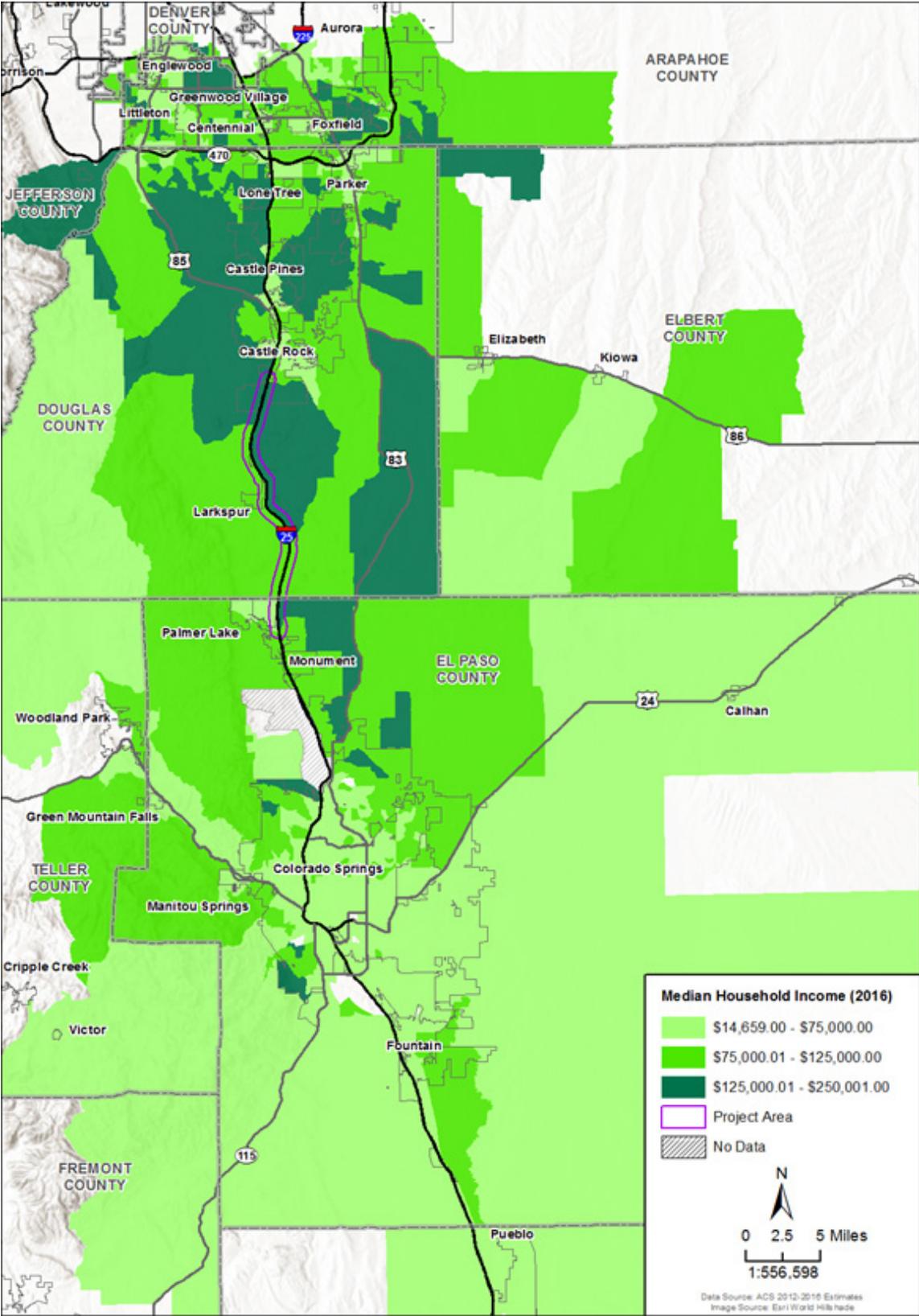
In general, the study area has experienced an increase in median ages as depicted in Figure 3. Larkspur and Black Forest have experienced very large increases in their median ages, which are now 50.1 and 47.3 years in 2016, respectively. Castle Pines and Centennial also have high median ages, both over 40 in 2016. Douglas County and Woodmoor also experienced sizeable increases in median age since 2000. Not surprisingly, the Air Force Academy had the lowest median age in 2016 at just under 21 years, due to the large number of students.



Source: U.S. Census, ArLand

Figure 3. Median Age, 2000 and 2016

2.2.4 Median Household Income



Source: U.S. Census, ArLand

Figure 4. Median Household Incomes, 2016

Figure 4 depicts three ranges of median household incomes, with darker green representing higher incomes. Pockets of higher incomes are scattered throughout the area, although a distinct concentration exists between C-470 and Larkspur, including most of the area west of I-25 from south of Highlands Ranch to southern Castle Rock.

A closer look at jurisdictional figures in Table 2 show that of the three counties, Douglas County had the highest estimated median household income in 2016, at about \$106,000. Communities within Douglas County also had very high median household incomes. In 2016, Castle Pines had the highest median household income at just under \$141,000 per year followed by Lone Tree at \$115,000. On the other hand, Larkspur's estimated median household income in 2016 was about \$35,500 which decreased from a 2010 median household income of \$57,250.

In El Paso County, Woodmoor and Black Forest had median household incomes well over \$100,000. The greatest percentage increases in median household incomes between 2010 and 2016 were seen in Monument and the Air Force Academy. Palmer Lake and Colorado Springs were in the mid-\$50,000 range, which is just under the statewide median household income of about \$62,500.

Table 2. Median Household Incomes, 2000-2016

	2000 ^a	2010 ^b	2016 ^c	Growth 2000-2016	CAGR ^d 2000-2016 (percent)
Arapahoe County	\$53,570	\$58,719	\$66,288	\$12,718	1.3
Centennial	--	\$87,007	\$96,422	--	--
Douglas County	\$82,929	\$99,198	\$105,759	\$22,830	1.5
Lone Tree	\$96,308	\$108,190	\$115,049	\$18,741	1.1
Castle Pines	--	\$137,019	\$140,764	--	--
Castle Rock	\$64,138	\$85,461	\$93,153	\$29,015	2.4
Larkspur	\$43,750	\$57,250	\$35,556	-\$8,194	-1.3
El Paso County	\$46,844	\$56,268	\$60,219	\$13,375	1.6
Palmer Lake	\$52,340	\$59,261	\$57,727	\$5,387	0.6
Woodmoor CDP	\$97,359	\$109,849	\$124,301	\$26,942	1.5
Monument	\$50,000	\$89,203	\$98,397	\$48,397	4.3
Black Forest CDP	\$77,085	\$103,551	\$112,544	\$35,459	2.4
Air Force Academy CDP	\$43,417	\$67,083	\$71,705	\$28,288	3.2
Colorado Springs	\$45,081	\$53,074	\$56,227	\$11,146	1.4

Source: U.S. Census, ArLand

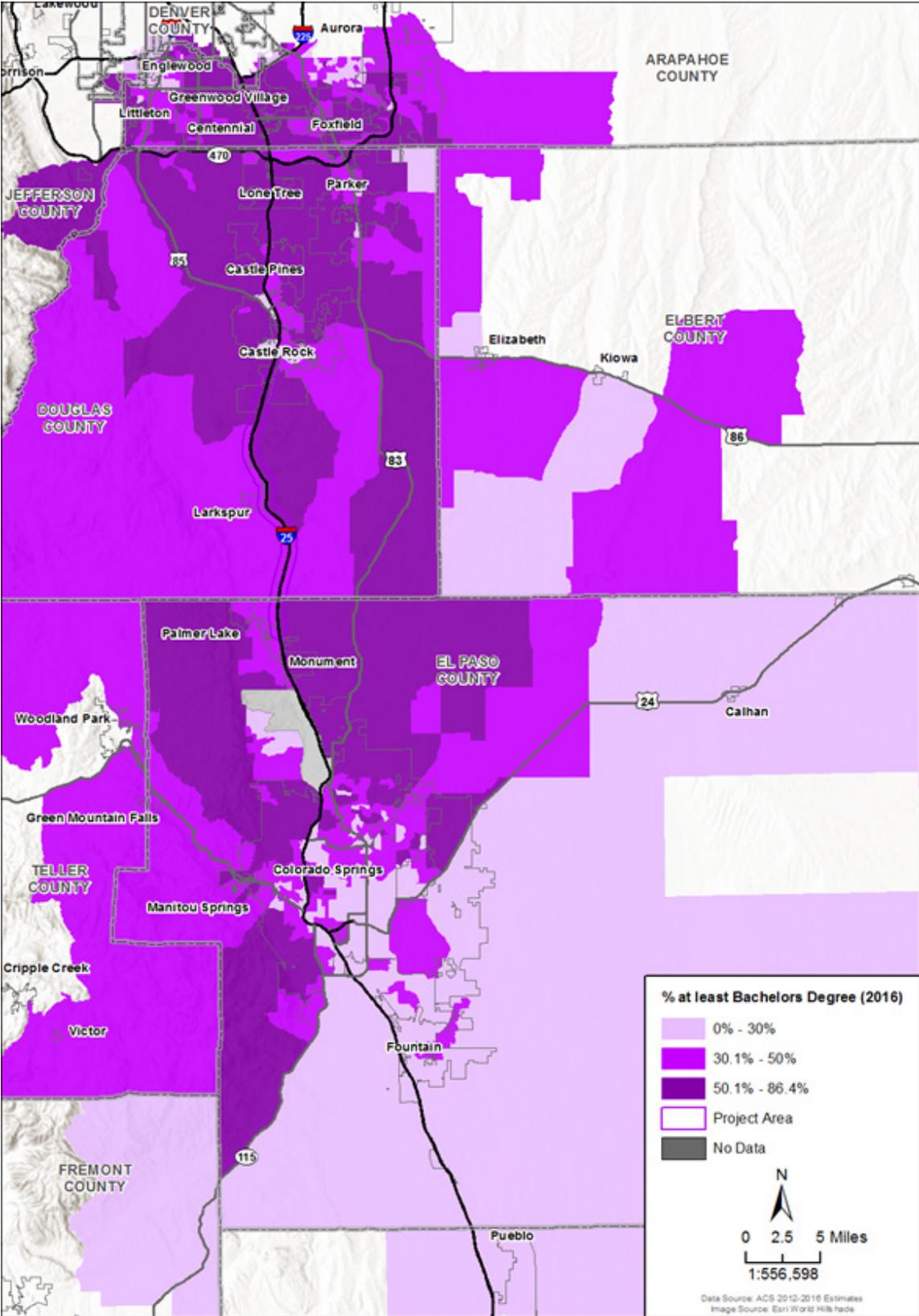
^aCensus 2000, Table P053

^b2006-2010 ACS 5-year estimates, Table B19013

^c2012-2016 ACS 5-year estimates, Table B19013

^dCompound Annual Growth Rate

2.2.5 Educational Attainment



Source: U.S. Census, ArLand

Figure 5. Percent of Population 25+ with a Bachelor’s Degree, 2016

As seen in Figure 5, the study area has very high levels of educational attainment. At least 30% of the population from the southern Denver metropolitan area to Colorado Springs has at least a bachelor's degree. Higher levels of educational attainment, shown in darker shades of purple, are concentrated in Douglas County along I-25 and north of Colorado Springs.

Those jurisdictions with very high median incomes also have higher levels of educational attainment as seen in Table 3. Almost 60% of Douglas County's population aged 25 years and over had a bachelor's degree or higher in 2016. About 40% of Arapahoe County and 37% of El Paso County's population over the age of 25 had a bachelor's degree or higher in 2016. Within Douglas County, Castle Pines and Lone Tree had approximately 70% of their populations with at least a bachelor's degree. Larkspur had the lowest educational attainment of any jurisdiction in the study area.

Table 3. 2016 Percentage of Population with Bachelor's Degree and Higher

	2016 ^a (percent)
Arapahoe County	40.7
Centennial	55.4
Douglas County	57.5
Lone Tree	69.3
Castle Pines	70.6
Castle Rock	48.5
Larkspur	15.1
El Paso County	36.6
Palmer Lake	38.4
Woodmoor CDP	64.9
Monument	55.0
Black Forest CDP	56.0
Air Force Academy CDP	28.6
Colorado Springs	37.8

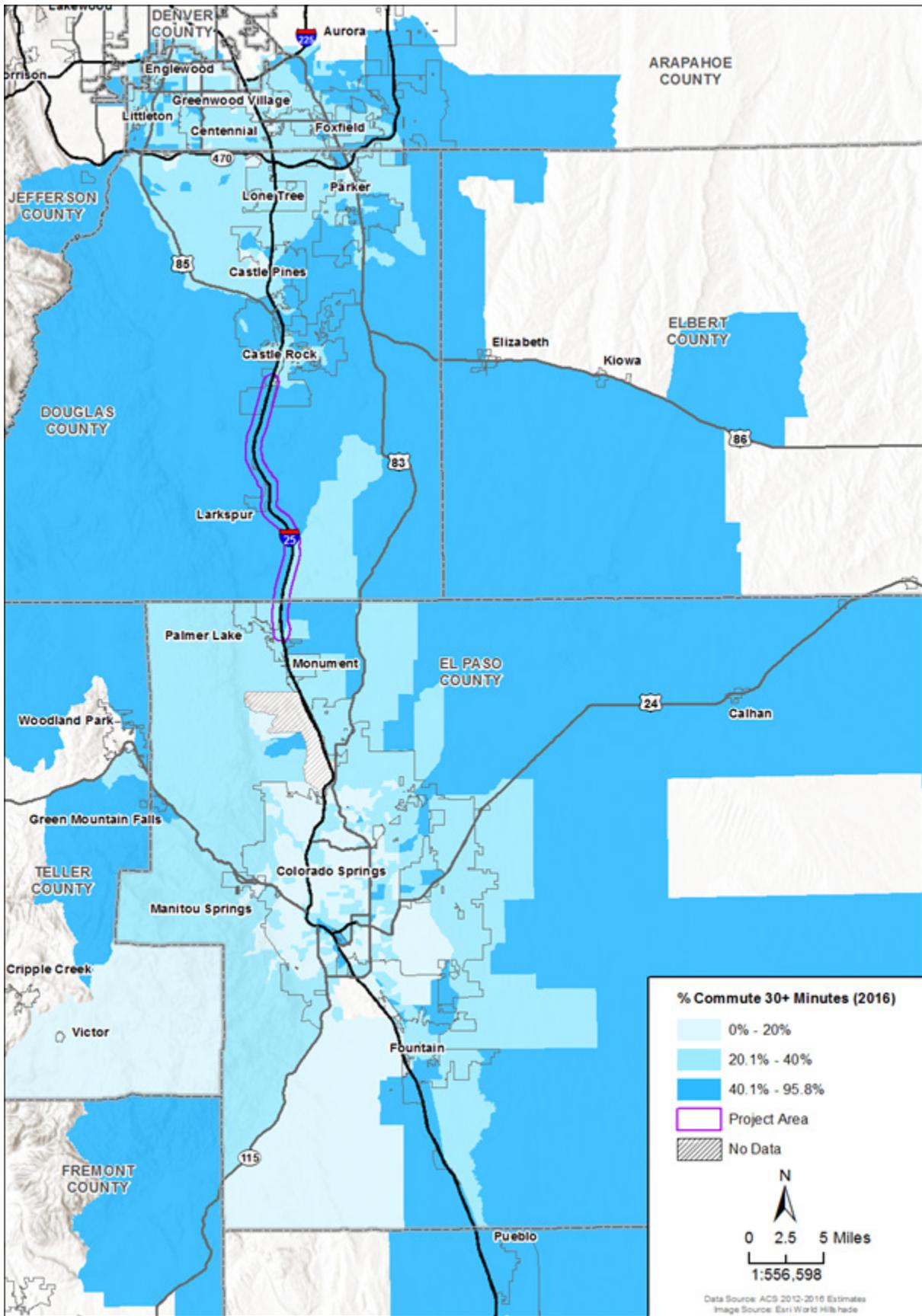
Source: U.S. Census, ArLand

^a2012-2016 ACS 5-year estimates, Table S1501

2.2.6 Journey to Work

Figure 6 depicts the percentage of the population with commuting times in excess of 30 minutes. Most areas along I-25 from the southern end of the Denver metropolitan area to northern El Paso County have at least 20% of the population with 30+ minute commute times. Areas in and around south Denver and in and around Colorado Springs have lower percentages of the population with long commute times compared with the area from Castle Rock to Monument where over 40% of the population has a commute time over 30 minutes.

It illustrates that the Colorado Springs metropolitan area and the Denver metropolitan area are their own respective labor sheds. The location of forecast growth and other workforce trends as further discussed will bring these two labor sheds even closer together with resulting impacts on traffic conditions.



Source: U.S. Census, ArLand

Figure 6. Journey to Work, 2010-2016

2.3 Population and Household Forecasts

Arapahoe, Douglas, and El Paso Counties are all expected to grow by over 1% per year through 2040. Combined, the three counties are forecast to grow by almost 700,000 people. El Paso County is forecast to add the most residents at more than 300,000 people, followed by Arapahoe County at just under 250,000. Many of the communities within each county are forecast to grow, but by varying rates as seen in Table 4.

Table 4. Forecast Population and Household Growth, 2015-2040

	2015 ACS ^a	2015 DRCOG / PPACOG	% Difference between ACS and DRCOG / PPACOG (percent)	2040 DRCOG / PPACOG	DRCOG / PPACOG Growth 2015-2040	CAGR 2015-2040 (percent)
Population						
Arapahoe County	608,310	633,268	-4.1	874,708	241,440	1.3
Centennial	106,604	105,216	1.3	113,185	7,969	0.3
Douglas County	306,974	286,572	6.6	422,912	136,340	1.6
Lone Tree	12,462	12,766	-2.4	15,945	3,179	0.9
Castle Pines ^b	10,399					
Castle Rock	52,143	52,506	-0.7	75,807	23,301	1.5
Larkspur	248	377	-52.0	428	51	0.5
El Paso County	655,024	664,407	-1.4	978,349	313,942	1.6
Palmer Lake	2,529	2,284	9.7	3,507	1,223	1.7
Woodmoor CDP	8,427	8,254	2.1	8,873	619	0.3
Monument	6,252	6,295	-0.7	12,218	5,923	2.7
Black Forest CDP	12,918	15,102	-16.9	34,702	19,600	3.4
Air Force Academy CDP	5,864	670	88.6	670	0	0.0
Colorado Springs	442,040	424,506	4.0	557,877	133,371	1.1
Households						
Arapahoe County	229,601	252,608	-10.0	364,030	111,422	1.5
Centennial	38,991	39,800	-2.1	43,402	3,602	0.3
Douglas County	111,113	117,772	-6.0	183,886	66,114	1.8
Lone Tree	4,955	4,695	5.2	6,140	1,445	1.1
Castle Pines ^b	3,683					
Castle Rock	18,721	18,928	-1.1	29,242	10,314	1.8
Larkspur	139	146	-5.0	166	20	0.5
El Paso County	245,287	261,279	-6.5	378,956	117,677	1.5
Palmer Lake	1,038	891	14.2	1,372	481	1.7
Woodmoor CDP	2,795	2,955	-5.7	3,172	217	0.3
Monument	1,909	2,045	-7.1	3,986	1,941	2.7
Black Forest CDP	4,703	5,374	-14.3	12,251	6,877	3.4

	2015 ACS ^a	2015 DRCOG / PPACOG	% Difference between ACS and DRCOG / PPACOG (percent)	2040 DRCOG / PPACOG	DRCOG / PPACOG Growth 2015-2040	CAGR 2015-2040 (percent)
Air Force Academy CDP	515	239	53.6	239	0	0.0
Colorado Springs	174,441	174,862	-0.2	228,802	53,940	1.1

Source: DRCOG, PPACOG, ArLand

^a2011-2015 ACS 5-year estimates, Tables S0101 and S1101

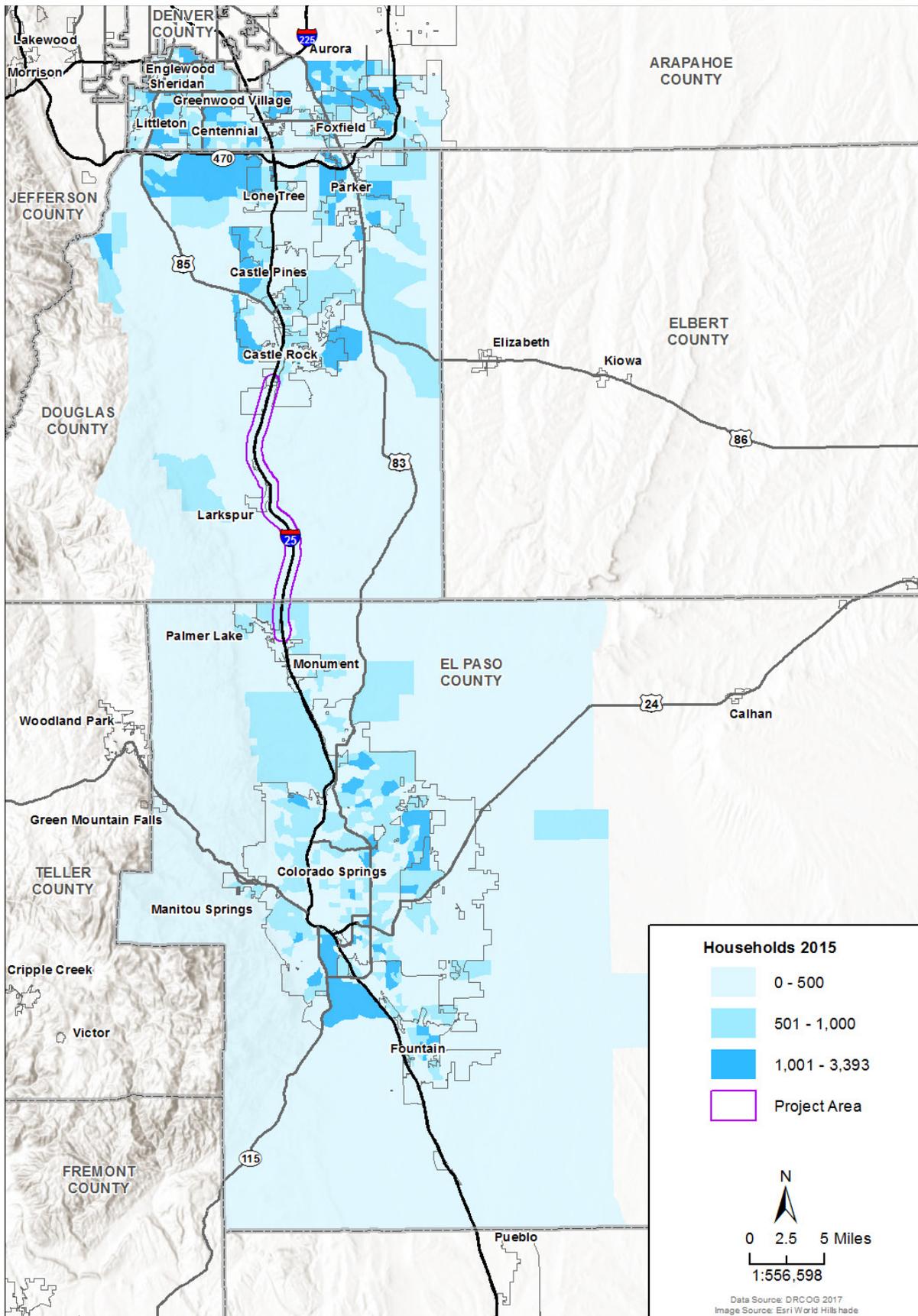
DRCOG Land Use Explorer - <http://landuseexplorer.drcog.org/>

^b Forecasts for Castle Pines not available from DRCOG. Community is undergoing significant expansion through annexation.

The relative concentration of households is expected to change by 2040, as seen in Figure 7. In 2015, higher household densities were present in the southwestern Denver metropolitan area, southeast Aurora, and in Colorado Springs and areas to its northeast. By 2040, household density is forecast to increase in northeastern Douglas County, along I-25 in the Castle Pines to Castle Rock area, and east/northeast of Colorado Springs in El Paso County.

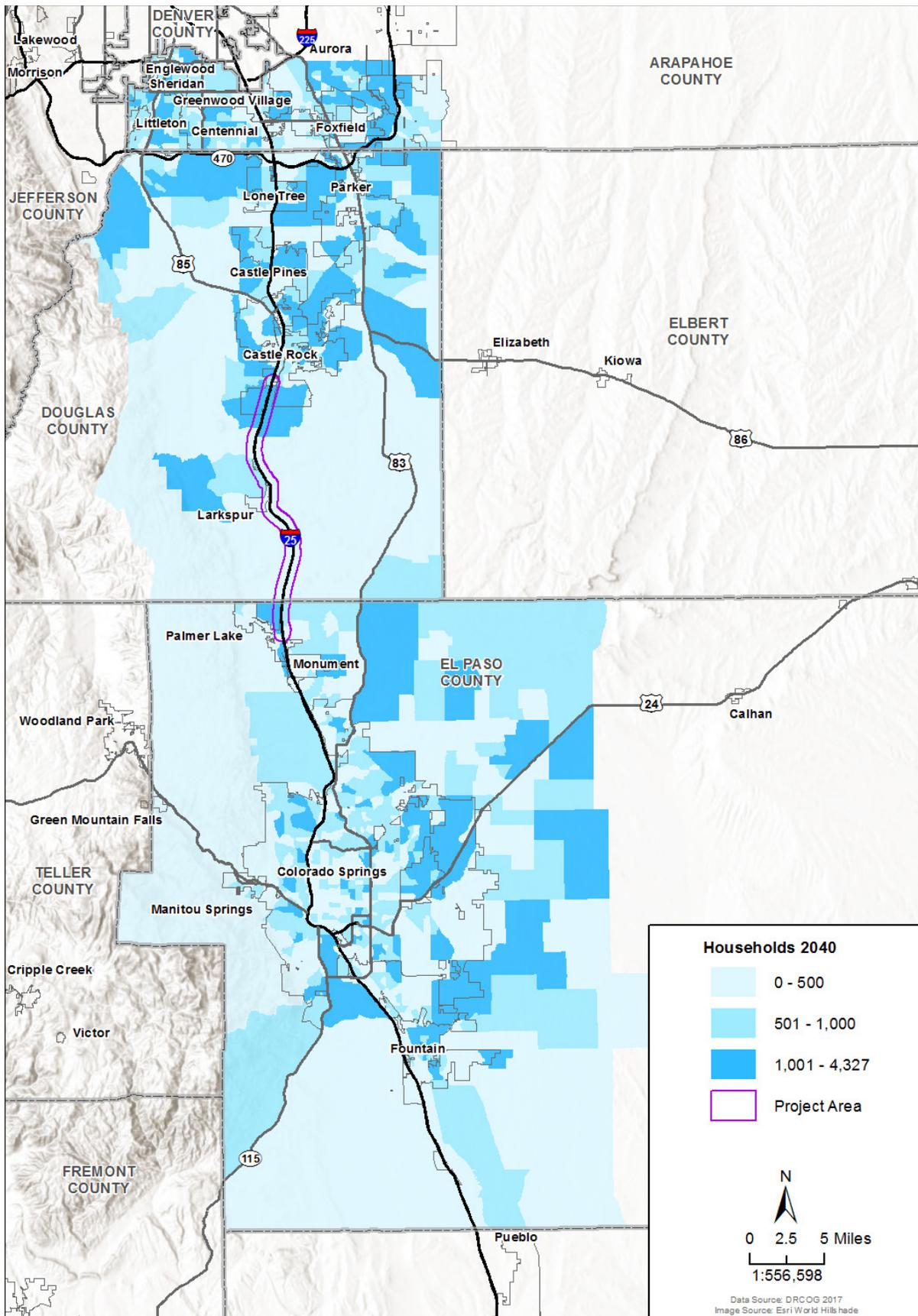
There is a significant amount of household and employment growth planned for the study area which extends beyond 2040. Table 5 and Figure 9 show the planned developments in more detail and where they are planned. Many of these projects are currently under construction and through interviews with the various planning departments, an estimate of what is remaining to be developed has been collected. There are several developments where information about what is being planned is currently not available including Monument Ridge in Monument, and Canyons South in Douglas County. In Douglas County, there is also a significant amount of planned development in and near Parker which is closer to Highway 83. Those Parker-area developments have not been included in this table.

According to Table 5, there are nearly 50,000 dwelling units, 15 million square feet of commercial space that are planned or remaining to be built that have been specifically identified. There are 960 acres of land designated for future commercial uses where square footages are unknown. Additionally, there are 20,000 acres where plans are currently unknown but likely to include a significant amount of residential and commercial development.



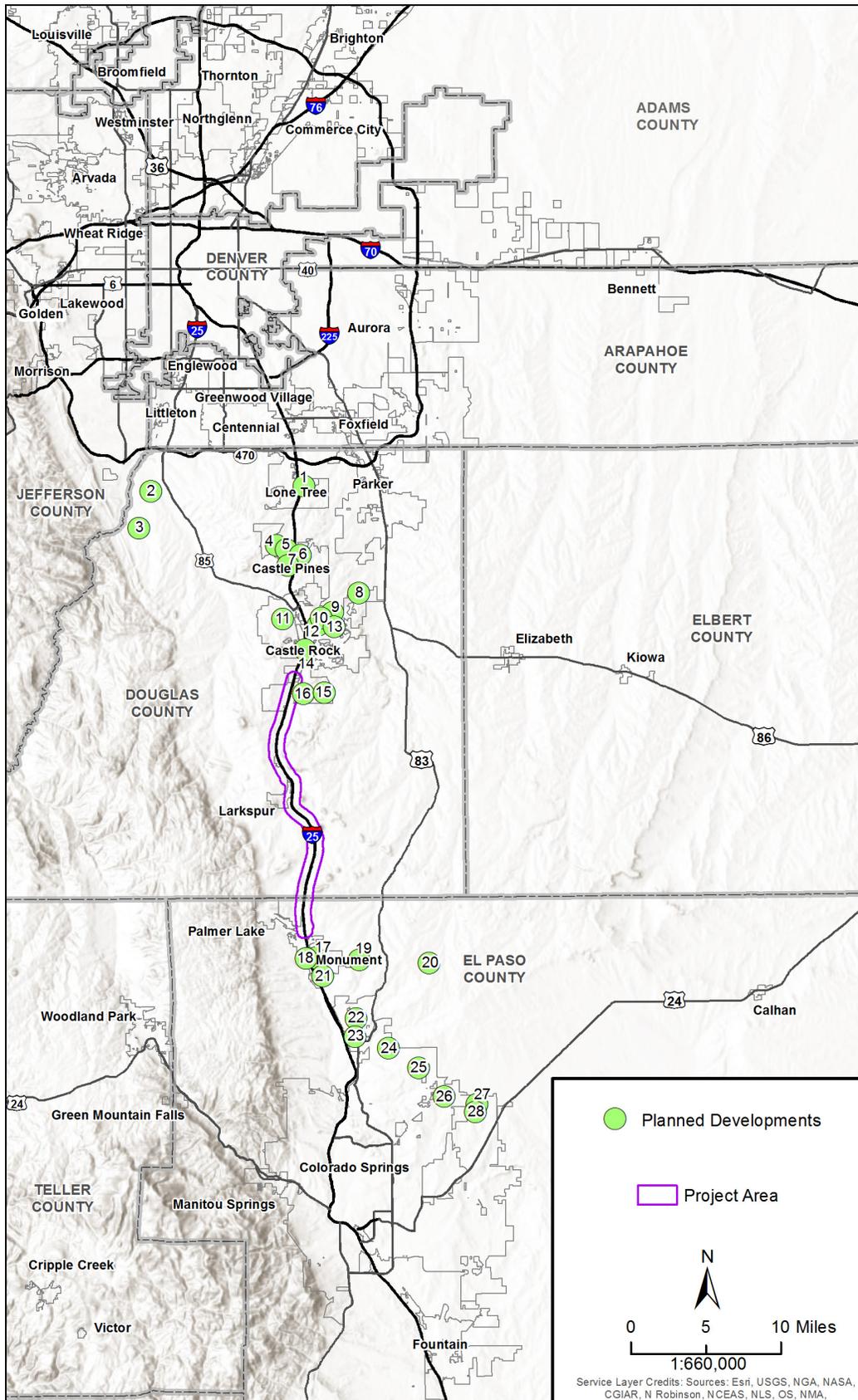
Source: DRCOG, ArLand

Figure 7. 2015 Households



Source: DRCOG, ArLand

Figure 8. 2040 Households



Source: ArLand

Figure 9. Planned Developments

Table 5. Planned Developments in Study Area, 2015-2040

Map #	Name	City/Town /County	Residential Units	Commercial Space (s.f.)	Commercial Acres	Total Acres (if Plans NA)	Status	Notes
1	RidgeGate	Lone Tree	10,000	10,000,000	---		Under Construction	3,500-acre, mixed-use development south of Lincoln Avenue, east and west of I-25. RTD has started construction of the SE light rail extension from Lincoln to RidgeGate. The RidgeGate station will be a hub for additional mixed-use development on the east side of I-25. Will include parks, trails, open space, and both retail and office development.
2	Plum Creek	Douglas County	1,100	0	---		Preapp Process	Single Family Residential
3	Sterling Ranch	Douglas County	12,050	---	500		Under Construction	3,400 acres including 1,200 acres of open space. 31,000 residents, 20-year buildout.
4	Legacy Village of Castle Pines	Castle Pines	176	0	---		Under Construction	176-unit assisted living facility on 7-acre property at the northwest corner of Castle Pines Parkway and Monarch Blvd.
5	Castle Pines Town Center	Castle Pines	675	500,000	---		Under Construction	A 354-acre planned development which allows for a mix of land uses including parks, open space, trails, schools, civic, community, commercial, retail, agricultural, up to 475 single family residences and 200 multifamily residences. 500,000+ s.f. of mixed use/non-residential, concentrated at Happy Canyon and I-25.
6	The Canyons	Castle Pines	2,500	2,100,000	---		Under Construction	The Canyons is a 3,343-acre master planned community which allows for a mix of land uses including parks, open space, trails, schools, civic, community, commercial, retail, agricultural, and single-family/multifamily land uses. 2,500 DUs. 2.1 million sf of mixed use/non-residential, which is generally concentrated at southeast corner of Hess Road and I-25. To include 20 acres for civic uses, a 4-acre transit station, 1.5 acres for fire stations, 113 acres of public parks, 50 acres for public schools, and 1,400 acres for open space.
7	Castle Pines Valley	Castle Pines	631	400,000	---		Under Construction	Two Phases, with home building in 2017-2019. Located just south of the Village Square Commercial District, Castle Pines Valley (formerly known as Lagae Ranch) is a 249-acre planned development which includes Elk Ridge Park, American Academy Charter School (completed in 2009), up to 231 single-family homes, 400 multi-family residences, an additional elementary school, 76 acres of open space, trails, civic uses, community uses, commercial uses and limited retail uses. Approx. 400,000 s.f. civic/commercial.

2.0 COMMUNITY PROFILE

Map #	Name	City/Town /County	Residential Units	Commercial Space (s.f.)	Commercial Acres	Total Acres (if Plans NA)	Status	Notes
8	Liberty Village	Castle Rock	515	0	---		Under Construction	Located west of Highway 83 along Castle Oaks Drive. 1,337.5 acres. 5,785 DUs total, with about 515 DUs remaining to be built.
9	Canyons South	Douglas County		---	---	1,580	Preapp Process	The property is currently zoned for development and Douglas County is currently working on the PD Plan. Approximately 1,580 acres.
10	Pioneer Ranch	Castle Rock	1,123	400,000	---		Under Review	Located west of Founders Parkway and east of Front Street. Metzler Ranch PD is located to the west, Pinion Soleil to the north and Pine Canyon (A-1) to the south. The site is approximately 388 acres, with 78 acres of open space and 40 acres dedicated public land to include a school site.
11	The Meadows	Castle Rock	4,320	0	---		Under Construction	The Meadows neighborhood (approx. 2,693 acres) on the northern edge of Castle Rock has added an estimated 5,200 homes to the town since 1987 with an additional 600 homes slated to be built in the area in the next three to four years. The town approved more than 10,800 residential units in total. About 60% built out, with approx. 4,320 DUs remaining to be built.
12	Pine Canyon	Castle Rock	1,320	815,000	---		Under Review	Located west of Founders Parkway, east of Front Street. The Woodlands PD is adjacent to the south and Douglas County A-1 property lies to the north. A portion of the property is located west of I-25 and east of Liggett Road. The site is approximately 540 acres. The proposed PD Plan, PD Zoning Regulations and traffic impact analysis are under staff review. 1,320 max. residential (mix of SF and multifamily (attached and detached). 815,000 max. commercial, office, retail (majority west of I-25). 133 acres of site maintained as public or private open space
13	Terrain at Castle Rock	Castle Rock	1,700	0	---		Under Construction	Located on 1,185 acres about three miles east of I-25, east of Founders Parkway. Nearly 600 acres of Terrain is dedicated for parks, open space and the preservation of historical markers. Approx. 1,700 DUs remaining to be built.
14	Riverwalk	Castle Rock	228	48,000	---		Under Construction	Mixed-use development of two blocks of downtown on both sides of Sellars Gulch on Wilcox Street. 228 apartments and about 48,000 s.f. of office, retail, and restaurant space (14,000 sf of ground level retail, 34,000 sf of commercial office space)

Map #	Name	City/Town /County	Residential Units	Commercial Space (s.f.)	Commercial Acres	Total Acres (if Plans NA)	Status	Notes
15	Crystal Valley Ranch	Castle Rock	980	0	---		Under Construction	Approximately 1,088 acres. 2,889 DUs total, with about 980 remaining to be built.
16	The Lanterns	Castle Rock	1,200	---	---		PD approved	The Lanterns Planned Development is located south of Plum Creek Boulevard and Crystal Valley Parkway. The site is approx. 848 acres. 663 of 1,200 DUs are planned for 55+ years of age. Initial construction phases planned for early 2018.
17	Jackson Creek Senior Living	Monument	150	0	---		Under Construction	Located at Jackson Creek Parkway & Harness Road. Approximately 150 estimated units.
18	Wagons West	Monument	131	---	---		Preliminary PD Approved	Located on Old Denver Highway. 131 DUs. Annexed and preliminary PD Site Plan approved, but not platted.
19	Sanctuary Pointe	Monument	600	---	---		Under Construction	Located along the western edge of the Black Forest at Sanctuary Rim Drive and Baptist Rd. Phase 1 is for 257 DUs, which are under construction. Phase 2 is for 273 DUs. Phase 2 is under review. Phase 3 max is 70 DUs.
20	Flying Horse North	El Paso County	283	---	---		PD approved	1,417 acres. 283 residential lots and either a golf course or open space with trails.
21	Monument Ridge	Monument	---	---	---		Sketch Plan	This residential project located at the southeast corner of Baptist Road and Struthers Road is at sketch plan review. The number of DUs is currently unknown.
22	Flying Horse	Colorado Springs	950	---	118		Under Construction	1,593-acre planned development. 2,715 DUs total at build out. 197-acre golf course; 62 acres office; 76.5 acres commercial. About 65% built out, with approx. 950 DUs and 118 acres of commercial remaining to be built.
23	The Farm	Colorado Springs	723	550,000	---		Under Construction	The property is a 475-acre PUD and located between I-25 and Voyager Parkway, north of Interquest Parkway. Max of 1,446 DUs. Max of 1.1 million s.f. of commercial and employment allowed per the annexation agreement. About 50% built out. Development remaining is approx. 723 DUs and 550,000 s.f. commercial.

2.0 COMMUNITY PROFILE

Map #	Name	City/Town /County	Residential Units	Commercial Space (s.f.)	Commercial Acres	Total Acres (if Plans NA)	Status	Notes
24	Cordera Commercial North	Colorado Springs	---	---	18		Concept Plan Approved	Concept Plan approved October 2017. Approx. 18 acres of commercial in compliance with the Briargate Master Plan.
25	Wolf Ranch	Colorado Springs	3,250	---	29		Under Construction	1,982 acres. About 7,500 DUs, 30 acres mixed use, and 28 acres neighborhood commercial total. About 50% built out. Approx. 3,250 DUs and 29 acres mixed use/commercial remaining to be built.
26	Woodmen Heights	Colorado Springs	1,046	---	295		Under Construction	816.25-acre Master Plan. 38 acres regional commercial/office; 62 acres community commercial/office; 90 acres neighborhood commercial/office, 5 acres office, 100 acres major assembly. Approx. 4,183 DUs. About 75% built out. Approx. 1,046 DUS and 295 acres commercial remaining to be built.
27	Banning Lewis Ranch Village Two	Colorado Springs	2,377	175,000	---		PD Approved	426.9 acres. 2,377 DUs. 175,000 s.f. commercial.
28	Banning Lewis Ranch	Colorado Springs	---	---	---		Under Construction	18,000 acres
29	Forest Lakes	El Paso County	467	---	---		Under Construction	Master planned community located north of the U.S. Air Force Academy, just one mile west of I-25, off Baptist Road. Public and private trails, two neighborhood parks, and 450 acres of open space and 2 community lakes in 990 acres..
TOTAL			48,495	14,988,000	960	20,000+		

Sources: Planning and Community Development Departments of Douglas County, El Paso County, Castle Pines, Castle Rock, Monument, Colorado Springs, ArLand

3.0 Study Area Economy

3.1 Gross Regional Product

In 2017, Colorado’s Gross State Product was estimated at approximately \$313 billion (Table 6). The Gross State Product (GSP) is the market value of all final goods and services produced in the state in 2017. The Gross Regional Product (GRP) is a similar measure for smaller geographic areas, in this case, the County. As seen in Table 6, the GRP of the three counties encompassing the project area represents about 30% of that of the State of Colorado. Arapahoe County has the highest GRP of the three counties, at almost \$45 billion.

Table 6. Gross Regional and State Products, 2017

	Gross Regional Product	Percent of Total
El Paso County	\$33,101,956,854	10.6%
Douglas County	\$16,560,482,439	5.3%
Arapahoe County	\$44,999,806,101	14.4%
Arapahoe, Douglas, and El Paso Counties	\$94,662,245,395	30.2%
State of Colorado	\$313,513,019,587	100.0%

Source: Arapahoe-Douglas Works, Emsi, ArLand

3.2 Jobs

There are a significant number of jobs in Arapahoe, Douglas, and El Paso Counties. As seen in Table 7, total 2016 average employment in the three counties was just over 700,000, which represented 27.5% of all Colorado jobs. In the three-county area, the three sectors with the highest average employment were Health Care and Social Assistance, Retail Trade, and Accommodation and Food Services. The Professional and Technical Services sector was a close fourth.

In the three-county area, as shown in Table 8, there has been an increase of about 124,000 jobs since 2000, which represents about 34% of the state’s job growth over the past decade and a half. Both the state and the three-county area saw the largest employment increase in the Health Care and Social Assistance sector. Employment in the manufacturing sector was hit the hardest since 2000, losing about 47,000 jobs statewide and 16,000 jobs in the three-county area, representing about 35% of statewide job losses in this sector.

Table 7. Average Annual Employment by Industry, 2016

	Arapahoe County	Douglas County	El Paso County	Arapahoe, Douglas, and El Paso Counties	State of Colorado	3-County Percent of State Total
Accommodation and Food Services	26,161	11,962	29,518	67,641	271,208	24.9%
Administrative and Waste Services	26,640	6,070	18,892	51,602	158,622	32.5%
Agriculture, Forestry, Fishing & Hunting	94	193	414	701	16,516	4.2%
Arts, Entertainment, and Recreation	5,470	3,273	5,152	13,895	58,670	23.7%
Construction	20,244	8,363	14,850	43,457	158,294	27.5%
Educational Services	22,930	10,732	26,918	60,580	216,536	28.0%
Finance and Insurance	28,516	8,231	12,077	48,824	109,478	44.6%

	Arapahoe County	Douglas County	El Paso County	Arapahoe, Douglas, and El Paso Counties	State of Colorado	3-County Percent of State Total
Health Care and Social Assistance	43,035	11,825	39,496	94,356	327,736	28.8%
Information	17,675	5,912	6,417	30,004	74,394	40.3%
Management of Companies and Enterprises	7,280	3,010	1,219	11,509	36,833	31.2%
Manufacturing	8,148	2,378	11,480	22,006	142,764	15.4%
Mining	714	478	65	1,257	23,575	5.3%
Other Services, Ex. Public Admin	8,959	3,957	10,853	23,769	78,887	30.1%
Professional and Technical Services	31,529	12,032	23,309	66,870	211,593	31.6%
Public Administration	13,051	3,236	13,157	29,444	146,379	20.1%
Real Estate and Rental and Leasing	6,312	1,694	4,653	12,659	49,645	25.5%
Retail Trade	34,614	17,924	32,687	85,225	270,137	31.5%
Transportation and Warehousing	5,069	1,302	5,234	11,605	81,957	14.2%
Unclassified	28	9	39	76	764	9.9%
Utilities	260	430	2,493	3,183	14,176	22.5%
Wholesale Trade	14,836	3,800	5,525	24,161	104,882	23.0%
Total, All Industries	321,566	116,808	264,447	702,821	2,553,045	27.5%

Source: Labor Market Information, Quarterly Census of Employment and Wages Program, ArLand

Table 8. Average Annual Employment Change, 2000 to 2016

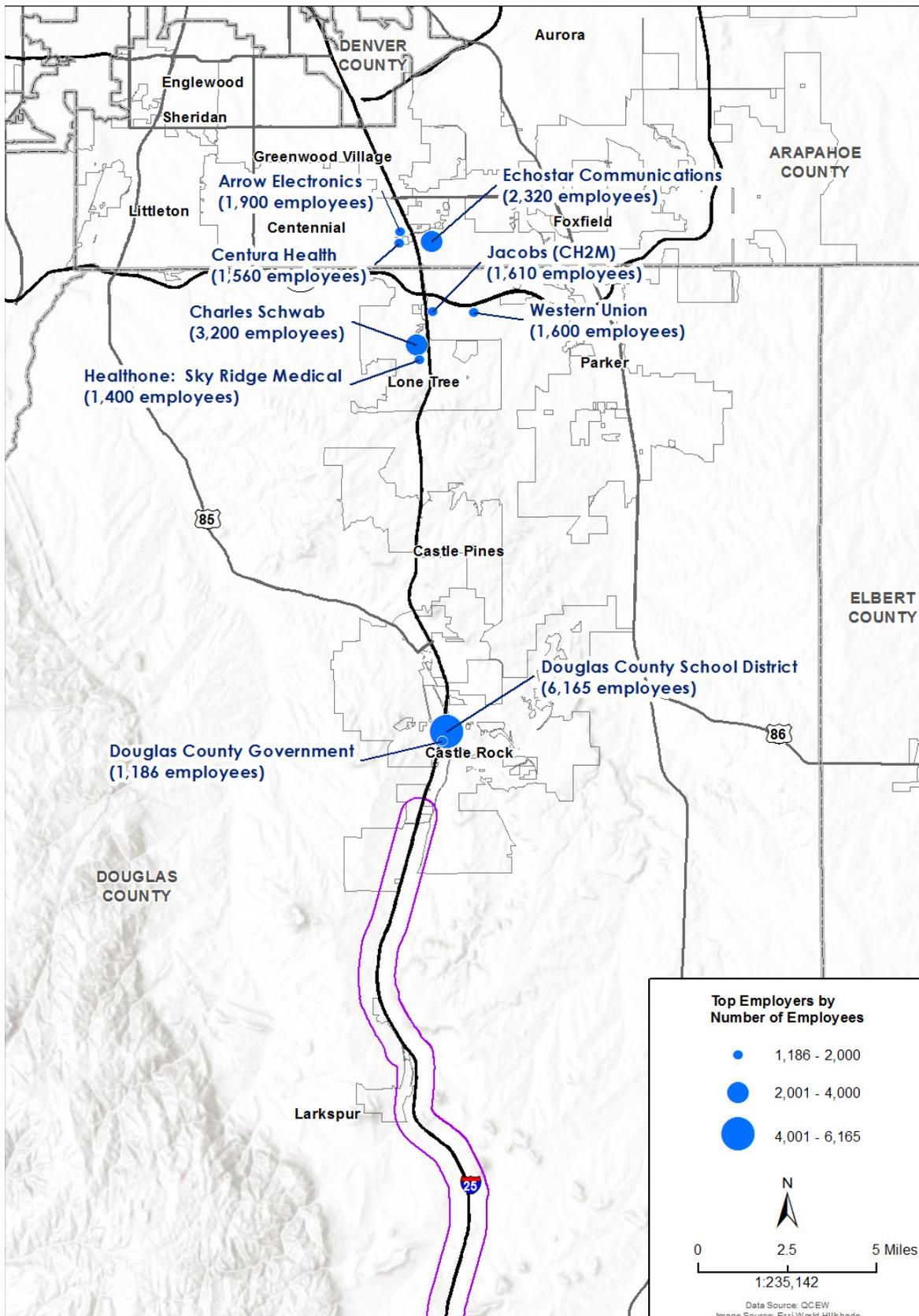
	Arapahoe County	Douglas County	El Paso County	Arapahoe, Douglas, and El Paso Counties	State of Colorado	3-County Percent of State Total
Accommodation and Food Services	5,474	5,773	6,427	17,674	67,017	26.4%
Administrative and Waste Services	4,092	4,177	1,167	9,436	12,996	72.6%
Agriculture, Forestry, Fishing & Hunting	-59	95	123	159	1,686	9.4%
Arts, Entertainment, and Recreation	1,376	2,190	1,241	4,807	12,183	39.5%
Construction	-2,090	242	-583	-2,431	-8,489	28.6%
Educational Services ^a	5,214	---	7,799	13,013	57,782	22.5%
Finance and Insurance	4,094	5,318	1,165	10,577	7,916	133.6%
Health Care and Social Assistance	20,915	9,647	18,257	48,819	138,302	35.3%
Information	-10,965	2,271	-5,829	-14,523	-34,186	42.5%
Management of Companies and Enterprises	1,705	2,670	270	4,645	18,735	24.8%
Manufacturing	-2,906	539	-13,731	-16,098	-46,614	34.5%
Mining	142	398	-24	516	11,883	4.3%
Other Services, Ex. Public Admin	1,250	1,870	1,366	4,486	13,424	33.4%
Professional and Technical Services	5,045	8,712	3,116	16,873	59,079	28.6%
Public Administration	2,427	1,429	2,258	6,114	22,338	27.4%

	Arapahoe County	Douglas County	El Paso County	Arapahoe, Douglas, and El Paso Counties	State of Colorado	3-County Percent of State Total
Real Estate and Rental and Leasing	-97	1,037	605	1,545	3,616	42.7%
Retail Trade	3,356	5,780	4,051	13,187	25,034	52.7%
Transportation and Warehousing	324	799	-71	1,052	-2,685	-39.2%
Unclassified ^b	---	---	---	---	730	---
Utilities	-133	160	-94	-67	801	-8.4%
Wholesale Trade	-1,853	1,372	-844	-1,325	4,839	-27.4%
Total	37,336	60,154	26,708	124,198	366,388	33.9%

Source: Labor Market Information, Quarterly Census of Employment and Wages Program, ArLand

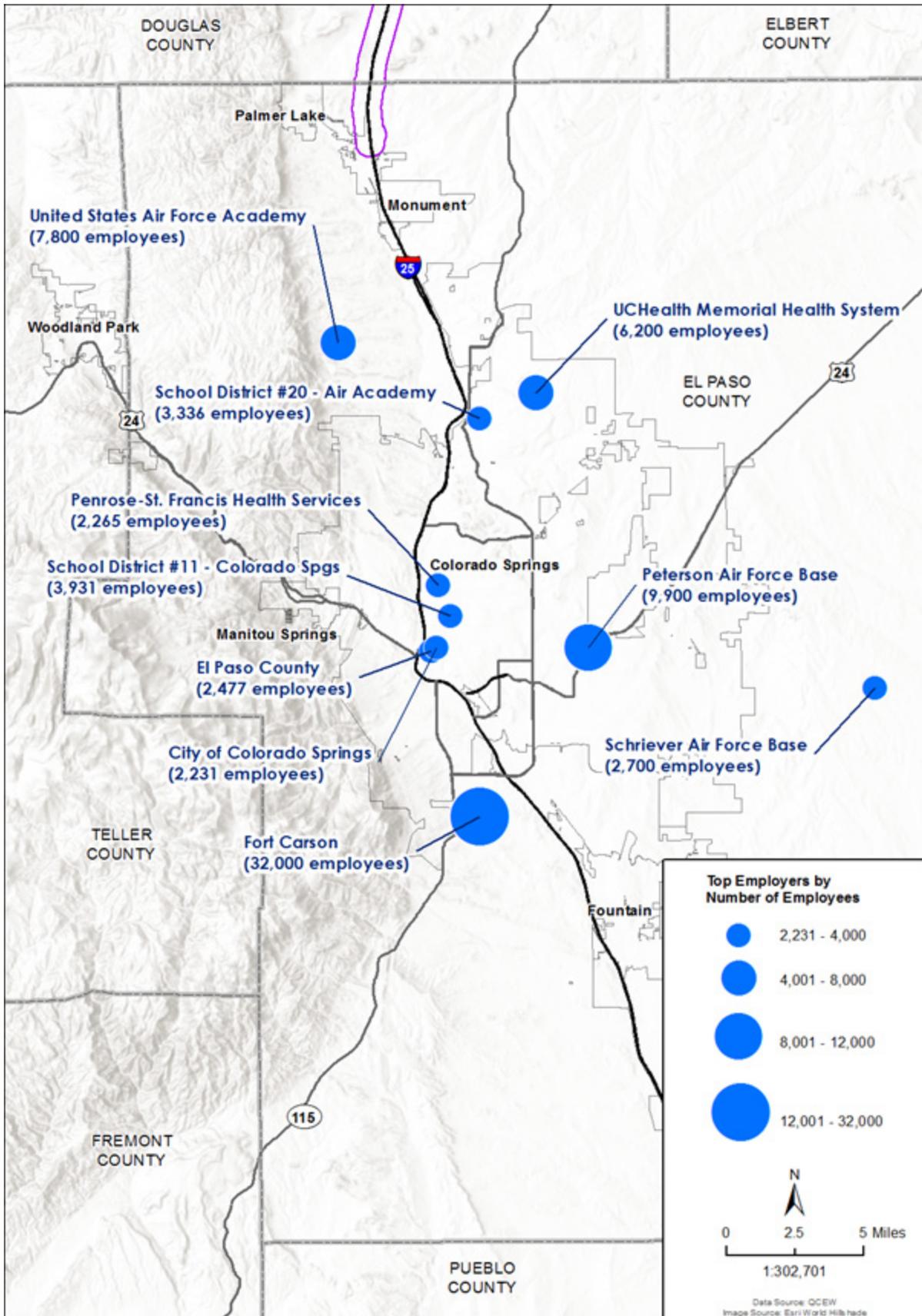
^a Douglas County Avg. Annual Employment Change not accounted for because 2000 data is confidential. 2016 avg. annual employment was 10,732.

^b Unclassified sector employment was confidential in 2000 in all three counties. 2016 three-county total was 76.



Source: 2016 Douglas County Comprehensive Annual Financial Report, ArLand

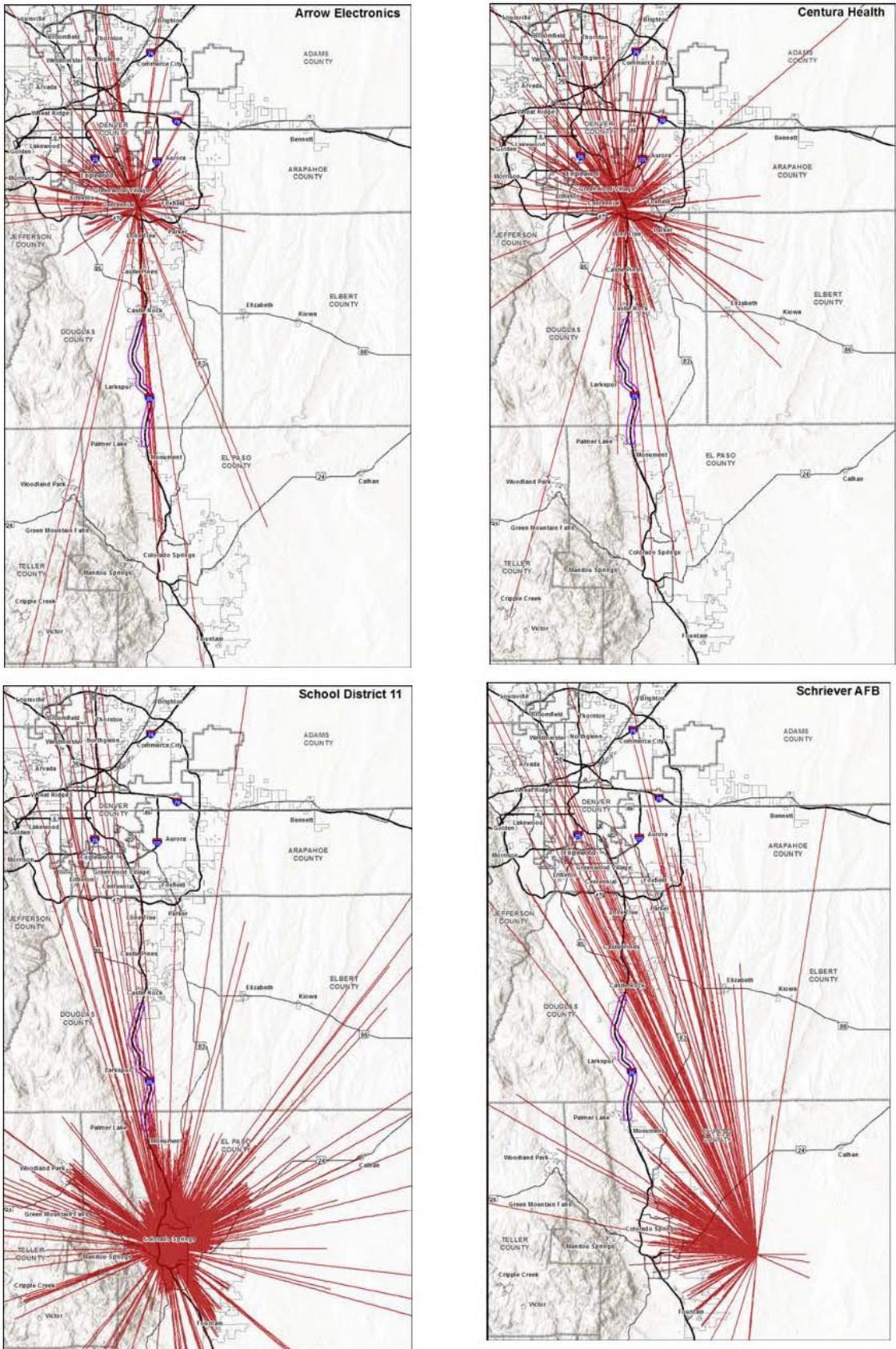
Figure 10. Major Employers in Southwest Arapahoe and Douglas Counties, 2016



Source: 2016 El Paso County Comprehensive Annual Financial Report

Figure 11. Major Employers in El Paso County, 2016

3.0 STUDY AREA ECONOMY



Source: Local Employment Household Dynamics, US Census

Figure 12. Origin-Destination of Select Major Employers (Block Group Analysis), 2016

There are a number of significant employers in southwest Arapahoe, Douglas and El Paso Counties as shown in Figure 10 and Figure 11. . The largest employer in southwest Arapahoe and Douglas Counties in 2016 was the Douglas County School District with just over 6,000 employees. The second and third largest employers were Charles Schwab (3,200 employees) and Echostar Communications (2,320 employees) (Figure 10). Douglas County School District jobs are shown in the head office location, although school district employees such as teachers are scattered throughout the County.

The largest employer in El Paso County (Figure 11) is the United State military with approximately 57,000 employees. The largest military installation is Fort Carson with 32,000 employees, followed by about 9,900 at Peterson Air Force Base, 7,700 at Schriever Air Force Base, and nearly 8,000 at the U.S. Air Force Academy. The largest private employer in the county is the UCHealth Memorial Health System with 6,200 employees.

Figure 12 shows generalized origin-destination information for the same census blocks as select major employers. The radial lines map workers' respective home location destinations. It helps to illustrate that while Denver metro and Colorado Springs commuting tends to take place within each respective labor shed, there are not insignificant numbers of commuters who travel long distances from their homes to work.

3.3 Major Industrial Clusters

State and regional economic development corporations monitor economic development “clusters”. Clusters are groups of interrelated industries which represent regional economic relationships that help drive regional economies. Clusters can also include universities and research laboratories that help drive innovation as well as supporting industries. Clusters help attract employees who have a larger potential job pool from which to choose.

The Metro Denver Economic Development Corporation and the Colorado Springs Chamber of Commerce and Economic Development Corporation monitor a number of key industry clusters closely because of their importance to the state and regional economy.

Key industry clusters in the Metro Denver/Northern Colorado region include:

- Aerospace
- Aviation
- Beverage Production
- Bioscience
- Broadcasting and Telecommunications
- Energy
- Financial Services
- Healthcare and Wellness
- Information Technology / Software

Key industry clusters in the Colorado Springs region include:

- Aerospace and Defense
- Information Technology / Cybersecurity
- Healthcare and Medical Technologies
- Sports
- Manufacturing (Advanced Manufacturing)

Our analysis focused on those clusters that were important to both metropolitan areas. Overlapping clusters include:

- Aerospace and Defense

- Information Technology / Computer Software / Cybersecurity
- Healthcare and Medical Technologies

Some of the clusters were collapsed in order to reflect the close relationships among the sectors. The clusters were defined using the North American Industrial Classification Codes as follows:

- Aerospace and Defense:
 - 332993 – Ammunition (except small arms) Manufacturing
 - 334511 - Search, Detection, Navigation, Guidance, Aeronautical, and Nautical System and Instrument Manufacturing
 - 33641 – Aerospace Product and Parts Manufacturing
 - 541713 – Research and Development in Nanotechnology
 - 541715 – Research and Development in Physical, Engineering, and Life Sciences
 - 9271 – Space Research and Technology
 - 92811 – National Security
- Information Technology / Computer Software / Cybersecurity:
 - 334614 – Software Reproducing
 - 511210 – Software Publishers
 - 518210 – Data Processing, Hosting, and Related Services
 - 541511 – Custom Computer Programming Services
 - 541512 – Computer Systems Design Services
 - 541513 – Computer Facilities Management Services
 - 541519 – Other Computer Related Services
- Healthcare and Medical Technologies:
 - 621-623 – Ambulatory Health Care Services, Hospital, Nursing and Residential Care Facilities
 - 3391 – Medical Equipment and Supplies Manufacturing
 - 3254 – Pharmaceutical and Medicines Manufacturing
 - 3334510 – Electromedical and Electrotherapeutic Apparatus
 - 42345 & 42346 - Medical and Dental Hospital Equipment and Supplies
 - 4242 – Drugs and Druggist Sundries Wholesalers
 - 541713 - Research and Development in Nanotechnology
 - 541714 – Research and Development in Biotechnology
 - 541715 - Research and Development in Physical, Engineering, and Life Sciences

Aerospace and Defense

The defense and aerospace cluster is a major economic driver in Colorado. The state is home to the second largest space economy in the United States after California and has a sizeable military presence. Colorado has major Department of Defense, NASA, and related commercial activities and is the US center for military space (National Space Defense Center). The presence of the state's four military commands – the primary customers for space based research, development, acquisition, and operations – ensure that this cluster remains strong.

The cluster is dependent on air freight connections and road and rail networks to develop, build, and deploy defense technologies and forces. It relies heavily on the state's highways, including I-25 South, part of the Strategic Highway Network (STRAHNET) system of public highways developed by the Federal Highway Administration (FHWA) and the Department of Defense (DOD) to provide access, continuity, and emergency transportation of personnel and equipment for national defense and security. Connectivity to STRAHNET and the Colorado freight network is very important to the state's active

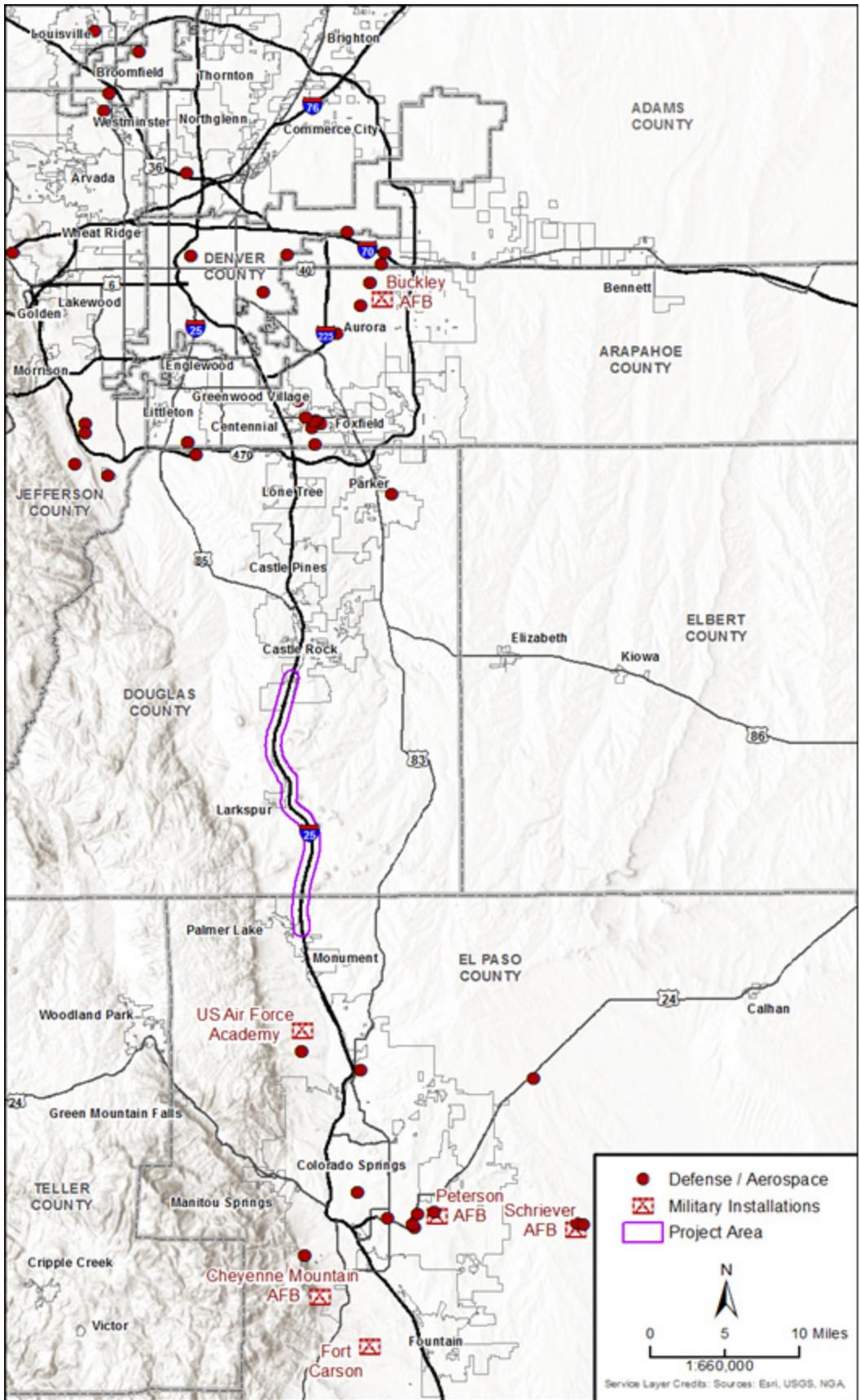
military installations and many Colorado National Guard facilities. It is particularly important to Fort Carson in El Paso County because it is a major military deployment site with direct access to I-25/U.S. Highway 87, which is on the STRAHNET and the Colorado freight network. Additionally, companies and military facilities in this cluster rely on a multimodal freight transportation options to receive and ship final goods. The state is currently competing with other states to launch the space industry and position space transport as a future freight and cargo mode.

This cluster has a significant presence in the study area. Major private companies in the Aerospace/Defense cluster in the southwestern Arapahoe, Douglas, and El Paso Counties include:

- The Harris Corporation
- Lockheed Martin
- Raytheon
- Sierra Nevada Corporation
- United Launch Alliance

The defense sector in El Paso County, in particular, is an important driver of the regional economy. El Paso County facilities include: Fort Carson, Peterson Air Force Base, Schriever Air Force Base, Cheyenne Mountain, and the Air Force Academy.

The Northern Colorado region also has a strong cluster of companies involved in geospatial technologies, remote sensing, imaging, and related industries. There are companies involved in research, development, design, and manufacturing of products and systems for commercial, military, and civil space applications. This includes products like navigation and detection instruments, guided missiles, spacecraft, satellites, and communications equipment. There are strong relationships among companies in this cluster throughout the Front Range.



Source: Colorado Department of Labor and Employment, QCEW, ArLand

Figure 13. Defense and Aerospace Cluster

Figure 13 shows the locations of the entities in this cluster in the study area. There is significant overlap between this and the following cluster, the Information Technology, Software and Cybersecurity Cluster shown in Figure 14. There is a relatively high concentration of entities in this cluster between I-25 and Foxfield in Arapahoe County and in and around Colorado Springs. Unlike other clusters, this cluster has a number of large employers employing between 200 and 500 persons each. The military bases also employ a significant number of military personnel with Fort Carson employing the most at over 30,000 military and civilian persons.

The Department of Defense (DOD) “industry” in Colorado not only plays a big role in supporting national security, but also the state’s economy. According to a recent study of the economic impact of defense, military, and veterans related activities:

- Total defense sector related employment in Colorado is estimated at nearly 250,000 jobs, 9% of total Colorado wage and salary employment. This includes both persons employed directly by the Defense Sector as well as the multiplier effect of indirect and induced jobs.
- Eight major military installations in the Front Range help generate \$11 billion in total labor income in El Paso, Arapahoe, Pueblo, and Weld Counties. About 41% of El Paso County’s labor income and 12% of Arapahoe County’s labor income is estimated to be defense-sector related.

Much of the state’s DOD-related employment is tied directly to the installations themselves, as seen in Table 9. In 2016, there were over 65,000 people working on military installations in Arapahoe, Weld, Pueblo and El Paso Counties. About 81% of this installation-specific employment was in El Paso County, and about 60% of that was at Fort Carson, which has more than 30,000 employees.

Table 9. Employment on Colorado’s Front Range Major Military Installations, 2016

	Military	Civilian	Total
Arapahoe County Installations (Buckley Air Force Base)	7,925	3,802	11,727
Weld County Installations (Greeley Air National Guard)	305	12	317
Pueblo County (Chemical Depot & Preparedness Program)	400	115	515
El Paso County Installations	39,285	13,377	52,662
Fort Carson	25,514	6,527	32,041
Peterson Air Force Base / Cheyenne Mountain	5,632	4,297	9,929
Schriever Air Force Base	2,026	675	2,701
Air Force Academy	6,113	1,878	7,991
3-County Total	47,915	17,306	65,221

Source: Department of Military and Veterans Affairs, ArLand

^aMilitary employment includes Active Duty, National Guard and Reserves (estimated on FTE basis); Civilian includes professional and non-professional categories

In part, jobs and labor income are also related to DOD contracts, which are another important component of the DOD “industry” in the state. As seen in Table 10, Arapahoe and El Paso Counties received over two-thirds of the \$6.5 billion in DOD contracts in the state in 2016. Arapahoe County received about 35.3% of all contract dollars, followed by El Paso County at 32.8%.

Table 10. DOD Contract Value by County, 2016

	Contract Value ^a	Percent of Statewide Total
Arapahoe	\$2,306,136,422	35.3%
Douglas	\$9,126,923	0.1%
El Paso	\$2,143,956,817	32.8%
Pueblo	\$53,135,687	0.8%
Weld	\$2,522,382	<0.1%
County Total	\$4,514,878,231	69.1%
State of Colorado	\$6,536,208,170	100.0%

Source: Summit Economics, ArLand

^a Direct contracts and assistance awards; dollars shown represent obligated amounts

Recipients of DOD contracts span multiple sectors but include one particularly important sector in the state - private aerospace contractors. According to the Denver Metro Chamber of Commerce, Colorado's aerospace industry employs more than 7 percent of the nation's aerospace workforce and multiple national aerospace contractors have operations in the state, with key locations along the Front Range.

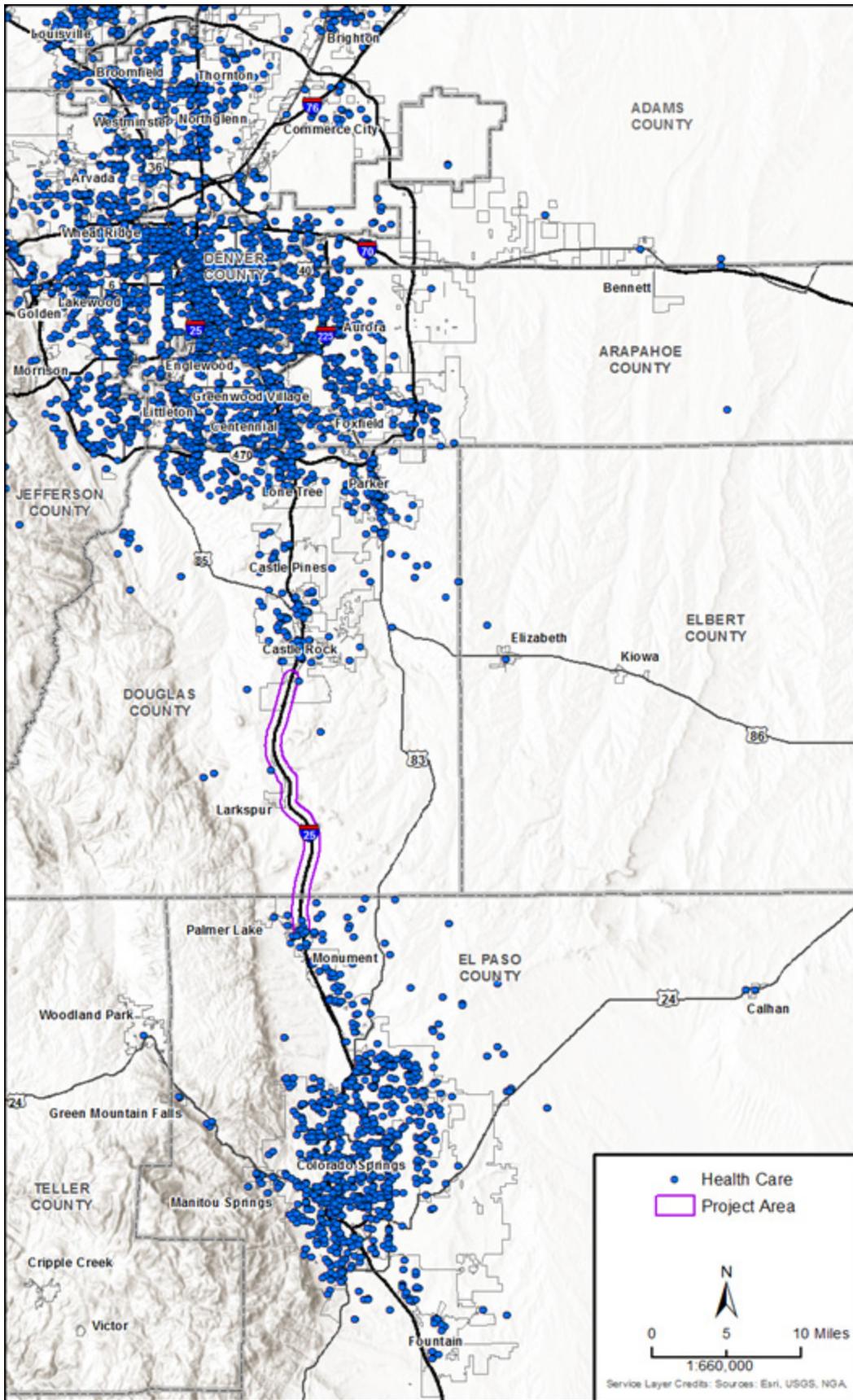
Arapahoe and El Paso Counties, in particular, are home to base installations and private defense contractors that support the DOD. In addition, defense and military activities benefit other counties in the state due to DOD contracts, National Guard and Reserve activity, and benefits received by Veterans. All of this translates into significant contributions to the state's total job base, labor income, and Gross Regional Product.

3.3.1 Information Technology, Software, and Cybersecurity

The information technology (IT) cluster is comprised of high-knowledge, human-capital-based businesses. The cluster is expanding globally, in Colorado and along the I-25 corridor. Enterprises in the IT/Computer Software portion of this cluster provide products and services in support of other business activities. The range of products and services provided is broad and includes computer security programs and virus protection, customer software and computer integrated systems design. The region is home to companies developing software for a variety of industries, including e-business, telecommunications, gaming, finance, and aerospace and defense.

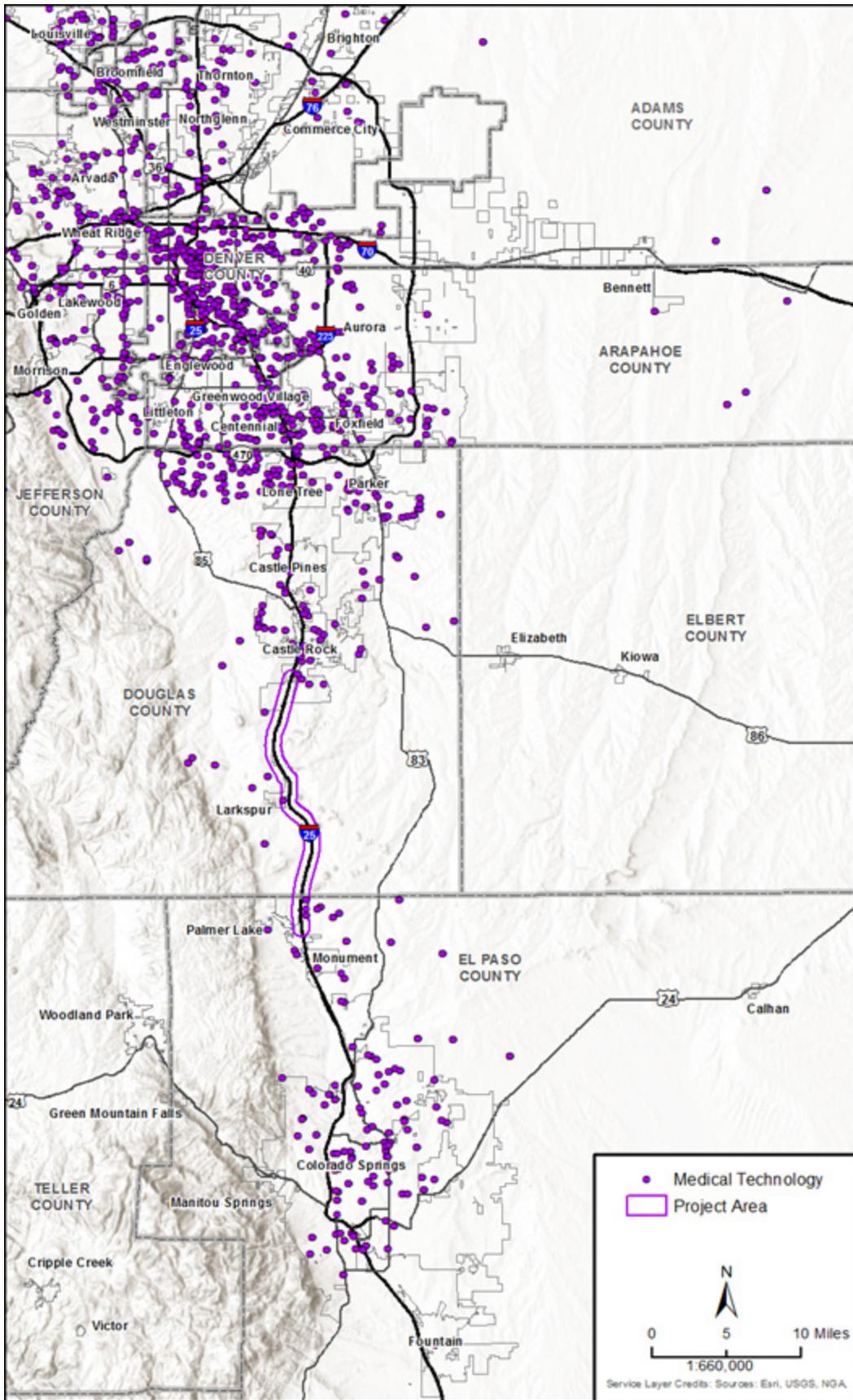
This cluster includes digital companies which rely on many types of supplies and products to be delivered or exported via the freight transportation network, such as computer components and electronics. Some companies in this cluster have a direct connection to the freight and logistics industry cluster, including those technology companies that provide logistics and supply chain management and big data services.

3.0 STUDY AREA ECONOMY



Source: CDLE QCEW, ArLand

Figure 15. Healthcare Cluster



Source: CDLE QCEW, ArLand

Figure 16. Medical Technology Cluster

The cybersecurity presence in the area is also robust and growing. Colorado Springs is home to the National Cybersecurity Center along with large companies that provide services to other businesses and the military. Cybersecurity companies provide a wide range of services including managing military satellite operations and control systems, tracking arsenal, and providing datacenter control and protection. There are strong interrelationships between this cluster and defense / aerospace.

As seen in Figure 14, there were almost 12,000 entities operating in the IT, software and cybersecurity cluster in 2016, including startups and major corporations. Businesses in this cluster are highly concentrated in southwestern Arapahoe County and northwestern Douglas County. Southeast Aurora, Parker, Castle Rock, and northern Colorado Springs are also home to relatively high numbers of businesses in this cluster.

3.3.2 Healthcare and Medical Technology

The healthcare sector has a large and growing presence in the Study Area. This includes healthcare providers, medical and specialty hospitals, kidney dialysis centers, among others. It is supported by renowned clinical systems and top-rated research institutions, as well as medical innovation and technology firms. Firms working in biotechnology and the medical device and diagnostic fields are working on everything from engineering custom artificial limbs to developing nutritional eye health support solutions.

This cluster has over 15,000 entities in the Study Area and is heavily scattered with concentrations along I-25 as depicted in Figure 15 and Figure 16.

As a net domestic importer of medical goods and products, this cluster is heavily reliant on the freight transportation system for receipt of supplies that are critical to their operations. This includes medicine and bandages, among many others. Over 97,000 tons of pharmaceuticals were shipped to Colorado from other states, primarily transported via truck on the state's highway system. Valued at over \$6.5 billion dollars, this is just one type of product this industry cluster requires for its day-to-day operations.

3.4 Jobs Forecast

Based on forecasts prepared by the Denver Regional Council of Governments (DRCOG) and the Pikes Peak Area Council of Governments (PPACOG), there will be an estimated 430,000 additional jobs in Arapahoe, Douglas, and El Paso Counties by 2040. This growth figure is based on 2015 DRCOG/PPACOG Council of Governments figures, which are quite different from the Quarterly Census of Employment and Wage (QCEW) data from the Colorado Department of Labor and Employment, as seen in Table 11. Job growth is expected to be particularly high in places like Monument and Black Forest. For purposes of this analysis, jobs growth forecast are based on the Council of Governments estimates.

El Paso County is forecast for an additional 185,000 jobs, followed by Arapahoe and then Douglas Counties. Figure 18 shows that both the southern Denver and the Colorado Springs metropolitan areas are forecast to see more employment infill in their respective areas.

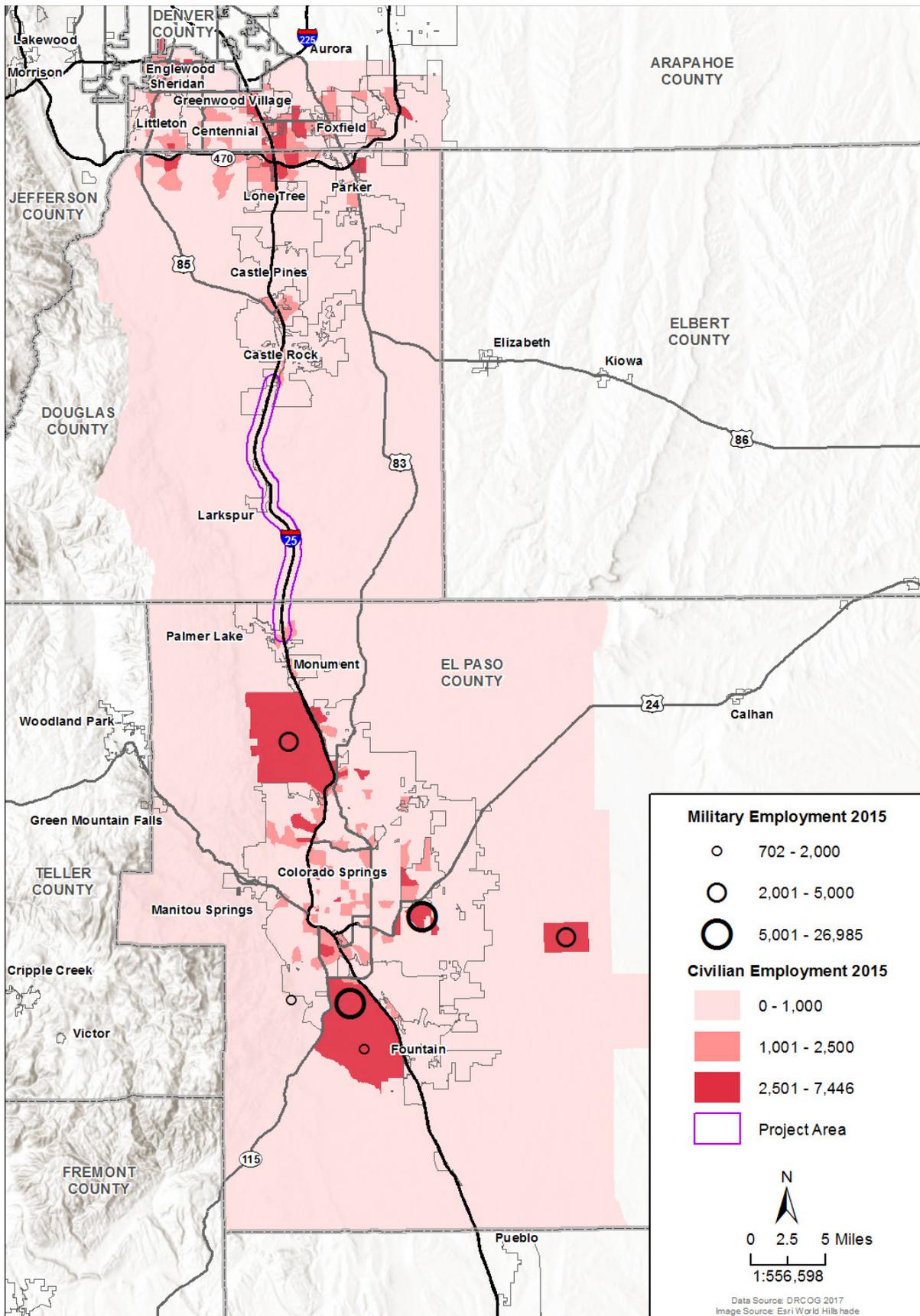
Table 11. Employment Forecasts, 2015-2040

	2015 QCEW ^a	2015 DRCOG / PPACOG	% Difference between QCEW and DRCOG / PPACOG	2040 DRCOG / PPACOG	DRCOG / PPACOG Growth 2015-2040	CAGR ^b 2015-2040
Employment						
Arapahoe County	316,597	345,259	-9.1%	504,770	159,511	1.5%
Centennial	--	66,033	--	81,129	15,096	0.8%
Douglas County	112,883	136,650	-21.1%	221,618	84,968	2.0%
Lone Tree	--	16,781	--	27,549	10,768	
Castle Pines	--	--	--	--	--	--
Castle Rock	--	17,106	--	24,772	7,666	1.5%
Larkspur	--	166	--	215	49	1.0%
El Paso County	256,705	313,133	-22.0%	499,222	186,089	1.9%
Palmer Lake	--	732	--	1,142	410	1.8%
Woodmoor CDP	--	2,490	--	4,250	1,760	2.2%
Monument	--	3,425	--	8,768	5,343	3.8%
Black Forest CDP	--	1,791	--	4,157	2,366	3.4%
Palmer Lake	--	732	--	1,142	410	1.8%
Air Force Academy CDP	--	1,273	--	1,273	0	0.0%
Colorado Springs	--	243,934	--	368,815	124,881	1.7%

Source: Labor Market Information-Quarterly Census of Employment and Wages Program, DRCOG, PPACOG, ArLand

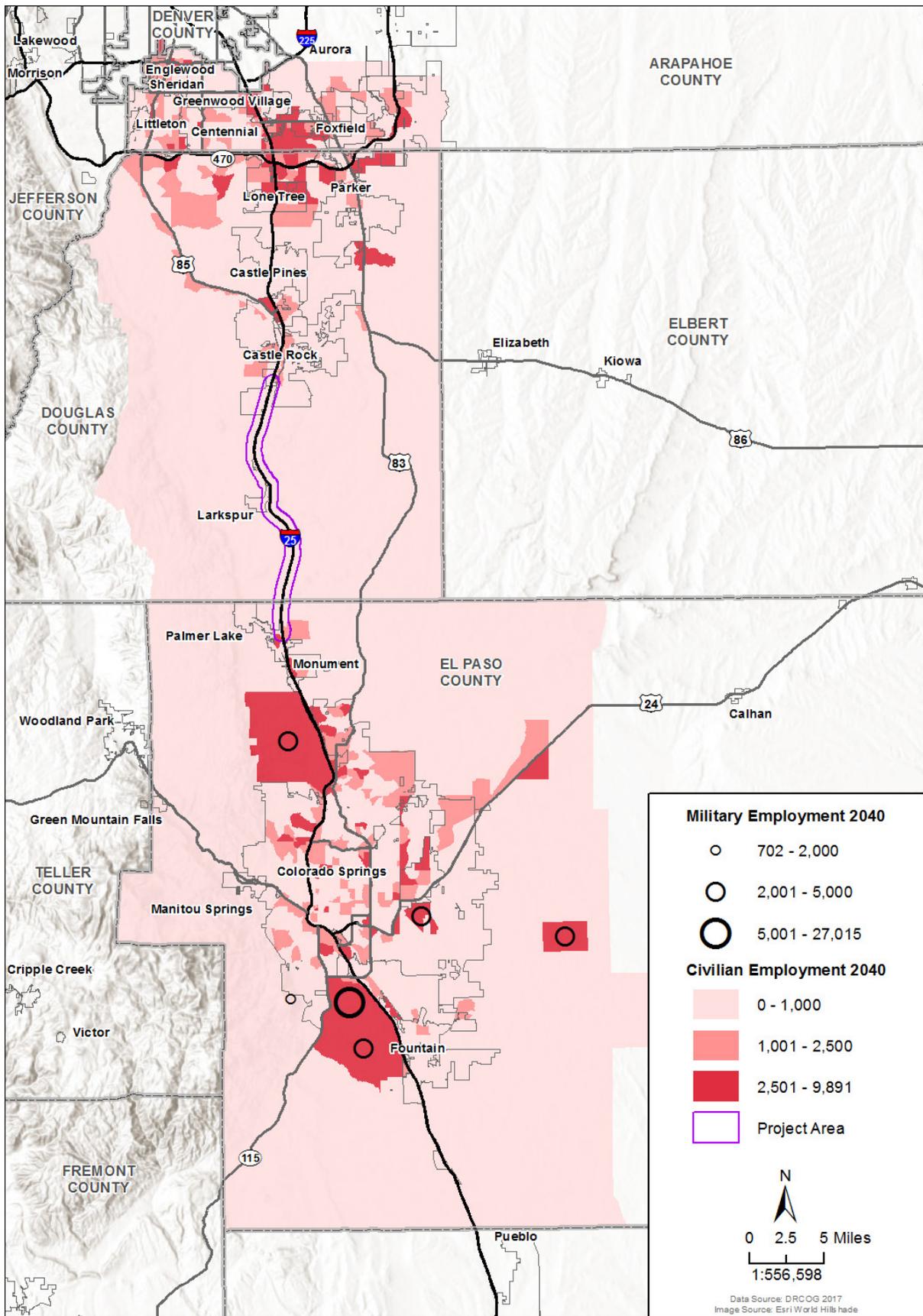
^a Average annual employment

^b Compound Annual Growth Rate



Source: DRCOG, PPACOG, ArLand

Figure 17. Civilian and Military Employment, 2015



Source: DRCOG, PPACOG, ArLand

Figure 18. Civilian and Military Employment, 2040

Although jobs line the I-25 corridor, in 2015 civilian jobs were very concentrated along I-25 from Greenwood Village to Lone Tree, in Englewood, and Highlands Ranch, as seen in Figure 17. Military jobs are located in El Paso County, with Fort Carson being home to the largest number of military jobs.

By 2040, the number of military jobs is not forecast to change significantly. Civilian jobs, however, are forecast to intensify in those areas where they are already relatively dense. In addition, southeast Aurora, Parker, and the area between Castle Pines and Castle Rock is expected to see an increase in jobs by 2040. Farther south, Monument and eastern Colorado Springs are also expected to see jobs increases by 2040.

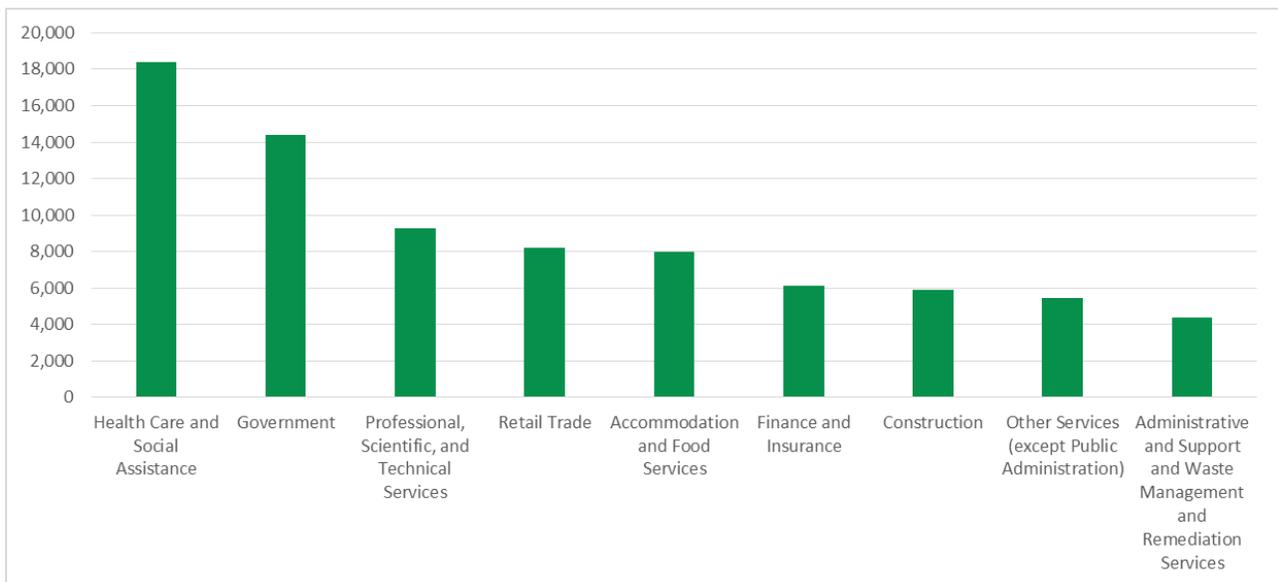
3.5 Workforce Demand

While forecasts are for continued growth in both the Denver and Colorado Springs metropolitan area economies, interviews indicate that there are huge workforce needs that are being unmet.

Housing affordability is becoming an increasingly significant challenge impacting workforce demand, the ability to fill jobs and grow the economy. This section will discuss area workforce needs, housing needs, and how the increased unaffordability of metro Denver housing, and the relative affordability of El Paso County has the potential to exacerbate traffic conditions in the I-25 corridor area between the two metropolitan areas.

This study is being conducted during a time of record low unemployment in Arapahoe, Douglas, and El Paso Counties. According to the Bureau of Labor Statistics, unemployment is estimated at 2% for all three areas. Businesses throughout the Front Range are anecdotally having difficulty finding workers.

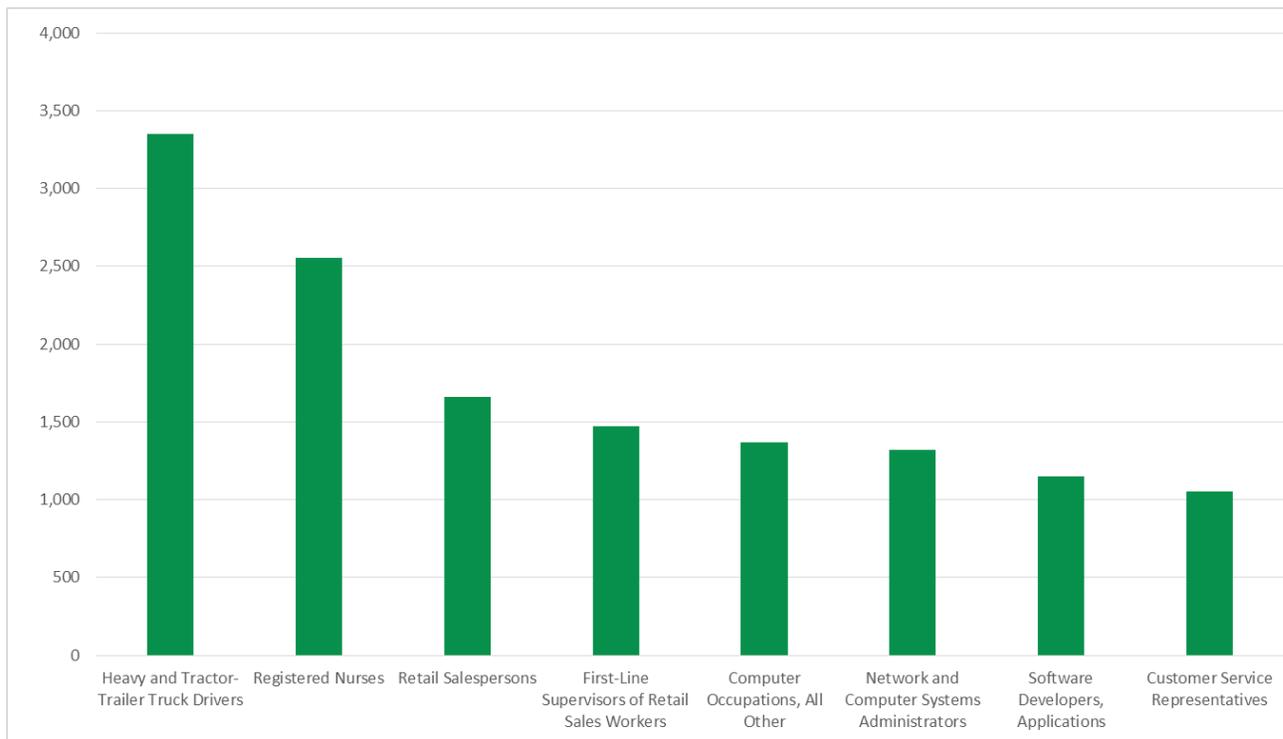
While low unemployment is potentially a temporary phenomenon, State and Arapahoe / Douglas County Workforce Center forecasts indicate that there will be demand for 90,000 jobs in 5 years in the 3-county region with most of the demand forecast in Health Care and Social Assistance, Government, Professional Scientific and Technical Services, and Retail Trade, as seen in Figure 19.



Source: Emsi, Arapahoe/Douglas Works!, ArLand

Figure 19. Workforce Needs in El Paso, Douglas, and Arapahoe Counties, 2016- 2021

Figure 20 shows the current unfilled top jobs in the 3-county region. Jobs driving trucks, nursing, retail, as well as a number of different high tech positions are currently not being filled.



Source: Emsi, Arapahoe/Douglas Works!, ArLand

Figure 20. Top Unfilled Jobs in El Paso, Douglas, and Arapahoe Counties, 2016-2021

While these jobs have a variety of educational and experience requirements, many of them are considered “Very Difficult” to fill, despite high salaries. Among the jobs listed, average salaries range from about \$29,000 for retail sales person jobs to over \$100,000 for software developer jobs (Table 12).

Table 12. Top Unfilled Jobs and Average Wage in 2016

Occupation	Average 2016 Colorado Wage
Heavy and Tractor-Trailer Truck Drivers	\$47,340
Registered Nurses	\$70,550
Retail Salespersons	\$29,040
First-Line Supervisors of Retail Sales Workers	\$49,170
Computer Occupations, All Other	\$95,600
Network and Computer Systems Administrators	\$83,470
Software Developers, Applications	\$105,640
Customer Service Representatives	\$35,440

Source: Emsi, Arapahoe/Douglas Works!, Occupational Employment Statistics, Bureau of Labor Statistics, ArLand

In choosing to take a job or relocate for a job, one of the most important considerations is the location, availability, and price of housing relative to the job location. The increase in price and availability of appropriately priced housing has important implications for fulfilling workforce needs as well as the transportation system that enable workers to get to their jobs.

3.5.1 The Rising Price of Housing

According to the US Census, Colorado was ranked the 8th fastest growing state in the US between 2010 and 2016 with the addition of half a million people. According to the State Demographers’ Office, the majority of growth came from migration, rather than natural increases in the population. Much of the

growth has been seen in the Denver metro area in the “core” Denver metro counties (Table 13). Denver County grew by the most at over 90,000 new residents, followed by Adams County at 57,000 new residents. Douglas County added nearly 30,000 residents. During this time period, El Paso County grew by approximately 43,000 new residents.

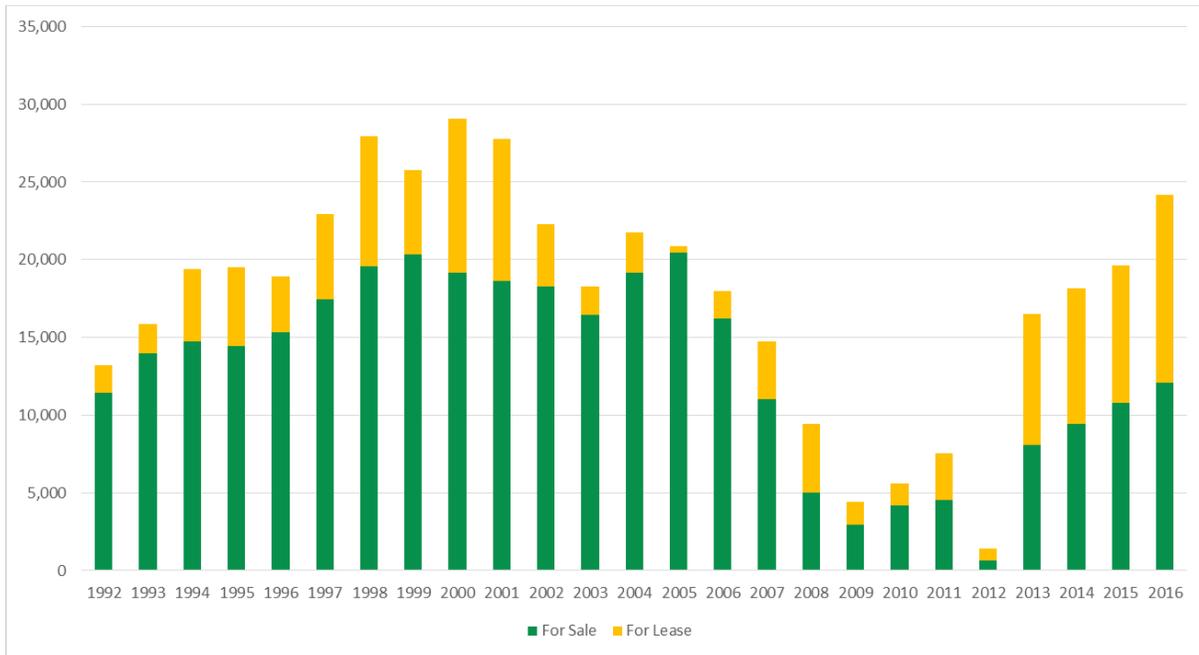
Table 13. Population Growth 2010-2016

	2010	2016	Growth 2010-2016	CAGR 2010-2016
Adams County	441,603	498,187	56,584	2.0%
Arapahoe County	571,914	617,668	45,754	1.3%
Boulder County	294,567	322,226	27,659	1.5%
Broomfield County	55,889	66,529	10,640	2.9%
Denver County	600,158	693,060	92,902	2.4%
Douglas County	285,465	314,338	28,873	1.6%
Jefferson County	534,543	571,837	37,294	1.1%
Total	2,784,228	3,117,539	333,311	1.9%
El Paso County	622,263	665,171	42,908	1.1%

Source: US Census, ArLand

Despite the unprecedented growth, the residential market hasn’t rebounded at the same levels and in the same way as seen in previous economic recoveries. Figure 21 shows new housing permits in the Denver metro area, divided by For-Sale and For Lease permits. It includes the same core Denver metro counties as shown in Table 13. Previous economic recoveries in the late 1990s and 2000s have seen a significant amount of single family detached housing built. When housing production restarted in 2013, the building mix shifted to many more for-lease apartments, targeted to the young adult population moving to the state.

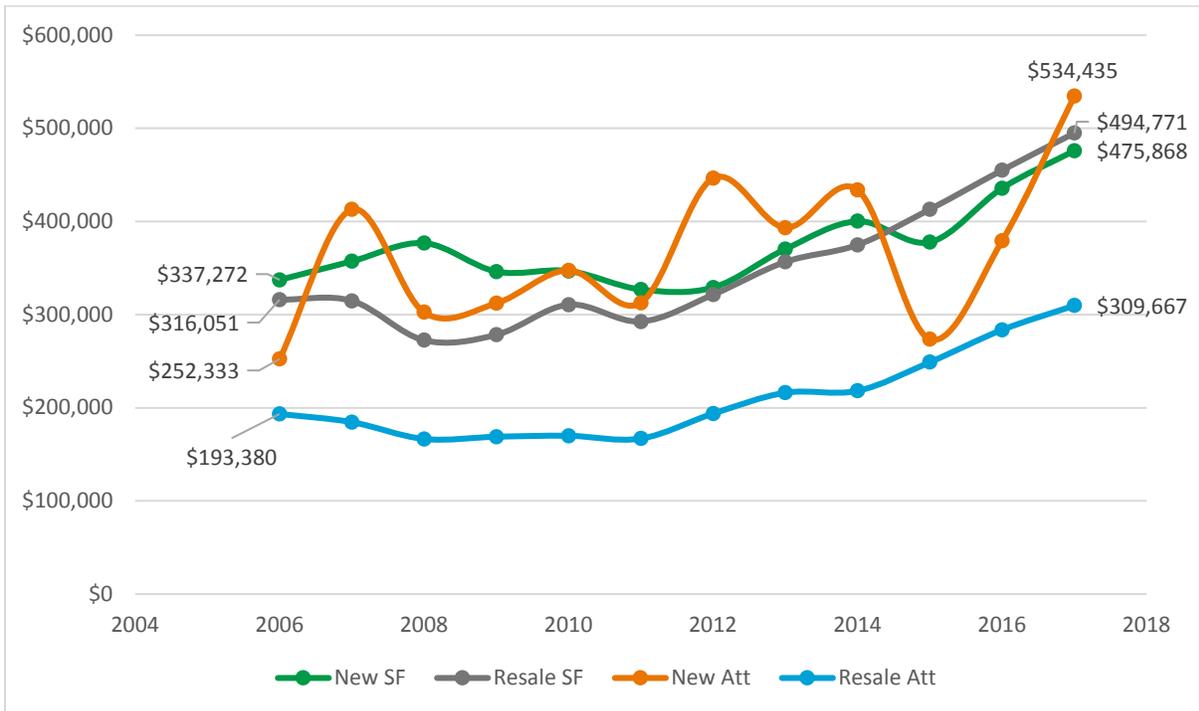
According to the Denver Metro Association of Realtors, developers are building fewer homes compared to previous economic recoveries, and when they do build, they’re building higher-end products to maximize profits in the face of more regulations, and rising construction and labor costs.



Source: Genesis Group, Home Builders Association of Metropolitan Denver, ArLand

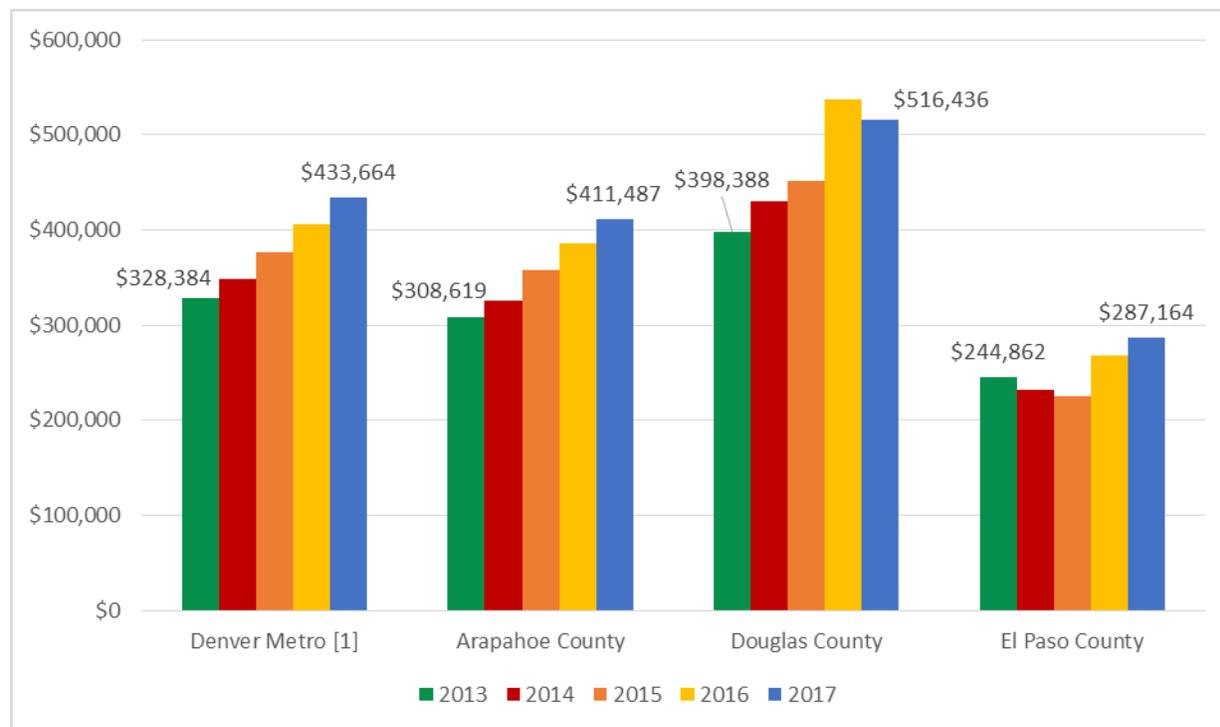
Figure 21. Denver Metro Building Permit Trends, 1992-2016

Partially as a result of more constrained supply, prices of all housing types have seen sustained increases over time. Figure 22 shows the change in Denver metro area housing values for different for-sale housing types. All housing types saw changes in values during this time period. While there have been fluctuations, prices have generally seen a sustained increase in the past ten years. While the average price of resale attached housing (condos and townhomes) in the Denver metro area is approximately \$300,000, the average price in the Denver metro area for other for-sale housing types is currently \$475,000 and higher.



Source: Genesis Group, Home Builders Association of Metropolitan Denver, ArLand

Figure 22. Average Denver Metro Housing Prices



Source: Colorado Comps LLC, ArLand

Figure 23. Average For-Sale Housing Prices

Figure 23 shows the change in average housing prices (for all housing types) for the Denver metro area, Arapahoe, Douglas, and El Paso Counties. Average housing prices in Douglas County are over \$500,000. In Arapahoe County and Denver metro area (defined as Adams, Arapahoe, Boulder, Denver, Douglas, Jefferson Counties), average housing prices are over \$400,000. El Paso County has seen increases in average housing prices, but they are generally under \$300,000. According to the Colorado Association of Realtors, the median price of a single family home in El Paso County is \$255,000.

Low interest rates and high apartment rents are contributing to the demand for for-sale units. Anecdotal evidence and broker interviews indicate that the “drive till you qualify” phenomenon is increasingly being seen in Monument and northern Colorado Springs. Monument is perceived as a location convenient to one spouse who may work in the Denver metro area and the other spouse who may work in the Colorado Springs area.

Although Denver is often cited as a top millennial destination, Colorado Springs has become a city within the U.S. with one of the fastest growing millennial populations. Local officials believe that job availability and housing affordability are contributors to the growing demand.

3.5.2 Workforce Demand and Housing Availability

Table 14 shows the workforce occupations in greatest demand in the 3 County area and associated incomes. It assumes that principal, interest, taxes and insurance are 28% of gross income. Assuming that up to 20% down, 5% interest, and a 30 year mortgage, the price an average full time worker can afford is shown on the bottom line. According to Table 14, the professions with the ability to afford an average priced house in Douglas County are those in the highest paid software and computer professions. While many households have dual incomes which this table doesn’t reflect, it begins to indicate that for many households, purchasing a home in the Denver metro area is increasingly difficult.

Table 14. Housing Prices for Professions Most in Demand

		Truck Drivers	RNs	Retail Sales	Sales Supervisor
Average Personal Income		\$47,340	\$70,550	\$29,040	\$49,170
Income Available for Housing, Insurance, and Taxes	28%	\$13,255	\$19,754	\$8,131	\$13,768
Monthly Payment		\$1,105	\$1,646	\$678	\$1,147
Mortgage Rate		5%	5%	5%	5%
Mortgage Term		30 years	30 years	30 years	30 years
Financeable Mortgage Amount		\$205,767	\$306,651	\$126,224	\$213,721
20% Down, Supportable Price		\$257,208	\$383,313	\$157,781	\$267,151

Source: Occupational Employment Statistics, Bureau of Labor Statistics, Arland

		Computer	Administrators	Software	Customer Service
Average Personal Income		\$95,600	\$83,470	\$105,640	\$35,440
Income Available for Housing, Insurance, and Taxes	28%	\$26,768	\$23,372	\$29,579	\$9,923
Monthly Payment		\$2,231	\$1,948	\$2,465	\$827
Mortgage Rate		5%	5%	5%	5%
Mortgage Term		30 years	30 years	30 years	30 years
Financeable Mortgage Amount		\$415,532	\$362,808	\$459,172	\$154,042
20% Down, Supportable Price		\$519,415	\$453,510	\$573,965	\$192,553

Source: Occupational Employment Statistics, Bureau of Labor Statistics, Arland

Apartment rents show similar differences between the Denver metro and Colorado Springs economies. Table 15 shows average asking rents, irrespective of unit types. It shows that average rents in El Paso County are 25% lower than average Denver metro rents and 33% lower than average Douglas County rents.

Table 15. Average Asking Apartment Rents, 2018

Area	Average Rent
Denver Metro	\$1,319
Arapahoe County	\$1,275
Douglas County	\$1,450
El Paso County	\$983

Source: CoStar, ArLand

4.0 Freight

Between Denver and Colorado Springs, truck traffic is estimated to comprise up to 12% of all traffic traveling the I-25 corridor. Given the dominant role of truck traffic, a safe and reliable highway system throughout the state, including I-25 South, is important to economic competitiveness and growth. Inefficiencies in supply chains, (including transportation safety and reliability issues), impose direct costs on businesses and reduce the state's economic competitiveness. It is estimated that transportation costs can represent anywhere from 5% to 50% of business revenue. Congestion can include additional costs in the form of additional wages, wasted fuel, and idle equipment. This downtime has the potential to disrupt the supply chain and production schedules. Congestion in the Denver metro area alone is estimated to cost each truck moving through the area an additional \$25,000 per year in lost time and added fuel costs.

In addition to congestion, degraded roads add to maintenance costs due to tire wear, etc. Removing such inefficiencies enhances the ability of businesses to utilize just-in-time inventory management systems and minimize transportation-related costs. Improving safety and reliability is an important part of connecting the economy and maintaining and enhancing the state's economic competitiveness.

This section provides details on the freight economy, the National Highway Freight Program, freight's importance to the Colorado economy as well as the jurisdictions within the project area, and key industry clusters and their reliance on the state's freight transportation network.

4.1.1 The Freight Economy and the National Highway Freight Network

The United States is the world's largest economy, and the freight economy plays a vital supporting role in the economies of the United States and the State of Colorado. Freight is moved throughout the country and Colorado over the freight transportation system, an extensive network of highways, railroads, waterways, pipelines, and airways. In Colorado, trucks carried about 77% of all shipments by tonnage to, from, and within the state in 2015.

I-25 is part of the Camino Real Corridor, which runs from El Paso, Texas, to the Canadian border in Montana. The Camino Real Corridor is a Congressionally designated High Priority Corridor established by the Intermodal Surface Transportation Efficiency Act of 1991.

A National freight policy was established in the Moving Ahead for Progress in the 21st Century (MAP-21) Act, signed into law in July 2012. Map-21 was soon replaced, in December 2015, by the Fixing America's Surface Transportation (FAST) Act as the overall federal surface transportation legislation. The FAST Act built on Map-21's provisions to make federal surface transportation more streamlined, performance-based, and multimodal. The FAST Act also established a new National Highway Freight Program to improve the efficient movement of freight. It had a series of goals including ongoing investment, improving safety, security, resiliency, and efficiency among others. It established several subsystems of roadways including the Primary Highway Freight System (PHFS) which is a network of highways identified as the most critical highway portions of the U.S. freight transportation system. The entire length of Interstate 25 (I-25) and Interstate 70 (I-70) in the state, as well as Interstate 225 (I-225) in the Denver metro area are all part of the PHFS (Primary Highway Freight System). I-25 (South) is a particularly critical component of I-25, helping connect the state's economy and the two largest metropolitan areas.

4.1.2 Freight's Importance to the Colorado Economy

On a statewide basis, Colorado's freight system has a significant impact on Colorado's economy. A few important statewide statistics are included below.

- \$155.8 billion of Colorado's Gross State Product of \$313 billion is generated by freight and freight-reliant industries.
- More than 1 in 6 jobs and one-third of the state's economy rely on the day-to-day movement of goods as a core business function.
- Over 420 million tons of products valued at over \$341 billion dollars moved within, in, and out of Colorado in 2016.
- Nearly \$8 billion in goods were exported from Colorado to destinations around the world in 2015.
- 4,500 new businesses are started in Colorado on average each quarter, many of which ship goods around Colorado, across the country, or overseas.
- Nearly 400,000 Colorado workers are employed in businesses that rely on freight movement. This includes 382,298 workers and 30,431 businesses operating in critical trades that, deliver packages, produce food, stock store shelves, manufacture goods, and supply other businesses.
- Colorado's robust freight, rail, transportation, trade, and logistics industries directly employed 40,836 workers in 3,405 companies in 2015. An additional 17,591 workers are self-employed in the transportation industry, including owner-operators of trucking, trade, and logistics businesses.
- The trucking industry in Colorado employs over 20,000 workers and generates significant motor fuel taxes and transportation fees.

And given other shifts in consumer behavior, like the rapid growth in e-commerce and on-demand shopping that is driving up demand for small package home delivery, among other trends, freight volume is expected to increase by more than 40 % by 2045 nationwide. In Colorado, the movement of goods by trucks is forecasted to increase 37 % and total over 279 million tons by 2045.

4.1.3 The Trading Economy

Colorado imports and exports goods from around the country and across the world. The state exports to more than 125 countries and simultaneously imports consumer products and production inputs, among others. Measured by the value of goods, Colorado is a net importer; measured by the weight of goods, Colorado is a net exporter of goods, albeit only slightly. Maintaining and enhancing the state's components of the global supply chain will enhance the state's economic competitiveness. A key part of this effort is connecting the economy through the multimodal freight transportation system.

Colorado's top domestic trading partners differ when comparing inbound and outbound movement of goods and whether goods are measured by weight (tons) or value (dollars). By the weight and value of goods, Colorado imports the most from Wyoming, Nebraska, Texas, California and Utah. Colorado imports the most goods by weight from, and by value from California. The state exports the most goods by weight to Wyoming, followed by Nebraska, Texas, Oklahoma, and Kansas. Colorado also exports the most goods by value to Wyoming. The other states in the top five by value are Nebraska, California, Utah, and Texas (Table 16).

Table 16. Colorado's Top Domestic Trading Partners (2015)

	Top Inbound Trading Partners					Top Outbound Trading Partners				
Forestry, Farm & Food										
Weight (tons)	Iowa 624,600 (6%)	Texas 649,200 (7%)	California 884,100 (9%)	Idaho 1,142,800 (12%)	Nebraska 3,317,600 (34%)	Nebraska 3,937,600 (16%)	New Mexico 798,400 (6%)	Kansas 701,500 (6%)	Texas 666,900 (5%)	California 637,100 (5%)
Value (dollars)	Idaho \$566,000 (5%)	Wyoming \$574,500 (5%)	Texas \$1,020,820 (9%)	Nebraska \$1,378,400 (13%)	California \$2,093,080 (19%)	Nebraska \$2,210,370 (16%)	California \$1,297,240 (9%)	Texas \$1,110,160 (8%)	New Mexico \$735,170 (5%)	Oregon \$695,790 (5%)
Manufactured Goods										
Weight (tons)	Illinois 475,900 (5%)	Oregon 634,300 (7%)	Utah 743,300 (8%)	California 1,123,200 (12%)	Texas 1,400,100 (14%)	Wyoming 1,180,700 (17%)	Texas 913,800 (13%)	Utah 657,000 (9%)	New Mexico 527,100 (7%)	California 478,300 (7%)
Value (dollars)	Michigan \$3,272,690 (5%)	Illinois \$3,467,810 (5%)	Utah \$5,095,400 (7%)	Texas \$11,583,410 (17%)	California \$13,364,380 (19%)	California \$5,194,260 (10%)	Texas \$4,918,040 (9%)	Utah \$4,414,660 (8%)	Wyoming \$3,448,200 (7%)	Arizona \$2,721,790 (5%)
Mining & Bulk Material										
Weight (tons)	Utah 419,100 (6%)	Texas 425,900 (6%)	Oklahoma 793,600 (11%)	Minnesota 1,106,400 (15%)	Wyoming 1,206,300 (17%)	Oklahoma 3,875,600 (44%)	South Dakota 738,200 (8%)	Utah 551,500 (6%)	New Mexico 496,300 (6%)	Wyoming 475,000 (5%)
Value (dollars)	California \$257,710 (6%)	Illinois \$285,050 (7%)	Tennessee \$321,720 (8%)	Texas \$372,750 (9%)	Minnesota \$787,450 (19%)	Utah \$1,019,690 (23%)	Missouri \$545,450 (12%)	Wyoming \$424,010 (10%)	Pennsylvania \$246,930 (6%)	Iowa \$226,630 (5%)
Coal, Oil & Natural Gas										
Weight (tons)	Louisiana 427,900 (1%)	Oklahoma 480,900 (1%)	Utah 923,000 (2%)	Nebraska 14,331,500 (29%)	Wyoming 32,412,400 (65%)	Wyoming 33,479,400 (34%)	Nebraska 19,733,100 (20%)	New Mexico 11,540,200 (12%)	Kansas 6,217,500 (6%)	Utah 5,174,600 (5%)

4.0 FREIGHT

	Top Inbound Trading Partners					Top Outbound Trading Partners				
Value (dollars)	Texas \$364,740 (3%)	Louisiana \$454,910 (4%)	Illinois \$516,060 (5%)	Nebraska \$3,651,040 (33%)	Wyoming \$4,733,790 (43%)	Wyoming \$7,863,540 (30%)	Nebraska \$4,765,420 (18%)	New Mexico \$2,317,380 (9%)	Kansas \$1,814,110 (7%)	Oklahoma \$1,434,980 (5%)
Chemicals, Plastics, Pharmaceuticals, etc.										
Weight (tons)	Oklahoma 265,400 (8%)	California 275,300 (8%)	Texas 276,400 (8%)	Wyoming 531,700 (16%)	New Mexico 554,300 (16%)	Illinois 370,500	Utah 283,500	New Mexico 230,900	Arizona 80,700	Wyoming 73,400
Value (dollars)	New York \$579,630 (4%)	Minnesota \$681,050 (5%)	Texas \$957,270 (8%)	California \$1,770,950 (13%)	Ohio \$3,219,480 (24%)	New York \$811,080 (9%)	Missouri \$697,740 (7%)	Nebraska \$549,940 (6%)	Wyoming \$542,090 (6%)	Texas \$528,450 (6%)

Source: Colorado Freight Plan, ArLand

In addition to domestic trade, Colorado is also heavily involved in international trade. Canada is Colorado's top export market, followed by Mexico and China. Colorado's highways, including I-25 South, serve as key corridors in the movement of goods between Colorado and Canada and Mexico. Movement of goods between Colorado and major shipping ports on California's west coast facilitate trade with China and other countries in Asia, including China, Japan and South Korea. Colorado's top international export categories by value include computer and electronic products and processed foods.

Within the state, a total of 5,810 companies exported from Colorado in 2014 and about 87% of these companies were small and medium sized. Of all exporters, the majority of these companies are located on the Front Range.

The Denver-Aurora-Lakewood metropolitan area exports the highest value of goods of all metropolitan areas in the state at \$3.9 billion in 2015. This represents just over 44% of state exports to foreign markets and makes it the 66th largest metropolitan area reporting international exports nationwide.

Table 17. Value of Colorado Metropolitan Area Exports (2015)

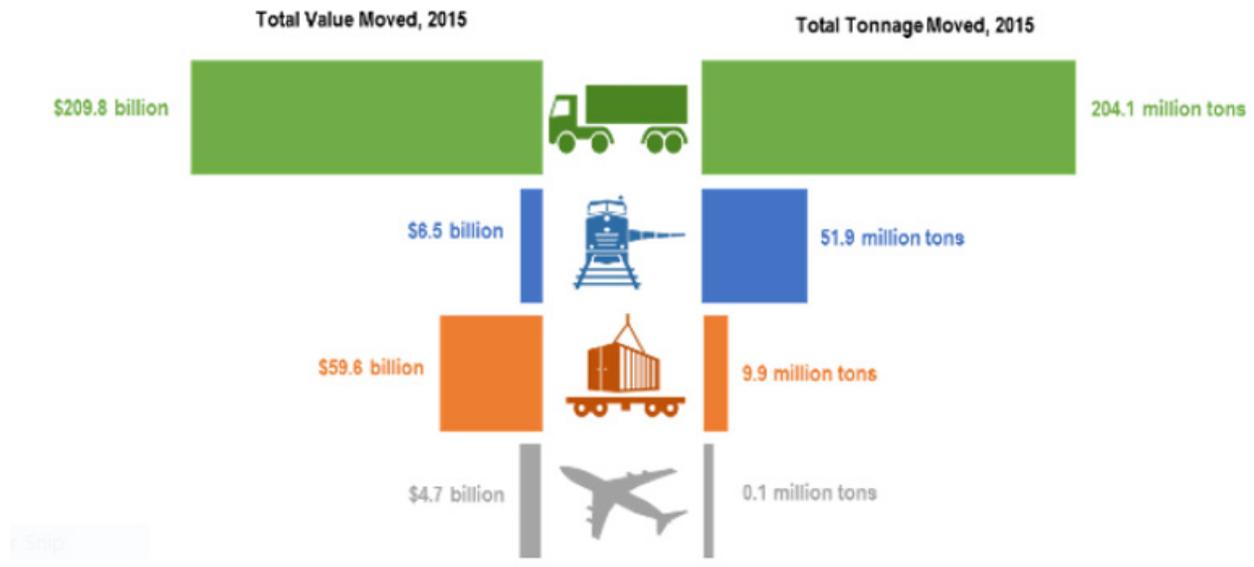
Metropolitan Area	2015 Goods Export Value	Share of State Exports
Denver-Aurora-Lakewood	\$3.9 billion	44.2%
Greeley	\$1.2 billion	14.0%
Boulder	\$1.0 billion	11.8%
Fort Collins	\$991 million	11.2%
Colorado Springs	\$832 million	9.4%
Pueblo	\$205 million	2.3%
Grand Junction	\$125 million	1.4%

Source: Office of Trade and Economic Analysis, International Trade Administration, U.S. Department of Commerce; ARLand

Colorado Springs has a very high export value of goods at \$832 million. The \$4.7 billion combined value of goods exported from the Denver-Aurora-Lakewood and Colorado Springs metropolitan areas represents over half (53.6%) of Colorado's total exports by value. And this doesn't account for other large Front Range metropolitan areas that rely on the efficient movement of goods to, from, and within the region, including Greeley, Boulder, and Fort Collins.

Every sector of the economy is heavily reliant on robust supply chains, including the safe and reliable movement of goods and supplies. For major industries in the state, including key industry clusters, their supply chains have many links and rely on a variety of inputs and materials. The efficient movement of freight is critical to business success and the state's economic competitiveness.

4.1.4 Value of Freight



Source: Colorado Freight Plan

Figure 24. Colorado Freight: Value and Tons Moved

According to the Colorado Freight Plan, trucks move the vast majority of freight throughout the state. The total estimated value of freight moved at the State level in 2015 is estimated at \$282.8 billion with \$209.8 billion of that moved by truck. In order to calculate the value of freight produced / moved in the study area, the ratio of freight value to State Gross State Product is assumed to apply to the 3-county region. An estimate was made of the portion of Arapahoe County’s Gross Regional Product just in the southern portion of the County near I-25. The total estimated value of freight moved in the study area is estimated at nearly \$60 billion.

Table 18. Estimate of Freight Value in Study Area, 2015-2016

County	Gross Regional Product	Estimated Value of Freight Flow
El Paso	\$33.1 billion	\$24.1 billion
Douglas	\$16.6 billion	\$12.1 billion
Arapahoe (southern portion only)	\$32.4 billion	\$23.6 billion
Total	\$82.1 billion	\$59.7 billion

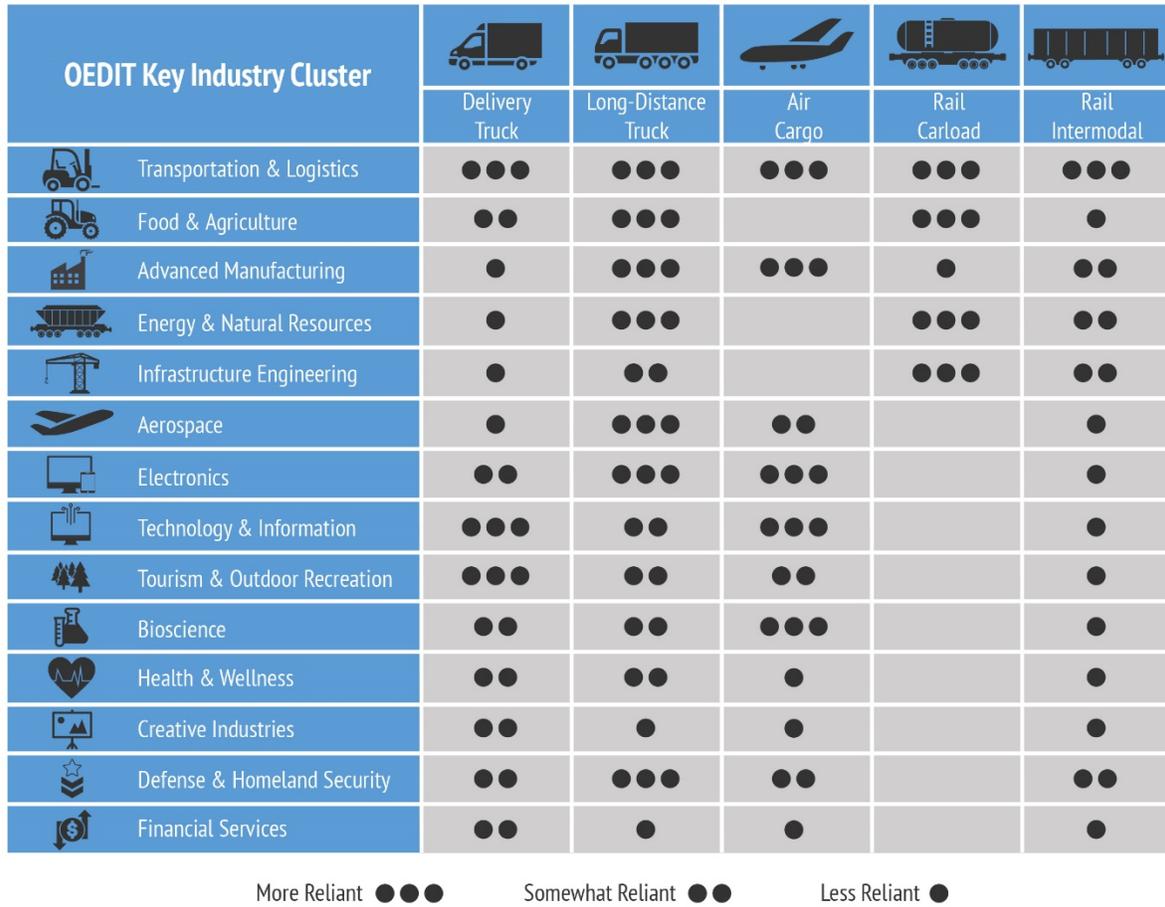
Source: Colorado Freight Plan, Arapahoe Douglas Works!, ArLand

4.1.5 Economic Development Clusters and Freight

There are fourteen key industry clusters that are vital to Colorado’s economic future according to the Colorado Office of Economic Development and International Trade (OEDIT). As seen in Figure 25, key industry clusters in the state have varying reliance on more than one mode of transporting goods and materials. All clusters rely to varying degrees on rail (carload or intermodal) and truck (long-distance and delivery) transportation. Three of the key industry clusters do not rely on air cargo to move freight.

The clusters previously profiled as particularly important to the economy in the study area includes Defense and Aerospace; Information Technology, Software and Cybersecurity, Health Care and Medical Technology.

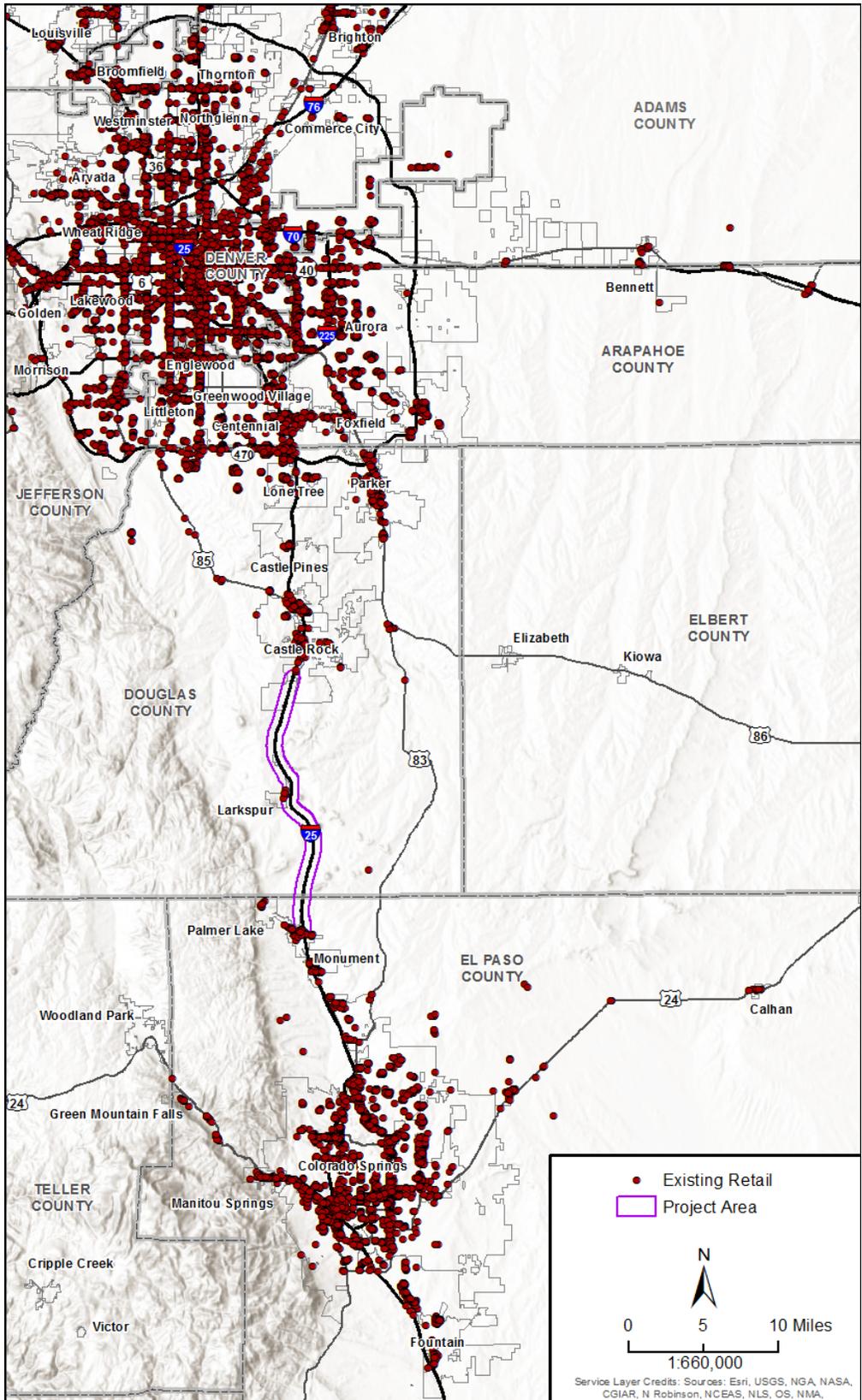
Aerospace and Defense are reliant on long distance trucks and to a lesser degree on delivery trucks. Technology and Information are reliant on delivery trucks and air cargo. Healthcare (Health and Wellness) are somewhat reliant on both delivery and long distance trucks.



Source: Colorado Freight Plan, 2018, ArLand

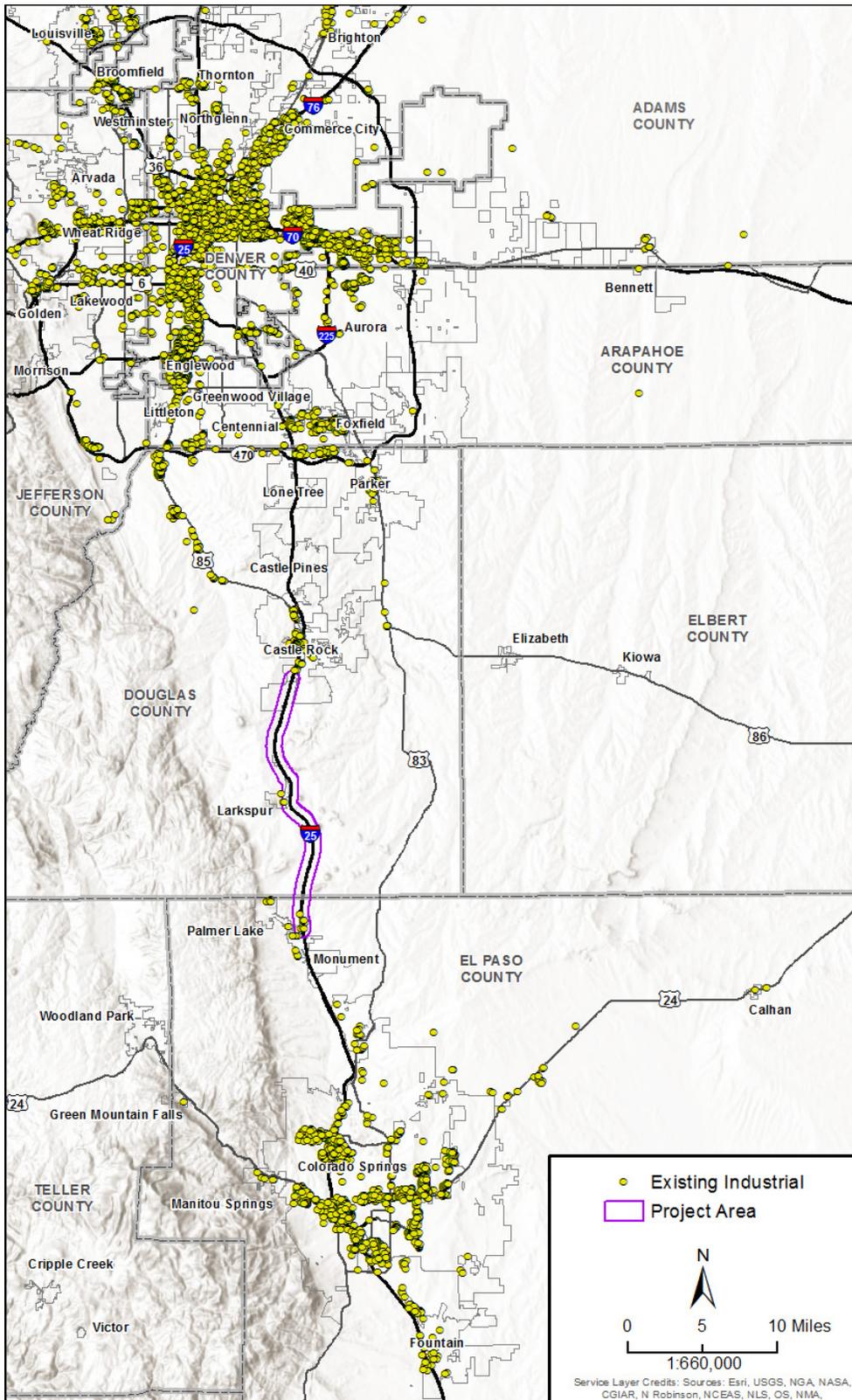
Figure 25. State Industry Clusters Freight Reliance

4.1.6 Retail and Industrial Activity



Source: Colorado Department of Labor and Employment, ArLand

Figure 26. Existing Retail Locations



Source: Colorado Department of Labor and Employment, ArLand

Figure 27. Existing Industrial Locations

Businesses in the retail and industrial sectors, in particular, are heavily reliant on trucks of all sizes for deliveries both to and from the business. The spatial arrangement of retail and industrial activity drives, and is driven by, the configuration of transportation infrastructure, particularly major roadways and interstates.

As seen in Figure 26, retailers are heavily concentrated in population centers like the Denver and Colorado Springs metropolitan areas. Within the Denver metropolitan area, retailers line major commercial corridors, including I-25. Within Colorado Springs, retailers are concentrated near downtown and along U.S. 24, as well as in the northern portion of the city, including along I-25. Additional concentrations of retail activity occur along U.S. 83 in and north of Parker, in Lone Tree, Castle Rock, and Monument. A few retailers are present in Castle Pines and Larkspur.

Location of industrial activity, in particular, has also historically been aligned with the location of railroads and waterways. As shown in Figure 27, industrial activity is heavily concentrated along major roadways and in areas where such activity is allowed and encouraged. Within the Denver metropolitan area, industrial activity is concentrated north of I-70 in parts of Denver, Commerce City, Aurora, and other surrounding communities. A general north-south concentration exists beginning (to the north) along I-76 in Commerce City. It then generally aligns with I-25 through downtown Denver and continues south along U.S. 85 (Santa Fe Boulevard) into Englewood and Littleton. Industrial activity is also heavily concentrated along I-70, particularly east of I-25 to its intersection with E-470. Some, albeit limited, industrial activity is present between the Denver metropolitan area and Colorado Springs. These areas include along U.S. 85 south of C-470, and in Castle Rock and Monument. Heavy concentrations are again present within Colorado Springs. Primary concentrations are present near the intersection of I-25 and West Garden of the Gods Road. In this area, most industrial activity occurs west and southeast of this intersection. Additional concentrations are present near downtown and along U.S. 24 to the east of downtown, especially just west of Peterson Air Force Base and the Colorado Springs Airport. Another concentration of activity is present in southeast Colorado Springs.

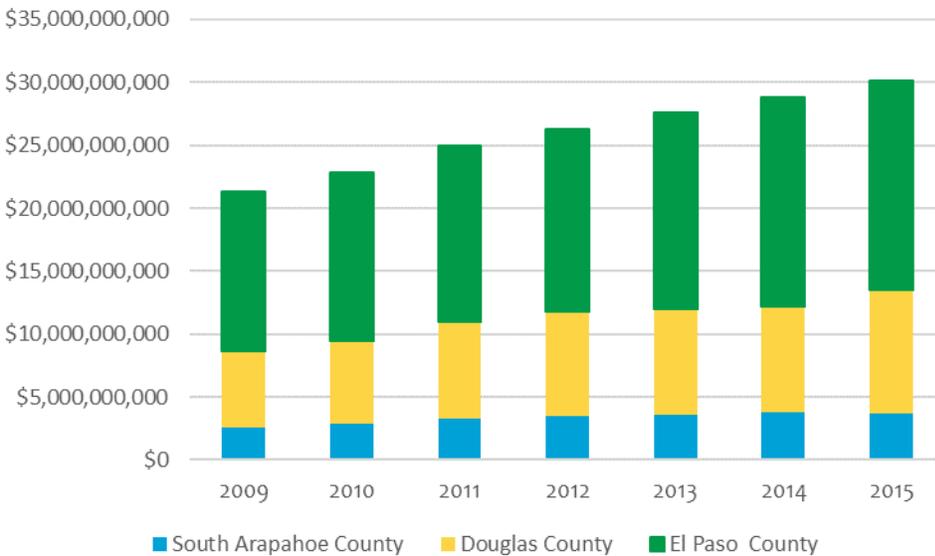
Table 19. Retail and Industrial Inventory in Study Area 2007-2016

Sector	2007	2016	Change 2007-2016	Change % of 2016
Retail Square Feet	55.3 Million	60.7 Million	5.4 Million	8.9
Industrial Square Feet	43.8 Million	45.8 Million	1.9 Million	4.2
Total	99.1 Million	106.5 Million	7.4 Million	6.9

Source: CoStar, ArLand

Table 19 shows the growth in retail and industrial inventory in the study area in the last ten years. It shows the addition of 5.4 million square feet of retail and 1.9 million square feet of industrial. In addition to regular trip generation by employees, customers, visitors, and others, this additional commercial inventory has generated a significant increase in truck traffic either delivering goods to and from the facility or addressing e-commerce demands.

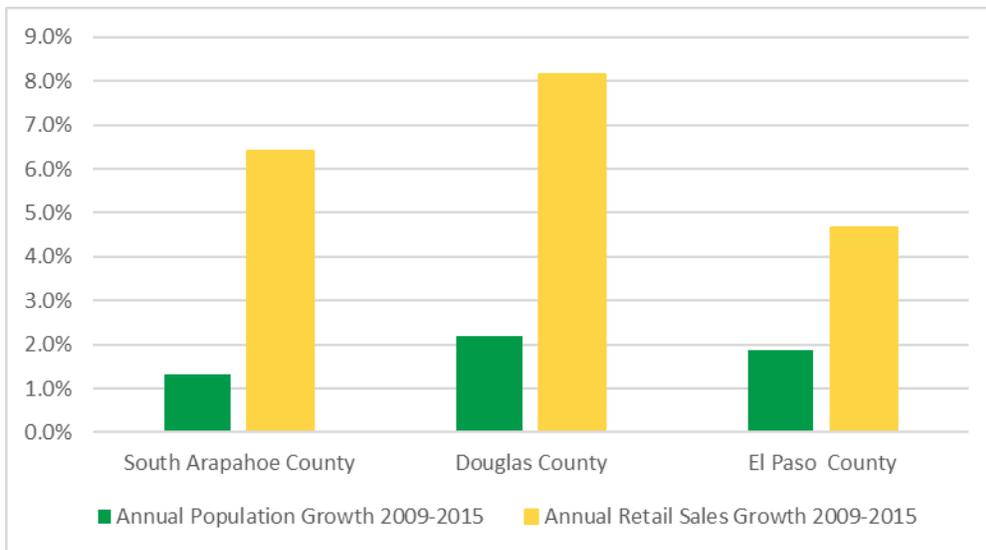
The significant growth in retail inventory over the past decade is tied to robust growth in retail sales. As seen in Figure 28, retail sales grew in south Arapahoe, Douglas, and El Paso Counties from 2009 through 2015, the most recent year data is available. The largest growth in retail sales was in Douglas and El Paso Counties. Combined estimated retail sales in the three areas was about \$30 billion in 2015. This is about a \$9 billion dollar (41%) increase from 2009 retail sales or about 6% annual growth over the six-year period. And, as will be discussed further, expected growth in e-commerce is expected to drive more demand for industrial facilities.



Source: Colorado Department of Revenue

Figure 28. Retail Sales, 2009-2015

Between 2009 and 2015, the population in South Arapahoe, Douglas and El Paso Counties grew between 1.3 and 2.2% annually as shown in Figure 29. During this same time period, retail sales grew by 4.7% in El Paso County, 6.4% in Southern Arapahoe County and 8.1% in El Paso County. While a recovering economy and greater spending per household are partial explanations, an increase in the number of restaurant and retail outlets, an increase in tourism, and an increase in the number of regional destinations are also other explanations. These changing variables all serve to put more cars on the road.



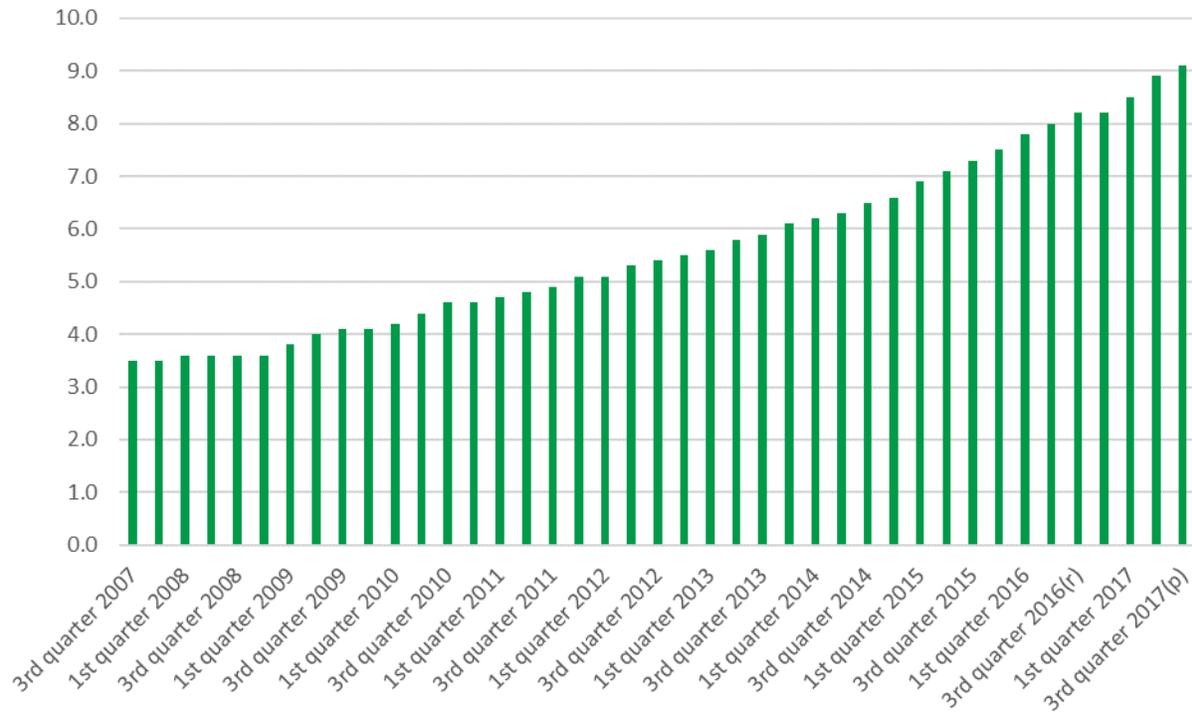
Source: US Census Bureau, Colorado Department of Revenue

Figure 29. Population Growth vs. Retail Sales Growth, 2009-2015

4.1.7 Changing Shopping Habits and Impact on Freight

Retail purchases are increasingly occurring online, which is altering demand for industrial space and increasing the number of trucks required to transport small packages to people’s door steps. It may also alter the number of shopping trips by vehicle.

As seen in Figure 30, the e-commerce share of total retail sales in the United States has climbed dramatically over the past 10 years. Ten years ago, e-commerce sales only accounted for about 3.5 % of total retail sales. As of the third quarter of 2017, the most recent data available, preliminary US Census Bureau estimates suggest that this figure had climbed to 9.1 %, with over \$115 billion in e-commerce sales during that quarter alone. This trend is expected to continue.



Source: U.S. Census Bureau, ArLand

Note: sales estimates are adjusted for seasonal variation, but not for price changes. Total sales estimates are also adjusted for trading-day differences and moving holidays. (p) is a preliminary estimate and (r) is a revised estimate.

Figure 30. Estimated Quarterly U.S. E-Commerce Retail Sales as a Percentage of Total Retail Sales, 2007-2017

Colorado's Front Range has and will continue to feel the effect of the growth of e-commerce and the related drive for companies to deliver their products to consumers very quickly (e.g. same-day and next-day delivery). The impact of this trend is continued growth in industrial warehouse space. The goal of expediting delivery is leading to construction of very large distribution centers near large population centers along with smaller warehouses in urban areas to facilitate last-mile delivery. A local example is Amazon's recently opened 450,000+ square foot sorting center in Aurora in close proximity to hubs for FedEx and the U.S. Postal Service. The growth of these centers results in more truck traffic to deliver goods to residences.

In addition to driving growth and utilization of industrial space, the rise of e-commerce sales has the potential to affect the number of shopping trips consumers make in a vehicle. More than one in five household trips is a shopping trip and as the volume of e-commerce continues to increase, travel associated with shopping may change. However, according to the U.S. Department of Transportation, this has yet to result in a significant effect on travel for household shopping trips.

Another factor that will impact traffic is the "buy online, pick up in store" option that many retailers now provide. The evolution of consumer behavior and demand for this type of shopping will also play a role in future shopping traffic. According to Nielsen, Millennials are more likely than Boomers to shop in this manner, which indicates that despite growth in e-commerce, some percentage of purchases will still involve the use of a vehicle to pick them up.

These trends point to an increase in truck traffic, particularly in urban areas like those along I-25, required to deliver goods to residences. All of these factors combined with population growth forecasts will likely result in more Front Range traffic related to shopping and the movement of goods.

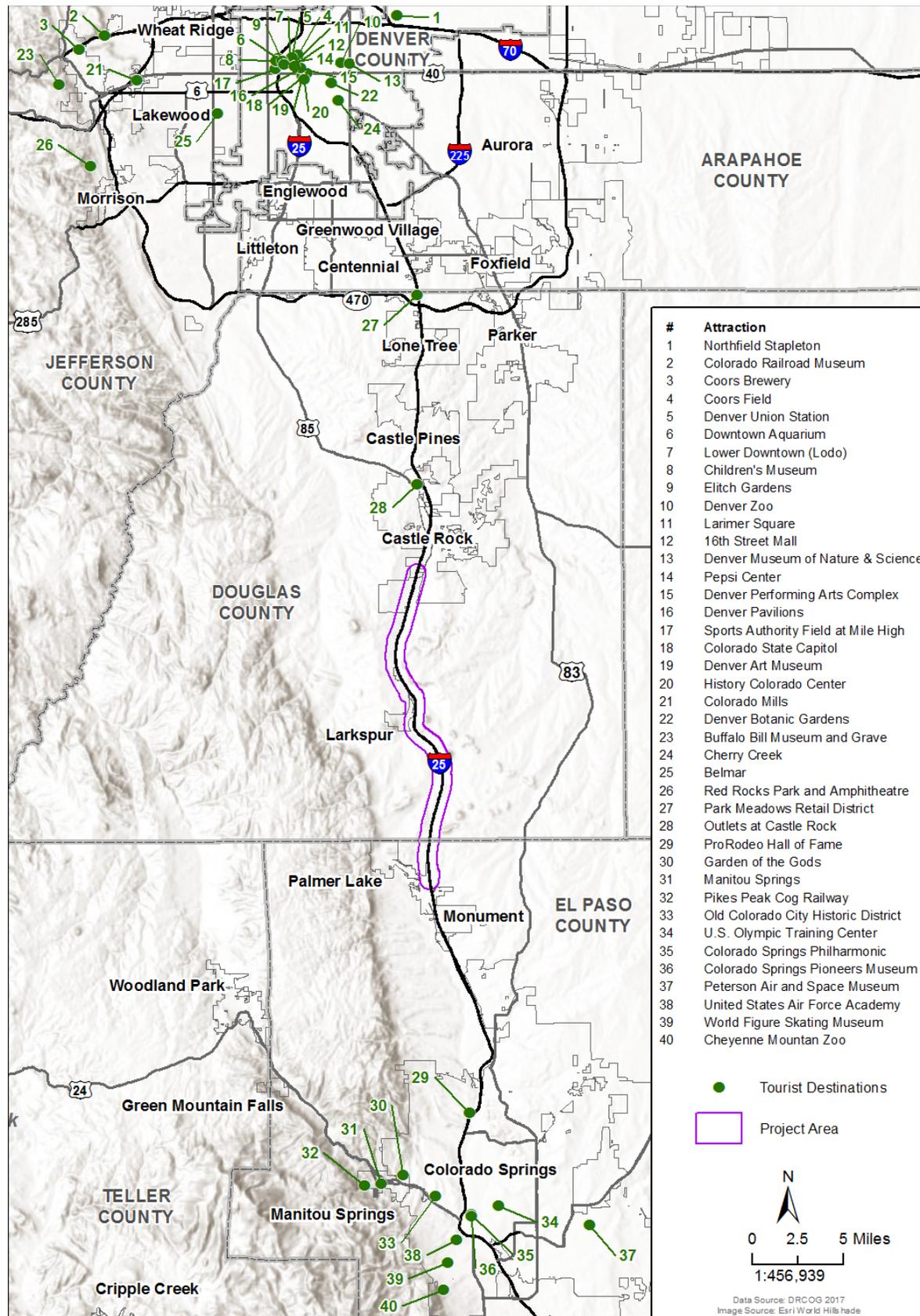
5.0 Tourism

Colorado's travel industry has grown in terms of number of visitors, travel spending, employment, and contributions to state and local tax revenues over the past 20 years. The Denver and Colorado Springs metropolitan areas have multiple tourist attractions that are of regional and national interest which are depicted in Figure 31, all of which are popular tourist draws. Many hold regular events drawing tens of thousands to hundreds of thousands of visitors annually. Because of marketing and other reasons, the state is transitioning from being a regional destination to a national destination, according to state tourism officials.

- *Pikes Peak Region:* According to Longwoods International and the Colorado Springs Convention and Visitors Bureau (El Paso, Teller, and Fremont Counties), visitors to the Pikes Peak Region welcomed 23 million people in 2016, which is a 12% increase over visitation in 2015 (20.5 million). More than half of visitors (57%) took a day trip, while 43% stayed overnight. The nature of visitation has changed. In previous years, "visiting friends and relatives" was the top reason for travelers to come to the area. In 2016, 50% surveyed reported that they visited, not because it was an obligation, but because they chose to visit.
- Since 2012, overnight travel has risen 32% with the top visitor states (other than Colorado) including California, Texas, Kansas, New York, Florida, Arizona, New Mexico, Missouri, and Illinois. Eighty nine percent (89%) of visitors arrive in a vehicle.
- *Denver:* According to Longwoods International and Visit Denver, there were an estimated 31.5 million visitors to Denver in 2016. 17.3 million of these visitors were estimated to be overnight visitors. Overnight leisure visitors have increased while business travelers have remained flat. Within the leisure travel category, the majority were visiting friends and relatives followed by "marketable" visitors, those who visit because they choose to visit. Denver also generated 14.2 million day visitors. While specific origin information isn't available, the majority of visitors are from other parts of Colorado. It can be assumed that residents from El Paso County, the rest of the Pikes Peak region and residents further to the south are among these visitors using I-25 as their conduit.

Beyond the tourist activities suggested by Figure 31, high traffic volumes on the weekends indicate that I-25 is a gateway for a wide variety of recreational and tourism-oriented activities.

- *Shopping:* Large retail and shopping districts along the Front Range attract visitors from surrounding states who visit and shop, often on weekends. Cabelas and Ikea are examples of retailers located in the southern Denver metro area with very large trade areas serving Colorado and adjoining states.
- *Special Events:* Larkspur holds an annual Renaissance Festival which attracts tens of thousands of visitors on weekends during the summer months. It's indicative of the growing number of festivals, concerts, and other activities taking place throughout the Front Range.
- *Sporting Events:* In addition to large professional sporting events which attract visitors from a wide ranging area, Colorado is a very active state with sports leagues and school-related sporting events which require travel for games and other activities.
- *Outdoor Access:* While there is a significant amount of local and regional outdoor space, the study area is also at the gateway to significant open space and recreational destinations in southern and western Colorado as well as adjoining states. The Pike and Isabel National Forests and the Great Sand Dunes, among other destinations, is located south of Colorado Springs.



Source: ArLand

Figure 31. Regional Tourist Destinations

Table 20. Overnight Travel Impacts, 2006-2016

	Arapahoe, Douglas, and El Paso Counties			Denver Metro Region ^b			Pikes Peak Region ^c		
	2006	2016p	CAGR ^a	2006	2016p	CAGR ^a	2006	2016p	CAGR ^a
Total Direct Travel Spending (\$Million)	\$1,922	\$2,626	3.2	\$5,963	\$8,932	4.1	\$1,185	\$1,543	2.7
Industry Earnings Generated by Travel Spending (\$Million)	\$476	\$706	4.0	\$1,797	\$2,720	4.2	\$273	\$410	4.2
Industry Employment Generated by Travel Spending (000 Jobs)	19.0	24.1	2.4	46.9	57.0	2.0	13.5	16.0	1.7
Government Revenue Generated by Travel Spending (\$Million)	\$104	\$166	4.8	\$317	\$523	5.1	\$60	\$95	4.7

Source: Dean Runyan Associates 2017, ArLand

^a Compound Annual Growth Rate

^b Denver Metro region includes Adams, Arapahoe, Broomfield, Denver, Douglas, and Jefferson counties.

^c Pikes Peak region includes El Paso, Fremont, and Teller counties.

As seen in Table 20, all metrics related to overnight travel have increased in the 10-year period from 2006 to 2016 in Arapahoe, Douglas, and El Paso Counties which are part of the Denver metro and Pikes Peak Regions. These three counties have seen a 3.2% average annual growth rate in total direct travel spending, 4% growth in earnings, an almost 5% annual growth rate in combined state and local tax generation, and over 2% growth in employment generated by travel spending.

Travel spending statewide has increased by about 4.0% per year on average since 1996. In 2016 direct travel spending was over \$19.7 billion and supported 165,000 jobs. The travel industry also contributes significantly to state and local tax revenue, generating about \$560 in tax revenue per household statewide in 2016 (Dean Runyan Associates 2017). And while travel and tourism is important throughout the state, the Denver metro and Pikes Peak Regions, including Arapahoe, El Paso, and Douglas Counties are particularly important to this industry, with almost half of all overnight travel spending in the state occurring in the Denver metro region, which includes Arapahoe and Douglas Counties.

6.0 Travel and Mobility: Benefits and Costs

6.1 Key Issues

There are three central issues related to the movement of people and goods along I-25 South - travel reliability, safety, and the movement of goods via truck. This section discusses all three of these issues within the context of CDOT's planned near-term corridor improvements at the Gap between Monument and Castle Rock. The project is designed to enhance the safety of roadway users, increase travel reliability, and allow for more efficient movement of goods while simultaneously enhancing safety and increasing travel reliability. The costs and benefits of these improvements are also discussed.

6.1.1 Travel Reliability

Travel reliability, or the ability of travelers to get to their destination without unexpected or unreasonably long traffic delays comes up often in public meetings and workshops. It is one of the central reasons for the I-25 Gap Project as well as future I-25 improvements. Increasingly, corridor travelers are experiencing long traffic delays, and the need to plan daily activities around traffic congestion is becoming the new norm. Travel delays are the result of congestion and events (e.g. crashes, weather, and sporting events).

Peak period delays typically occur in northbound lanes during weekday mornings and in southbound lanes during weekday afternoons. Events, however, also cause travel delays during the week. In the Gap, travel delays are equally the result of events (50 %) and congestion (50 %), and these delays are getting worse. Travel delays are worse in the southbound direction than northbound, worse on weekday afternoons, and during summer and fall. The longest travel times are regularly an hour or longer, which is three times as long as similar trips should take without delay. Delays that are multiple hours in duration now also occur in the Gap area. From 2015 to 2016 there was a 20% increase in congestion-related delays.

6.1.2 Safety

Safety has become an increasingly important issue in the corridor. From 2011 to 2015 there were 4,710 total crashes (accidents), or 2.5 crashes per day on average. Most of these (72%) resulted in property damage, and about 28% of all crashes during the 5-year period resulted in human injury or death. There were 13 fatal crashes during this time, or approximately 2 to 3 per year.

Information gathered over a 5-year period through 2015 shows that numerous crash types occur along I-25 South at all times of day and night throughout each year. The majority of crashes (57 %) are rear-end or sideswipe events where vehicles are traveling in the same direction. Incidents where vehicles crash into fixed objects like concrete barriers or guardrails account for about 24% of crashes. About 6% of crashes are the result of vehicle-wildlife collisions, although this type of crash may be underreported. In terms of month, day of the week, and time of day, the highest crash frequencies occur during summer months (June, July, August), on Saturdays, and during commuting times - 7:00-8:00am and 3:00-4:00pm. Most crashes occur below posted speed limits and about one quarter occur on wet road surfaces. Other physical roadway elements pose safety issues, such as interchanges and changes in the number of lanes. When compared to all crashes in the corridor, fatal crashes are much more likely to involve alcohol or drugs and to occur at dark or on curves.

6.1.3 Freight/Truck Traffic

The inability of heavy trucks to maintain their speed on roadway grade changes is another central issue of the project, impacting safety and travel reliability. Heavy trucks account for 8 to 11% of corridor traffic and they must deal with climbing grades in both directions in the corridor. Climbing is difficult for heavy trucks and they often must slow down anywhere from 10 to 20 miles per hour depending on the grade. This impacts traffic flow as faster vehicles then try to pass, which leads to a lot of variability in travel speed and reduced travel safety for truck drivers and all other roadway users.

6.1.4 Gap Project Improvements

The Planning and Environmental Linkages study identified a long term vision for I-25 between South Denver and Northern Colorado Springs. During the course of this study, it identified the “Gap” as the most urgent priority. I-25 is 6 to 8 lanes wide from just south of Castle Rock through Denver, and 6 lanes wide from Monument south to Colorado Springs. The 18 mile “Gap section between Monument and Castle Rock narrows to only 4 lanes (2 lanes in each direction) and has been identified as the most critical area for immediate improvements. Specific project improvements include:

- Widen 18 miles of I-25 from south of Castle Rock to Monument by adding an Express or General-Purpose Lane in each direction. In the express lane option drivers would have the choice of using the Express Lane for a reliable trip in exchange for a variable toll or ride the two existing general-purpose lanes for free. Carpoolers (vehicles with 3 or more people) could ride the Express Lane for free. In the general-purpose lane option, an additional general-purpose lane would be added.
- Widen shoulders outside and inside the travel lanes
- Add new wildlife crossings
- Add new overlay to existing pavement
- Repair or replace bridges
- Modernize highway with communications and power to enable advanced technology

As previous sections have outlined, I-25 in this section of the state connects two of the state’s largest urban centers, fast growing, well-educated and dynamic communities that contribute significantly to the State’s Gross Domestic Product. The Gap project improvements will continue to help support these vibrant local economies in this region.

6.1.5 Benefit Cost Analysis

A Benefit Cost Analysis (BCA) was conducted for the I-25 South Gap project to help quantify the return on investment to the region through a comparison of costs and benefits that measure productivity and safety for all. The BCA was conducted following a process consistent with U.S. Department of Transportation (US DOT) guidance where annual costs and benefits are projected over the 20 year life-cycle of the project. Per this guidance, future costs and benefits are also discounted at a 7% rate.

Other cost and benefit assumptions are outlined below.

Costs

- *Capital Costs:* Capital costs for the Gap project are estimated at \$320 million, expressed in 2016 dollars. Assuming that construction starts in November 2018 and finishes in 2020, the net present value (NPV) at a 7% discount rate for the capital costs is estimated to be \$255 million as shown in Table 21.
- *Annual Operating and Maintenance Costs:* Based on CDOT’s previous project experience, annual operating and maintenance costs are estimated at approximately \$7,200 per lane mile between the 2021 and 2030 (while the improvements are newer) at nearly \$1.5 million annually through 2030. In

2031, that cost increases to \$14,200 per lane mile and nearly \$2.9 million annually. Total Operations and Maintenance through 2040 is estimated at \$43.7 million. As seen in Table 22 at a 7% discount rate, the net present value (NPV) of operations and maintenance up to 2040 is estimated at \$15.8 million.

Table 21. Estimated Gap Capital Costs

Cost Item	Amount (2016\$)
Project Schedule	
2018	\$15,950,000
2019	\$191,400,000
2020	\$111,650,000
Total Costs	\$319,000,000
Net Present Value at 7%	\$255,348,012

Source: CH2M/Jacobs

Table 22. Operating and Maintenance Costs

Cost Item	Unit Cost (\$/lane mile)	Lane Miles	Annual O&M (2016\$)	Total O&M (2016\$)
O&M 2021-2030	\$7,200	204	\$1,468,800	\$14,688,000
O&M 2031-2040	\$14,200	204	\$2,896,000	\$28,968,000
Total O&M (2021-2040)				\$43,656,000
Net Present Value at 7%				\$15,760,703

Source: CH2M/Jacobs

Benefits

The U.S. Department of Transportation outlines the benefits that can be measured including travel time savings, operating cost savings, safety benefits, and residual value of the project after 20 years. While these guidelines also suggest including benefits such as the environmental benefits of reduced congestion (fewer pollutants released into the environment), these measures have not been included at this point in order to focus on the benefits which appeared to be most important to the public. The three most significant measures include travel time savings, decrease in vehicle operating costs, and safety improvements.

- *Travel Time Savings:* Travel time savings account for the greatest benefit provided by the I-25 Gap project. Travel time savings are shown in Table 23. In 2040 for morning peak period drivers, time savings are estimated at 12 to 20 minutes through the Gap from Monument to Castle Rock. The time savings is most significant for Friday, southbound traffic where time savings range from 50 to 87 minutes, depending on the alternative.

Table 24 describes the projected travel times through the Gap by 2040 with and without proposed project improvements. It shows that by 2040, weekday morning northbound travel through the Gap will take 43 minutes without improvements. Express lane travelers will need just 23 minutes. Friday southbound travelers will need nearly 2 hours to get through the Gap by 2040 without improvements. With the express lane concept, that time will be shortened to 32 minutes by 2040.

- *Vehicle Operating Costs:* This benefits measures the decrease in vehicle operating costs for drivers which incorporates vehicle costs related to fuel, maintenance, regular repairs and depreciation.

Project improvements are the result of improved average speeds, greater reliability, improved road conditions, and increased safety.

- *Safety*: Safety improvements are becoming a growing concern along the I-25 South corridor. The additional lanes are expected to significantly reduce congestion and stop-start conditions which in turn are expected to reduce the occurrence of crashes.

With both the build alternatives (2 general-purposes + 1 express and 3 general-purpose lanes), there would be a reduction in severe crashes where there is an injury or someone is either incapacitated or killed.

By 2040,

- Non-incapacitating (although an injury is involved): there would be a reduction from 58 to 49 accidents of this type
- Incapacitating: a reduction of 12 to 10 accidents of this type
- Killed: a reduction of 5 to 4 fatalities

Table 23. Travel Time Savings with Additional Lanes (in minutes) by 2040

Time Period	General-Purpose Lanes (2040)	Express Lanes (2040)
Weekday NB 6:30 – 9:00 AM	12	20
Weekday SB 3:00-7:00 PM	36	53
Friday SB 3:00-7:00 PM	50	87
Sunday NB 11:30 AM-3:00 PM	11	17

Source: CH2M/Jacobs

Table 24. Projected Travel Times through the Gap (in minutes)

Time Period	No Build (2040)	General-Purpose Lanes (2040)	Express Lanes (2040)
Weekday NB 6:30 – 9:00 AM	43	31	23
Weekday SB 3:00-7:00 PM	79	43	26
Friday SB 3:00-7:00 PM	119	69	32
Sunday NB 11:30 AM-3:00 PM	40	29	23

Source: CH2M/Jacobs

Benefit-Cost Calculation

The BCA considers both capital and operating costs, as well as annual benefits that accrue during operations.

Assumptions include the following:

- A 20 year life cycle assumed (US DOT 2017 BCA Guidance)
- 7% real discount rate (US DOT 2017 BCA Guidance)
- Percent Car Traffic: 91.5% (based on traffic demand model)

- Percent Truck Traffic: 8.5% (based on traffic demand model)
- Passengers per Car: 1.39 (based on US DOT 2017 BCA Guidance)
- Passengers per Truck: 1.00 (based on US DOT 2017 BCA Guidance)
- Value of Time for Private Vehicle (\$ per person-hour): \$14.10 (2016\$) (US DOT 2017 BCA Guidance)
- Value of Time for Commercial Vehicle Truck Driver (\$per person-hour): \$27.20 (2016\$) US DOT 2017 BCA Guidance)
- Annual Growth in Value of Time: 1% (2014 US DOT Guidance)
- Crashes and injuries are valued (based on US DOT 2017 BCA Guidance)
 - No Injury: \$3,700
 - Possible Injury: \$63,900
 - Non-Incapacitating: \$125,000
 - Incapacitating: \$459,100
 - Killed: \$9,600,000
 - Injured (severity unknown): \$174,000
 - Accidents (unknown if injury): \$132,200
- Traffic Growth Rates (based on traffic demand model)
 - No Build:
 - Vehicle Hours Travelled: 3.3% average annual growth rate between 2021-2040
 - Vehicle Miles Travelled: 2.2% average annual growth rate between 2021-2040
 - 2 General-Purpose and 1 Express Lane Option:
 - Vehicle Hours Travelled: 3.2% average annual growth rate between 2021-2040
 - Vehicle Miles Travelled: 2.2% average annual growth rate between 2021-2040
 - 3 General-Purpose Lanes Option:
 - Vehicle Hours Travelled: 3.1% average annual growth rate between 2021-2040
 - Vehicles Miles Travelled: 2.2% average annual growth rate between 2021-2040

The summary benefit cost calculation for the build alternatives is shown in Table 25. In this particular analysis, benefits were somewhat limited to the measures as described. Additional measures such as the benefits derived from a reduction in pollutants would undoubtedly change the calculations. Assuming a 7% real discount rate, an investment in capital, operating and maintenance costs would result, in both scenarios of a net present value benefit for the project of approximately \$1.7 billion. The net present value of costs is approximately \$270 million. The project would result in greater productivity, lower vehicle costs, and greater safety for the traveling public in this important part of the state of Colorado.

Table 25. Summary of Net Present Value, Benefits and Costs

Benefits and Costs	2GP + 1 Exp	3 GP
Benefits		
Value Travel Time Savings	\$1,584,137,481	\$1,652,436,283
Vehicle Operating Cost Savings	\$5,907,704	\$3,372,538
Safety Benefits	\$83,760,080	\$83,769,080
Residual Value	\$37,733,863	\$37,733,863
Total Benefits	\$1,711,548,128	\$1,777,311,764
Costs		
Capital	(\$255,348,012)	(\$255,348,012)
Operating & Maintenance	(\$15,760,703)	(\$15,760,703)
Total Costs	(\$271,108,714)	(\$271,108,714)

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